

A BYLAW OF THE CITY OF VICTORIA

The purpose of this Bylaw is to amend the Official Community Plan to make changes to Urban Place Designations and Development Permit Areas in order to implement the future land use, urban form and character directions identified in the Victoria West Neighbourhood Plan.

Under its statutory powers, including section 472 and 488 of the *Local Government Act*, the Council of the Corporation of the City of Victoria, in a public meeting, enacts as follows:

Title

- 1 This Bylaw may be cited as “OFFICIAL COMMUNITY PLAN BYLAW, 2012, AMENDMENT BYLAW (NO. 25)”.

Definitions

- 2 “OCP Bylaw” means the City’s Bylaw No. 12-013, the Official Community Plan Bylaw, 2012.

Official Community Plan Bylaw

- 3 The OCP Bylaw is amended in Schedule A, Table of Contents, under the heading “List of Map” by:
 - a) Replacing the text, “Map 31: Victoria West Strategic Directions” with the text “Map 31: Victoria West”.
 - b) Inserting the following text immediately after the text “Map 32: Composite Map of Development Permit Areas and Heritage Conservation Areas”:

“Map 32A: Composite Map of Attached Residential Development Permit Areas”;
 - c) Inserting the following text immediately after the text “Map 48B: DPA 6A: Small Urban Villages – Burnside at Harriet”:

“Map 48C: Small Urban Villages: Craigflower Village and Catherine at Edward Village”;
 - d) Inserting the following text immediately after the text “Map 56: DPA 7A: Corridors – Shelbourne Avenue”:

“Map 56A: Corridors – Esquimalt Road”
 - e) Inserting the following text immediately after the text “Map 72: DPA 15C: Intensive Residential – Rockland”:

“Map 72A: DPA 15F: Intensive Residential – Attached Residential Development”;
 - f) Renumbering the page number references accordingly.
- 4 The OCP Bylaw is amended in Schedule A, Section 6: Land Management and Development, as follows:
 - a) By repealing Map 2: Urban Place Designations and replacing it with a new Map 2: Urban Place Designations, attached to this Bylaw in Schedule 1.

- b) In Figure 8: Urban Place Guidelines, in the Designation “Traditional Residential,” under the category “Built Form,” by adding the following words immediately after the phrase “Ground-oriented buildings up to two storeys.”:

“Ground-oriented buildings up to two and one-half storeys may be considered in the Victoria West neighbourhood for certain infill housing types, as indicated in the Victoria West Neighbourhood Plan”.

- c) In Figure 8: Urban Place Guidelines, in the Designation “Traditional Residential,” under the category “Built Form,” by adding the following words immediately after the phrase “Multi-unit buildings up to three storeys, including attached residential and apartments on arterial and secondary arterial roads”:

“, and in other locations indicated in the Victoria West Neighbourhood Plan.”

- d) In Figure 8: Urban Place Guidelines, in the Designation “Traditional Residential,” under the category “Uses,” by adding the following language:

“Limited local-serving commercial uses in specific locations identified in the Victoria West Neighbourhood Plan.”

- e) In Figure 8: Urban Place Guidelines, in the Designation “Traditional Residential,” under the category “Density,” by adding the following language:

“Additional density may be considered where appropriate in certain limited areas in the Victoria West Neighbourhood up to a maximum indicated in the Victoria West Neighbourhood Plan.”

- 5** The OCP Bylaw is amended in Schedule A, Section 14: Economy, by repealing Map 14: Employment Lands and replacing it with a new Map 14: Employment Lands, attached to this Bylaw in Schedule 1.

- 6** The OCP Bylaw is amended in Schedule A, Section 21: Neighbourhood Directions, as follows:

- a) By repealing Map 24: Harbour Strategic Directions, and replacing it with a new Map 24: Harbour Strategic Directions, attached to this bylaw in Schedule 1.
- b) By repealing Map 31: Victoria West Neighbourhood Directions and replacing it with a new Map 31: Victoria West Neighbourhood, attached to this Bylaw in Schedule 1.
- c) In section 21.26.4., by replacing the words “Develop a local area plan” with the words “Implement, monitor and regularly update the local area plan”.
- d) By deleting Policies 21.26.5. and 21.26.7., and renumbering remaining policies accordingly.

- 7** The OCP Bylaw is amended in Schedule A, Appendix A: Development Permit Areas and Heritage Conservation Areas by:

- a) Repealing Map 32: Composite Map of Development Permit Areas and Heritage Conservation Areas and replacing it with a new Map 32: Composite Map of Development Permit Areas and Heritage Conservation Areas, attached to this Bylaw in Schedule 1.
- b) Inserting immediately after Map 32: Composite Map of Development Permit Areas and Heritage Conservation Areas, Map 32A: Composite Map of Attached Residential Development Permit Areas, attached to this Bylaw in Schedule 1.

- 8 The OCP Bylaw is amended in Schedule A, Appendix A, DPA 5: Large Urban Villages by:
- a) Adding the following guidelines in section 5(b)(viii) after the words “Policy Plan and Design Guidelines for the Songhees Area of Victoria West (2008).”:
 - Design Guidelines for: Multi-Unit Residential, Commercial and Industrial (2012).
 - Revitalization Guidelines for Corridors, Villages and Town Centres (2017).”
 - b) Repealing Map 39: DPA 5: Large Urban Villages, and replacing it with a new Map 39: DPA 5: Large Urban Villages, attached to this Bylaw in Schedule 1.
 - c) Repealing Map 41: DPA 5: Large Urban Villages – Victoria West Village and replacing it with a new Map 41: DPA 5: Large Urban Villages – Westside Village, attached to this Bylaw in Schedule 1.
- 9 The OCP Bylaw is amended in Schedule A, Appendix A, DPA 6A: Small Urban Villages, as follows:
- a) In section 1, by deleting the words “Maps 48A and 48B” and replacing them with the words “Maps 48A, 48B and 48C”.
 - b) By adding the following sections immediately after section 5(b)(ii):
 - “(iii) to Craigflower Village:
 - Design Guidelines for: Multi-Unit Residential, Commercial and Industrial (2012).
 - Revitalization Guidelines for Corridors, Villages and Town Centres (2017).
 - (iv) to Catherine at Edward Village:
 - Design Guidelines for: Multi-Unit Residential, Commercial and Industrial (2012).
 - Revitalization Guidelines for Corridors, Villages and Town Centres (2017).”
 - c) By repealing Map 48: DPA 6A: Small Urban Villages and replacing it with a new Map 48: DPA 6A: Small Urban Villages, attached to this Bylaw in Schedule 1.
 - d) By inserting, immediately after Map 48B, a new map 48C: DPA 6A: Small Urban Villages – Craigflower Village and Catherine at Edward Village, attached to this Bylaw in Schedule 1.
- 10 The OCP Amendment Bylaw is amended in Schedule A, Appendix A, DPA 7A: Corridors, as follows:
- a) In section 1, by deleting the words “and 56” and replacing them with the words “, 56 and 56A”.
 - b) In section 4(c), by adding the phrase “, Esquimalt Road” immediately after the words “Oak Bay Avenue”.
 - c) By adding the following section immediately after section 5(b)(iii):
 - “(iv) Esquimalt Road corridor:
 - Design Guidelines for: Multi-Unit Residential, Commercial and Industrial (2012).
 - Revitalization Guidelines for Corridors, Villages and Town Centres (2017).”
 - d) By repealing Map 50: DPA 7A: Corridors and replacing it with a new Map 50: DPA 7A: Corridors, attached to this Bylaw in Schedule 1.

- e) By inserting, immediately after Map 56, a new map 56A: DPA 7A: Corridors – Esquimalt Road, attached to this Bylaw in Schedule 1.
- 11** The OCP Bylaw is amended in Schedule A, Appendix A, DPA 13: Core Songhees by:
- a) Repealing Map 64: DPA 13: Core Songhees and replacing it with a new Map 64: DPA 13: Core Songhees, attached to this Bylaw in Schedule 1.
- b) Adding the following new section 5.(b)(v):
- “(v) Core Songhees Employment Corridors:
- › Policy Plan and Design Guidelines for the Songhees Area of Victoria West (2008)
- › Design Guidelines for: Multi-Unit Residential, Commercial and Industrial (2012).
- › Revitalization Guidelines for Corridors, Villages and Town Centres (2017).”
- 12** The OCP Bylaw is amended in Schedule A, Appendix A, immediately after DPA 15E: Intensive Residential, by inserting:
- a) Map 72A: DPA 15F: Intensive Residential – Attached Residential Development, which is attached to this Bylaw in Schedule 1.
- b) DPA 15F: INTENSIVE RESIDENTIAL – ATTACHED RESIDENTIAL DEVELOPMENT” which is attached to this bylaw as Schedule 2.
- 13** The OCP Bylaw is amended in Schedule A, Appendix A, DPA 16: General Form and Character in section 2.(b)(i)(1)(B), by adding the following text immediately after the text “DPA 15E, Intensive Residential – Garden Suites”:
- “DPA 15F, Intensive Residential – Attached Residential Development”
- 14** The OCP Bylaw, 2012, is amended in Schedule A, including appendices, wherever the words “Victoria West Village” occur, by replacing those words with “Westside Village”.
- 15** This Bylaw comes into force on adoption.

