



## **Council Report**

**For the Meeting of April 26, 2017**

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<b>To:</b>	Council	<b>Date:</b>	April 16, 2018
<b>From:</b>	Jonathan Tinney, Director, Sustainable Planning and Community Development		
<b>Subject:</b>	Proposed Victoria West Neighbourhood Plan and Associated Official Community Plan Amendment Bylaws		

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### **RECOMMENDATION**

1. That Council give first and second readings of Official Community Plan Amendment Bylaw (Bylaw No. 18-056) concerning Urban Place Designations and Development Permit Areas in the Victoria West Neighbourhood at the April 26, 2017 Council meeting;
2. That Council consider the Official Community Plan Amendment Bylaw in conjunction with the City of Victoria 2018-2022 Financial Plan, the Capital Regional District Liquid Waste Management Plan and the Capital Regional District Solid Waste Management Plan pursuant to Section 477(3)(a) of the Local Government Act, and deem those Plans to be consistent with the proposed Official Community Plan Amendment Bylaw.
3. That Council consider approval of the Victoria West Neighbourhood Plan, 2018, at the same Council meeting at which the above Bylaws are considered and allow public comment.
4. That upon approval of the Victoria West Neighbourhood Plan, 2018, that Council rescind the Victoria West Neighbourhood Community Plan (1988).

### **EXECUTIVE SUMMARY**

The purpose of this report is to present Council with the proposed Victoria West Neighbourhood Plan, 2018 for consideration of approval (Attachment 1), and associated Official Community Plan bylaw amendments (Attachment 2) for consideration of 1<sup>st</sup> and 2<sup>nd</sup> readings. The plan has been created over the last 24 months with participation from the community, Council and staff. Council provided feedback on the draft Victoria West Neighbourhood Plan on October 19, 2017, considered the proposed Official Community Plan (OCP) amendments on December 7, 2017 and directed staff to undertake public consultation on the revised draft plan and proposed OCP amendments.

Engagement results showed support for the revised draft plan and OCP amendments, with specific concerns related to some topics. Staff have responded by making some additional changes to the neighbourhood plan policies and associated OCP amendments to address this feedback. The changes include:

- New revisions to the Employment-Residential urban place designation in certain areas to emphasize retention of, and flexibility in, employment use if residential uses are included;
- Re-instatement of the Traditional Residential urban place designation for some properties on the south end of Mary Street and at Skinner and Russell Streets that were considered

for Urban Residential in earlier versions of the plan, with new changes to support multi-unit housing up to 3 storeys in height;

- Revisions to the proposed development permit area boundaries and guidelines to further emphasize sensitive transitions to lower-scale residential development in specific areas;
- Revisions to the development permit guidelines for attached residential developments to further address townhouse compatibility;
- Revisions to clarify uses and provide more design guidance for Pioneer Co-op site; and
- More detail related to parks, transportation, waterfront access and food systems, and new content related to population trends.

## PURPOSE

The purpose of this report is to seek Council's approval of the proposed Victoria West Neighbourhood Plan, 2018, and introduce an Official Community Plan amendment bylaw for consideration of 1<sup>st</sup> and 2<sup>nd</sup> reading to align with the proposed neighbourhood plan.

## BACKGROUND

The draft Victoria West Neighbourhood Plan, proposed Official Community Plan (OCP) amendments and proposed consultation came before Council on December 7, 2017 where the following resolutions were approved:

1. Consider consultation under Section 475(1) and 475(2) of the *Local Government Act* and direct staff to undertake consultation with those affected by the proposed amendments to the Official Community Plan through online consultation and a public open house concurrent with public review of the proposed Victoria West Neighbourhood Plan.
2. Consider consultation under Section 475(2)(b) of the *Local Government Act* and direct staff:
  - a. to refer the proposed Official Community Plan amendments to the Songhees Nation, the Esquimalt Nation, the Township of Esquimalt and School District 61;
  - b. that no referrals are necessary to the Capital Regional District Board, Island Health or the provincial or federal governments.
3. Direct staff to prepare Official Community Plan amendment bylaws following consultation to adjust urban place designations, adjust development permit area boundaries and guidelines in accordance with feedback received on the proposed Official Community Plan amendments.
4. Refer the proposed Victoria West Neighbourhood Plan to the meeting of Council at which the above Official Community Plan amendments Public Hearing is held, for consideration of final approval.
5. Following approval of the proposed Victoria West Neighbourhood Plan, rescind the Victoria West Neighbourhood Community Plan (2002).

## PUBLIC FEEDBACK

Between January 10 and February 28, 2018, staff sought input on revisions to the first draft plan and proposed OCP amendments (Attachment 3) as per Council direction. A summary and compilation of the engagement results is attached (Attachment 4).

Engagement results showed support for many of the proposed OCP amendments and revisions to the draft plan, with specific concerns related to some topics. The Victoria West Community Association also led a targeted review of the revised plan and OCP amendments with their members, which indicated different results from City-led engagement on some issues.

Key results from the latest round of engagement include:

- People were **generally satisfied with their involvement** in the plan, and with some feeling they had not been as involved as they wanted to be due to lack of awareness or busy schedules.
- Some **expressed concern regarding additional housing and associated impacts** on neighbourhood character, traffic, parking and food-growing potential outside of the master planned communities, particularly for building heights or new Urban Residential areas (particularly south of Esquimalt Road). Many highlighted the need for sensitive transitions.
- Others were **concerned the plan is not ambitious enough**, particularly in supporting new housing, future housing attractive to families with children, sufficient height or density to support non-market housing, or missed opportunities to locate more housing near transit and downtown.
- There was **general support for changes to employment areas, with some concerns** that employment be the top priority, that flexibility of use (between light industry and office) be maintained, that different areas be treated the same, and that employment uses transition sensitively to residential areas. The Community Association is concerned about the building heights proposed in these areas.
- There was **general support for changes to urban villages**, with some differing opinions on appropriate heights or densities, and extent of commercial uses at/adjacent to small urban villages. Community Association feedback recommended removing the proposed expansion of Craigflower Village and limiting the proposed Catherine at Edward Village to 2 ½ storeys.
- There was a **mix of opinions regarding three proposed changes from Traditional to Urban Residential designation**, with support for changes along Esquimalt Road, mixed opinions for the change adjacent to Craigflower Village, and low support for a change on south Mary Street.
- There was **support for changes to increased building heights within Traditional Residential areas, with some concerns** for impacts on neighbours, character and traffic, particularly for Pioneer Housing Cooperative, south of Esquimalt Road and for new infill housing. The Community Association feedback recommended building heights throughout traditional residential areas be 2 ½ storeys.
- There was a desire for additional detail for park and transportation improvements, waterfront access, and stronger food systems content.
- Community Association feedback recommended more information including population projections, visualization of future development, prior zoning and consultation on future public amenities.

## STATUTORY CONSULTATION

Correspondence received from the Township of Esquimalt supports the proposed amendments to the OCP, particularly increased density along Esquimalt Road and the retention of employment lands. The Township indicated an interest in working with the City of Victoria to establish a prominent gateway at 760 Esquimalt Road.

Staff met with administration from School District 61 and Victoria West Elementary School to receive feedback, and have incorporated suggested changes regarding future childcare and public trails across the playing fields.

## CHANGES AND CONSIDERATIONS



After considering all of the feedback received, staff have made some changes to the proposed OCP amendments that were first presented to Council on December 7, 2017. Staff have also made revisions to the proposed Victoria West Neighbourhood Plan. Major changes, and their rationale, are detailed as below. Additional changes and rationale are included in a matrix in Attachment 5.

#### Changes to Proposed OCP Amendments

1. **Employment Lands:** In response to some concerns from the community and staff regarding the need for strong but more flexible policies for employment lands, staff propose the following:
  - a) revising proposed urban place designations to apply the same urban place designation, Employment-Residential, to most Vic West employment lands. This designation supports both employment and light industrial uses, and provides the option for limited residential uses, thus maximizing the flexibility of employment areas over time. The revised designation will maintain the maximum height limits specified in the plan policies and will slightly increase maximum floor space ratio for those areas previously proposed as Industrial- Employment (from 2.2 to 2.5);
  - b) revising the Employment-Residential designation to limit residential floor area to one half of the total floor area; and
  - c) revising plan policies to encourage light industrial, artisan or flexible uses at grade in certain locations.
2. **South Mary Street Urban Place Designation:** In response to a mix of community opinions on the proposed change from Traditional Residential to Urban Residential (up to 4 storeys) on the east side of Mary Street, south of Esquimalt Road, staff propose re-instating the Traditional Residential designation for this area with a new amendment to support apartment buildings or townhouses up to 3 storeys in height and 1.2 FSR (Housing Sub-area 2). This revision strikes a balance between maintaining the traditional residential character, while providing the opportunity for adding some additional housing density and diversity near transit and the waterfront.
3. **Development Permit Areas and Guidelines:** In response to community and staff feedback regarding development permit guidelines, staff have made the following revisions:
  - a) revised Design Guidelines for Attached Residential Developments (formerly referred to as townhouse guidelines) to strengthen design guidance for sensitive transitions to parks, natural areas, waterfronts and trails; and for the location and screening of mechanical equipment;
  - b) revised Guidelines for Corridors, Villages and Town Centres to include area-specific guidelines for sensitive transitions to adjacent residential areas at Catherine Street and Edward Street small urban village, and between employment areas and residential areas on Esquimalt Road and at Alston Street/ Tyee Road/ Langford Street;
  - c) added new plan policies to emphasize sensitive transitions to adjacent lower scale development in relevant locations
  - d) Skinner Street corridor shifted from proposed Development Permit Area 7A (which is a DPA for the purpose of revitalizing areas where commercial use is permitted) to Development Permit Area 15F (which is a DPA for the purpose of guiding form and character of intensive residential) given Skinner Street's traditional residential character.
4. **Density Bonus:** In response to recent Council direction to consider on-site affordable



housing as a community amenity contribution within the current limits of the Official Community Plan, staff have removed the OCP amendment that would have permitted an additional 10% density with the provision of affordable housing and other community amenities for certain urban place designations. Plan policies have also been updated to reflect this change.

5. **Russell Street at Skinner Street:** In response to mixed community feedback on the suitability of 3-4 storey mixed use buildings on this half-block adjacent to Craigflower Village, staff have revised the proposed OCP amendment to re-instate the original Traditional Residential urban place designation for this area, with a new amendment to support townhouses or apartments up to 3 storeys on this half-block. Plan policies have been revised to reflect this change.
6. **Westside Village:** In response to further urban design analysis, the opportunity for buildings over 6 storeys in height has been removed from this area. Due to the large property sizes, the anticipated densities in the plan can be achieved within the current 6 storey height limit. An OCP amendment for additional height is no longer proposed, and the plan policies have been revised reflect this change. The OCP has been amended to refer to the area as "Westside Village", consistent with the neighbourhood plan.

#### Changes to Proposed Plan Policies

7. **Pioneer Co-op:** In response to some community concern about potential impacts of buildings up to 3 storeys and allowing limited commercial or service uses on the Pioneer Co-op site, staff have revised the plan policies to clarify that any commercial uses should be limited in scale and not result in significant traffic increases (6.4.6., 6.10.2); and that buildings should be designed and located to minimize shading and privacy impacts on neighbouring properties (6.4.5.)
8. **Parks:** In response to community feedback requesting more detail for potential park improvements and waterfront features, staff have made the following revisions:
  - a) added new sub-sections for most neighbourhood parks to outline potential improvements
  - b) revised Map 6 to show current and potential waterfront access points
9. **Transportation:** In response to community feedback suggesting additional transportation improvements, staff have made the following revisions:
  - a) revised policy 3.4.1. to provide more detail about future Bay Street Bridge improvements
  - b) referred Alston and Langford Streets for evaluation as part of update to pavement management plan (3.3.10)
  - c) added new policy to evaluate Craigflower Road, Tyee Road, Bay Street, Wilson Street and Esquimalt Road for consistency of speed limits along the corridor and across municipal borders (3.6.9.)
  - d) revised policy 3.4.4. to add evaluation of pedestrian crossing at Craigflower Road and Raynor Street
10. **Food Systems:** In response to community feedback for more neighbourhood food system content in the plan, staff have met with the Vic West Food Security Collective and revised the plan policies to provide more recognition of Vic West's contribution to food security,

encourage and support the growing of food in a variety of places (12.2.3), and provide examples of how food features can be included in new multi-unit development (12.3.1).

- 11. Gateway at Esquimalt/ Victoria border:** In response to feedback from the Township of Esquimalt, staff have revised the plan to encourage the City to work collaboratively with Esquimalt to establish a gateway at Esquimalt and Dominion Roads to recognize the transition between the two municipalities.
- 12. Victoria West School:** In response to feedback from School District 61, staff have revised the plan to show potential trail connections on the school field in a more conceptual manner, and to support potential expansion of out-of-school care services at Victoria West School.
- 13. Population Estimates:** In response to community feedback, a new sub-section on population and housing trends has been added to the plan's introduction.
- 14. Neighbourhood Visualization:** In response to community feedback for a comprehensive visualization of what Vic West could like in the future, staff have added a new map to show anticipated building heights in the neighbourhood. A more detailed visualization would require significant work to prepare accurately, and is outside the resources of the neighbourhood plan project.
- 15. Other Revisions:** In response to community feedback, staff have made a number of minor revisions for clarity, brevity, tone and detail throughout the plan. These are indicated in blue in the proposed plan.

Staff have carefully considered all feedback and do not recommend additional changes to the OCP amendments or proposed neighbourhood plan beyond those outlined here. In revising the draft plan, staff feel the proposed plan strikes a careful balance between accommodating future housing needs and types, maintaining neighbourhood character, supporting transit and local businesses, supporting the urban forest and preparing for a future population that is less reliant on vehicles.

The urban place designation amendments proposed to the Official Community Plan are consistent with the infrastructure needs identified in the Capital Regional District Liquid Waste Management Plan and the Capital Regional District Solid Waste Management Plan.

## **OPTIONS & IMPACTS**

### *Accessibility Impact Statement*

The neighbourhood plan contains a number of policies that will make public spaces more accessible to people of all abilities, including improving pedestrian crossings, sidewalks and trails.

### *2015 – 2018 Strategic Plan*

This milestone in the Victoria West Neighbourhood Plan process supports Strategic Plan, Objective 3: Strive for Excellence in Planning and Land Use, which contains actions and outcomes to undertake local area planning focused on urban villages and transportation corridors.

### *Impacts to Financial Plan*

Some initiatives related to the proposed Victoria West Neighbourhood Plan will have associated capital and operating costs. The funding status of identified implementation actions is outlined in



the Action Plan in Chapter 16 of the proposed plan. Implementation of the plan will need to be balanced with available resources and other City priorities. The actions may be accomplished through a combination of funding sources, including City capital programming, amenity contributions from development, senior government grants and other partnerships.

#### *Official Community Plan Consistency Statement*

While the proposed Victoria West Neighbourhood Plan contains policy directions that are in line with the broad growth objectives and policies of the OCP, some amendments to the OCP are required to align the two plans given the finer grain of detail that emerged out of the neighbourhood plan process and to implement the urban design objectives.

### CONCLUSIONS

Consideration of the proposed OCP amendments concurrent with approval of the Victoria West Neighbourhood Plan will ensure the two documents are in alignment immediately, which will provide more clarity for the public, landowners and developers seeking to submit development proposals to the City, and for staff in providing development advice to applicants.

Respectfully submitted,

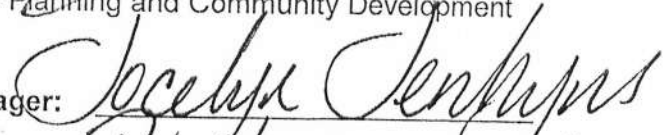
  
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-Att.

  
Jonathan Tinney  
Director  
Sustainable Planning and Community Development

Report accepted and recommended by the City Manager:

Date:

  
April 20, 2018

#### List of Attachments

- Attachment 1: Proposed Victoria West Neighbourhood Plan, 2018
- Attachment 2: Official Community Plan Amendment Bylaw 18-056
- Attachment 3: Engagement Materials (January 2018): Summary of Revisions to First Draft of Victoria West Neighbourhood Plan
- Attachment 4: Engagement Summary and Feedback on Proposed OCP Amendments and Revised Draft Plan (April 2018)
- Attachment 5: Matrix of Revisions to OCP Amendments and Proposed Victoria West Neighbourhood Plan (April 2018)
- Attachment 6: Minutes from Advisory Design Panel, November 29, 2017



# Victoria West

## Neighbourhood Plan

April 2018 **PROPOSED PLAN FOR ADOPTION**





The City of Victoria acknowledges that the land and water of the Victoria West neighbourhood is the traditional territory of the Lekwungen people.



## **With thanks**

The new Victoria West Neighbourhood Plan has been shaped by the participation of many residents, businesses and other stakeholders. The City of Victoria is appreciative of the contributions made by all members of the community.

The City of Victoria would like to recognize and extend special thanks to the Victoria West Neighbourhood Plan Working Group for their guidance and support for community engagement during the creation of this plan:

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Note: Blue text is used throughout this version to indicate changes made relative to the earlier proposed version of the neighbourhood plan.

Note: Red text remains in this document indicating changes made relative to the initial draft plan version of the neighbourhood plan.

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# Big Moves in the Plan

-  Create Strong Village Hearts
-  Knit old and new Vic West together
-  Connect the community
-  Add housing that fits
-  Create more places to live near transit and amenities
-  Strengthen connections to the waterfront
-  Support jobs and businesses in the neighbourhood
-  Strengthen Parks and Food Systems



Figure 1. Big Moves in the Plan





## Create Strong Village Hearts

### What we heard

The shops and services at Westside Village are a great community asset. In the future, improving cycling and walking access would enhance the village as a community gathering place. Future development should be sensitively designed to enhance streets and public places, add places to live and work and continue to provide access to shops, services and community facilities.

Craigflower Village is valued for its small shops, local flavour and friendly ambiance. Neighbours would like to maintain the character while supporting the existing businesses and providing more space for a few new shops and additional housing. The public space along Craigflower Road should be enhanced and connections to Banfield Park strengthened.

Other locations, including Harbour Road, Esquimalt Road near the E&N Trail, the Roundhouse, and Catherine Street at Edward Street present opportunities for neighbourhood gathering, [shops](#) and services.

### How the plan addresses what we heard

The plan supports the evolution of Westside Village as a mixed-use heart for the neighbourhood, with:

- multi-storey buildings for housing and employment

- an enhanced sense of place along streets and in public spaces, with a focus on Wilson Street. This includes ground-floor shops facing onto streets, enhanced street furniture and trees, and places to gather
- improved connections to and from the village for pedestrians and cyclists
- an opportunity for [an indoor community space](#)

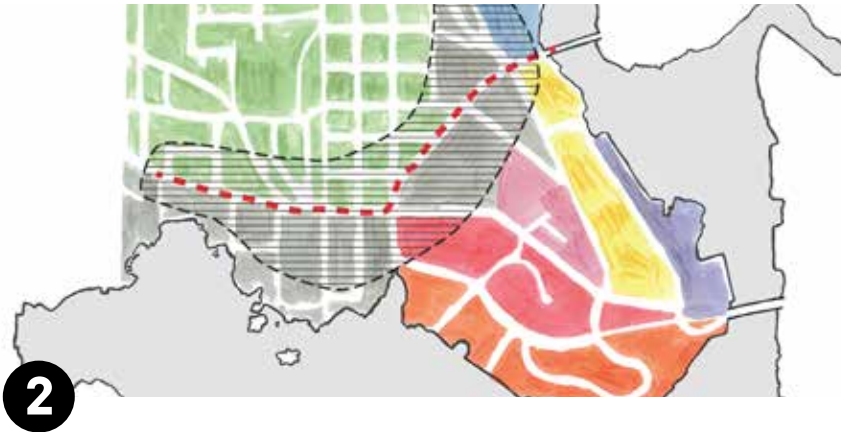
The plan supports small changes to the Craigflower Village area to:

- allow a broader mix of housing types [including options for small apartments or attached housing](#)
- improve places to gather on both sides of Craigflower Road
- [provide the option for](#) some additional shops while maintaining the [character](#) of the village

The plan also designates a small village in the block of Catherine Street north of Edward Street, supporting new mixed-use buildings of up to three storeys.

### For more information on this Big Move

See Chapter 7, Urban Villages.



## Knit old and new Vic West together

### What we heard

The Vic West neighbourhood is an eclectic, vibrant and diverse mix of old and new. People value the unique contrast of older, historic residential areas, new higher-density areas and employment lands. We heard the new and old parts of Vic West could be better knit together. There is differing urban design, busy roads, and people who don't know each other. There is a particular opportunity to enhance sense of place and connections along the Bay Street and Esquimalt Road corridor with centrally-located shops, housing and community facilities.

### How the plan addresses what we heard

The plan identifies ways to bring older and newer parts of Vic West together:

- completing north-south pedestrian and cycling routes
- recognizing that future retail spaces, parks and public places at Bayview Place (including Roundhouse) and Dockside will be important destinations for residents from across Vic West
- adding housing mixed with employment along Esquimalt Road, to create a more vibrant place
- enhancing the pedestrian environment on Bay Street
- reinforcing Westside Village as the heart of the neighbourhood
- exploring the opportunity for a future community facility near Westside Village

### For more information on this Big Move

See Chapter 3, Transportation and Mobility, Chapter 7, Urban Villages, Chapter 9, Core Songhees and Master Planned Areas, Chapter 13, Neighbourhood Well-Being and Chapter 14, Arts, Culture and Placemaking



## Connect the community

### What we heard

Vic West has several busy streets that are used for commuting traffic between Victoria and other municipalities. While two regional trails run through the community, it is often hard to get comfortably from neighbourhood destinations to the trails. Transportation improvements are needed to increase safety and connectivity, including better crossings, traffic calming and wayfinding.

### How the plan addresses what we heard

The plan identifies new routes for pedestrians and cyclists and locations that may need upgrading. Future improvements include:

- Complete pedestrian and cycling routes and develop new ones to connect different parts of the neighbourhood
- Address key intersections, crossings and areas for improvement to make walking and cycling more comfortable and safe
- Better connect the neighbourhood to existing regional trails
- Improve the network of pathways that connects through the complicated block pattern of the neighbourhood

### For more information on this Big Move

See Chapter 3, Transportation and Mobility.



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## Add housing that fits in older residential areas

### What we heard

Vic West celebrates the diversity of people and housing in the neighbourhood. There is also a deep appreciation of the older residential areas, with their low-scale housing, [green spaces and eclectic character](#). The community wants to see this character maintained, while increasing the range of housing choices and improving affordability.

### How the plan addresses what we heard

The plan supports the creation of more [housing diversity](#) while maintaining the character of the older residential area:

- [New opportunity for small apartment buildings near Esquimalt Road](#)
- [Support townhouses and houseplexes on larger lots in certain locations](#)
- [Reduce lot size appropriate for duplexes and consider smaller lots for small lot houses](#)
- Option for suites in duplexes, small lot houses, and townhouses fronting streets
- Protect and re-use heritage buildings by allowing additional housing
- Support revitalization of existing housing cooperatives
- Provide updated urban design guidelines for new development

### For more information on this Big Move

See Chapter 6, Residential Areas, and also Chapter 10, Heritage.



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## Create more places to live near transit and amenities

### What we heard:

[There is an opportunity to add more housing along Vic West's transit routes](#). The area around Esquimalt Road would benefit from more vibrancy. New housing near trails and transit routes will help revitalize the area and provide places to live into the future.

### How the plan addresses what we heard

The plan identifies new moderately-scaled housing opportunities near Skinner Street, along Esquimalt Road, and in the Lime Point Area:

- Enliven Esquimalt Road with residential units above employment, [with potential for townhouses and some small apartment buildings up to three storeys stepping down toward the water](#).
- Support two and a half storey townhouses [and houseplexes](#) along Skinner Street
- [Option for](#) three-storey mixed-use buildings, townhouses, and conversions of existing houses along Craigflower Road near the village, [and three-storey mixed-use buildings in the half block northeast of Skinner and Russell Street](#)
- Encourage new housing in mixed use buildings within Westside Urban Village
- [More](#) urban design guidance [to ensure that new development is](#) sensitive to neighbours, [public spaces](#) and the shoreline

### For more on this big move:

See Chapter 6, Residential Areas, and also Chapter 8, Employment Lands





## Strengthen connections to the waterfront

### What we heard:

The waterfront is a precious asset and source of neighbourhood identity.

### How the plan addresses what we heard

- Improve access to the waterfront through swimming docks and trail improvements
- Add features and improvements that encourage gathering and animate the waterfront
- Work with partners to restore water and environmental quality [along](#) the Gorge Waterway

### For more on this big move:

See Chapter 4, Parks, Open Space and Waterways



## Support jobs and businesses in the neighbourhood

### What we heard:

The neighbourhood has a rich history of employment connected to the E&N Rail and the harbour. [The industrial and employment uses are part of what make Vic West unique, and are important for the region.](#) The community values preserving existing [jobs and](#) employment lands, renewing and maintaining space for neighbourhood-compatible artisan and light industries, creating offices for new employment, and finding opportunities for mixing living and working.

### How the plan addresses what we heard

- Maintain the Upper Harbour waterfront for industry
- Encourage the retention and renewal of light industrial spaces and add new upper floor commercial and office spaces
- [Maintain employment uses \(including office, artisan, and light industrial\)](#) along parts of Esquimalt Road and [north of Westside Village, with opportunities for housing or employment above.](#)
- Support new upper-floor offices in Westside Village

### For more on this big move:

See Chapter 8, Employment Lands, and Chapter 7, Urban Villages.







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## Strengthen parks and food systems

### What we heard

The neighbourhood has great parks [and open spaces](#) that provide areas for play and recreation, access to nature, and places to gather. Residents have many ideas for improving [these places](#) to draw more people and offer even more activities and recreation for all ages and abilities.

The community prides itself on its urban food production and envisions more food [growing](#) in private green spaces, public parks, open spaces and boulevards, and on sites of new multi-unit buildings.

### How the plan addresses what we heard

- Identify future improvements to parks and open spaces that add activities, improve comfort and safety, restore nature and encourage people to gather. These ideas will guide future park improvement plans.
- Support the creation of community gardens and orchards as community-initiated projects. The plan identifies several locations, including Hereward and Triangle Parks
- Support the creation of new allotment or community gardens through private redevelopment, [and food growing features](#) in new multi-unit developments

### For more on this big move:

See Chapter 4, Parks, Open Spaces and Waterways, and Chapter 12, Neighbourhood Food Systems

# 1. Introduction



Victoria West, or “Vic West”, is a waterfront neighbourhood nestled between the Gorge Waterway and Victoria Harbour, in the traditional territories of the Lekwungen People. Long before the first Europeans arrived, First Nations people hunted, fished and harvested the waterways and lands of Vic West. The Gorge Waterway was rich in fish and waterfowl, and provided shelter from the outer harbour. Travel routes on water and land ran through Vic West. Oral histories tell of occasional small villages along the shore.

Vic West today is home to 9% of Victoria's population (2016 Census). It is a thriving, vibrant and diverse neighbourhood, a mix of historic residential neighbourhoods, stunning parks and waterfront, light industrial and office areas, parks and recent higher density master planned developments that are leaders in Canadian urban design.

The new Victoria West Neighbourhood Plan, which began in the spring of 2016, was developed with the community to ensure future development is shaped by those who know the neighbourhood best.

Residents, business owners, community groups and development professionals were all involved in shaping the neighbourhood plan. The plan acts as a framework for guiding new growth, development and public investment within Vic West, and will be implemented over the years to come through development, partnerships and City initiatives.

Vic West has seen tremendous change since the last plan was created in 1986. Former industrial lands have been transformed into higher density areas of Songhees, Dockside, Railyards and Bayview Place including Roundhouse, that provide new housing, retail spaces and public spaces within easy walking access of downtown. New housing has flourished in the residential neighbourhoods west of Alston Street, and Vic West has pioneered small lot houses and new passive houses. Regional active transportation corridors such as the Galloping Goose and E&N Trail have been built and connect Vic West to downtown and the rest of the region. Community-led initiatives such as public food gardens, placemaking, public art and the community centre have transformed

public spaces, and built community. Future development in master planned areas will see new commercial spaces, public gathering spaces and new housing, which will continue to add vibrancy and livability to the neighbourhood.

As Victoria and Vic West continue to grow, the neighbourhood plan is intended to guide growth in a way that meets the needs of the Vic West community, Victoria as a whole, and the region over the next 20-25 years. The plan complements City initiatives on livability, affordability, environmental sustainability, economic vitality and other improvements to support the well-being of Vic West residents and businesses. The plan addresses issues identified by the Vic West community such as types and locations of new housing, creating strong village centres, knitting together the old and new, supporting and evolving employment areas, improving active transportation connections and supporting a more sustainable and walkable forms of development, and new public amenities that support a growing population.



# What is a Neighbourhood Plan?

By 2041, the city of Victoria is expected to have grown by 20,000 people. The City's Official Community Plan provides high level guidance for where and how those people may live, work, shop and play in the city. The neighbourhood plan translates this guidance to the local level, including:

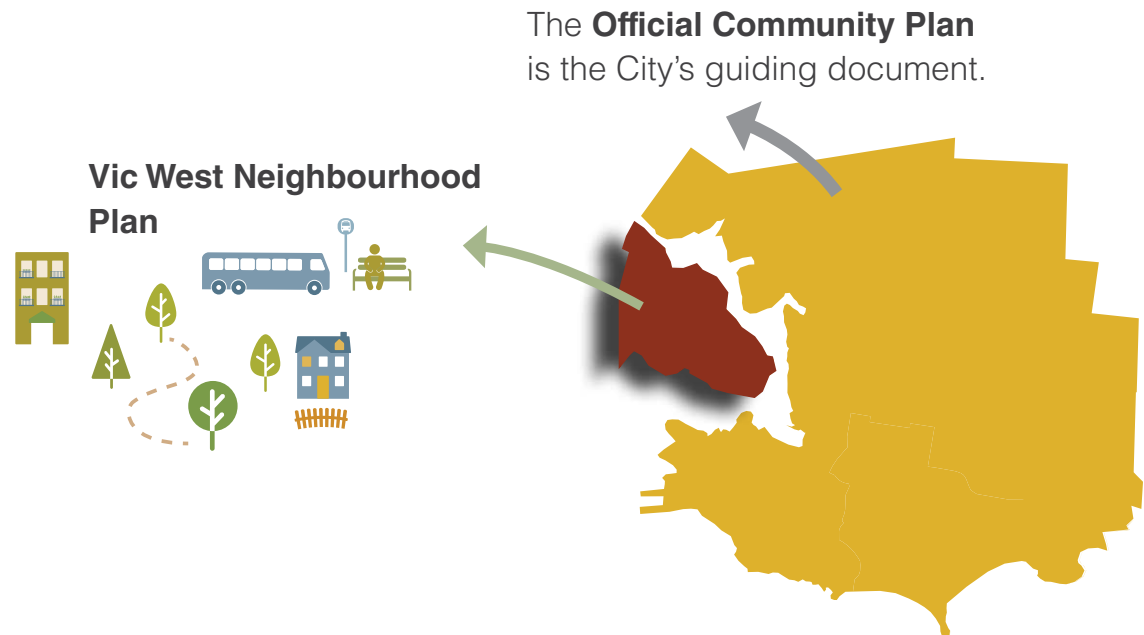
- What kind of housing is desirable? Where should housing, shops and services be located? And what should they look like?
- How will people move around the neighbourhood?
- How can parks and public spaces be improved?
- What will future residents and businesses need?

The primary role of a neighbourhood plan is to provide guidance for future housing and other development. Private development projects will be reviewed for their ability to help achieve the plan's vision and goals. City planners and City Council use a neighbourhood plan with other related policies, guidelines and regulations to evaluate the impact and suitability of public and private projects and initiatives related to land use, development, infrastructure, parks, community facilities and transportation.

The City also uses a neighbourhood plan as a guide in preparing operating and capital budgets, planning work priorities and determining public improvements.

The neighbourhood plan will be implemented over a 20-30 year time frame, although regular monitoring will take place throughout the life of the plan.

The Vic West Neighbourhood Plan provides more certainty about the community's vision for the area – for developers, for the City and for residents.



# How to use this Plan

The neighbourhood plan helps guide future development by refining the Official Community Plan (OCP) direction for appropriate density, form and design of new development and adjacent streetscapes. The OCP remains the City's guiding bylaw for considering new development, and where conflicts arise, the OCP takes precedence. The OCP defines Development Permit Areas and Heritage Conservation Areas which include guidelines for the form and character of new development. The neighbourhood plan may contain recommendations to make changes to the OCP so that the two documents are in alignment.

The neighbourhood plan also provides guidance to consider programs, projects and partnerships which can be considered for inclusion in the City's capital plan or general operations which can be funded in whole or in part as amenities resulting from development; or which might be achieved through partnerships with the community.

Specific initiatives are identified in the Action Plan in Chapter 16.