READ A FIRST TIME the **26th** day of **April** 2018

READ A SECOND TIME the **26th** day of **April** 2018

Public hearing held on the day of 2018

READ A THIRD TIME the day of 2018

ADOPTED on the day of 2018

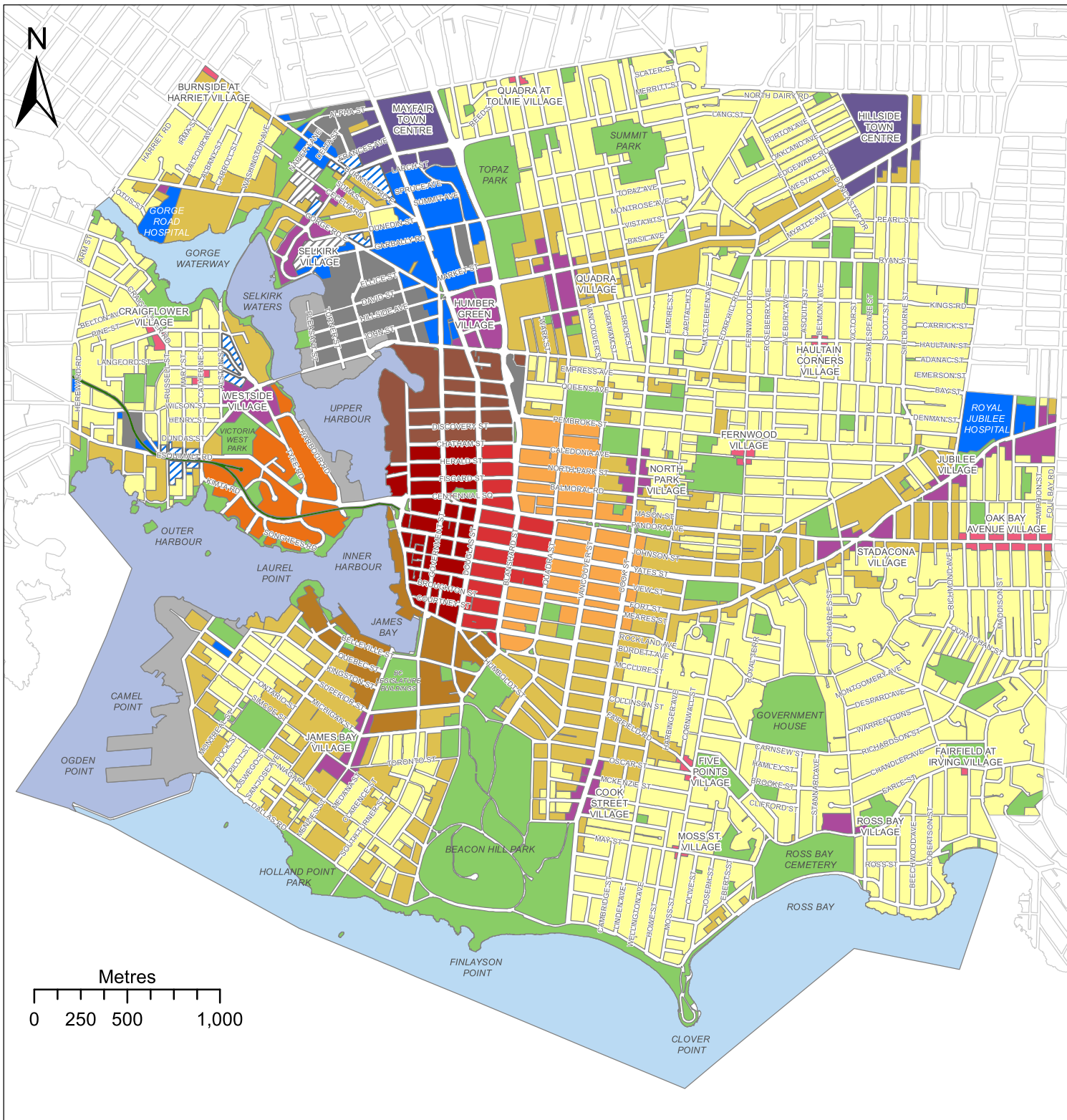
CITY CLERK

MAYOR

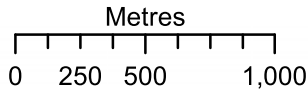
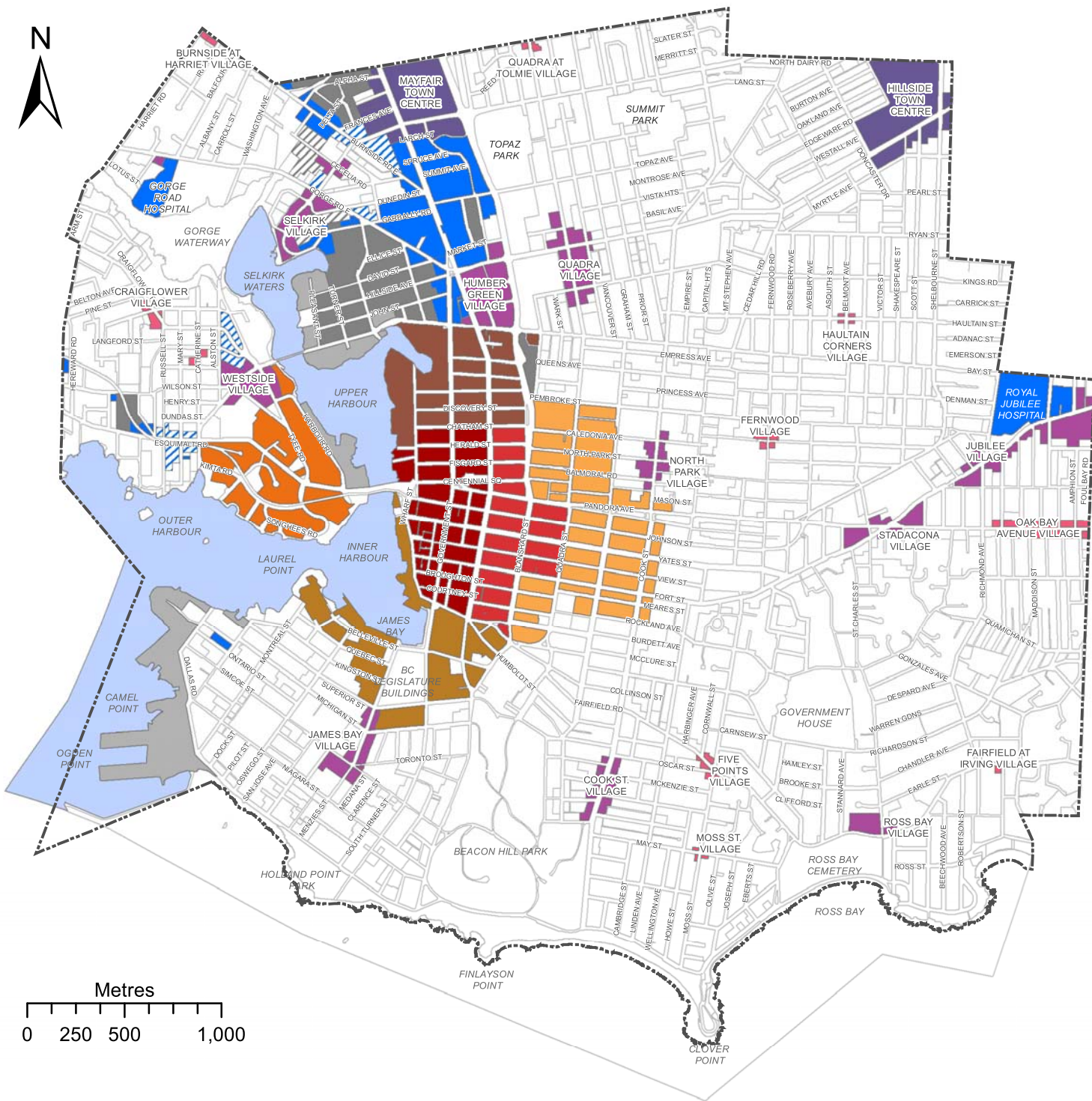
MAP 2

Urban Place Designations

- Core Historic
- Core Business
- Core Employment
- Core Inner Harbour/Legislative
- Core Songhees
- Core Residential
- General Employment
- Employment-Residential
- Industrial Employment
- Industrial Employment-Residential
- Marine Industrial
- Town Centre
- Large Urban Village
- Small Urban Village
- Urban Residential
- Traditional Residential
- Public Facilities, Institutions, Parks and Open Space
- Rail Corridor
- Working Harbour
- Marine



Urban Place Designations extend to the centerlines of adjacent streets.

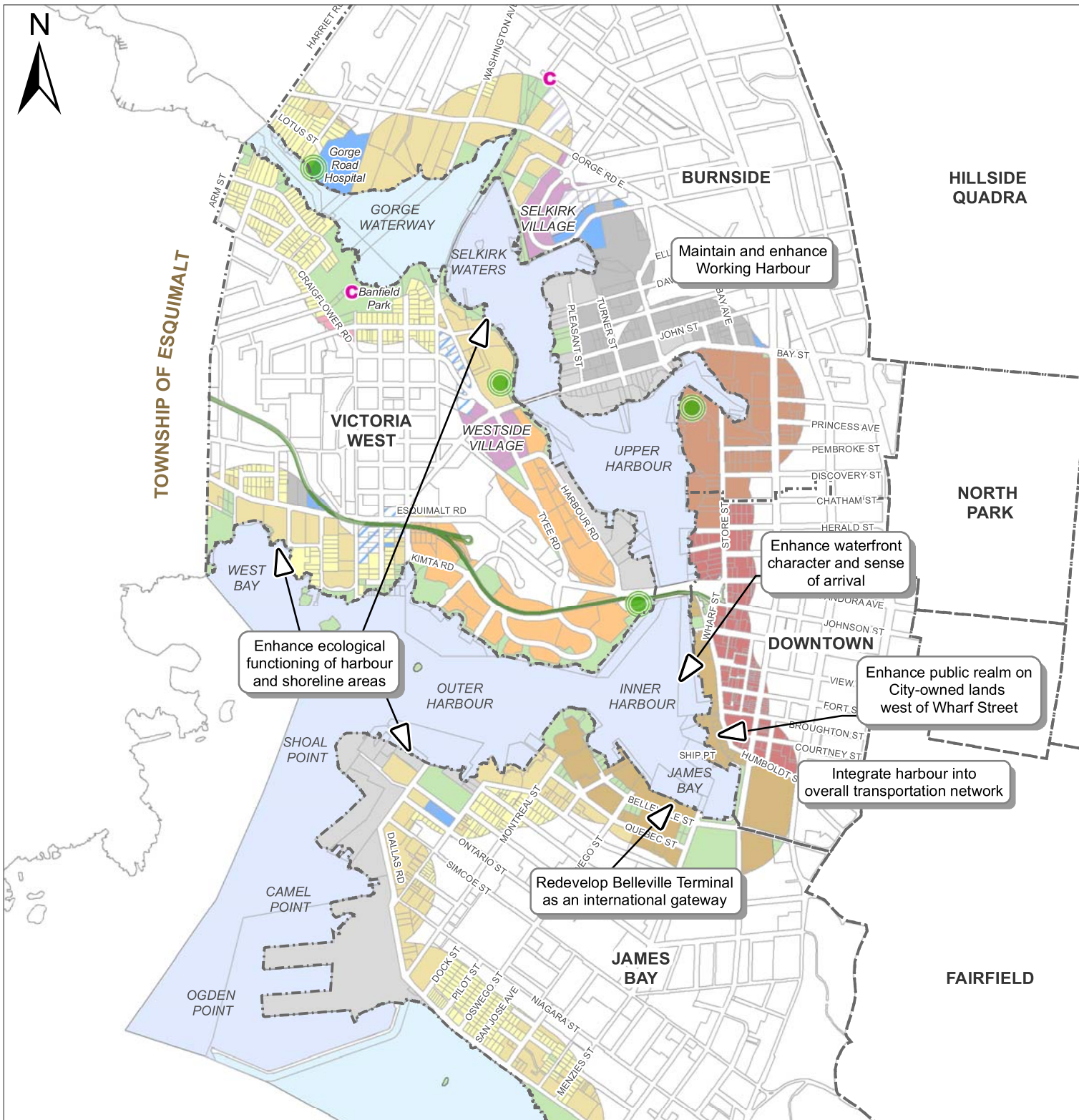


MAP 14

Employment Lands

- Core Historic
- Core Business
- Core Employment
- Core Inner Harbour/Legislative
- Core Songhees
- Core Residential
- General Employment
- Industrial Employment
- Marine Industrial
- Town Centre
- Large Urban Village
- Small Urban Village
- Marine - Harbour
- Employment-Residential
- Industrial Employment-Residential

Urban Place Designations are provided for information purposes only. Please refer to Map2 and Figure 8 for designation information.



*Urban Place Designations are provided for information purposes only. Please refer to Map 2 and Figure 8 for designation information.



MAP 31

Victoria West Neighbourhood

Urban Place Designations*

- Core Songhees
- General Employment
- Marine Industrial
- Large Urban Village
- Small Urban Village
- Urban Residential
- Traditional Residential
- Public Facilities, Institutions, Parks and Open Space
- Rail Corridor
- Working Harbour
- Marine
- Employment-Residential
- Industrial Employment

Public Facilities

- Existing Public School
- Proposed Park (approximate location)
- Community Centre

*Urban Place Designations are provided for information purposes only. Please refer to Map 2 and Figure 8 for designation information.



MAP 32

COMPOSITE MAP OF DEVELOPMENT PERMIT AREAS AND HERITAGE CONSERVATION AREAS

- DPA 1 (HC): Core Historic
- DPA 2 (HC): Core Business
- DPA 3 (HC): Core Mixed-Use Residential
- DPA 4: Town Centres
- DPA 5: Large Urban Villages
- DPA 6A: Small Urban Villages
- DPA 6B (HC): Small Urban Villages Heritage
- DPA 7A: Corridors
- DPA 7B (HC): Corridors Heritage
- DPA 8: Victoria Arm Gorge Waterway
- DPA 9 (HC): Inner Harbour
- DPA 10A: Rock Bay
- DPA 10B (HC): Rock Bay Heritage
- DPA 11: James Bay and Outer Harbour
- DPA 12 (HC): Legislative Precinct
- DPA 13: Core Songhees
- DPA 14: Cathedral Hill Precinct
- HCA 1: Traditional Residential
- DPA 15C: Intensive Residential Rockland