# Vic West Plan Process

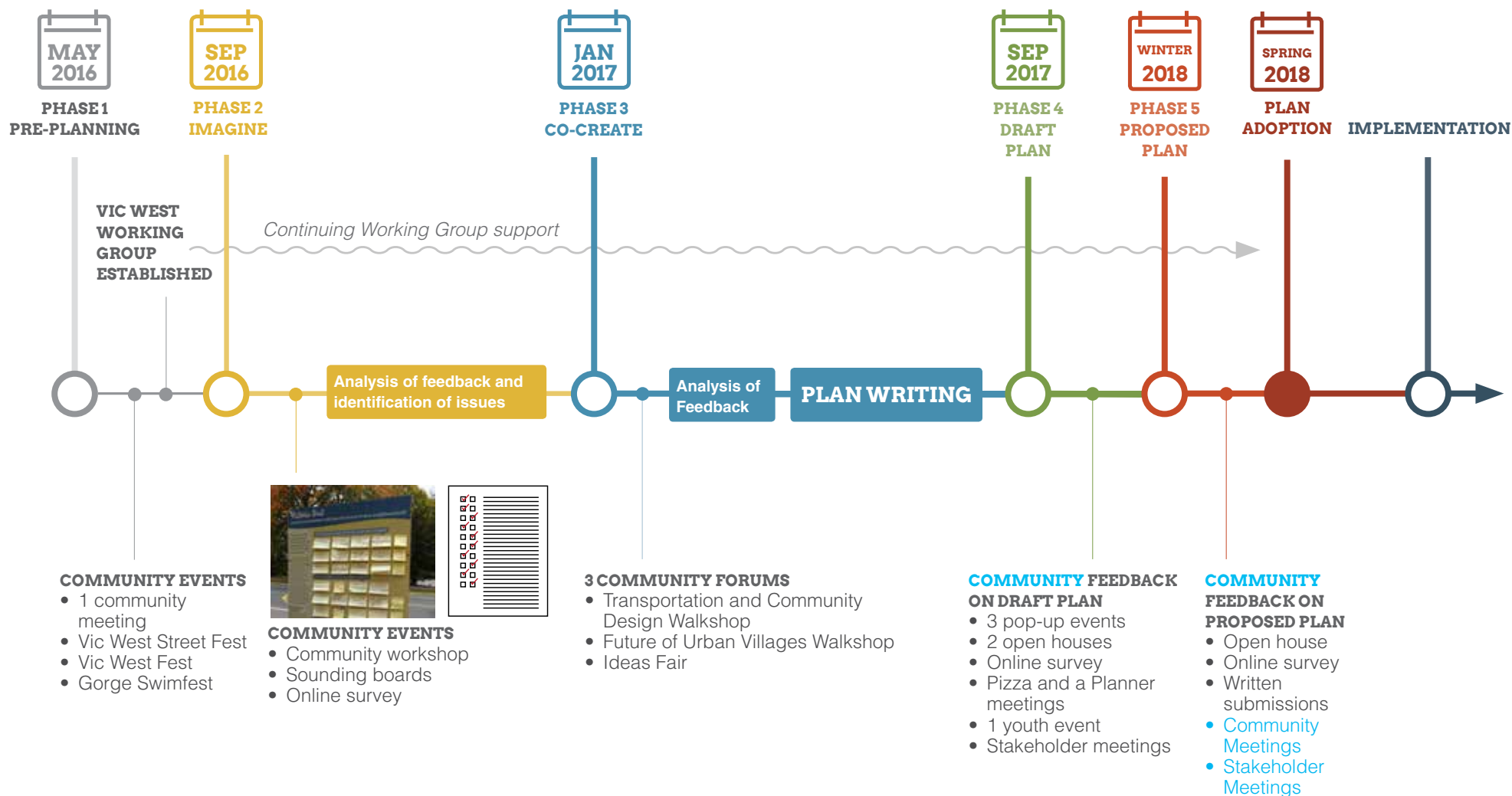


Figure 2. Plan Process

# A New Neighbourhood Plan

The plan vision and goals set a future direction for how Victoria West will develop, evolve and change over time. During the first phase of community engagement, citizens were asked to help build the vision and goals for the future of Victoria West. The vision and goals were used to guide the development of the strategies, policies and actions in this plan.

## *Neighbourhood Vision Statement*

“*In 25 years, Victoria West is a safe and welcoming community with diverse people, housing and businesses. The old and new are knitted together to create an eclectic and exciting mix of people, places and culture. It is a leader in neighbourhood sustainability, showcasing strong businesses, active transportation, a vibrant waterfront, healthy ecosystems, local food and innovative housing.*”

## Neighbourhood Plan Goals

The following neighbourhood plan goals were developed based on community input.

Topic	Goals	Chapter
<b>Transportation and Mobility</b>	<ol style="list-style-type: none"><li>1. Improve active transportation connections to destinations inside and outside Vic West</li><li>2. Tame the major roads to make them safer and more pleasant for people</li><li>3. Manage traffic on local roads to improve safety and neighbourhood livability</li><li>4. Manage parking to support business vitality and housing affordability</li><li>5. Connect Vic West to the rest of the city and region by transit, rail and water</li></ol>	<b>3</b>
<b>Parks, Open Space &amp; Waterways</b>	<ol style="list-style-type: none"><li>1. Protect and enhance Vic West's shoreline ecosystems</li><li>2. Improve access, amenities, wayfinding and facilities along the waterfront</li><li>3. Enhance neighbourhood parks for all ages and a variety of activities</li><li>4. Maintain and enhance the urban forest, habitat and native ecosystems</li></ol>	<b>4</b>

Figure 3. Plan Goals

# Neighbourhood Plan Goals (cont'd.)

<b>Urban and Traditional Residential Areas</b>	<ol style="list-style-type: none"> <li>1. Encourage a mix of housing sizes, costs, tenures and types</li> <li>2. Create more affordable housing</li> <li>3. Showcase new, innovative housing types</li> <li>4. Encourage the retention and re-use of historic buildings</li> </ol>	<b>6, 7</b>
<b>Urban Villages</b>	<ol style="list-style-type: none"> <li>1. Strengthen and diversify the mix of businesses in urban villages</li> <li>2. Improve the walkability, bikeability and public realm in and around urban villages</li> <li>3. Support new housing within a 5 minute walk of urban villages</li> </ol>	<b>7</b>
<b>Employment Lands</b>	<ol style="list-style-type: none"> <li>1. Maintain the working harbour and adjacent industrial lands that support the ocean and marine sector</li> <li>2. Protect and enhance light industry and other employment in Vic West</li> <li>3. Celebrate the industrial heritage of the neighbourhood</li> </ol>	<b>8</b>
<b>Heritage</b>	<ol style="list-style-type: none"> <li>1. Protect the historic character of significant buildings and important sites</li> <li>2. Celebrate and interpret the heritage of the neighbourhood</li> </ol>	<b>10</b>
<b>Infrastructure and Green Development</b>	<ol style="list-style-type: none"> <li>1. Ensure sufficient infrastructure capacity to meet the future needs of residents and businesses</li> <li>2. Use stormwater management to restore ecological processes.</li> <li>3. Become a leader for sustainable buildings and infrastructure</li> <li>4. Identify and address neighbourhood climate change impacts</li> <li>5. Develop strategies for mitigation and adaptation to climate change and sea level rise</li> </ol>	<b>11</b>
<b>Neighbourhood Food Systems</b>	<ol style="list-style-type: none"> <li>1. Create more places to get and grow food close-to-home</li> <li>2. Integrate food production into public places</li> </ol>	<b>12</b>
<b>Neighbourhood Well-being</b>	<ol style="list-style-type: none"> <li>1. Refresh indoor community facilities to support residents of all ages in a growing community</li> <li>2. Support the creation of more childcare options to support families with children living in Vic West</li> <li>3. Foster a safe and inclusive community for all residents</li> </ol>	<b>13</b>
<b>Arts, Culture and Placemaking</b>	<ol style="list-style-type: none"> <li>1. Encourage public art and placemaking that celebrate Vic West's identity</li> <li>2. Support creative entrepreneurs</li> </ol>	<b>14</b>

Figure 3. Plan Goals, cont'd.



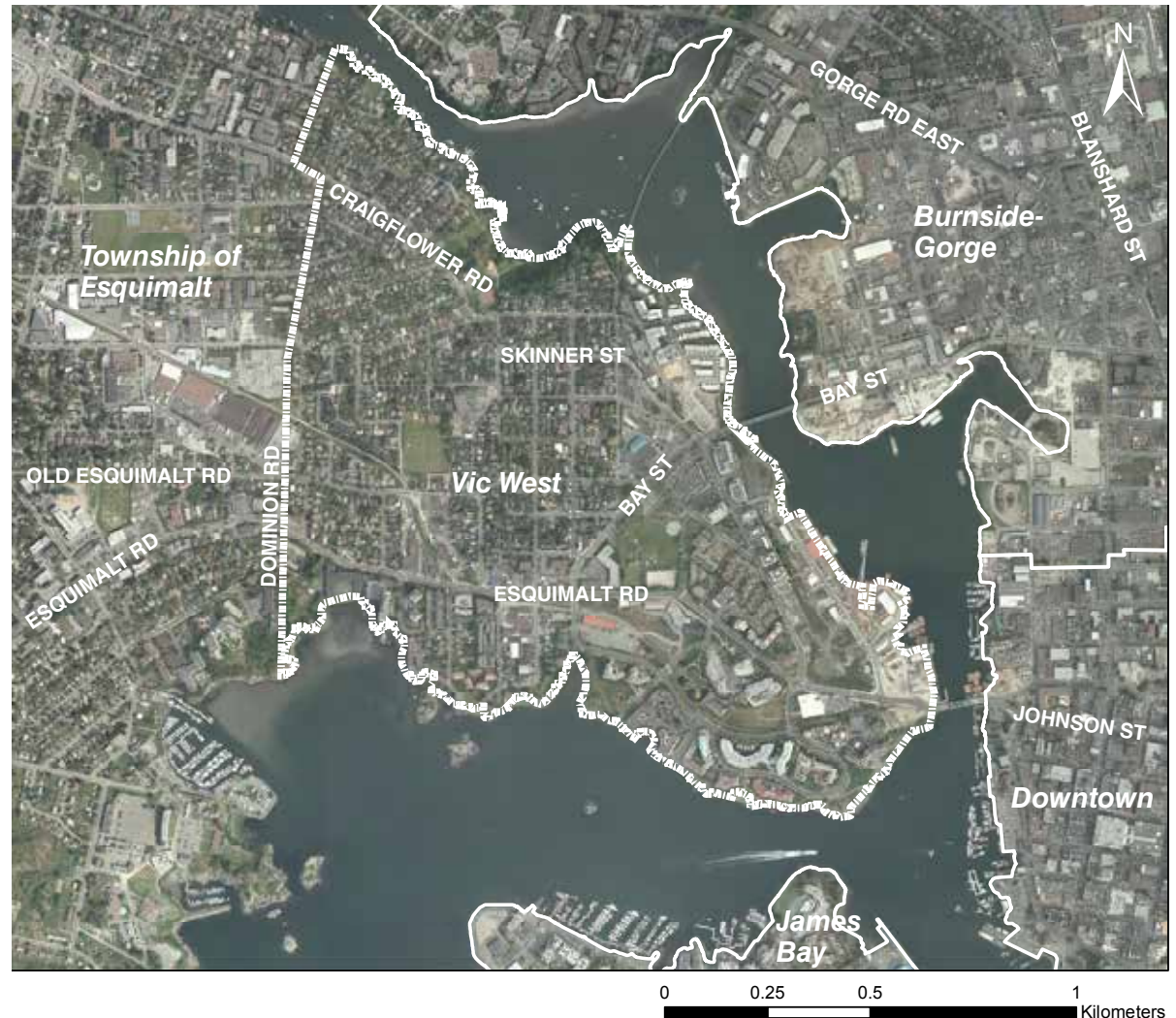
# 2. Neighbourhood Context

## Plan Area

Victoria West is an established neighbourhood of approximately 6,800 residents, representing 9% of the City's population.\*

The neighbourhood is approximately 158 hectares (390 acres). Located in the west portion of the City, it is connected to the rest of Victoria by water, by two bridges and the Selkirk Trestle (Galloping Goose Trail). Esquimalt lies on the western border.

Victoria West is a diverse and rapidly growing area, with a relatively new commercial centre and several large scale, high density developments (e.g., Bayview Place, Dockside Green, Railyards, Bayview Place (including Roundhouse) to the east of Bay Street, and older lower-profile residential areas to the west, with tree lined streets and heritage buildings. Pockets of light industrial businesses reflect Vic West's working past and the presence of the railroad, while historic commercial storefronts have evolved in walkable locations like Craigflower village. At the Upper Harbour, the Harbour Road Industrial lands continue to be a key anchor for Victoria's maritime industries, providing quality jobs for the region and maintaining the working harbour and its economic opportunities.



\*Footnote: Statistics Canada National Household Survey, 2016

Figure 4. Vic West Plan Area

# Existing Urban Structure



**Map 1. Figure ground map of Vic West (2017)**



# Neighbourhood Features

Some of the unique features of Vic West are shown here.



*Craigflower Village*



*Banfield Park and other neighbourhood parks provide a variety of amenities*



*Westside Village is a central shopping destination*



*New, urban communities have been developed on former industrial lands which were once the Songhees reserve*



*Westsong Walkway and Songhees area*



*Vic West Community Centre*



*Housing cooperatives add to housing diversity (above: Tyee Co-op)*



*The Galloping Goose Regional Trail at the historic Selkirk Trestle*



*E&N Rail Roundhouse  
(Heritage Designated building)*



*Employment areas have long been part of the neighbourhood*



*A mix of historic older homes and new development in a greatly varied lot pattern is a characteristic of the neighbourhood*





# Moments in the History of Vic West

First Nations communities fish, gather, and hunt in the area. There are oral histories of occasional small villages along the shore.



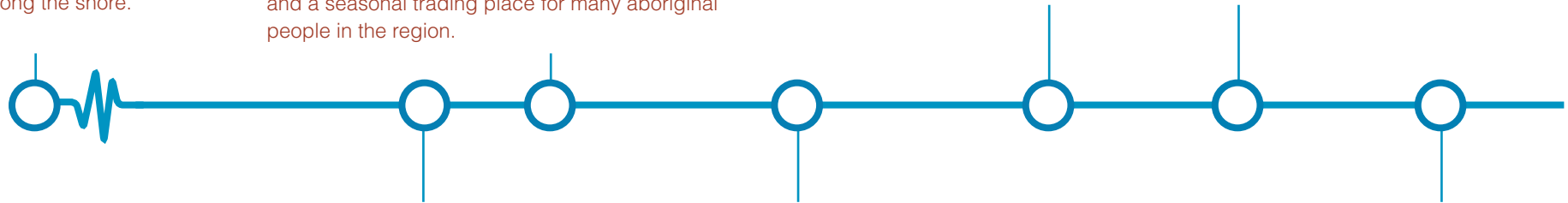
**1853:** The Hudson's Bay Company relocates the Songhees village the west shore of the Inner Harbour. The village was a reserve until the land was sold in 1911. It is home to local Lekwungen people and a seasonal trading place for many aboriginal people in the region.



**1896:** An overcrowded streetcar crashes through the Point Ellice (Bay Street) Bridge, claiming 55 lives.



**1911:** The Songhees People are moved to a new reserve in Esquimalt, and land is sold. The Songhees became an industrial area.



**1843:** The Hudson's Bay Company established Fort Victoria in the traditional territory of the Lekwungen people. The Fort's economy was dependent on trading with First Nations people. Farmland in Vic West sustained the Fort until the late 1850s.



**1874:** The Marine Hospital is established in Victoria West (Songhees village in foreground)



**1910-1920:** Streetcar and rail access make Vic West a popular place to live and establish industrial companies, close to the city centre. Canadian National Railway (CNR) completes its rail yards and passenger service from Victoria to Sooke begins in 1922.

Figure 5. History Timeline



1930: From the 1890s to the 1930s, the Gorge Waterway is a popular recreation area for swimming, boating, picnicking and camping.



1970s: City makes land available for Victoria's first housing co-op, now Pioneer Co-op, as part of a larger neighbourhood revitalization program



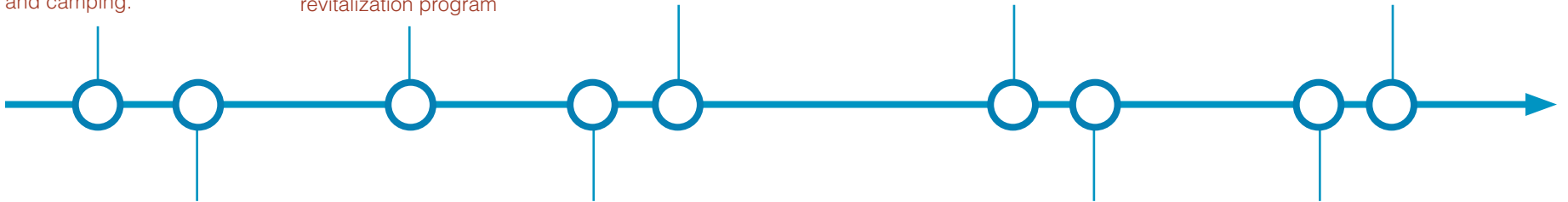
1980s: Redevelopment of Songhees industrial lands begins.



1996: The Selkirk Trestle is reopened and the Galloping Goose Regional Trail is established on former railway bed.



2009: Bayview One building completed as first phase of Bayview Place development.



1948: Banfield Park established.



1984: Victoria's first Brew Pub is established, originally known as the Lime Bay Inn, now Spinnakers.



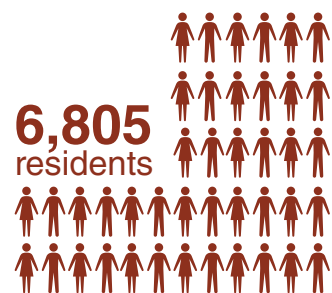
1998: Development of Westside Village shopping centre.