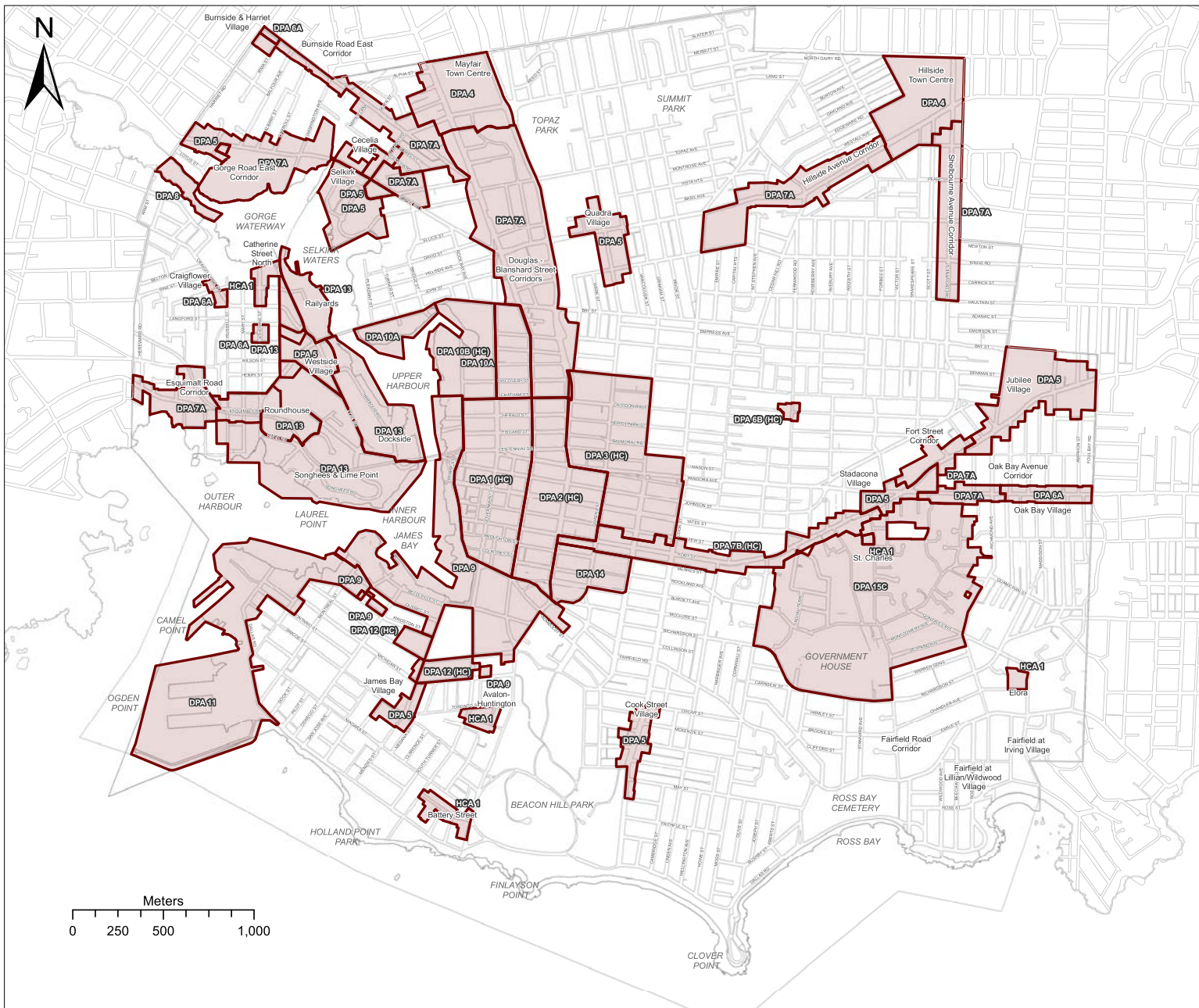
See Map 32A for the following Intensive Residential Development Permit Areas:

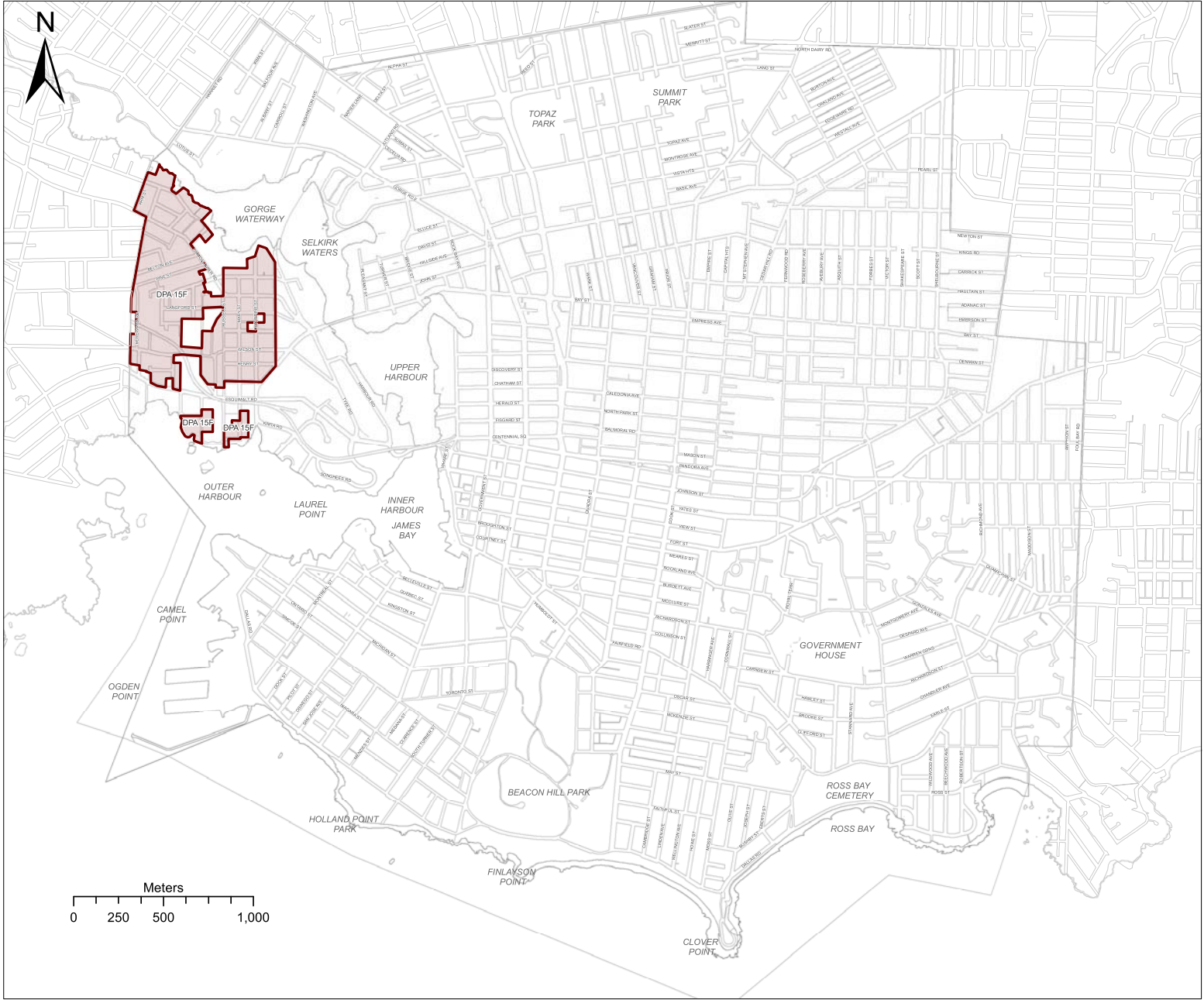
DPA 15F: Intensive Residential - Townhouse and Rowhouse

The following designations apply to all areas within the City of Victoria and are not shown on this map:

- DPA 15A: Intensive Residential Small Lot
- DPA 15B: Intensive Residential Panhandle Lot
- DPA 15D: Intensive Residential Duplex
- DPA 15E: Intensive Residential Garden Suites
- DPA 16: General Form and Character

This composite map is provided for reference only. Please see the map and provisions for each designated DPA and HCA for legal information.