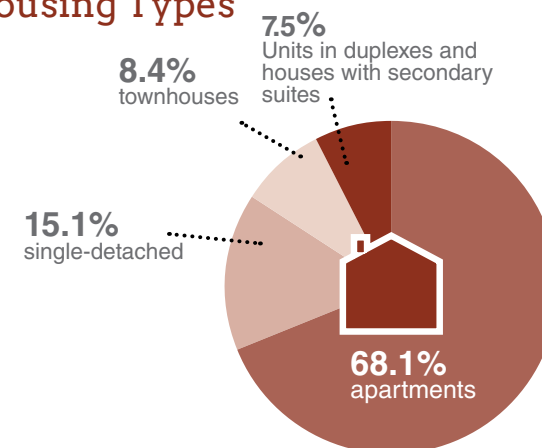
2008: First residents move into Dockside Green, a green mixed use development on a former industrial site.

# Neighbourhood Snapshot

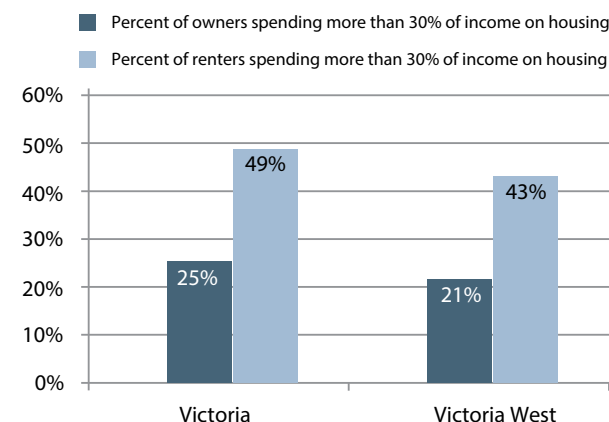
## Residential



## Housing Types



## Housing Affordability



Source: Statistics Canada National Household Survey, 2011

## Rental vs. Ownership



of Victoria West housing  
units are rented

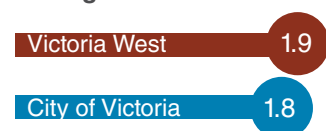


of City of Victoria housing  
units are rented

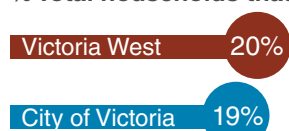
### Median Age



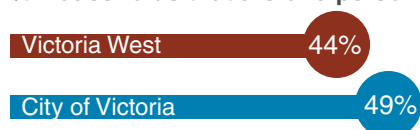
### Average household size



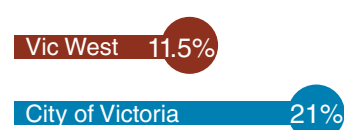
### % Total households that have children at home



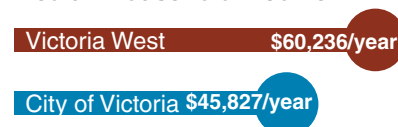
### % Households that are one-person households



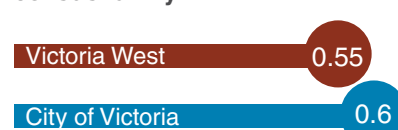
### Low-income Households



### Median Household Income



### Average number of children at home per census family



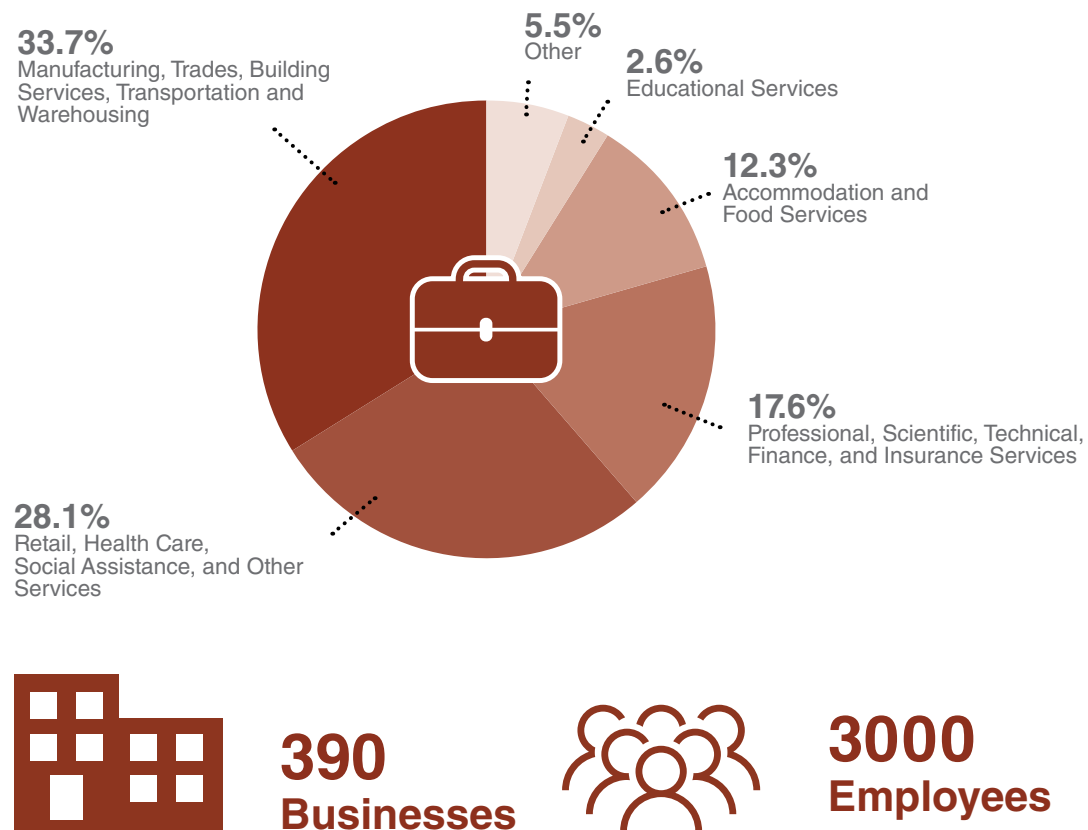
Source: Statistics Canada Census and  
National Household Survey, 2011

Figure 6. Neighbourhood Snapshot - Residential Statistics



# Neighbourhood Snapshot

## Employment



### Did you know?

Vic West hosts a diversity of industry sectors including:

- Ship building & Repairing
- Navigational & Guidance Instruments Manufacturing
- Breweries
- Machine Shops
- Cabinet & Counter-top Manufacturing
- Financial Institution Head Office
- Computer Systems Design & Related Services
- Research and Development in Biotechnology

Source: Capital Regional District Employers Database, 2011

Figure 7. Neighbourhood Snapshot - Employment Statistics

# Housing and Population Trends in Vic West

## Neighbourhood Population and Housing Trends

Over the next 20-25 years, Victoria is expected to grow by 20,000 people. The City's Official Community Plan envisions that approximately 50% of these new residents will be accommodated in the city's core (including the Songhees area of Vic West), 40% within a five minute walk of large urban villages, and approximately 10% in the remainder of the city. This growth pattern will shape a more sustainable community where shops, facilities and jobs are located close to where people live and more residents in the area help villages and downtown to thrive.

Population change is influenced by housing demand, demographic shifts and economic, policy and environmental limits to development. Over the last few decades, Vic West has grown faster than other neighbourhoods in Victoria due to the recent development of new master planned areas on former waterfront industrial lands.

Over the next 20-25 years, Vic West is expected to grow by approximately 3,500 new housing units (5,200 residents). This includes approximately 2,150 additional housing units (3,200 residents) identified in approved agreements for new master planned areas (Dockside, Railyards and Bayview).

Outside of master planned areas, historical trends are often the best predictor of future housing growth. An estimated 1,130 units of housing (approximately 1,725 residents) are anticipated in parts of the Core, Large Urban Village, Employment-Residential, and some Urban Residential areas outside of the master planned areas (see Map 9 Neighbourhood Land Use Map).

An estimated 275 more ground oriented units are anticipated mainly in the older, Traditional Residential parts of Vic West. Real growth rates may be higher or lower due to a number of variables.

The Official Community Plan identifies how infrastructure, transportation, parks, community facilities and housing will accommodate future population growth. The neighbourhood plan and the Official Community Plan will be reviewed regularly to consider changing trends.



Census year	1991	1996	2001	2006	2011	2016
Population of Vic West	4,978	5,310	5,575	5,985	6,805	7,668
% change in Vic West population		6.7%	5.0%	7.4%	13.7%	12.7%
Housing Units in Vic West	2,290	2,515	2,760	3,155	3,675	4,162
% change in units in Vic West		9.8%	9.7%	14.3%	16.5%	13.3%
People per unit in Vic West	2.17	2.11	2.02	1.90	1.85	1.84

Source: Statistics Canada

Figure 8. Population and housing trends in Vic West 2001-2016

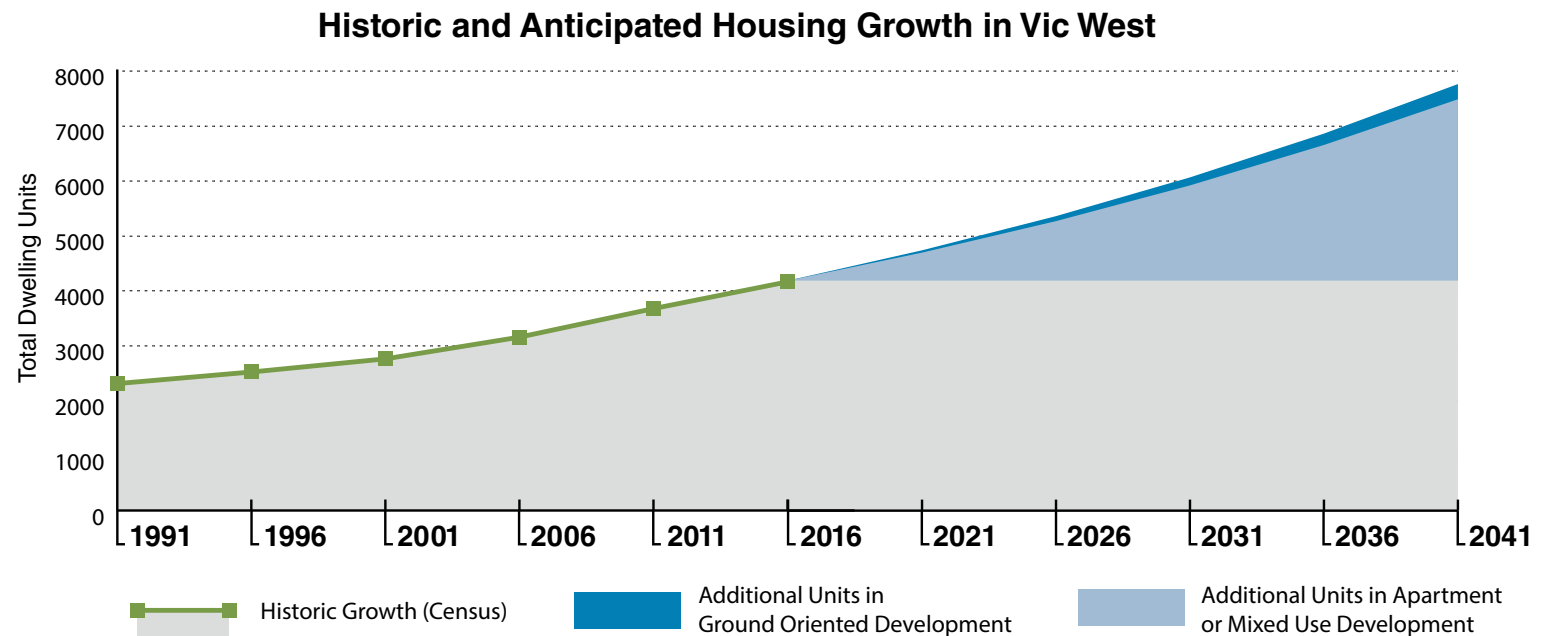
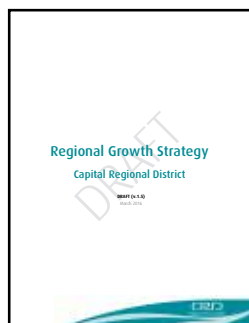


Figure 9. Historic and anticipated housing growth in Vic West



# Policy Context : City-Wide Plans

This plan is to be read in conjunction with the following City plans and policies



**Regional Growth Strategy** – guides regional decision making on transportation, population growth and other regional planning issues.



**Official Community Plan** – contains city-wide objectives and specific direction for areas of growth to guide neighbourhood planning.



**Transit Futures Plan** – prepared by BC Transit, identifies key corridors and improvements for Rapid, Frequent and local transit.



**Bicycle Master Plan** – identifies future active transportation network and priorities.



**Growing Business in BC's Capital City** - Victoria's Economic Development Strategy



**Making Victoria** - The Mayor's Task Force on Economic Development and Prosperity Economic Action Plan



**Parks and Open Spaces Master Plan** – identifies key goals and priorities for next 25 years.



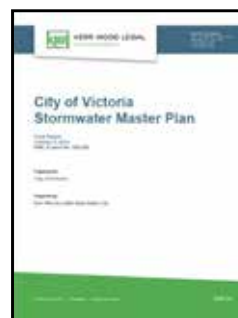
**Pedestrian Master Plan** – identifies priorities and guidelines for sidewalk network completion.



**Water System Master Plan** - 30-year plan examining the needs of the community with respect to water.



**Arts and Culture Master Plan** – identifies the City's vision, role and initiatives for supporting local arts and culture.



**Stormwater Master Plan** - assesses the capacity of the City's storm sewer network, including potential effects of climate change.



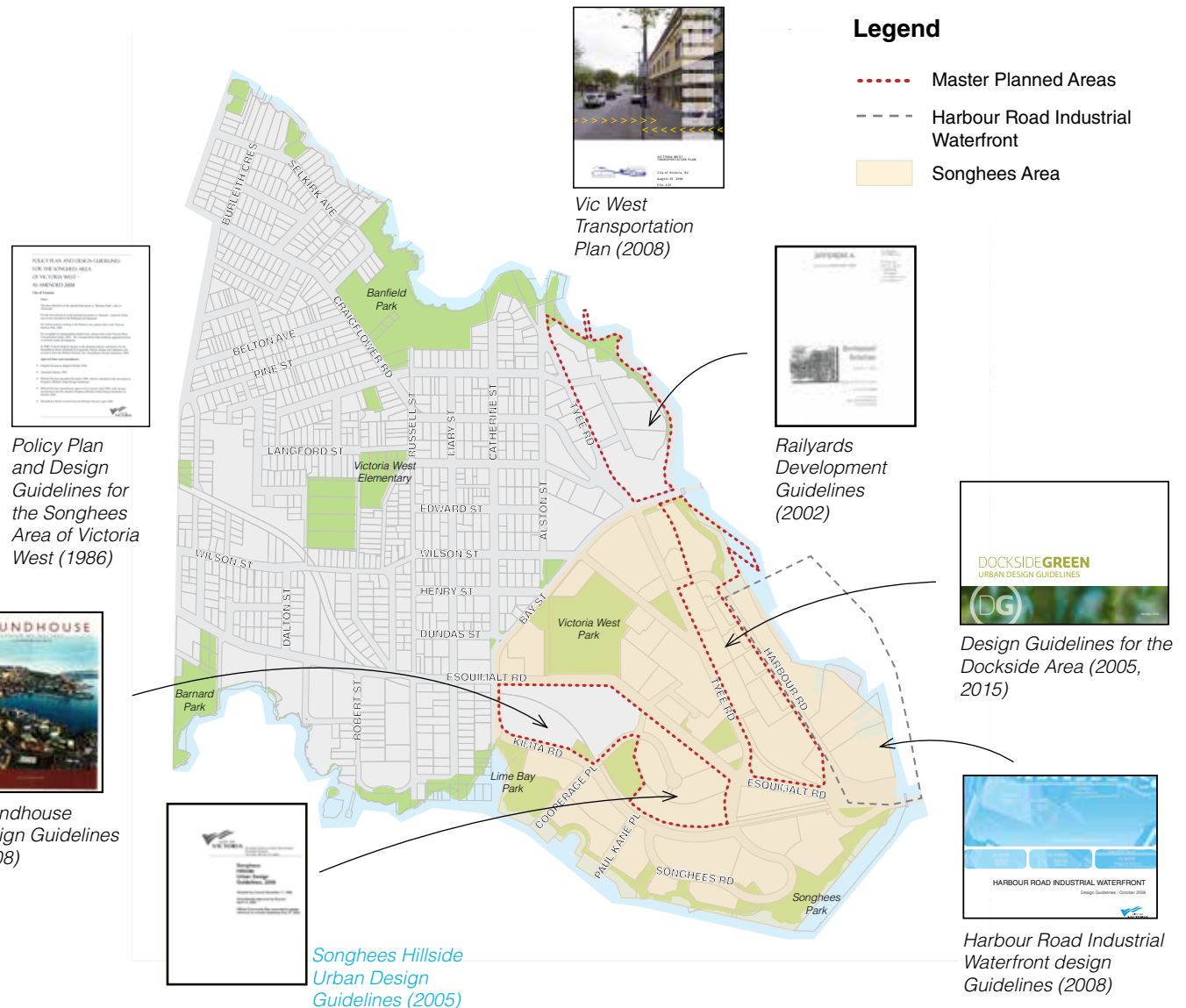
**City of Victoria Wayfinding Strategy** - facilitates transportation around the city, primarily by pedestrians, cyclists, and transit users.