MAP 32A

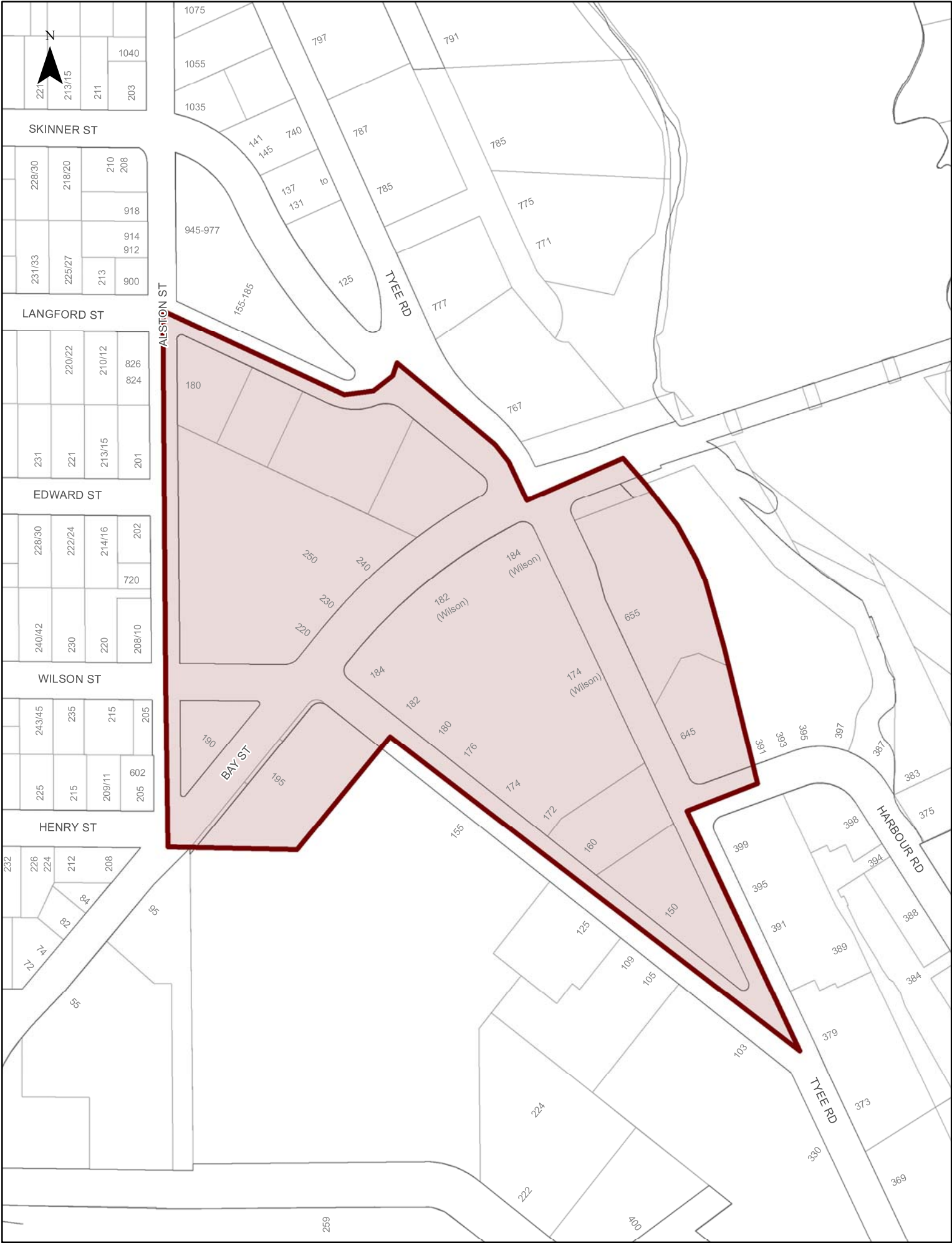
COMPOSITE MAP OF ATTACHED RESIDENTIAL DEVELOPMENT PERMIT AREAS

DPA 15F: Intensive Residential -
Attached Residential Development

Map 39: DPA 5: Large Urban Villages



Map 41: DPA 5: Large Urban Villages - Westside Village

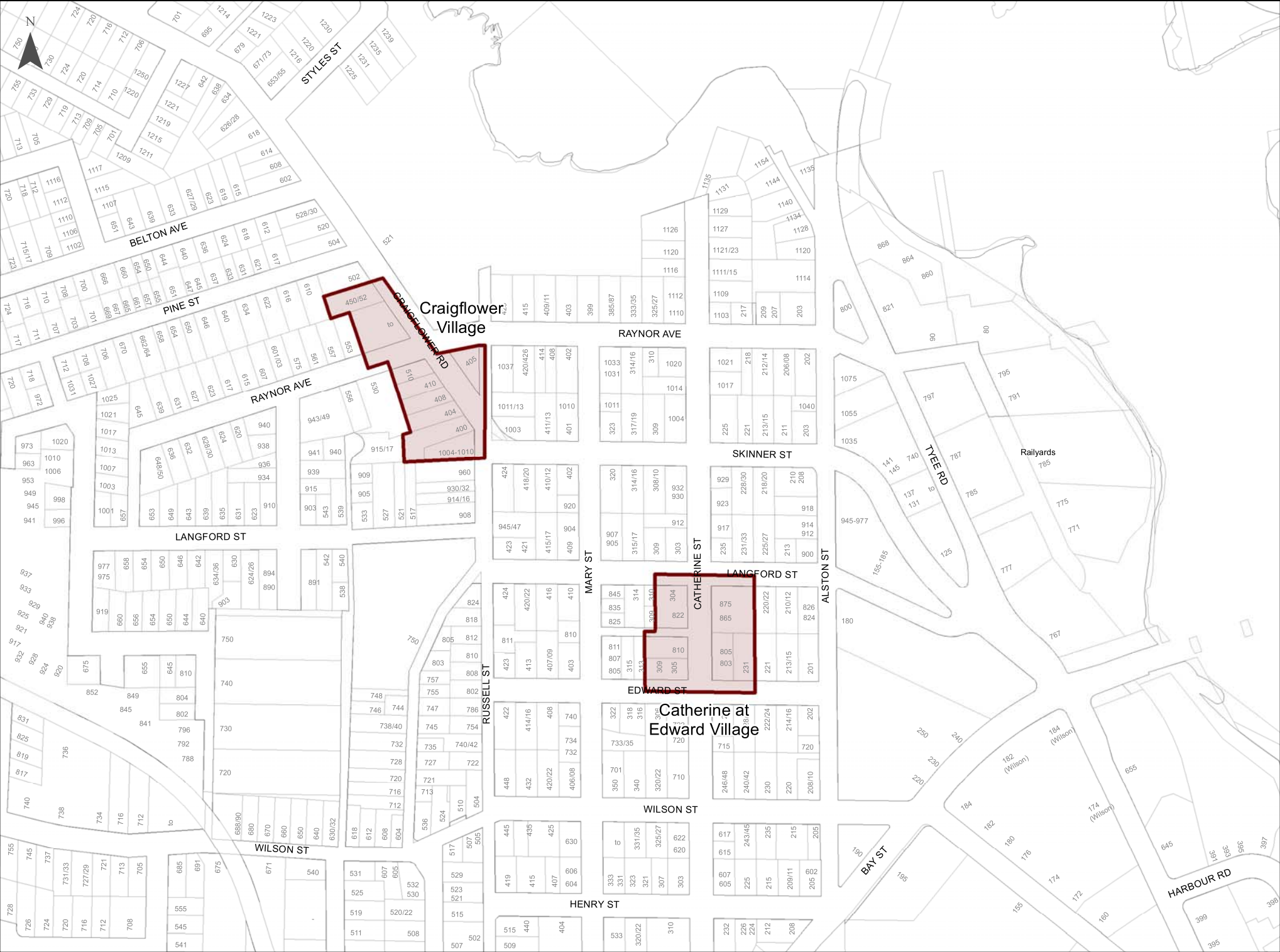


Map 48: DPA 6A: Small Urban Villages

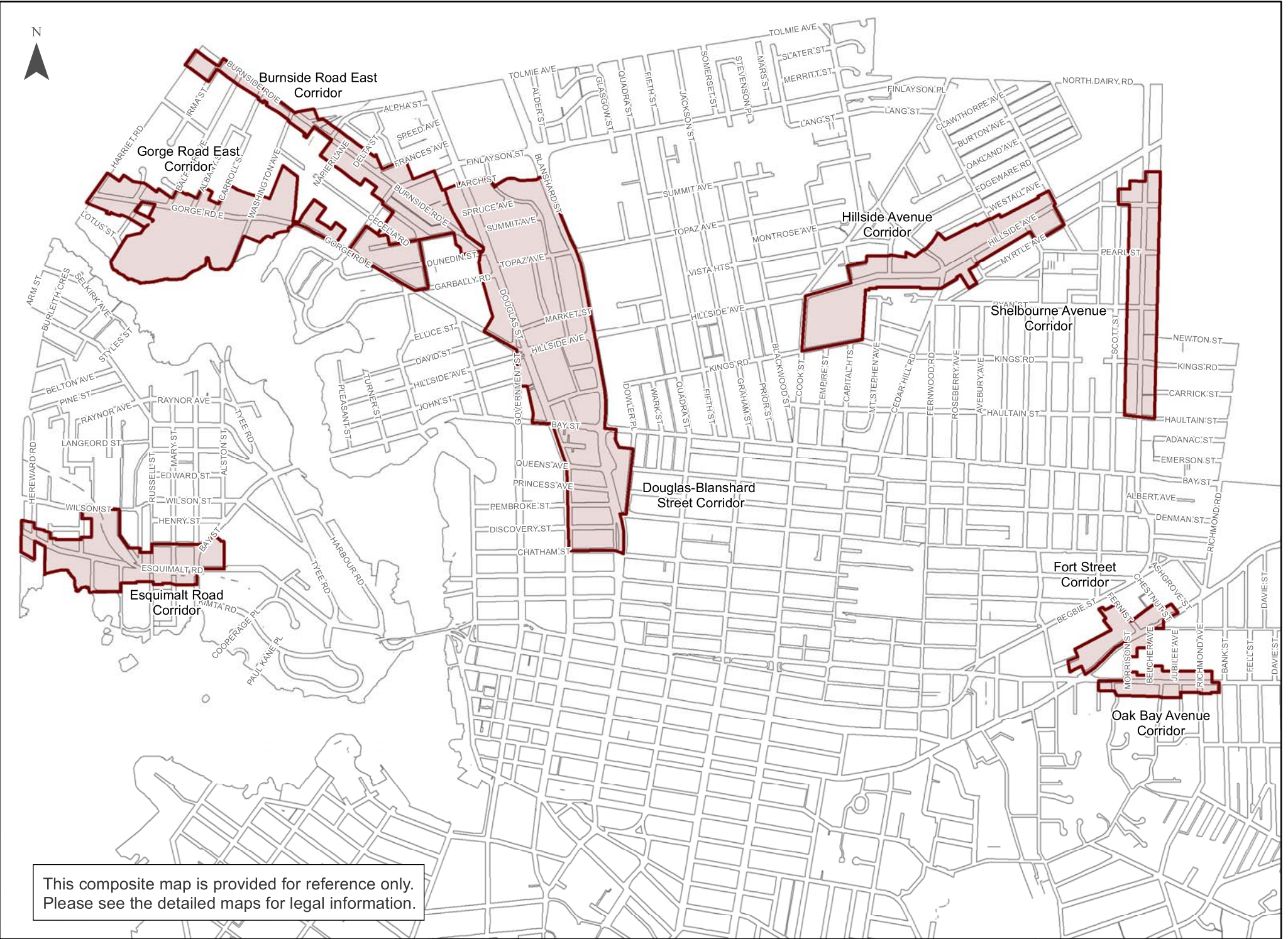


This composite map is provided for reference only.
Please see the detailed maps for legal information.

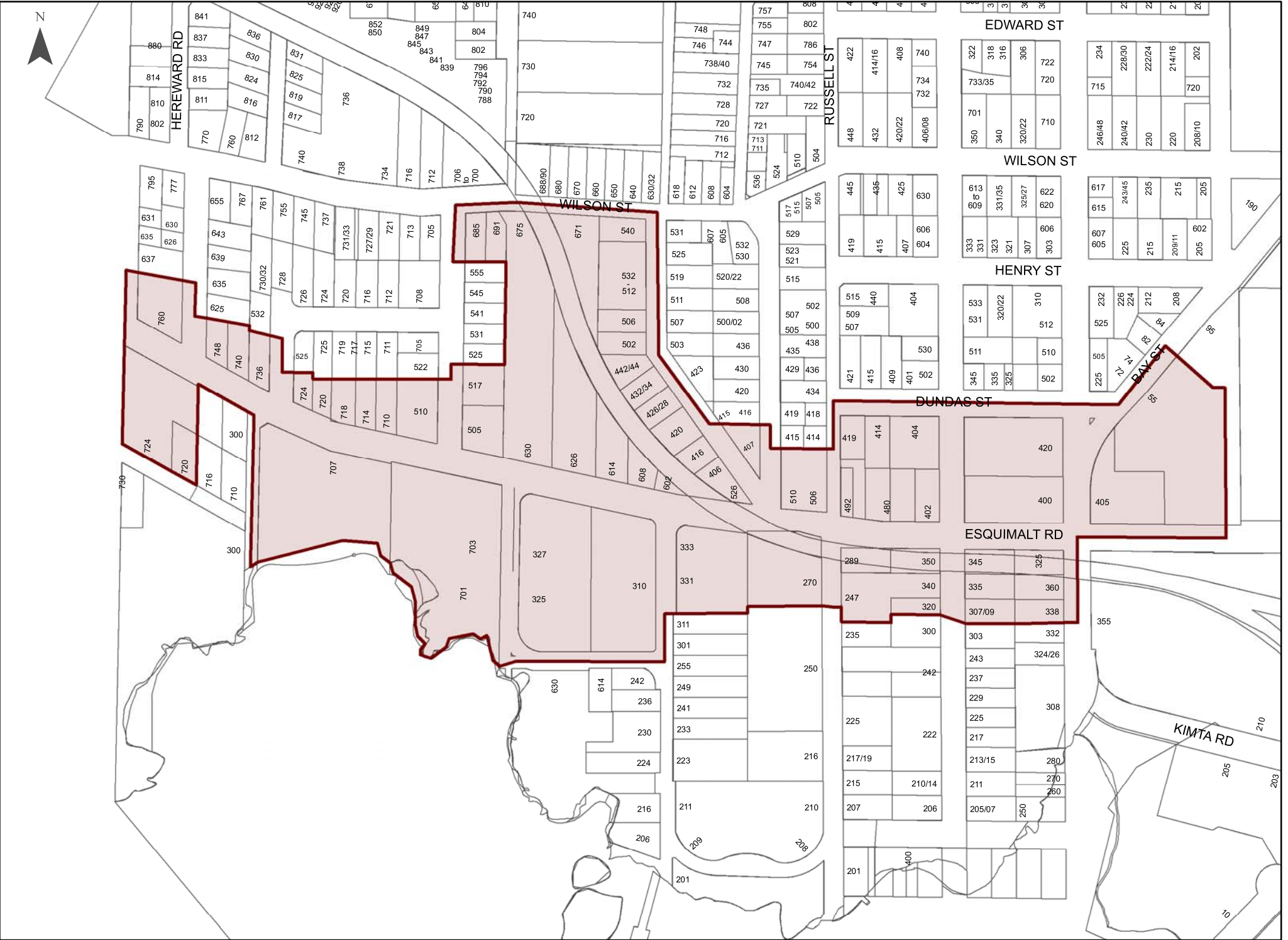
Map 48C: DPA 6A: Small Urban Villages - Craigflower Village and Catherine at Edward Village



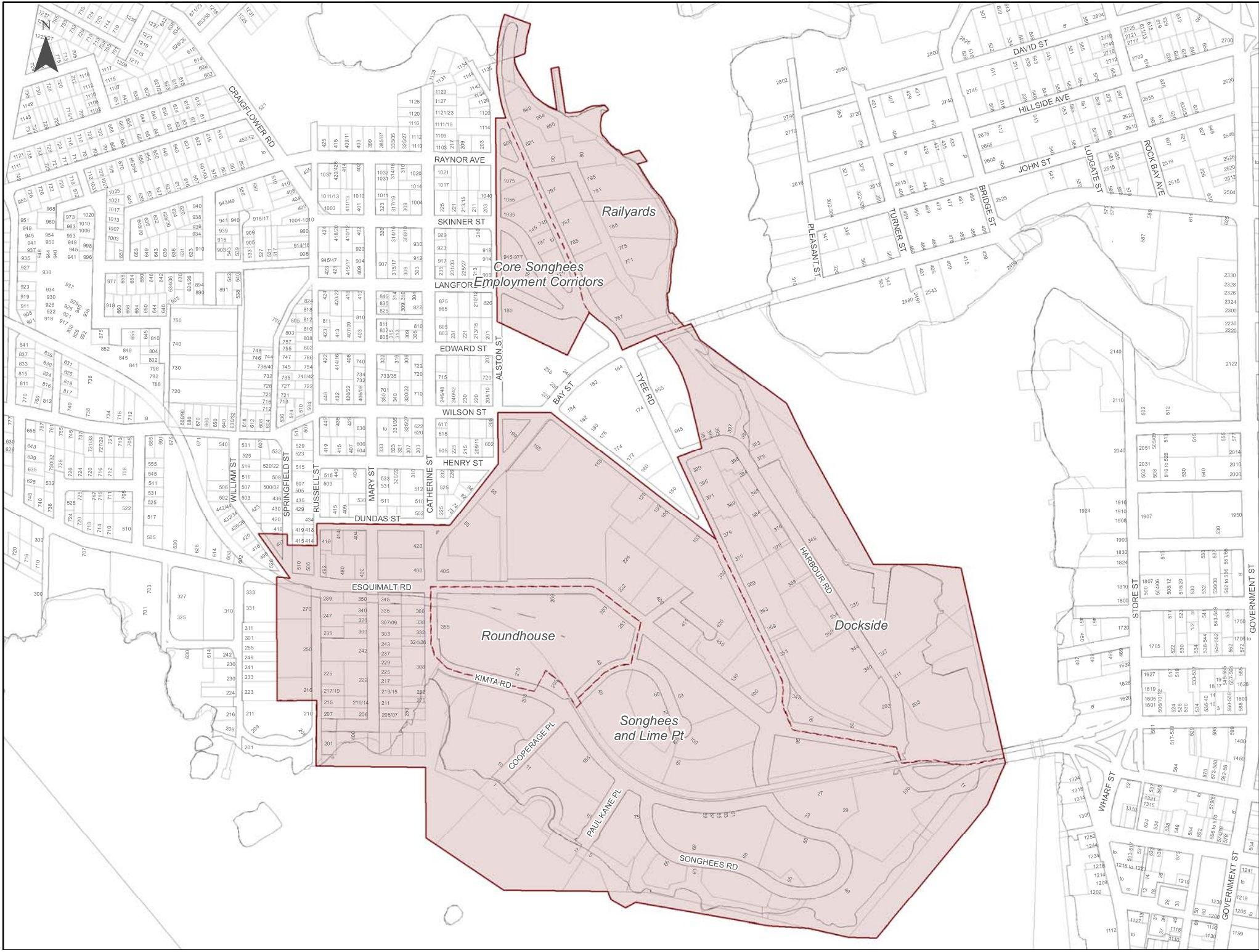
Map 50: DPA 7A: Corridors



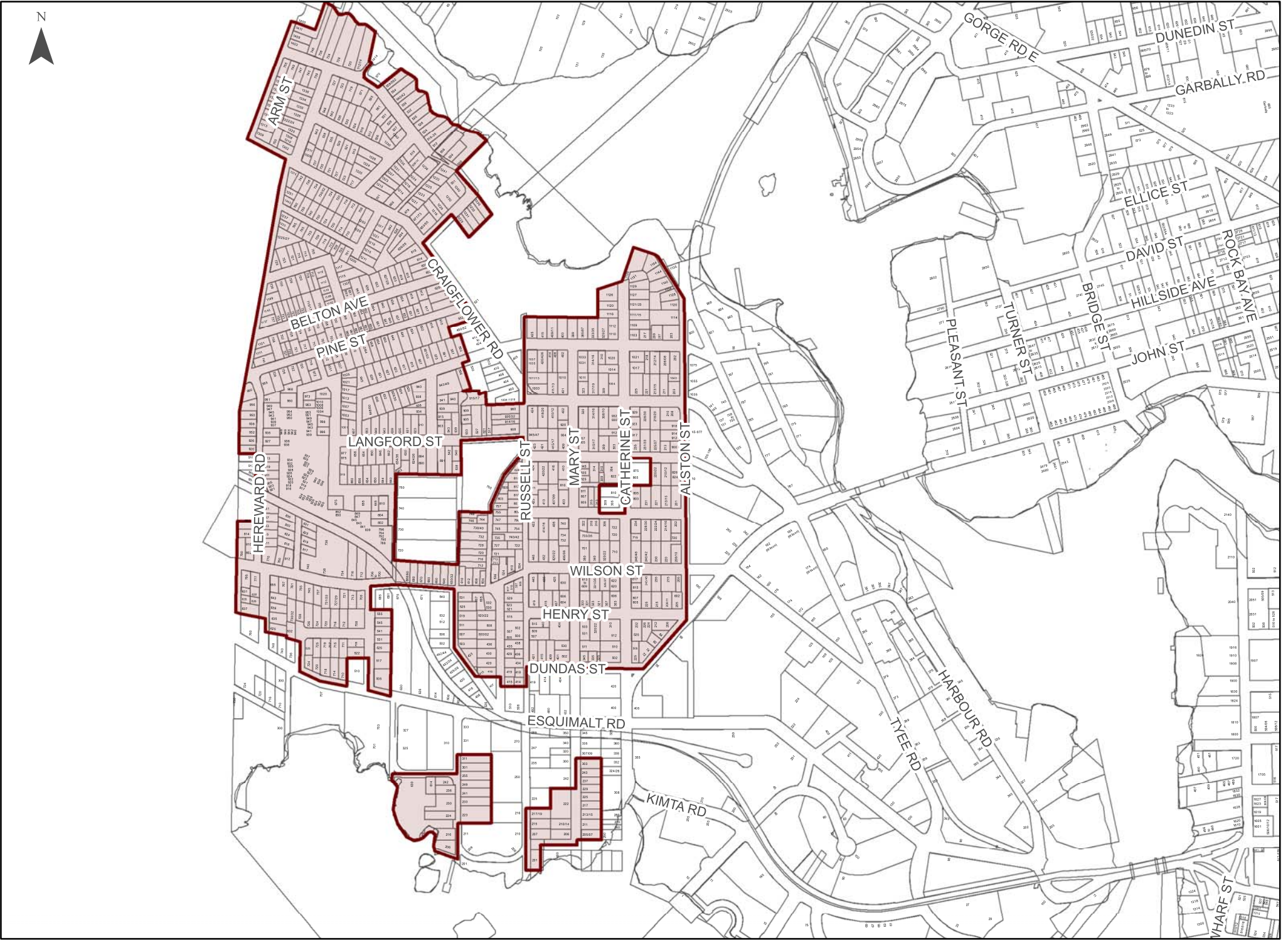
Map 56A: DPA 7A: Corridors - Esquimalt Road



Map 64: DPA 13: Core Songhees



Map 72A: DPA 15F: Intensive Residential - Attached Residential Development



DPA 15F: INTENSIVE RESIDENTIAL – ATTACHED RESIDENTIAL DEVELOPMENT

1. Pursuant to Section 488 (1) (e) of the Local Government Act, the area that is shaded and circumscribed by solid lines on Map72A is designated as Development Permit Area DPA 15F, Intensive Residential – Attached Residential Development, for the purpose of establishing objectives for the form and character of intensive residential development.

2. Application and Exemptions:

(a) In this area:

“Intensive Residential – Attached Residential Development” means construction of, addition to or alteration of a building, accessory structure or landscape on a lot which includes three or more attached, ground-oriented housing units.

(b) Development Permits are required for Intensive Residential – Attached Residential Development in accordance with the Local Government Act, subject only to the General Exemptions identified in the “Overview” section of this Appendix and the following exemptions.

(c) Specific Exemptions for DPA 15F, Intensive Residential – Attached Residential Development:

(i) A Development Permit is not required for:

- (1) development that is not Intensive Residential – Attached Residential Development;
- (2) residential single-family dwellings and their accessory buildings and structures unless the single-family dwelling is located on a lot that also includes Intensive Residential – Attached Residential Development;
- (3) duplex development and their accessory buildings and structures unless the duplex is located on a lot that also includes Intensive Residential – Attached Development;
- (4) house conversion;
- (5) commercial or industrial development;
- (6) alterations to landscaped areas which neither expand the extent of paved or impervious areas nor remove trees or shrubs which are shown in a previously approved Development Permit.

3. The special conditions that justify this designation include:

- (a) Victoria’s Traditional Residential areas are primarily characterized by low density, single-family dwellings.
- (b) These neighbourhoods each have a unique sense of place, traditional scale and character.
- (c) Attached residential development provides an opportunity to integrate other types of ground-oriented housing forms into neighbourhoods to provide more housing diversity.

4. The objectives that justify this designation are:

- (a) To accommodate 10% of Victoria’s anticipated population growth and associated housing growth in Small Urban Villages, and residential areas, to encourage and support future and existing commercial and community services.

- (b) To integrate more intensive residential development in the form of ground-oriented attached residential development within Traditional Residential areas in a manner that respects the established character of the neighbourhoods.
- (c) To accommodate housing growth in Traditional Residential areas in a manner that is gradual, of a compatible scale and adaptive to the local contexts.
- (d) To achieve a high quality of architecture, landscape and urban design to enhance neighbourhoods.
- (e) To integrate infill development in Traditional Residential areas that is compatible with existing neighbourhoods through considerations for site planning and building designs that promote livability and provide sensitive transitions to adjacent developments.

5. Guidelines:

These Guidelines are to be considered and applied for Development Permits:

- (a) Design Guidelines for Attached Residential Development (2018)
- (b) Guidelines for Fences, Gates and Shutters (2010)



Revitalization Guidelines for Corridors, Villages and Town Centres

July 2017
Updated May, 2018



Revitalization Guidelines for Corridors, Villages and Town Centres

Preamble:

These guidelines apply to designated Corridors, Villages and Town Centres and are intended to supplement the *Design Guidelines For: Multi-Unit Residential, Commercial and Industrial, July 2012* which address form and character of developments across the city.

It is intended that both guideline documents will be considered together in conjunction with other applicable guidelines noted in each designated development permit area as detailed in the *Official Community Plan*. Collectively, the guidelines are intended to guide applicants in achieving new development and additions to existing buildings that result in design excellence, livability, and high-quality pedestrian environments. This is intended to contribute to sense of place and urbanism that is responsive to Victoria's context, while enabling flexibility and fostering creativity.

All visuals in this document are provided for illustrative purposes only to support description of the guidelines.

General Guidelines

1) Context and Streetscapes:

- a. Buildings flanking streets should create a sense of enclosure and human scale. To achieve this, buildings fronting streets should provide a “street wall” that is at a height approximately 1/2 to 1/3 the width of the flanking street. This can be expressed as a street-wall-to-street-width ratio range of approximately 1:2 to 1:3. For buildings located on corner sites, this principle should be applied to the facades facing both streets where possible.

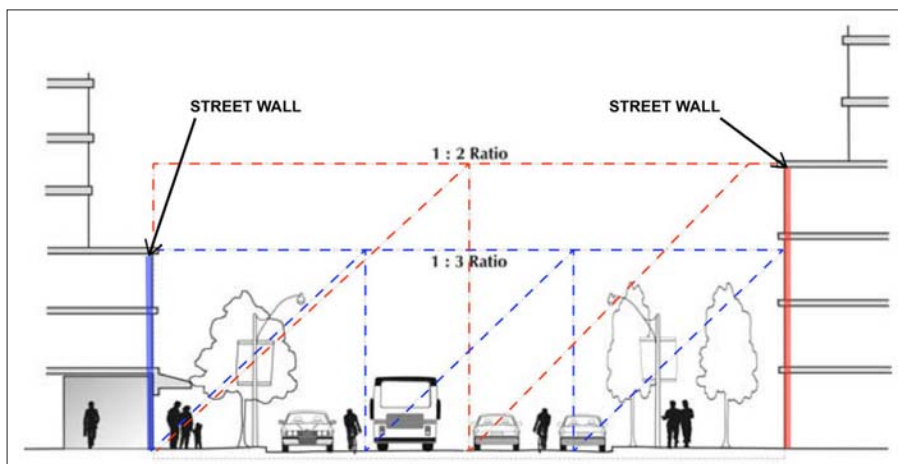


Figure 1: A building height-to-street-width ratio of between approximately 1:3 and 1:2 is recommended to frame streets and provide human scale. Portions of buildings above the street wall are encouraged to step back.

- b. To mitigate the visual impact of building height and to maximize sunlight exposure to the street, the upper portions of buildings above the street wall should be set back by at least two metres.
- c. Where an established pedestrian-friendly street wall exists, the front facade of new buildings should be generally aligned with adjacent buildings to create visual continuity along the streetscape.
- d. Buildings with commercial uses at grade should generally be built up to the sidewalk. Portions of the front facade may be set back from the front property line to accommodate features such as patios, courtyards or seating areas.
- e. Buildings should create “eyes on the street” and public spaces by orienting doorways, windows and balconies to overlook sidewalks, walkways, parks and other open spaces.
- f. Consider unique rooflines for taller buildings that have a visually prominent location (e.g. at corners, or at terminating vistas of streets) in order to create a distinct landmark.

2) Building Design:

- a. Building facades, especially those facing streets, should be well-designed and articulated with human-scale architectural features that create visual interest for pedestrians. Facade designs should consider the rhythm and pattern of existing building facades and architectural elements in the surrounding context, such as building articulation, rooflines, window placement, entryways, canopies and cornice lines.
- b. Large expanses of blank walls should be avoided. Where this is not possible, design treatments such as vertical plant materials, landscaping, art (e.g. mosaic, mural or relief) or the use of other building materials and building elements are encouraged to add visual interest.
- c. Weather protection for pedestrians should be provided in the following manner:
 - a) Individual canopies or awnings of sufficient depth should be provided to protect pedestrians from inclement weather, especially at building entrances.
 - b) The underside of canopies should be illuminated.
 - c) Canopies with translucent or frosted glazing are encouraged to maximize winter sunlight, particularly for north-facing facades.
- d. For buildings located on a corner, the corner design should include an architectural feature that addresses and emphasizes the corner. Strategies to achieve this include but are not limited to a chamfered or setback corner, prominent glazing, or a primary building entrance oriented to the corner.
- e. The first storey of a mixed-use or commercial building should be designed with a minimum floor-to-ceiling height of at least 4m and a minimum depth of approximately 10 metres to accommodate a range of commercial uses.
- f. Buildings with commercial uses at grade should be designed with a series of modulated storefronts and entrances, with transparent glazing. This design strategy is encouraged even where the building has a single tenant or use.



Figure 2: Modulated, transparent storefronts create interest for pedestrians and encourage activity along the street.

- g. Buildings that extend along sloping sites should be designed to follow and respond to the natural topography while maintaining a strong relationship of facades and building entrances to the street. Where retaining walls are unavoidable, they should be designed to ensure that they do not negatively impact the pedestrian experience along adjacent sidewalks.

3) Parking:

- a. Parking should be located underground or to the rear of buildings to provide human scale pedestrian environments. Where rear yard surface parking is proposed, building designs and landscaping interventions should be employed so that parking is integrated into sites in a manner that results in an attractive and safe environment.

4) Livability:

- a. Where two or more buildings are located on a single site, or where a single structure contains two or more building elements above a common base or podium, a comfortable separation space should be provided for residential units, with consideration for window placement, sunlight penetration to residential units, and adequate spaces for landscaping.