# Policy Context : Plans Specific to Victoria West

Although the neighbourhood plan encompasses the whole of Vic West, this plan is less detailed for some of the newer parts of the neighbourhood, including Dockside Green, Railyards, Bayview Place (including Roundhouse) and Songhees. Detailed master plans exist for these sub-areas, with specific guidance for the land uses, building heights, urban design, public realm, transportation network and affordable housing.

The intent of this plan is to maintain, implement and refer to the existing Master Development Agreements for these areas, and focus on knitting these newer parts of Victoria West with each other and with the other parts of this diverse neighbourhood.

More details about master-planned areas is provided in Chapter 9.



**Map 2. Existing Neighbourhood-specific Plans (2017)**



# 3. Transportation and Mobility

With its central location within the region, improving **connections** to transportation routes and destinations for all modes of travel are a key goal for the Vic West Plan.



## Goals:

1. Improve active transportation connections to destinations inside and outside Vic West
2. Tame the major roads to make them safer and more pleasant for people
3. Manage traffic on local roads improve safety and neighbourhood livability
4. Manage parking to support business vitality and housing affordability
5. **Connect Vic West to the rest of the city and region by transit, rail and water**

Active transportation refers to any form of human-powered transportation – walking, cycling, using a wheelchair, in-line skating or skateboarding. Active transportation provides important health, social, transportation, environmental and economic benefits.

Making it easier to use active transportation to connect with Vic West and to other regional destinations is a priority for Vic West. Two major transportation corridors – the E&N Rail Trail and Galloping Goose Trail – run through the neighbourhood. Better connecting these corridors and neighbourhood travel

routes will provide more direct and convenient access to important destinations – like the waterfront, work, schools, urban villages, downtown and destinations in adjacent municipalities.

The plan also supports **managing** major roads to make them more pleasant for people, more efficient use of the road network through traffic and parking management, and connecting Vic West by transit, rail and water to the rest of the region.

[See Chapter 16, Action Plan, for details regarding implementation.](#)

## Other Relevant Policies & Bylaws

Several City-wide policies guide transportation planning, priority-setting and funding at the neighbourhood level:

- Official Community Plan
- Pedestrian Master Plan
- Greenways Plan
- Bicycle Master Plan
- All Ages and Abilities Active Cycling Network
- Pavement Management Plan
- Zoning Regulation Bylaw Schedule C – off-street parking requirements
- Subdivision and Development Servicing Bylaw – design of roads, sidewalks and boulevards
- Streets and Traffic Bylaw – on-street parking



## Transportation and Mobility

# Neighbourhood Active Transportation Network

### Intent:

Improve active transportation connections to destinations inside and outside Vic West.

### 3.1. Active Transportation Network

3.1.1. Create and maintain a well-defined pedestrian and cycling network providing complete north-south and east-west connections to important destinations such as schools, parks, shopping areas, the City-wide All Ages and Abilities network and adjacent neighbourhoods and municipalities (see Map 3).

3.1.2. Complete gaps in the neighbourhood sidewalk network to the standards, and at locations, outlined in the Pedestrian Master Plan.

3.1.3. Create strong connections between active transportation routes and transit through features such as abundant and convenient bicycle parking at stops, and safe connections between transit stops and the active transportation network.

3.1.4. Include pedestrian and cyclist- focused public realm improvements in urban villages to encourage walkability and bikeability. This may include new benches, lighting, landscaping, street trees, wayfinding, bicycle parking and other features.

3.1.5. Look for opportunities to showcase public art, green infrastructure and community-led placemaking opportunities along pedestrian and cycling routes.

3.1.6. In developing urban forest succession management strategies, ensure continuous street trees along pedestrian and cycling routes to beautify the experience for users.



**Map 3. Neighbourhood Active Transportation Network**

## Transportation and Mobility

# Neighbourhood Active Transportation Network

### 3.2. All Ages and Abilities Network

3.2.1. Develop an All Ages and Abilities two-way protected bike lane on the east side of Harbour Road to connect downtown with the Galloping Goose Trail via the Johnson street bridge multi-use trail.

3.2.2. Work with private landowners in Railyards and Westside Village to improve pedestrian and cycling access and wayfinding to the Galloping Goose Trail.

3.2.3. Work with the CRD and private landowners to complete the E&N Rail Trail from the Esquimalt Road crossing to Johnson Street Bridge.

**All Ages and Abilities (AAA) bicycle routes** are designed to provide an inviting and low stress cycling experience. They appeal to a broader spectrum of the population, such as children and seniors, by establishing a safer and more comfortable environment for riding bicycles. On streets *with* low vehicle speeds and traffic volumes, *it means bicycles share the road with other traffic*. On busy streets, it means routes with physical separation from vehicles.

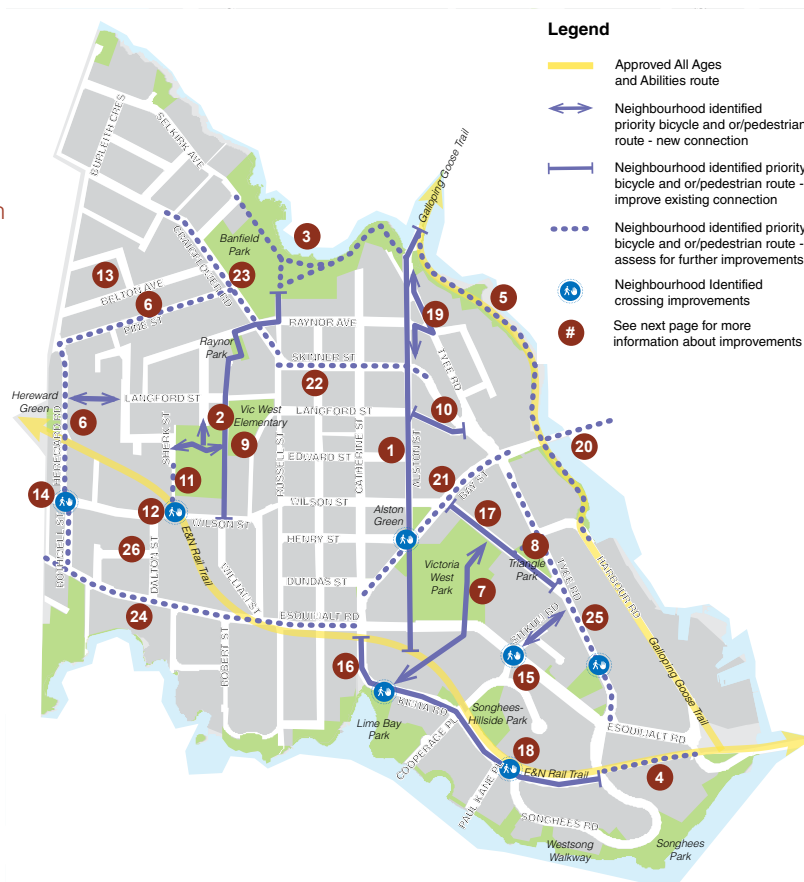


Figure 10. Harbour Road

# Neighbourhood-Identified Priority Transportation Improvements

The following improvements were identified through community feedback, and are presented in no particular order:

- 1 Alston Street: Establish as pedestrian-oriented corridor (3.3.1, 3.3.10, 3.4.3)
- 2 William Street Greenway: Improve and widen trail, add wayfinding (3.3.2)
- 3 Banfield Park: Assess waterfront trail for improvements (3.3.3)
- 4 E&N Rail Trail near Delta Hotel: Assess for improvements (3.3.4)
- 5 Galloping Goose Trail: Assess for eco-sensitive lighting (3.3.5)
- 6 Pine Street and Hereward Road: Assess for cyclist and pedestrian safety and add wayfinding (3.3.6)
- 7 Connect Westsong Walkway to Westside Village via Roundhouse redevelopment and Victoria West Park (3.3.7)
- 8 Triangle Park: Consider re-orienting trails to improve pedestrian connections to Galloping Goose (3.3.8)
- 9 Vic West Elementary School: Explore east-west trails. Encourage active travel program participation to reduce vehicle congestion (3.3.9)
- 10 Alston Street and Langford Street: Complete sidewalk network through redevelopment; assess for short-term improvements (3.3.10)
- 11 E&N Rail Trail West of Esquimalt Road: Improve informal connection to Sherk Street (3.3.11)



Map 4. Neighbourhood-Identified Priority Transportation Improvements

- 12 E&N Rail Trail at Wilson Street: Assess crossing for safety (3.3.12)
- 13 Belton Ave at Reno Street: Permanent, landscaped traffic diverter (3.3.13)
- 14 Wilson Street: Assess for vehicle speeds and speed limits. Evaluate for pedestrian and cyclist safety between Viewfield Road and Rothwell Street; improve pedestrian crossing and conditions at Hereward Road (3.3.14, 3.6.5, 3.6.9)

- 15 Esquimalt Road at Sitkum Street: Consider signalized crossing and pedestrian route through Tyee Cooperative (3.3.15)
- 16 Kimta Road: All Ages and Abilities bike route pilot project (3.3.16)
- 17 Wilson Street: Improve pedestrian experience and streetscape during redevelopment of village (3.3.17) assess for vehicle speeds (3.6.9)
- 18 Kimta Road: Evaluate need for crossing to connect to waterfront (3.3.18)
- 19 Establish and/or sign Galloping Goose to Raynor Avenue connection (3.3.19)
- 20 Point Ellice Bridge: Improve cycling and pedestrian facilities (3.4.1)
- 21 Bay Street: More pedestrian-oriented environment, through redevelopment (3.4.2) assess for vehicle speeds (3.6.9)
- 22 Skinner Street: Slower, more comfortable pedestrian and cycling (3.4.3)
- 23 Craigflower Road: Slow traffic, pedestrian-oriented urban village, evaluate crossing at Raynor Ave (3.4.4) assess speed limits (3.6.9)
- 24 Esquimalt Road: Assess for vehicle speeds. Through redevelopment, improve pedestrian-oriented design between Esquimalt border and Catherine Street. (3.3.20, 3.4.5)
- 25 Tyee Road: Pedestrian-oriented design, evaluate new & current crossings (3.4.6), assess speed limits (3.6.9)
- 26 Suffolk and Dalton Streets: Assess for short-cutting and speed (3.6.7)



# Transportation and Mobility

## Neighbourhood-Identified Priority Active Transportation Improvements

### 3.3. Neighbourhood-Identified Active Transportation and Connectivity Improvements

**3.3.1. Outer Harbour to Gorge Waterway via Alston Street:** Through redevelopment, establish Alston Street as pedestrian-oriented corridor, linking Victoria West Park with the Gorge Waterway. Enhance green areas along boulevard between Wilson Street and Skinner Street. Add wayfinding (See also 3.3.10 and 3.4.3).

**3.3.2. William Street Greenway to Banfield Park:** Improve trail between Wilson Street and Langford Streets. Add wayfinding. Widen trail and improve visibility at Raynor Park.

**3.3.3. Banfield Park:** Assess waterfront trail for visibility, trail surfacing, cyclist speed and ecological impact as part of long-term park improvements. Assess paved trail near playground for improvements for pedestrian safety; consider trail and playground location as part of long-term park improvements.

**3.3.4. E&N Rail Trail near Delta Hotel:** Assess for improvements as part of Kimta Road All Ages and Abilities Pilot Project.

**3.3.5. Galloping Goose Trail between Harbour Road and Banfield Park:** Assess for lighting improvements. Consider downlighting to minimize ecological impacts.

**3.3.6. Pine Street and Hereward Road:** Assess for improvements for cyclist and pedestrian safety as part of the long-term cycling network development. Key concerns include crossings, vehicle speed and narrow road conditions along Here-

ward Road. Work with BC Hydro and Esquimalt to improve E&N Rail Trail access in Hereward Park. Add wayfinding.

**3.3.7. Westsong Walkway to Westside Village via Victoria West Park:** Connect Westsong Walkway and Bayview Place (including Roundhouse) development with Westside Village. Implement existing park improvement plan for Victoria West Park to widen existing multi-use trail, add lighting, and add additional east-west connections. With Bayview Place development, complete a crossing of Kimta Road.

**3.3.8. Triangle Park:** Consider re-orienting trails and assessing crossings across Wilson Street and Tyee Road improve connections to the Galloping Goose to better reflect existing pedestrian travel patterns.

**3.3.9. Victoria West Elementary School:** Encourage school to participate in an active and safe routes to school program to encourage use of active travel modes, and reduce vehicle congestion near school. Work with School District to establish links across the playing field to Griffiths Street, the E&N Trail, and the William Street Greenway. Add wayfinding.

**3.3.10. Alston Street and Langford Streets:** Over long-term, complete sidewalk network through redevelopment. Evaluate opportunities for short-term sidewalk improvements on Langford Street between Alston Street and Tyee Road, and potential improvements to Alston Street, through update to pavement management plan (see also 3.4.3.).

**3.3.11. E&N Trail at Sherk Street:** Seek ways to improve the informal connection to the E&N trail.

**3.3.12. E&N Trail at Wilson Street:** Assess crossing for safety and visibility.

**3.3.13. Belton Avenue at Reno Street:** Establish a permanent, landscaped traffic diverter.

**3.3.14. Wilson Street at Hereward Road:** Improve crossing for pedestrians to better knit the neighbourhood together.

**3.3.15. Esquimalt Road at Sitkum Street:** Consider a signalized crossing through development in the area. Consider an eventual pedestrian network extension through the Tyee Cooperative as part of any redevelopment.

**3.3.16. Kimta Road Pilot Project:** Complete the Kimta Road All Ages and Abilities pilot bicycle route. Add wayfinding.

**3.3.17. Wilson Street:** With redevelopment, improve the pedestrian experience, and streetscape of Wilson Street between Tyee Road and Bay Street.

**3.3.18. Kimta Road:** Evaluate the need for a crossing at Songhees Road or Paul Kane Place to better link residences and Songhees Hillside Park to the waterfront.

**3.3.19. Galloping Goose to Raynor Avenue connection:** Establish a cycling connection between the Galloping Goose and Raynor Avenue / Catherine Street / Alston Street. This may be accomplished through infrastructure improvements and/or wayfinding signage bringing cyclists through the Railyards area.

**3.3.20. Esquimalt Road:** Monitor and review speed limit along Esquimalt Road following completion of new Johnson Street Bridge.



## Transportation and Mobility

# Neighbourhood-Identified Priority Active Transportation Improvements, cont'd.

### 3.4. Major Roads

#### Intent:

Tame the major roads to make them more pleasant for people.

**3.4.1. Point Ellice Bridge:** [Complete rehabilitation and resurfacing of bridge](#). Improve cycling and pedestrian facilities for Point Ellice Bridge as part of [long-term](#) bridge improvements.

**3.4.2. Bay Street:** Through redevelopment and on-going capital projects, create a more pedestrian-oriented environment with visual cues/design treatments that signal to motorists that this is a high pedestrian use space, including but not limited to:

- a) Improved pedestrian crossings and intersection improvements at Wilson Street and Bay Street
- b) Evaluate the need for a crossing connecting residential neighbourhoods with Vic West Park at Alston/Henry/Bay Streets intersection.
- c) Other enhancements to improve the pedestrian and cycling experience along Bay Street, through street design, boulevards, street trees, public realm improvements and cycling facilities improvements, where warranted.

**3.4.3. Skinner Street:** Consider transportation improvements to create a slower, more comfortable pedestrian and cycling experience

along Skinner Street. Assess for new crossing at Alston Street.

**3.4.4. Craigflower Road:** In the short-term, and consider community-led initiatives to slow traffic and animate public realm along Craigflower Road, and [assess the crossing at Raynor Avenue to determine if improvement is warranted](#). Through longer-term redevelopment, create a more pedestrian-oriented environment through the urban village and improve connections to Banfield Park. Retain street and park trees. Consider relocation of sidewalk to park property to create additional on-street parking on east side of Craigflower Road at Banfield Park to slow traffic and better support commercial areas.

**3.4.5. Esquimalt Road:** Encourage pedestrian-oriented public realm in the area between the Esquimalt border and Catherine Street through redevelopment of mixed use, residential and industrial properties.

**3.4.6. Tyee Road:** Through redevelopment, create a more pedestrian-oriented environment through visual cues/design treatments that signal to motorists that this is a high pedestrian use space, including but not limited to:

- a) Evaluate the need for a new crossing between Tyee Co-op and Dockside Green.
- b) Assess crossing location between Triangle Park and Dockside Green to better reflect existing travel patterns.

# Transportation and Mobility

## Traffic Management

### Intent:

Manage traffic to improve safety and neighbourhood livability.

### 3.5. Road Network

3.5.1. Design and manage the road network to direct through traffic to arterial and collector routes, and to discourage cut through traffic on local roads.

3.5.2. Street classification shall comply with the Official Community Plan, or other guidelines for street standards as approved by Council.

3.5.3. By way of development approvals, continue to require and negotiate space and rights-of-way to achieve City standards for arterials, secondary arterials, collector roads, local roads, All Ages and Abilities routes and Neighbourhood Connectors.

### 3.6. Neighbourhood-Identified Priority Traffic Management Improvements

**3.6.1. Skinner Street:** Evaluate road conditions for improvements for pedestrian and cyclist comfort and safety, including visibility, crossings, vehicle speed and intersection improvements.

**3.6.2. Langford Street at Russell Street:** Assess intersection for visibility and stop sign across from school.

**3.6.3. Mary Street:** Evaluate road conditions for speeding and cut-through traffic, and improvements for pedestrian safety, including intersection visibility and crossings at Wilson Street.

**3.6.4. Selkirk Street:** Evaluate for vehicle speed and cut-through traffic.

**3.6.5. Wilson Street from Rothwell Street to Viewfield Road:** Collaborate with the Township of Esquimalt to evaluate for pedestrian and cyclist safety, including visibility, crossings, vehicle speed and intersection improvements.

**3.6.6. Hereward Road and Rothwell Street:** Assess for vehicle speed and cut-through traffic. *Assess Rothwell Street for vehicle speeds, commercial traffic volume and road designation. Assess crossing at Esquimalt Road for safety and visibility.*

**3.6.7. Suffolk Street and Dalton Street:** Assess for vehicle speed and cut-through traffic.

**3.6.8. Bay Street between Tyee Road and Esquimalt Road:** Assess for vehicle speed, visibility, crossings and other improvements for pedestrian safety to improve connections between residential neighbourhood and Vic West Park and Westside Village.

**3.6.9. Assess Speed Limits:** *Assess Craigflower, Tyee, Bay, Wilson and Esquimalt for consistency of speed limits along corridors, and across municipal boundaries.*



# Transportation and Mobility

## Parking

### Intent:

Manage parking to support business vitality and housing affordability.

### 3.7. Vehicle Parking Management

3.7.1. While the long-term goal is to reduce the car dependency of urban villages, support a sufficient combination of on-street and off-street parking around village centres which, in combination with other modes, supports business vitality.

3.7.2. Support business viability in Craigflower Small Urban Village [by providing flexibility in on-site parking requirements for ground-floor commercial uses, where a change of use occurs in existing buildings](#) (see also Urban Villages, 7.4.8.).

3.7.3. Periodically review parking needs upon request and explore new parking management strategies as required.

3.7.4. Continue to refer resident concerns regarding on-street parking to the existing Residential Only Parking process.

### 3.8. Bicycle Parking Management

3.8.1. Prioritize end-of-trip cycling facilities such as secure and weather-protected bike parking at neighbourhood destinations including urban villages and neighbourhood parks.

### 3.9. Car Sharing and Low-Carbon Vehicles

3.9.1. Make at least one on-street location available for car sharing in or near each large and small urban village, and in new multi-unit buildings to reduce single vehicle dependency.

3.9.2. Partner with private industry to provide electric-vehicle charging locations in key multi-unit developments, near urban villages and the waterfront.

### 3.10. Bike Sharing

3.10.1. Support private and community partners in the development of a bicycle sharing system at key neighbourhood destinations and areas with high cycling potential.



# Transportation and Mobility

## Transit, Rail and Marine Network

### Intent:

Connect Vic West to the rest of the city and region by transit, rail and water. Create additional opportunities for housing to be located near frequent transit.

### 3.11. Multi-Modal Connections

3.11.1. Add opportunities for more people to live along frequent transit corridors on Esquimalt Road, Craigflower Road, Skinner Street and Tyee Road.

3.11.2. Maintain transit network consistent with BC Transit's Victoria Transit Future Plan (2011), as shown in Map 5.

- a) Continue to work with BC Transit to improve levels of transit service in Vic West to meet future ridership demand and increase capacity along frequent routes.

3.11.3. Continue to assess the re-establishment of train service along the E&N rail corridor, and the maintenance of the corridor alignment for transportation purposes.

3.11.4. Support the development of a regional marine trail network linking the Inner Harbour, Outer Harbour, Portage Inlet and Gorge Waterway.

**The Frequent Transit Network** will service major corridors with convenient, reliable and frequent transit service seven days a week. The Frequent Transit Network will carry a large share of the transit system's total ridership and for this reason justifies capital investments such as transit priority, right-of-way improvements and a high level of transit stop amenities. (Adapted from BC Transit, Transit Future Plan)



**Map 5. Neighbourhood Transit Network**  
(BC Transit, Victoria Transit Future Plan 2011)





# 4. Parks, Open Space and Waterways



## Other Relevant Policies & Bylaws

- Official Community Plan
- Parks and Open Spaces Master Plan
- Greenways Plan
- Urban Forest Master Plan
- Tree Preservation Bylaw
- Parks Regulation Bylaw
- Park Management and Improvement Plans

### Goals:

1. Protect and enhance Vic West's shoreline ecosystems
2. Improve access, amenities, wayfinding and facilities along the waterfront
3. Enhance neighbourhood parks for all ages and a variety of activities
4. Maintain and enhance the urban forest, habitat and native ecosystems

The parks, open spaces and waterways of Vic West are a vital piece of the neighbourhood's character, history, culture and landscape. They support important habitat and ecological functions, and provide residents and visitors with opportunities to socialize, recreate, relax, play, learn and connect with nature.

The Official Community Plan, Parks and Open Spaces Master Plan and the Urban Forest Master Plan provide overarching direction for neighbourhood plan's directions related to parks, open space, the urban forest and waterways. The neighbourhood plan emphasizes the ecological role of these areas and how they support active transportation, community gathering and neighbourhood character.

Analysis completed through the development of the Parks and Open Spaces Master Plan indicate that the Victoria West neighbourhood has an above average supply of park land per resident and currently meets the Official Community Plan target of 99% of all residents having access to a park or open space within 400 metres of home. New parkland will be provided with new housing in Railyards, Dockside, Songhees Waterfront, Bayview Place (including Roundhouse) areas, and through the Johnson Street Bridge project.

# Parks, Open Space and Waterways

## Shoreline and Waterways

### Intent:

Protect and enhance Vic West's shoreline ecosystems

Improve access, **amenities**, **wayfinding** and facilities along the waterfront.

Along the Upper Harbour industrial lands, promote visual connections to the water while supporting continued operation of the working harbour.

### 4.1. Waterways

4.1.1. Continue to work with community, CRD, industry and provincial and federal partners to improve and restore water and environmental quality in the Gorge Waterway.

4.1.2. Continue to support community-led stewardship and restoration of the Gorge Waterway.

4.1.3. Continue to manage Gorge and Outer Harbour areas as migratory bird sanctuaries. Support compatible on-shore and on-water uses and activities in this area.

4.1.4. Support restrictions on motorized watercraft north of the trestle bridge.

4.1.5. Investigate incorporating Green Shores principles for ecologically-sensitive shoreline development in waterfront parks.

4.1.6. Consider future changes to Development Permit Area guidelines for waterfront properties considering provincial guidance on sea level rise.

### 4.2. Waterfront Access and Trails

4.2.1. Complete a continuous waterfront pedestrian route around the Gorge Waterway and Outer Harbour as shown in Map 6.

4.2.2. As part of future planning for Arm Street Park and Burleith Park, reconsider opportunities for acquisition of easements for an environmentally sensitive public pathway near the waterfront.

4.2.3. Support community efforts to establish a continuous marine trail linking View Royal, Esquimalt and Saanich.

4.2.4. Support community-led efforts **in partnership with the City** to create new and/or improve existing public docks, wharves and other public access for neighbourhood swimming and/or watercraft with possible locations at Banfield Park, Arm Park, Burleith Park, **on the shoreline adjacent to** Railyards and along the Westsong Walkway, subject to habitat and environmental considerations.

See park-specific sub-sections of this chapter for suggested improvements to waterfront parks.

### 4.3. Water Lots

4.3.1. The Victoria Harbour Traffic Scheme (Transport Canada) identifies the north shore of the Outer Harbour as a location for non-motorized recreation. Consideration of proposed works on, or rezoning of, existing water lots along the Outer Harbour west of Cooperage Place should be guided by the following (for other water lots, consult the Victoria Harbour Plan):

- Any works or rezoning proposed for water lots should consider impacts to the ecology, wildlife, non-motorized recreation, enjoyment of adjacent park lands and trails, the Harbour Traffic Scheme, and impacts to on-shore traffic circulation and parking. Low-impact uses, particularly related to non-motorized water recreation, may be considered.
- Further uses which generate motorized boat traffic are discouraged.



## Legend

- Waterfront pedestrian routes
- ..... Possible future opportunity
- Existing public water access
- Potential public water access
- ⊙ Beach/informal access
- Private marina



**Map 6. Existing and potential waterfront pedestrian routes and public access**





*Playground and Community Centre in Banfield Park*



*Vic West Park*



*Songhees Park*



*Banfield Park Community Orchard*



*Hereward Park by E&N Trail*



*Arm Street Park*



*Bicycle parking and totem pole by Galloping Goose Regional Trail*



*Westsong Walkway*



*Arm Street Park Waterfront*

# Parks, Open Space and Waterways

## Neighbourhood Parks

### 4.4. Parks and Open Space Network

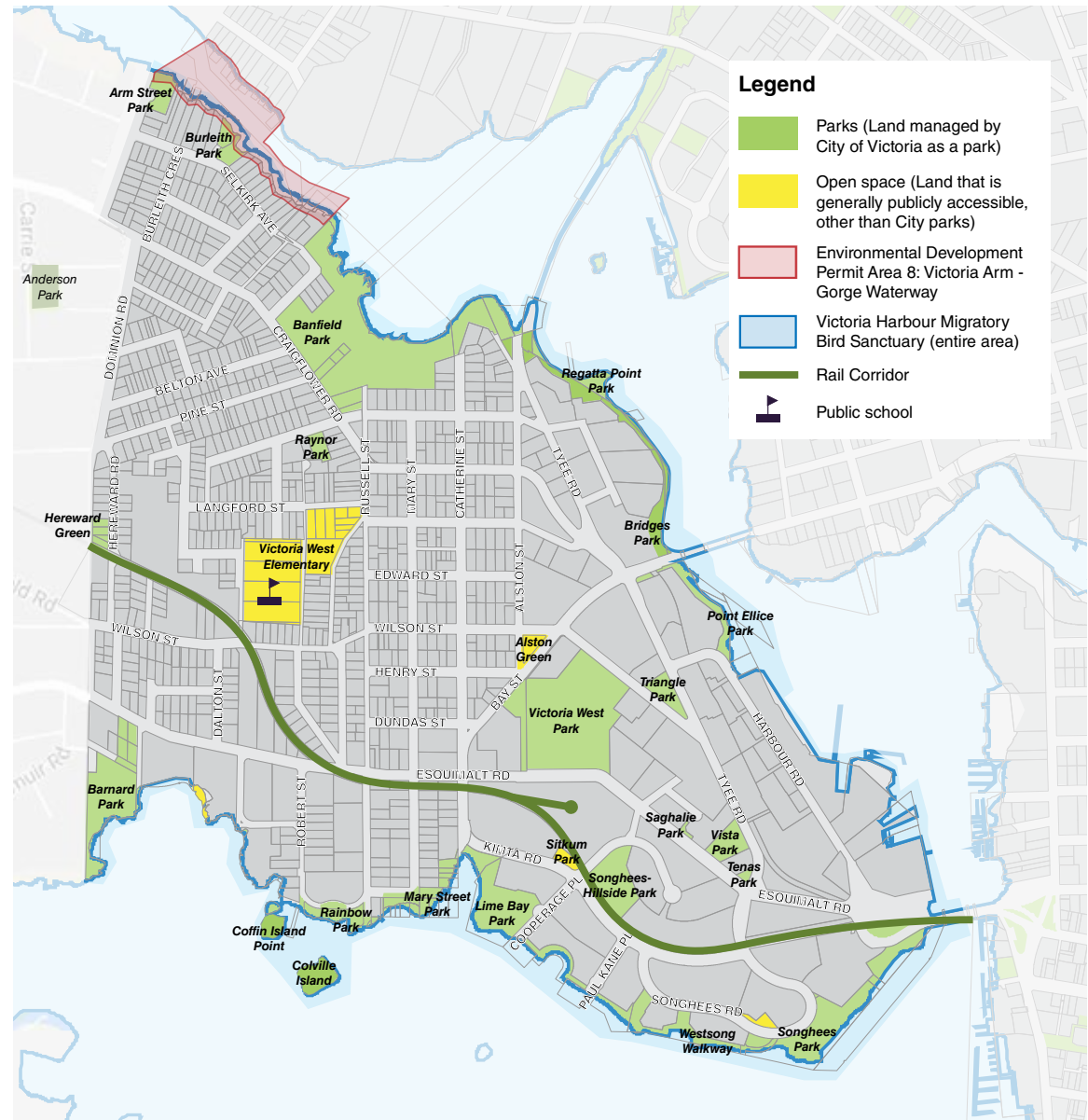
#### Intent:

Enhance parks to welcome all ages and abilities and provide diverse activities.

4.4.1. Protect and maintain the existing parks and open space network identified on Map 7: Parks and Open Space Network.

4.4.2. Design and refresh amenities in neighbourhood parks and open spaces to meet the needs of a range of ages, abilities and activities.

4.4.3. Improve access and create a more enjoyable walking and cycling experience to Vic West parks through the addition of new crossings and public realm improvements such as street trees, seating, bike racks, lighting and wayfinding.



**Map 7. Parks and Open Space Network**



## Parks, Open Space and Waterways

# Neighbourhood Parks, cont'd.

### 4.5. Community Gathering and Placemaking in Parks

4.5.1. Continue to work with recreation service providers to offer programming in neighbourhood parks.

4.5.2. Look for opportunities through park upgrades to add features to encourage neighbourhood gathering. Through engagement, residents suggested clusters of benches to encourage conversation, community ovens, and more picnic tables.

4.5.3. Use interpretive signage in parks for educational opportunities, and to connect people to the human and natural history of the neighbourhood.

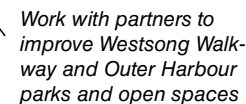
4.5.4. Engage the Songhees and Esquimalt First Nations to determine sites of **cultural significance** in parks and **public** open space and how to appropriately recognize these sites.

4.5.5. Explore opportunity for limited commercial activities in parks, such as food trucks and markets.

4.5.6. Encourage public art and placemaking in parks, and use of parks for community special events such as festivals and concerts.



# Park Improvements Summary



### Map 8. Park and Open Space improvements Summary



## Parks, Open Space and Waterways

# Park Improvements, cont'd.

### 4.6. Arm Street Park and Burleith Park

4.6.1. Undertake further planning for Arm Street Park and Burleith Park to add improvements to activate these parks, **support shoreline ecosystems** and draw people to the waterfront.

- a) Consider improvements to Arm Street and Burleith Parks that would allow further water access, **including swimming** docks, consistent with protection of sensitive habitat in the Victoria Arm of the Gorge Waterway. Conduct habitat assessment and feasibility studies to determine potential dock locations (see 4.2.4).

4.6.2. Evaluate the desirability of acquiring easements for a future extension of the waterfront walkway between these parks if consistent with environmental protection (see 4.2.2.).

### 4.7. Barnard Park

4.7.1. Improve wayfinding and pedestrian connections leading to Barnard Park.

4.7.2. Enhance Esquimalt Road edge of the park, including improved pathway and sightlines into the park.

4.7.3. Consider a pathway connection from Esquimalt Road to the playground.

### 4.8. Banfield Park

4.8.1. Create a park improvement plan for Banfield Park in collaboration with community organizations, provincial and federal partners to guide long-term investment such as:

- a) Maintain natural, green feel of the shoreline.

- b) Update existing amenities, including the playground, docks and trails (see 4.2.4.). **Explore opportunities for improved access to the beach.**
- c) Improve pedestrian and cycling safety while balancing ecological health.
- d) Consider new amenities **to encourage community gathering and use**, including a small stage for music performances, community oven, public barbecue facilities, additional picnic tables, or exercise equipment.
- e) Assess shoreline health and needs of migratory waterfowl, and recommend protection and management strategies.
- f) Protect and restore the natural areas of the park.
- g) Improve the delineation of the off-leash dog area.
- h) Add features along Craigflower Road to animate the edge of Banfield Park, create a place for gathering, which invites people to cross from the commercial area across the road.
- i) Consider relocating sidewalk along Craigflower Road to the inside of the trees, maintaining the trees to create a more pleasant pedestrian environment, and create opportunity for more on-street parking to support the adjacent small urban village and slow auto traffic.



*Galloping Goose Trail through Railyards development*



*Temporary use of open space in Dockside Green*



*Tennis courts in Banfield Park*

#### 4.9. Sitkum Park and Roundhouse Open Spaces

4.9.1. Sitkum park provides a landscaped natural feature to enhance the public realm and act as a gateway to the Roundhouse area.

4.9.2. Develop the publicly accessible Turntable Plaza and Roundhouse Mews, and conserve habitat in the Garry oak – camas meadow southeast of Esquimalt Road and Kimta Road, as identified in the Roundhouse Master Development Agreement (see Chapter 9).