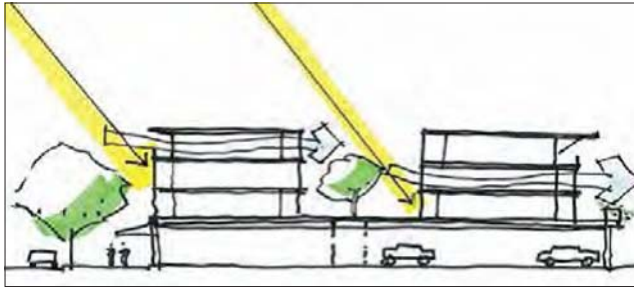


Figure 3: Comfortable separation space allows for sunlight access to individual units and outdoor spaces.

- b. Multi-unit buildings should be designed to provide a sensitive transition in scale to adjacent, smaller developments through considerations for massing and other design features. Strategies to achieve this may include but are not limited to setting upper storeys back, varying roof lines, siting or scaling buildings to reduce shading, etc.
- c. Residential building designs are strongly encouraged to include common outdoor space such as landscaped courtyards or rooftops, where possible.
- d. Buildings with residential use should be designed so that units receive daylight and natural ventilation from at least two sides of the building, or from one side and a roof. Where possible, provide dwelling units with a choice of aspect: front and back, or on two sides (for corner units).
- e. Residential buildings located along busy arterial streets should incorporate design features that minimize noise and pollution impacts (e.g. triple-pane glazing, residential units oriented towards courtyards, design of residential units with multiple orientations or side orientations, and building air intakes located away from the road).
- f. As a means to improve privacy between adjacent buildings, consider design solutions such as window size, window height, window placement and orientation, exterior landscaping, privacy screens or the use of frosted glazing on balconies.
- g. Pedestrian walkways that connect the primary entrance of multi-unit residential or commercial buildings with the adjacent public sidewalk should be a minimum of 2 m wide and distinguishable from driving surfaces by using varied paving treatments.

5) Materials and Finishes:

- a. Exterior materials that are high quality, durable and capable of withstanding a range of environmental conditions throughout the year are strongly encouraged, particularly on lower portions of buildings that are more closely experienced by pedestrians. High quality building materials include but are not limited to:
 - Natural wood
 - Composite materials
 - Brick masonry
 - Glazed tile
 - Stone
 - Concrete
 - Flat profile “slate” concrete tiles
 - Glass and wood for window assemblies
 - Standing seam metal roofing
- b. Light-coloured, heat reflective and permeable paving materials are encouraged for hard surfaces such as parking areas, walkways, patios and courtyards as a means to reduce storm water run-off and reduce heat-island effects. Light-coloured or heat reflective materials are also encouraged for rooftops to reduce heat island effects.

6) Landscaping and Open Space:

- a. Buildings that include residential units should include private open space (e.g. balconies, porches) or easily accessed shared open space in the form of courtyards, green spaces, terraces, yards, play areas or rooftop gardens.
- b. The rear yard of multi-unit or mixed-use buildings adjacent to lower scale residential development should provide landscaping and trees that mitigate the appearance of massing and contribute to a transition in scale.
- c. Landscape design should consider the local climate and water efficiency through species selection, including selection of draught-tolerant species, efficient irrigation systems or design of unirrigated landscapes, use of run-off for irrigation, presence of rain gardens and other approaches.
- d. Consider features in landscaping or open space that add to sociability, such as shared areas to sit, garden plots, play areas, balconies fronting courts, etc.

Area-Specific Guidelines:

In addition to the General Guidelines, the following guidelines apply to each specific designated area.

1) Town Centres

a. Mayfair Town Centre

- i. Taller buildings should generally be focused in the western part of the site, near Douglas Street.
- ii. Design taller buildings to have a clear architectural distinction between the base (podium or street wall portion), middle and upper portion of the building.
- iii. The podium base or street wall portion of buildings are encouraged to be three to five storeys (approximately 10–15 m) in height.
- iv. Major redevelopment of the Mayfair Shopping Centre should incorporate an internal network of pedestrian-friendly streets and connections between Speed Street, Nanaimo Street and Oak Street in order to create a structure of city blocks and to support permeability for pedestrians, cyclists and vehicles.

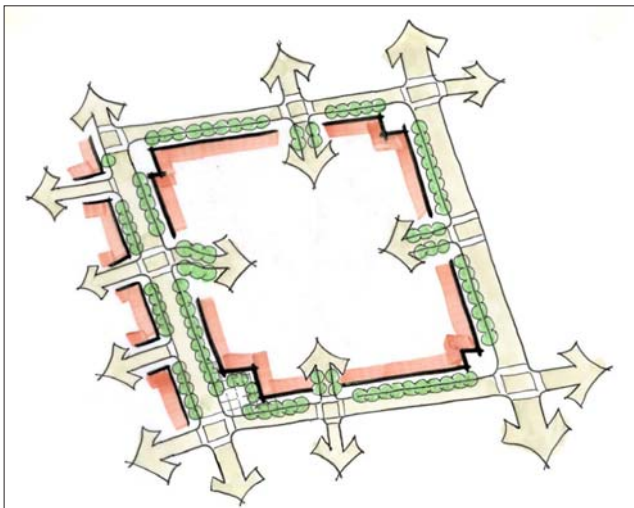


Figure 5: Major redevelopment of the Mayfair Shopping Centre site should establish an internal block structure connecting to adjacent streets.

- v. Building design should emphasize Douglas Street as the primary retail street of the Mayfair Town Centre. However, building designs should not “turn their back” on adjacent streets. Instead, provide facades that address all street frontages and are consistent with the General Guidelines for Building Designs (SECTION 2).
- vi. Building design that results in a landmark expression is encouraged at the intersection of Douglas and Finlayson Streets.
- vii. The tower portions of buildings above six storeys in height should generally be sited and designed to maintain access to sunlight, with a sufficient face-to-face separation distance between towers on the same site, and a sufficient clear distance to lot lines abutting other developable parcels. A desired face-to-face separation distance for towers at the Mayfair Shopping Centre site (the area bounded by Douglas Street, Nanaimo Street, Blanshard Street and Tolmie Avenue) is 25 metres.

2) Villages

a. Gorge at Irma Village

- i. Development within this village should create multiple smaller storefronts facing Gorge Road and turning the corner onto Irma Street to support a variety of neighbourhood-oriented commercial uses.

b. Craigflower Village

- i. Craigflower Village is envisioned as a neighbourhood-serving area with smaller storefronts, facades activating street or park edges regardless of use, and with scale transitioning to the adjacent neighbourhood. To achieve this:
 1. Buildings with commercial uses at grade should feature frequent entries and smaller storefront modules. A maximum distance of 10 metres for each module should be maintained.
 2. Between Raynor Street and Russell Street, new buildings should be sited close to the sidewalk's edge, regardless of use. Greater setbacks are encouraged where they accommodate residential or commercial patios or porches.

c. Catherine at Edward Village

- i. Buildings fronting along Catherine Street between Edward Street and Langford Street are encouraged to establish a consistent streetwall close to the sidewalk's edge, regardless of use, with individual entries on the ground floor. Greater setbacks are encouraged where they accommodate residential or commercial patios or porches.
- ii. Features which activate the frontage (e.g. glazing along the ground floor, entries, patios or porches, and windows or balconies providing for surveillance of the public realm) should wrap around the corner to activate the laneway (Bella Street) as a pedestrian or gathering space and provide for surveillance.
- iii. Establish sensitive transitions to adjacent lower-scale residential development, considering building massing, access to sunlight, privacy, and landscape.

d. Westside Village

- i. The section of Wilson Street stretching from its intersection at Bay Street and running east along the edge of Vic West Park is envisioned as an important gathering area. In addition to the General Guidelines, development in this area should provide frequent entries and be set back to provide sufficient space for pedestrian circulation, patio dining and street furniture along the Wilson Street frontage.
- ii. With new development, maintain or enhance the pedestrian connection that links Tyee Road to Wilson Street through the site as illustrated conceptually in Figure 6.



Figure 6: Conceptual illustration for Westside Large Urban Village

- iii. Buildings fronting on the Wilson Street – Bay Street intersection should engage the intersection with corner entries, enhanced pedestrian treatments or small plazas.
- iv. Buildings at the two identified terminating vistas on the west side of Bay Street, identified in Figure 6, should create a distinct landmark consistent with the General Guidelines.
- v. With redevelopment of the block west of Bay Street, new development is encouraged to create an internal pedestrian and/or road network linking the neighbourhood to the west, Bay Street, and Langford Street. (See, for example, conceptual illustration in Figure 6.)
- vi. In order to transition sensitively to the neighbourhood to the west, buildings fronting Alston Street should establish a streetwall of no more than 3 storeys as viewed from Alston Street, with modest landscape setbacks, architectural design and features (such as windows and doors) which activate the public realm.
- vii. Langford Street between Alston Street and Tyee Road is envisioned as an opportunity to establish a physical environment accommodating a mix of employment uses, possibilities for artisan, production, and similar uses mixed with pedestrian activity. Buildings fronting Langford Street are encouraged to be designed with frequent entries, individual storefront modules, and features such as roll-up doors or loading bays which can accommodate a variety of uses.
- viii. See also Supplementary Guidelines for Light Industrial Uses in Victoria West Corridors and Villages (section 11 of these Guidelines), if such uses are anticipated.

3) Corridors

a. Douglas-Blanshard Corridor

- i. In the Humber Green area between Douglas and Blanshard Streets, residential units are encouraged to be oriented to inner courtyards or quieter interior streets to mitigate noise impacts from adjacent arterial traffic. However, building designs should not “turn their backs” to Douglas and Blanshard Streets. Instead, provide facades that address all street frontages and are consistent with the General Guidelines for Building Designs (SECTION 2).

b. Gorge Road East Corridor

- i. Redevelopment along Gorge Road East should consider site planning and building massing to preserve and enhance view corridors looking south from Balfour Street and Carroll Street toward the Olympic Mountains.

c. Core Songhees Employment Corridors

The following guidelines apply to the area bounded by Alston Street, Tyee Road, and Langford Street, as well as lands fronting directly onto Langford Street between Alston Street and Tyee Road. These areas contain existing employment uses and are intended to accommodate future employment uses mixed with, or transitioning to, residential uses.

- i. In order to sensitively transition to the lower-scale residential areas to the west, buildings fronting Alston Street should establish a streetwall of no more than 3 storeys as viewed from Alston Street, with modest landscape setbacks, architectural design and features (such as windows and doors) which activate the public realm.
- ii. The design of buildings fronting onto the south side of Tyee Road should mitigate impacts on residential development across the street, considering massing and access to sunlight, given changes in elevation on these sites.
- iii. Langford Street between Alston Street and Tyee Road is envisioned as an opportunity to establish a physical environment accommodating a mix of employment uses, possibilities for artisan, production, and similar uses mixed with pedestrian activity. Buildings fronting Langford Street are encouraged to be designed with frequent entries, individual storefront modules, and features such as roll-up doors or loading bays which can accommodate a variety of uses.
- iv. See also Supplementary Guidelines for Light Industrial Uses in Victoria West Corridors and Villages (section 11 of these Guidelines), if such uses are included.

d. Esquimalt Road Corridor

- i. New development within the corridor and located between Mary Street and Victoria West Park should be designed to respect the public view corridors identified from Catherine Street at Edward Street, and from Mary Street at Henry Street, looking south to the Olympic Mountains, by considering the location, siting, massing and design of new development.
- ii. Uses along the south side of Esquimalt Road which are adjacent to the (future) E&N Rail Trail should provide for active frontages facing the trail, with entries, transparent glazing, and upper floor windows or balconies which provide for “eyes on the trail.” Pedestrian areas with a mix of hard and soft landscaping should be provided adjacent to the trail and delineated from the portion of the trail which is for active movement.
- iii. Development adjacent to the intersection of Esquimalt Road and Dominion Road should help create a gateway recognizing the transition between Esquimalt and Victoria.
- iv. Buildings fronting onto Dundas Street, or other streets where they are located across from Traditional Residential development, should transition sensitively across the street. Strategies to achieve this include emphasizing front yard landscaping, individual unit entries, and streetwall height.
- v. Buildings located adjacent to lower-scale residential development should transition sensitively to that development, considering building massing, access to sunlight, privacy, and landscape.

4) Supplementary Guidelines:

a. Light Industrial Development in Victoria West Corridors and Villages

Context and Intent: The Victoria West neighbourhood is a mixed neighbourhood of largely residential, commercial and mixed uses with pockets of light industrial uses in specific corridors. These light industrial areas accommodate employees and customers, interface with public spaces such as streets and trails, are found adjacent to or across the street from residential or commercial areas, and may contain a mix of uses. Therefore, it is important that care be taken in designing light industrial development to provide a positive environment for users, to minimize impacts on adjacent non-industrial development, and to support the pedestrian environment and public realm.

In addition to the General Guidelines, the following additional guidelines apply to any development which permits a light industrial use:

- i. Where development with an industrial use is located adjacent to or across the street from residential development, ensure a sensitive transition by:
 1. Screening any outdoor storage areas, work areas or loading areas, incorporating generous landscape where adjacent to residential uses. Where light industrial uses or loading are likely to generate noise, screening may include more substantial features (e.g. walls) which provide noise attenuation.
 2. Locating outdoor loading, work and storage areas away from adjacent residential uses.
 3. Providing sufficient building separation between light industrial uses and adjacent residential uses.

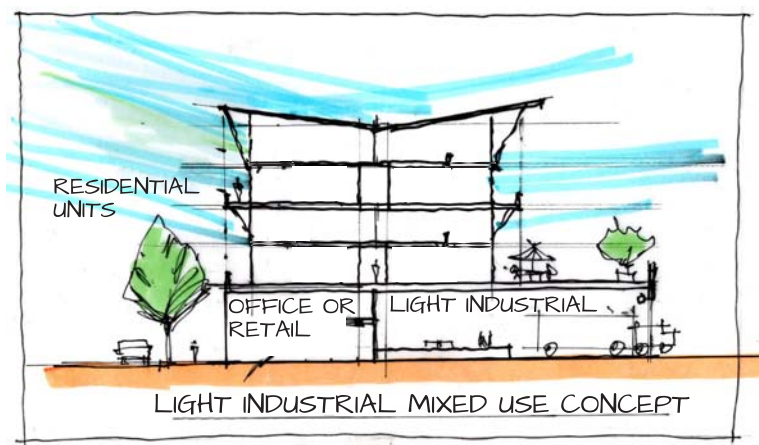


Figure 7: Example of a light industrial and residential mixed use building, showing how residential units might be set back from the loading areas.

4. Locating and screening ventilation and other equipment so as to minimize noise and visual impacts on residential uses.
- ii. In development which mixes light industrial uses with other uses (including commercial and residential uses), design should mitigate impacts of industrial uses on non-industrial uses. These impacts include but are not limited to noise, odours, glare and visual impacts of outdoor storage and activity. Strategies to achieve this include but are not limited to:
 1. Avoid residential overlook of loading areas or outdoor storage areas, through organization of building massing and orientation of windows and balconies.
 2. Incorporate methods for noise attenuation (e.g. triple-glazed windows; organization of building massing).
 3. Locate air intakes away from loading, circulation or work areas.



Design Guidelines for Attached Residential Development

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Design Guidelines for Attached Residential Development

Introduction

The purpose of these guidelines is to encourage high quality design that enhances neighbourliness and social vitality and creates a good fit with the existing neighbourhood.

These guidelines apply to residential developments of three or more self-contained units on a site, each having direct access to the outside at grade level, and at least three of which share common walls. Attached Residential Developments can be designed in different forms (e.g. townhouses, rowhouses, etc.). They can also be designed in different configurations, and may involve more than one building complex on a site, which may be organized in more than one row where supported in plan policies and permitted by zoning.

1) Site Planning

Objectives: *To site buildings in a manner that considers and maintains the pattern of landscaped front and back yards, that makes a positive contribution to the streetscape and that achieves a more compact residential building form, while maintaining livability.*

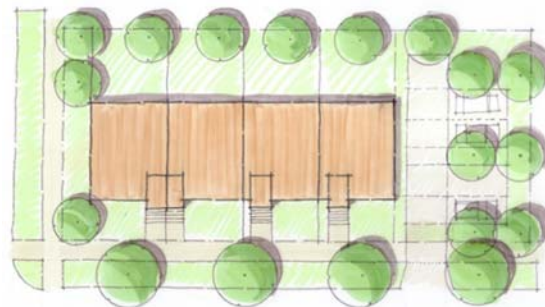
- a. Building Placement
 - i. Attached residential buildings should be designed parallel to the street with unit entrances oriented to, and directly accessed from, the fronting street. Both front and rear yards should be provided.
 - ii. For properties that include buildings of heritage value (Heritage Designated or listed on the City's Heritage Register) alternative siting of new buildings or additions may be considered to facilitate heritage conservation.



Townhouses can contribute positively to streetscapes and the overall neighbourhood, supporting walking and creating greater housing diversity within existing neighbourhoods.



Illustrative examples of how townhouses along a street might be arranged with well-articulated street frontages (above and below).



Illustrative example of how townhouses might be organized on a corner lot to minimize curb cuts and provide for on-site open space.

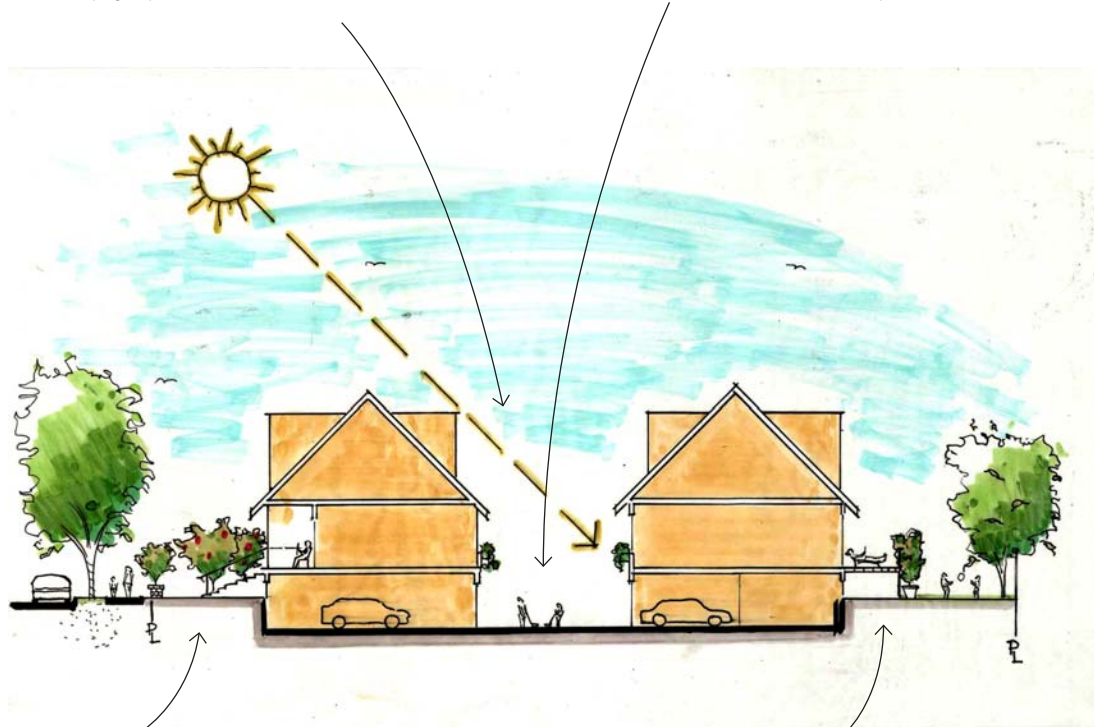
- iii. For properties that include significant natural features (e.g. significant trees, topography, rocky outcrops), buildings and landscape should be sited and designed to respond to natural topography and protect significant natural features wherever possible. Strategies to achieve this include but are not limited to alternative siting or clustering of buildings to avoid disturbance of natural features, and clustering of parking to reduce pavement on the site. (See also Section 4)
- iv. Some locations and lot sizes, as noted in local area or neighbourhood plan policies, may permit developments sited in more than one building complex on a site (i.e. more than one row). For these developments, the following should be achieved:
 1. Site planning should ensure that dwelling units face the street;
 2. Units located in the interior of lots should be designed with adequate separation from other buildings and have access to open space;



3. Vehicle access, parking and circulation should be integrated sensitively so it is not the dominant aspect of the development. See Section 1. vi. for further guidance.
 4. Dwelling units located in the interior of a site should have rear yard and side yard setbacks sufficient to support landscape and sensitive transitions to adjacent existing development and open spaces.
 5. Sufficient building separation should be provided between buildings to maximize daylight and minimize shadowing and overlook.
 6. Buildings which do not front onto the public street should be sited to provide sufficient separation from shared property lines and adjacent development in order to reduce overlook and shading, protect privacy for residents and neighbours, and provide space for landscaping.
- v. “Galley-style” developments, where building complexes are sited perpendicular to streets with residential unit entries oriented internally, are strongly discouraged. This layout is discouraged because it does not orient as many residential units towards the street, and typically provides less landscaped open space.

Ensure adequate building separation to enable natural daylight penetration (8 metres is desired)

Design driveways and parking access as flex-use shared spaces

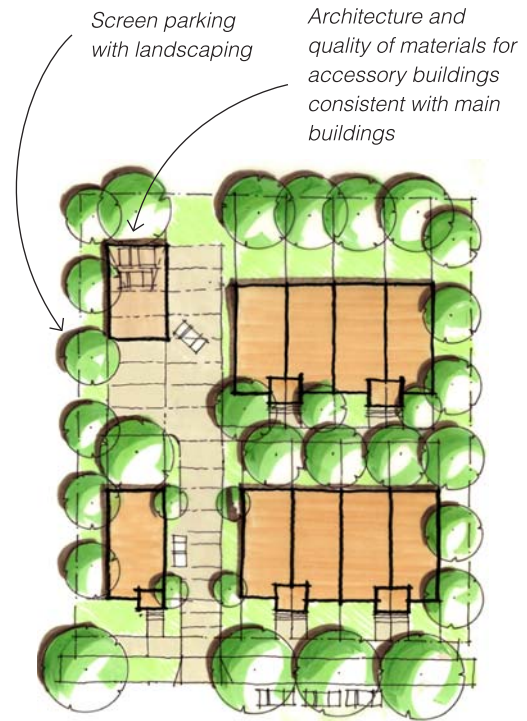


Orient and animate entry ways towards public streets

Incorporate semi-private and private usable outdoor amenity spaces for residents

vi. Vehicular access, circulation, garage doors and parking should not be the dominant aspect of developments and should be integrated to minimize impacts on fronting streets and adjacent public and private open spaces. Design strategies should be employed to minimize the impact of accommodating vehicles on site, including but not limited to the following:

1. Integrate parking in a manner that provides substantial landscaped areas in rear yards;
2. Locate and consolidate off-street parking areas to minimize extent of driveways and eliminate need for driveway access to individual units (refer to site plan showing shared/clustered parking);
3. Consider grouping driveway access points to minimize the number of driveway cuts and maximize space for landscaping and on-street parking;
4. Location of driveway access should strive to preserve existing canopy trees or provide opportunities for new canopy trees within the boulevard by providing enough planting space. See Section 4 Open Space Design for further guidance;
5. Front-accessed parking may be appropriate in some areas in order to avoid excessive pavement in rear yard areas. In these cases, attention to design is required to emphasize front yard landscape, provide tree planting space, and ensure a pedestrian-friendly building façade.
6. Minimize the impact of garage doors and vehicular entries by recessing them from the facade to emphasize residential unit entries.
7. Use high quality and, where appropriate, permeable paving materials for driveways;
8. Use attractive, high quality materials and consider incorporating glazing in garage doors;
9. See Section 4, Open Space Design for additional design guidelines related to landscaping and screening.



Example of a site layout which clusters parking in order to minimize the area of the site dedicated to vehicle circulation, and enhances the relationship of individual units to open spaces.

2) Orientation and Interface - A Friendly Face

Objectives: To ensure new development is oriented and designed to enhance public streets and open spaces and encourage street vitality and safety through increased “eyes on the street.”

- a. Residential buildings should be sited and oriented to overlook public streets, parks, walkways and open spaces balanced with privacy considerations.
- b. Developments should maintain a street-fronting orientation, parallel to the street.
- c. All residential units facing streets should have entries oriented towards, and be clearly accessible and visible, from the street.
- d. Where some units do not front onto a public street, a clear, legible and welcoming pedestrian pathway from the public street should be established.
- e. For developments that have interior-facing units, ensure unit entries are legible and emphasized through design features.
- f. Consider design strategies to delineate private front yard spaces, porches or patios from the public realm, while maintaining visibility of unit entrances. Design strategies may include but are not limited to:
 - i. elevating the front entryway or patio slightly above the fronting sidewalk level; or
 - ii. where a change in grade is not desired to provide accessibility, delineate the space through other means such as landscaping features, low fencing or planters.
- g. The design and placement of buildings and landscape should establish a sensitive transition to adjacent parks, trails, open spaces, and natural areas, considering a landscaped edge; respect the root zones of adjacent trees; and minimize impacts on ecologically sensitive areas and natural features.
- h. For new development adjacent to parks and larger public outdoor open spaces, design should clearly delineate private from public spaces, to avoid “privatizing” of public space.
- i. The location of blank walls or extensive parking areas adjacent to parks, trails and natural areas is strongly discouraged.