#### 4.10. Point Ellice Park and Dockside Green Open Spaces

Through the development of Dockside Green:

4.10.1. Develop Point Ellice Park consistent with the Dockside Master Development Agreement, including:

- a) Restoration of shoreline and upland plant communities, providing habitat for a variety of flora and fauna.
- b) Integration of shoreline habitat with existing features providing continuity to the site's history as working harbour.
- c) Assessment of the inter-tidal zone for its potential to be restored as a functioning marine system.

4.10.2. Develop publicly accessible open spaces consistent with the Dockside Master Development Agreement, including: a central, accessible Greenway; a children's playground; Dockside Commons, with a large lawn area with formal and informal seating; and Dockside Landing, a plaza fronting onto Harbour Road (see Chapter 9).

#### 4.11. Galloping Goose Trail and Adjacent Green Spaces

4.11.1. Work with City departments, CRD and provincial and federal partners to:

- a) Improve lighting on the Galloping Goose through Vic West, including the area between the Selkirk Trestle Bridge and Harbour Road.
- b) Address safety concerns at the intersection by the Trans-Canada Trail pavilion.
- c) Improve the **visitor experience** along the waterfront through on-going maintenance and other improvements such as visibility and trail design.
- d) Address speed and use concerns for electric or motorized devices (bikes, scooters) along the Galloping Goose.
- e) Consider improvements to reduce conflicts between pedestrians and cyclists.
- f) Consider street vending along the Galloping Goose Trail in limited locations, such as plazas or at key intersections.

#### 4.12. Hereward Park

4.12.1. Work with the CRD and BC Hydro to secure land to widen and improve the access trail between Hereward Road and E&N Trail for cyclists, strollers and people with mobility issues.

4.12.2. Work with residents and organizations to maintain and improve health of fruit trees.

#### 4.13. Bridges Park and Railyards Open Spaces

4.13.1. Improvements are to be completed as part of the Railyards Master Development Agreement, including:

- a) A children's play lot.
- b) Restoration of the shoreline environment to increase diversity and habitat.
- c) Consideration of opportunities for historical interpretation.

4.13.2. Complete the trail connection to, and entry plaza at, the Bay Street and Tyee Road intersection consistent with the Railyards Master Development Agreement.

#### 4.14. Triangle Park

4.14.1. Re-align trails in Triangle Park to provide a more accessible and direct connection between Dockside Green, the Galloping Goose, Wilson Street and Westside Village.

4.14.2. Consider opportunities for more fruit trees or a community orchard, **where there is community interest**.

#### 4.15. Vic West Park

4.15.1. Continue to implement park improvement plan (2016), including sports field improvements, community garden space, washroom and pathways.

4.15.2. **Assess vehicle drop-off/ parking conditions for the Skate Park.**

4.15.3. Add features to northern edge of Vic West Park along Bay Street to animate the edge of the park and invite people from the residential neighbourhood to use the park.

#### 4.16. Songhees Hillside Park

4.16.1. Preserve vistas of the Strait of Juan de Fuca and the Olympic Mountains.

#### 4.17. Westsong Way and Outer Harbour Parks and Open Spaces

4.17.1. Work with City departments, CRD, Greater Victoria Harbour Authority and provincial and federal partners to:

- a) Improve wayfinding and visibility of access points between the Westsong Walkway and the surrounding neighbourhood.
- b) Add bike parking at key access points to the trail.
- c) Add more seating in parks along Westsong way, including picnic tables and clustered benches or other seating to encourage conversation.
- d) Consider opportunities to animate locations along the walkway such as public art, temporary installations, events or vending.
- e) Assess needs for additional waste receptacles.
- f) Consider street vending along Westsong Way in limited locations, such as plazas or at key access points.

#### 4.18. Rainbow Park and Mary Street Park

4.18.1. In addition to policies 4.20.1:

- a) Enhance these parks with features which encourage activation and use. Suggestions from the community include additional picnic tables, enhanced landscape, enhanced connections to the trail, and a community garden.

#### 4.19. Lime Bay Park

4.19.1. In addition to the policies in 4.20.1., provide improvements to Lime Bay park consistent with the Roundhouse Master Development Agreement and previous planning for the Songhees area:

- a) Maintain the natural environment of the shoreline and adjacent slopes through the use of native plant materials.
- b) Provide terraced seating areas capitalizing on views, as a resting point along the Westsong Walkway.
- c) Incorporate sustainable stormwater practices.
- d) Establish a visual and pedestrian connection to Turntable Plaza (through the design of the Roundhouse development and associated crossing improvement at Kimta Road).
- e) Provide picnic areas.
- f) Improve access to the natural beach.

#### 4.20. Johnson Street Bridge Public Realm

4.20.1. Expand Songhees Park through improvements to the Johnson Street Bridge lands consistent with the Council-approved concept design (2017) that includes:

- a) Open, flexible green space.
- b) Landscaped terraces and stepped seating for harbour views.
- c) Grade accessible pathways and lighting.
- d) Connections to trails (E&N and Galloping Goose) and the Westsong Walkway.
- e) Look-out view deck at the water.
- f) Ecologically based native plantings.



# Parks, Open Space and Waterways

## Urban Forest

Victoria's urban forests provide important ecosystem services **resulting in** cleaner air and water, habitat for wildlife and improved rainwater absorption and contribute to Vic West's character and sense of place.

### Intent:

Maintain and expand the urban forest on public and private lands.

#### 4.21. Trees and Native Ecosystems in Public Parks

4.21.1. Identify tree planting strategies as part of park improvement plans. Select species that are resistant to climate change impacts and native species, where possible.

4.21.2. Restore and expand native ecosystems and natural areas at Banfield Park, Gorge waterway and public waterfront access points.

4.21.3. Identify suitable locations for community orchards in parks and open space, where residents and community organizations express interest in stewardship.





## Parks, Open Space and Waterways

# Urban Forest, cont'd.

### 4.22. Boulevards and Street Trees

4.22.1. In residential areas, ensure that neighbourhood character is maintained **or enhanced** through the planting and/or replacement of street trees that help preserve and **enhance** the tree canopy. Given the number of smaller lots located in Vic West, the boulevards represent one of the most important opportunities to protect and enhance the urban forest.

4.22.2. **Where possible**, stagger replacement of street trees to minimize impacts to neighbourhood character and aesthetics, where possible. Consider in-fill replanting of trees in areas where trees are aging in order to minimize impacts to canopy over time.

4.22.3. Select tree species that maximize urban forest benefits and are resistant to climate change impacts.

4.22.4. In Small and Large Urban Village areas **and other new multi-unit developments**, add new street trees where possible as part of public realm improvements.

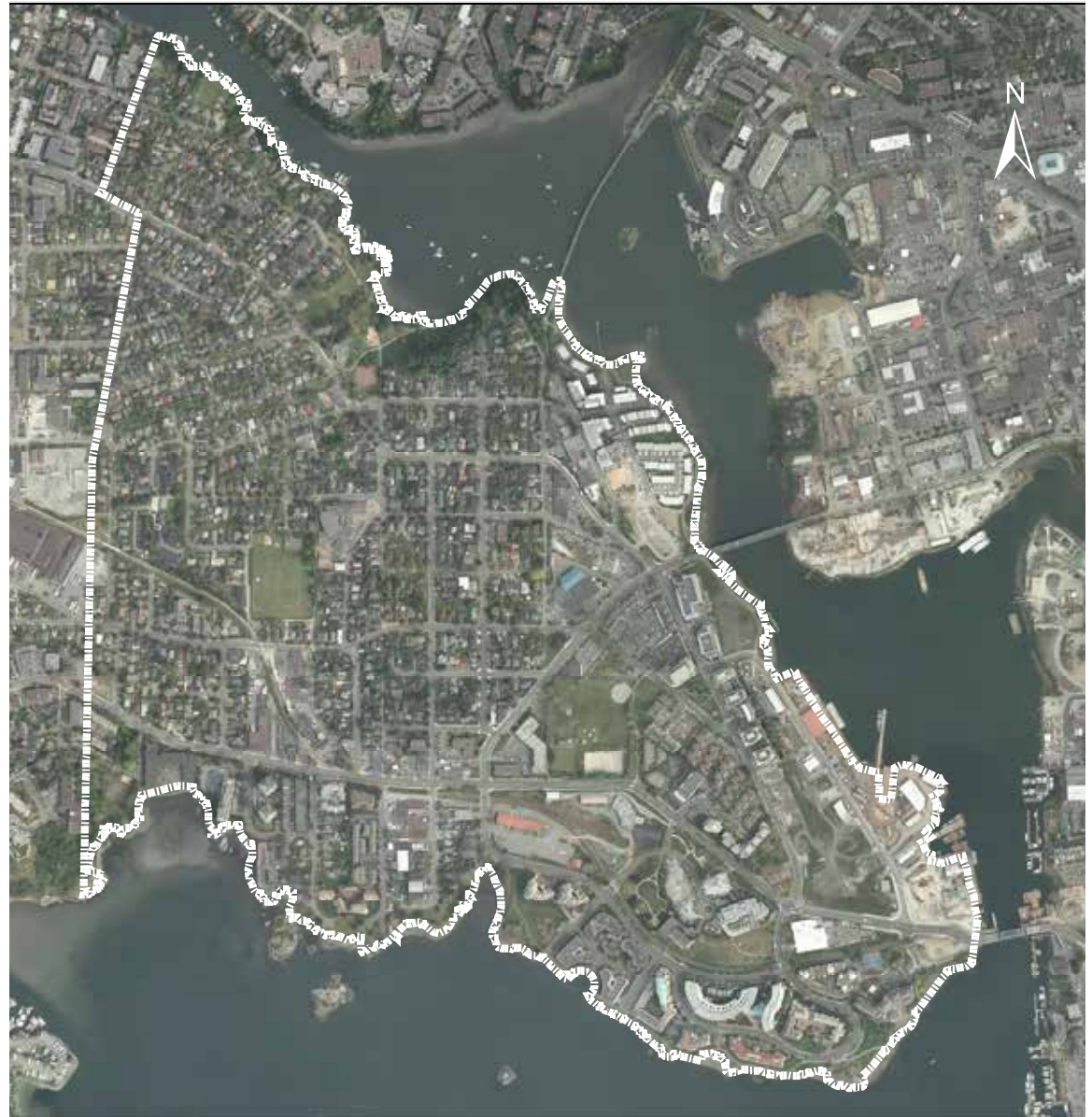


Figure 11: Tree coverage in Vic West (2017)

# 5. Future Land Use

The primary role of a neighbourhood plan is to guide how land should be used in the future. This chapter provides an overview of policies guiding what types of land uses, building forms, and densities are appropriate in different parts of Victoria West. The subsequent chapters 6-10 provide more detailed guidance. These chapters provide advisory policies which add further detail to the City's Official Community Plan (OCP) bylaw.

Together with the OCP, the Neighbourhood Plan provides direction for what is envisioned in the future. Within this framework, policies provide for a range of possible uses and leave room for individual decisions to be made related to the needs of owners, the interests of the community, and context of the site.

The OCP and Neighbourhood Plan do not take the place of zoning. It is the City's Zoning Bylaw which confers legal development rights for any property. When a change in zoning is sought, the OCP and the Neighbourhood Plan provide guidance for considering the appropriate zoning for a property.

Vic West contains a diversity of places and contexts. By managing change, this plan envisions moving Vic West closer to the vision of a diverse, sustainable community which maintains its sense of place and is a desirable place in which to live, work, shop or recreate.



Map 9. Neighbourhood Land Use Map

## Future Land Use Categories at a Glance

	Uses	Density	Building Types
<b>Traditional Residential</b>	<ul style="list-style-type: none"> <li>Residential</li> <li>Commercial uses may be supported at the intersection of major roads or in limited areas identified in Policy 6.4 and 6.10.</li> </ul>	<p>Density generally up to 1.0 FSR</p> <p>Density in most areas limited by housing types identified in Chapter 6.</p> <p>Opportunities for density up to 1.2 to 1.5 FSR in limited areas as identified in Chapter 6.</p>	<p>Single-detached dwellings, detached dwellings with accessory suites and duplexes.</p> <p>Generally ground-oriented residential buildings, including houseplexes, townhouses, and house conversions consistent with the policies in Chapter 6.</p> <p>Multi-unit residential buildings up to three storeys along Esquimalt Road, south of Esquimalt Road, adjacent to Catherine at Edward Street Village, and in the Pioneer Housing Cooperative site.</p>
<b>Urban Residential</b>	<ul style="list-style-type: none"> <li>Residential</li> <li>Commercial uses may be supported at grade fronting Esquimalt Road</li> </ul>	<p>Up to 1.2 FSR</p> <p>Opportunities for bonus density up to approx. 2.0 FSR.</p>	<p>As above, plus:</p> <p>Multi-unit residential buildings up to approximately 4 to 5 storeys are generally supported, where indicated in Chapter 6, with variable setbacks and front yard landscaping.</p> <p>Residential or mixed use buildings fronting Esquimalt Road</p> <p>Upper floors above the streetwall generally set back.</p>
<b>Small Urban Village</b>	<ul style="list-style-type: none"> <li>Active commercial uses* on the ground floor in most locations, with residential or commercial uses above.</li> </ul>	<p>Up to 1.5 FSR</p>	<p>Commercial or mixed use buildings up to 3 storeys.</p> <p>Conversions of single detached houses to commercial or mixed use.</p> <p>In some areas, residential or live-work buildings (see Chapter 8).</p> <p>For new buildings, ground level generally built up to the sidewalk, with parking located to the rear of buildings or underground.</p>
<b>Large Urban Village</b>	<ul style="list-style-type: none"> <li>Commercial uses on the ground floor, with active commercial uses* in most locations (see Chapter 7)</li> <li>Residential or commercial uses in upper floors</li> <li>Live-work or work-live**</li> </ul>	<p>Up to 1.5 FSR</p> <p>Opportunities for bonus density up to approx. 2.5 FSR</p>	<p>Commercial or mixed use buildings mostly up to 6 storeys.</p> <p>Buildings set close to the street to define the public realm along retail streets, with landscaped setbacks in more residential areas.</p> <p>Upper floors above the streetwall generally set back.</p> <p>Parking located in structures or underground.</p>
<b>Core Songhees</b>	<ul style="list-style-type: none"> <li>Varied commercial, residential, and limited light industrial uses</li> </ul>	<p>Up to 2.5 floor space ratio or as identified in a Master Development Agreement.***</p>	<p>Commercial, residential or mixed use buildings of varying heights</p> <p>Buildings set close to the street to define the public realm along retail streets, with landscaped setbacks in more residential areas.</p> <p>Upper floors above the streetwall generally set back.</p> <p>Parking located in structures or underground.</p>

\* Active commercial uses are those that tend to generate foot traffic and pedestrian interest, and include retail stores, food and drink establishments, personal services, theaters and entertainment, and may include some professional services, medical services or personal services.

Figure 12: Future Land Use Category Matrix

## Future Land Use Categories at a Glance, cont'd.

	Uses	Base/Max Density	Building Types
<b>General Employment</b>	<ul style="list-style-type: none"> <li>Commercial uses or light industrial uses****</li> </ul>	Up to approx. 2.0 FSR	<p>Commercial or light industrial buildings where the ground level is built up to the street.</p> <p>Parking generally located to the rear or underground.</p> <p>Any retail uses should generally be located at ground level.</p>
<b>General Employment with limited residential</b>	<ul style="list-style-type: none"> <li>Light industrial**** or commercial uses on the ground floor. Limited retail uses only.</li> <li>Commercial or residential on upper floors</li> <li>Work-live**</li> </ul>	<p>Up to approx. 2.5 FSR along Esquimalt Road Corridor and between Skinner Street and Vic West Large Urban Village</p> <p>Up to approx. 2.0 FSR elsewhere</p> <p>Residential space resulting from rezoning is considered bonus density</p> <p>Residential floor area is limited to one-half of total floor area</p>	<p>Commercial or mixed use buildings where the ground floor is built up to the street and consists of commercial or light industrial uses.</p> <p>Buildings up to approximately 4 to 6 storeys (see Chapter 8).</p> <p>Parking generally located to the rear or underground.</p>
<b>Industrial Employment</b>	<ul style="list-style-type: none"> <li>Light industrial uses and ancillary uses on any floor, including ancillary retail sales</li> <li>Commercial uses (except those ancillary to a light industrial use) are limited to upper floors</li> </ul>	Up to approx. 2.0 FSR	<p>Various building forms, where the ground level supports light industrial uses, with a floor-to-ceiling height suitable for contemporary light industry (with or without mezzanine), loading bays, and adequate separation from separate uses above.</p> <p>Up to approximately 3-4 storeys (see Chapter 8)</p>
<b>Marine Industrial</b>	<ul style="list-style-type: none"> <li>Primary processing, marine industrial, marine transportation, warehousing, shipping, bulk materials handling, and other industrial uses and accessory offices on upper floors</li> <li>Work-live***</li> </ul>	Up to approx. 3.0 FSR	<p>Industrial buildings and structures consistent with the Harbour Road Industrial Waterfront Guidelines and related zoning.</p>

\*\* Work-live or live-work units should be designed so that residential space and work space may be leased and occupied separately. (See also Work-Live Planning and Design Guidelines, as updated)

\*\*\* Many sites in this former industrial area have contributed to improvements in the area which support goals such as public spaces and housing diversity. For sites where amenity contributions have not been previously identified (for example, through a Master Development Agreement), rezoning for additional density should consider amenities to mitigate impacts of development.