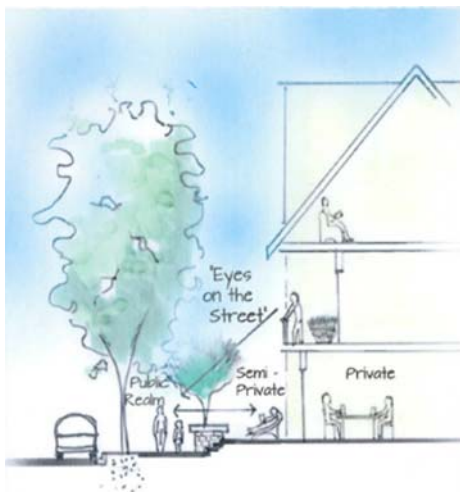


Illustration of interface with street.



Example of delineation of private front yard and visible entrance.

3) Building Form, Features and Context

1) Building Form and Design

Objectives: *To achieve buildings of high architectural quality and interest with human-scale building proportions that are oriented towards and are compatible with the established streetscape character and pattern. Human scale refers to the use of architectural features, details and site design elements that are human proportioned and clearly oriented towards pedestrian activity. Building articulation refers to the many street frontage design elements, both horizontal and vertical, that help create an interesting and welcoming streetscape.*

- a. Building design elements, details, and materials should create a well-proportioned and cohesive building design and exhibit an overall architectural concept.
- b. Incorporate a range of architectural features and design details into building facades that are rich and varied in detail to create visual interest when approached by pedestrians. Examples of architectural features include:
 - i. building height, massing, articulation and modulation
 - ii. bay windows and balconies
 - iii. fenestration pattern (proportions and placement of windows and entry ways)
- c. Modulation in facades and roof forms are encouraged to break up building mass, differentiate individual units within attached residential developments, and to provide architectural interest and variation along the street.

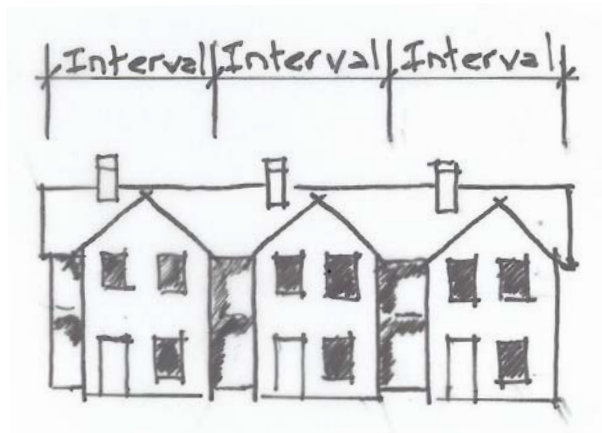


Development which exhibits a cohesive architectural expression, with variation in units, clear front entries, and architectural interest for pedestrians.



Historic traditional townhouses (left) demonstrate human scale architecture, relationship to the public street, and cohesiveness of architectural expression. These same principles should guide the design of more modern developments which may be expressed in varied architectural styles (example at right).

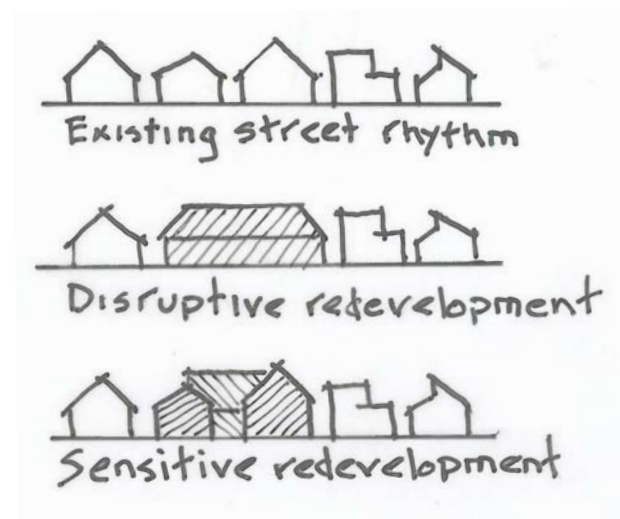
- d. Entrances should be located and designed to create building identity, to distinguish between individual units, and generally create visual interest for pedestrians. Well-considered use of architectural detail and, where appropriate, landscape treatment, should be used to emphasize primary entrances, and to provide “punctuation” in the overall street-scape treatment.
- e. Upper floor areas should be integrated into roof forms to help further mitigate the scale of new developments.
- f. Balconies should be designed as integral to the building. Overly enclosed balconies should be avoided, as these limit views and sunlight access.
- g. Building sidewalls should be designed to be attractive and interesting when viewed from adjacent buildings, street, and sidewalks through the use of materials, colours, textures, articulation, fenestration, and/or plant material.
- h. Creative use of landscaping or other screening should be used to reduce the perceived scale of adjacent development without compromising surveillance of public areas.
- i. Accessory structures should be compatible in architectural expression and quality of materials to main structures.



2) Neighbourliness/Compatibility

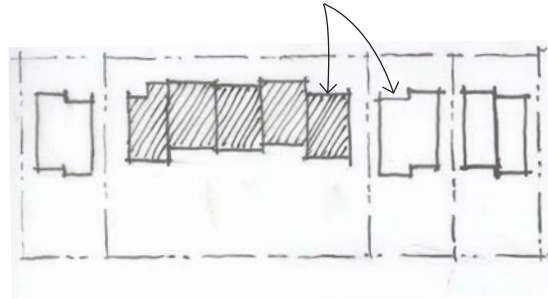
Objectives: To respond to the established form and architectural characteristics of surrounding buildings in order to achieve new buildings which are compatible with their context and minimize impacts on neighbours..

- a. New development should ensure a good fit with existing development by incorporating architectural features, details and building proportions that complement and respond to the existing architectural context, and by referring to distinctive and desirable architectural qualities of existing adjacent buildings in new development. Consideration should be given to the following aspect of development:
 - i. building articulation, scale and proportions
 - ii. similar or complementary roof forms
 - iii. building details and fenestration patterns
 - iv. materials and colour



- b. In some cases where a contextual architectural form and pattern does not exist, architectural character may be created rather than reflecting contextual precedent. In such cases, a well designed, new project can become a contribution to the context that may inform future development considerations.
- c. New townhouse development should transition in scale to existing residential buildings. Strategies to achieve this include but are not limited to the following:
 - i. A maximum one storey height difference between the end units of new street fronting developments and adjacent existing development should be achieved.
 - ii. The end units of new street fronting townhouse developments should be sited to match or transition to the front yard set back of adjacent existing residential buildings.
- d. The views from upper stories of new buildings should minimize overlook into adjacent private yards, especially in less intensive areas. Strategies to achieve this include but are not limited to the following:
 - i. Increased setback.
 - ii. Stagger windows to not align with adjacent, facing windows.
 - iii. Primary windows into habitable spaces, and also decks and balconies, should not face or be oriented to interior side-yards
 - iv. Locate and screen upper level windows, decks, and balconies to minimize overlook.
 - v. Use of skylights, translucent windows and clerestory windows are encouraged to minimize overlook of side yards.
 - vi. Landscape screening.
- e. Site, orient and design buildings to minimize shadowing impacts on adjacent properties.

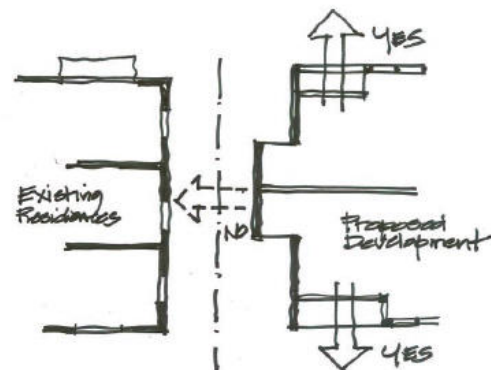
End units should be set back to match or transition to existing neighbouring houses



3) Mechanical equipment and service areas

Objective: To site and screen mechanical equipment and service areas to minimize impacts on neighbours and the public realm.

- a. Mechanical equipment, vents and service areas (e.g. for the collection of garbage or recycling) should be integrated with architectural treatment of the building, and screened with high quality, durable finishes compatible with building design.
- b. Mechanical equipment, vents and service areas should be located to minimize impacts on adjacent development by avoiding proximity to windows, doors and usable outdoor spaces.
- c. Location and installation of gas and electrical meters and their utility cabinets, as well as other mechanical or service apparatus should be carefully integrated into building and site design. Gas and electrical metres and utility cabinets on building frontages should be screened.



Orientation and placement of windows, balconies and porches to respect privacy of adjacent development

New developments should transition in height to existing adjacent residential buildings





High quality materials in buildings and landscape elements

4) Materials

Objective: *To use materials which are high quality, weather gracefully, and contribute to the overall neighbourhood image.*

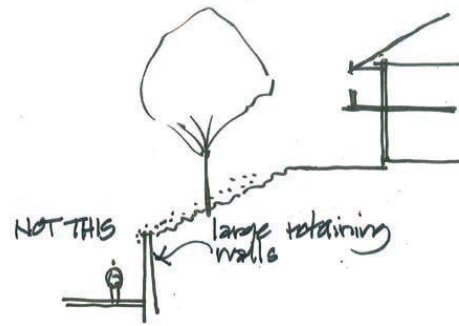
- a. An integrated, consistent range of materials and colours should be used, and variety between buildings and building frontages should be provided that is consistent with the overall streetscape.
- b. In general, new buildings should incorporate substantial, durable and natural materials into their facade to avoid a 'thin veneer' look and encourage graceful weathering of materials over time. Materials such as masonry, stone, natural wood, etc. are encouraged. Vinyl siding and large areas of stucco are discouraged and should generally be avoided.

4) Open Space Design

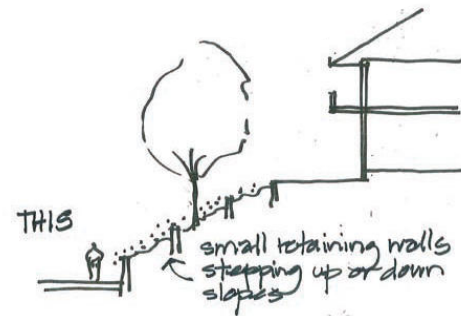
Objective: *To enhance the quality of open space, support the urban forest, provide privacy where needed, emphasize unit entrances and pedestrian accesses, reduce storm water runoff, and to ensure that front and rear yards are not dominated by parking.*

- a. Landscape treatments including use of front patios, accented paving treatments, fence and gate details, and other approaches are encouraged to help call out a residential entry and add interest along the street and sidewalk
- b. Areas within setbacks should incorporate plantings to create a green interface between buildings and streets
- c. Topographic conditions should be treated to minimize impacts on neighbouring development, for example by using terraced retaining walls of natural materials or by stepping a project to match the slope.
- d. Development should avoid significant reworking of existing natural grade.
- e. Where a building's ground floor is elevated above a pedestrian's eye level when on the sidewalk, landscaping should be used to help make the transition between grades. Some techniques for achieving this guideline include:

- i. rockeries with floral displays, live ground cover or shrubs.
- ii. terraces with floral displays, live ground cover or shrubs.
- iii. low retaining walls with raised planting strips
- iv. stone or brick masonry walls with vines or shrubs.
- f. Accessibility should be provided, where possible, in open space design.
- g. Landscape areas are encouraged to include a mixture of tree sizes and types
- h. Landscape on sites with significant natural features (e.g. significant trees, topography, rocky outcrops) should be located and designed to be sympathetic to the natural landscape.
- i. Consider planting tree species and other landscape plants that will tolerate a degree of drought and will survive the summer water restrictions and dry conditions of southern Vancouver Island.
- j. In considering tree placement along boulevards or in the front yard setback adjacent to street rights-of-way, consider tree sizes and spacing indicated by the City's specifications and policies for street trees.
- k. Landscaped screening along circulation and parking areas which abut lot lines is strongly encouraged, while maintaining site lines and enabling casual surveillance. Other surface parking areas should be screened with landscaping.
- l. Integration of landscaping to soften hardscape areas associated with vehicle circulation and parking is encouraged.
- m. Site design should integrate features to mitigate surface runoff of stormwater. This may include a variety of treatments (e.g. permeable paving for driveways and parking areas, landscape features designed for rainwater management, cisterns or green roofs, and/or other approaches) which are consistent with approved engineering practices and other city policies.
- n. Non-glare lighting should be provided at residential unit entrances, along pedestrian paths and common areas to contribute to safety. Lighting strategies that mitigate undue spill-over for adjacent residential units are strongly encouraged.



Avoid large retaining walls and significant reworking of natural grade.



Transition sensitively between grades.