Figure 12: Future Land Use Category Matrix, cont'd.

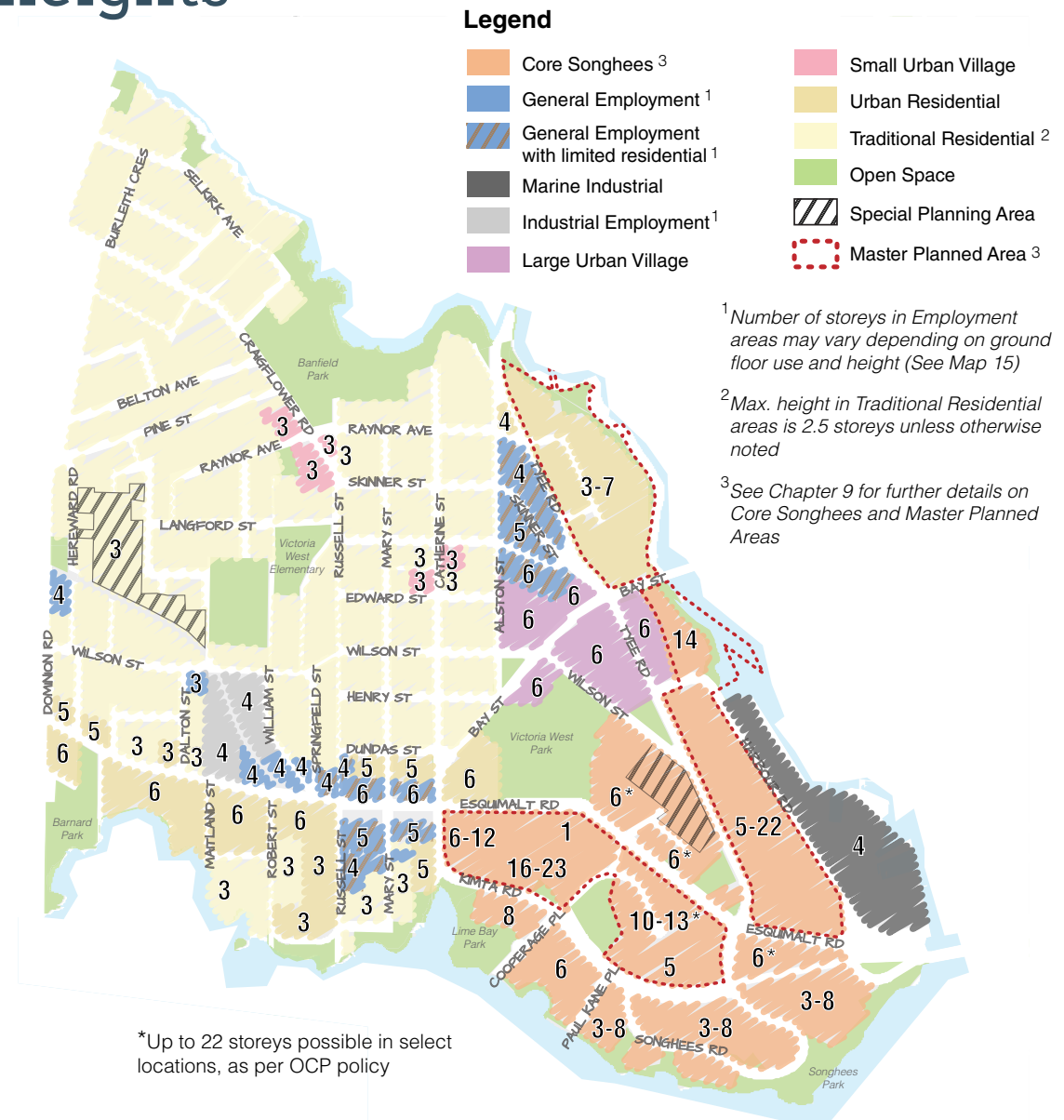


## Future Land Use

# Anticipated building heights

This plan contains general policies regarding anticipated maximum building heights for areas outside of the Core Songhees area. While Map 10 summarizes these policies, further detail on these and other policies regarding urban form can be found in Chapters 6-8.

Policies regarding building height are only one type of policy that guides future development. Setbacks and design guidelines provide further guidance, and encourage sensitive transitions to adjacent development. While building heights are envisioned for different areas, the maximum building height may not be achievable on all lots.



**Map 10. Anticipated maximum building heights at a glance**

Note: Storeys are approximate. See Maps 12, 13, 14, 15 and associated policies for further detail.

# Future Land Use

## View Corridors

This map is a compilation of public view corridors for reference only. Public view corridors indicate that future buildings in these corridors should be designed and massed sensitively to preserve or frame certain views from public spaces. For more detailed policy regarding view corridors, please see Chapter 6. Residential Areas, Chapter 8. Employment Lands, as well as the documents listed on Map 2 and the City of Victoria Downtown Core Area Plan.



**Map 11. View corridors at a glance**



# 6. Residential Areas

This plan aims to support a range of housing types, including both ownership and rental opportunities for different household types and sizes. Vic West features a range of housing types including medium- and high-density condominiums and rental apartment buildings, townhouses, duplexes and single-detached houses, heritage house conversions, small lot houses and new types of infill development. Different housing types are appropriate in master planned sites, urban villages and along busier streets than on local streets. The plan also recognizes that existing lot patterns, dimensions, and topography may influence the type of housing that is appropriate on a particular site.

## Goals:

1. Encourage a mix of housing sizes, costs, tenures and types
2. Create more affordable housing
3. Showcase new, innovative housing types
4. Encourage the retention and reuse of historic buildings

## Other Relevant Policies & Bylaws

- Official Community Plan
- Victoria Housing Strategy
- Market Rental Revitalization Strategy (upcoming)



*Vic West is characterized by a mix of housing types, ages, styles and lot configurations.*



*East of Alston Avenue, significant redevelopment has occurred, often replacing former industry with a mix of taller buildings and townhouses.*



*West of Alston Avenue, the neighbourhood consists primarily of ground-oriented housing including single detached homes, duplexes, house conversions, townhouses, and small apartments along tree-lined streets.*



*Somewhat larger lots tend to be found in the blocks east of Russell Street.*



*Western parts of the neighbourhood are characterized by many smaller lots on an irregular pattern of streets.*



*Two larger housing cooperatives – Tyee and Pioneer – add to the diversity of housing options.*



## Residential Areas

# Urban Residential Areas

### Intent:

Support mixed use and multi-unit development near urban villages, transportation corridors, parks and other amenities.

### 6.1. Land Use Policies

6.1.1. Consider maximum building heights consistent with Map 12.

6.1.2. The location, siting and design of new development should consider the view corridor identified from Catherine Street at Edward Street, to maximize views of the Olympic Mountains.

6.1.3. Residential buildings up to 1.2:1 floor space ratio are supported in this area.

6.1.4. Support the retention or replacement of existing rental units on sites of four or more rental units by considering an increase in zoned density on these sites only if, as a voluntary amenity, an equivalent number and kind (e.g. number of bedrooms) of units is maintained on-site and secured as rental housing consistent with the Official Community Plan and city-wide housing policies. A voluntary contribution to the Housing Reserve Fund in lieu of rental retention or replacement **on-site is strongly discouraged in these areas.**

6.1.5. Consider the following additional density:

- up to 2.0 floor space ratio where any required rental replacement or retention of units is secured (6.1.9.) and where an affordable housing contribution consistent with city-wide policy is secured for the life of the building.
- For projects providing additional affordable housing benefit (e.g. deeper affordability, 3 bedroom units, additional

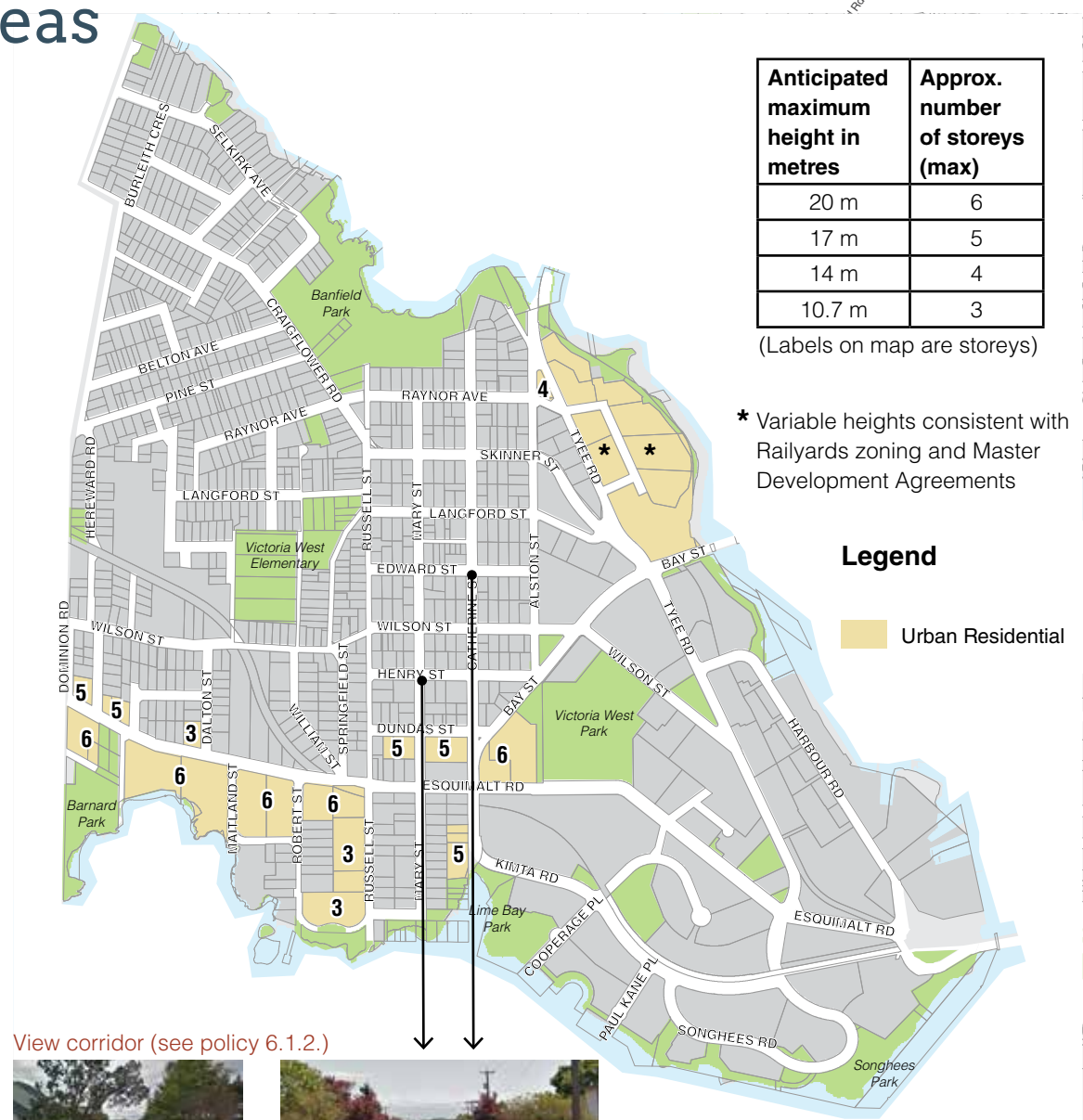
Anticipated maximum height in metres	Approx. number of storeys (max)
20 m	6
17 m	5
14 m	4
10.7 m	3

(Labels on map are storeys)

\* Variable heights consistent with Railyards zoning and Master Development Agreements

### Legend

Urban Residential



Map 12. Urban Residential Areas



on-site non-market units, etc.), additional density beyond 2.0 floor space ratio may be considered consistent with city-wide policies.

6.1.6. Developments fronting onto Esquimalt Road may include commercial uses on the ground floor.

## 6.2. Urban Form and Character Guidance for All Urban Residential Areas

6.2.1. In the area south of Esquimalt Road, developments should step down to the waterfront, with lower-scale development (for example, townhouses or heritage conversions) located in the lots facing the waterfront.

6.2.2. The scale of street walls (that portion of the building façade closest to the street) should frame the street at lower levels, and consider stepping back at upper levels to maximize sunlight penetration to the street.

6.2.3. Minimize the impacts of off-street parking on the quality of site designs and the pedestrian environment. Underground or enclosed parking is strongly encouraged.

6.2.4. Where parking is located in a structure but not underground, avoid blank walls adjacent to the street.

6.2.5. Ground-level units are encouraged to contain individual entries and semi-private open spaces (e.g. porches or patios) facing the street, especially along local and collector streets, to reinforce the sense of neighbourliness.



6.2.6. Development adjacent to lower-density residential uses should sensitively transition through massing, design, setbacks and landscape that minimizes shading and overlook and provides for building separation and privacy.

6.2.7. The height of street walls (that portion of a building closest to the street) to frame streets and provide definition and enclosure while maintaining sensitivity to and access to sunlight for development across the street.

6.2.8. The siting and access of new development should provide opportunities to create sufficient boulevard planting space for at least medium-sized canopy trees.

6.2.9. Include landscape and on-site open spaces that contribute to urban forest objectives, provide environmental benefits, and support sociability and livability. Where a pattern of landscaped yards adjacent to streets exists, this pattern should be continued.

6.2.10. Consider features which support urban food production for residents, including edible landscaping, allotment or rooftop garden area.

6.2.11. Development adjacent to the intersection of Esquimalt Road and Dominion Road should help create a gateway recognizing the transition between Esquimalt and Victoria (see 14.1.8.).



## Residential Areas

# Traditional Residential Sub-Areas



### Intent:

Development within the Traditional Residential areas is intended to:

- provide a range of ground-oriented forms of housing appropriate to lot size and context
- support additional ownership and rental opportunities for different household sizes
- support street trees and the urban forest with planting spaces on private lands and public boulevards
- encourage the retention and re-use of historic buildings

### 6.3. Traditional Residential Housing Sub-Areas

Within its Traditional Residential areas Vic West contains a wide variety of lot sizes, configurations, topography and varying proximity to amenities such as transit, parks and commercial areas. Based on these conditions, Vic West's Traditional Residential areas can be divided into several sub-

areas. Different conditions support different types of housing appropriate to each of these sub-areas (see Map 13).

- 1 Catherine Street at Edward and Craigflower Village Adjacent Sub-Area:** These properties provide opportunities for development which complements the adjacent small urban village. On Catherine Street between Edward and Langford Streets, cohesive and high quality urban design is desired for future development of the entire blockface, whether commercial mixed-use, or residential. **On Russell Street between Skinner Street and Raynor Avenue, there is opportunity for residential development which complements the village in form, scale and design, downslope from adjacent lower-density sub-areas.**
- 2 South of Esquimalt Road Area:** The Traditional Residential areas here comprise part of a community with a mix of housing types, situated near the Esquimalt Road corridor, adjacent to waterfront amenities, within walking distance of

downtown, and near the Roundhouse development. These Traditional Residential areas provide opportunity for further ground-oriented development complementing existing townhouses and courtyard apartments with lower scale buildings located near the waterfront, stepping up towards taller buildings along Esquimalt Road.

- 3 Esquimalt Road Sub-Area:** The Traditional Residential area along Esquimalt Road supports a variety of development up to 3 storeys, with design responsive to topography and supporting its location along a frequent transit route.
- 4 Pioneer Housing Cooperative Special Planning Area:** This area provides an important community with affordable housing options and is one of the few large properties found in the Traditional Residential areas. Should the cooperative choose to redevelop, the Special Planning Area policies in section 6.4 provide guidance.



## Residential Areas

# Traditional Residential Sub-Areas, cont'd.

- 5 Skinner Street Sub-Area:** The Skinner Street Corridor between Craigflower Village and Alston Street contains a mix of housing, including historic and more recent buildings characterized by green front yards. This area is intended to support retention and adaptive reuse of heritage homes and a variety of new infill housing up to 2.5 storeys (with or without a basement) with front yards and characteristics compatible with its context.
- 6 General Areas:** These areas contain a mix of lot sizes and types, with a regular street grid and larger lots found east of Russell Street and a mix of lot sizes and block configurations elsewhere. These areas are proximate to Westside Village, the Esquimalt Road corridor, or the E&N Rail Trail, and accommodate a variety of infill housing forms, including townhouses on larger lots, where sensitive transitions, green spaces and tree planting are maintained.
- 7 North Catherine Street Sub-Area:** This area encompasses the Catherine Street North Heritage Conservation Area and adjacent streets which have a strong heritage character. Within this area, house conversions and compatible infill development is supported.
- 8 The Northwest Sub-Area:** Characterized by generally small lots and a pattern of older houses in various styles along tree-lined streets. Lot sizes, shapes, and setbacks vary widely. Gentle infill which respects the existing lot pattern and maintains tree planting sites on private property is encouraged. A small area west of Craigflower Village supports commercial uses where they reuse and integrate existing houses.
- 9 Gorge Arm Waterfront:** This area contains sensitive shoreline and aquatic habitat which should be

maintained and is protected in part by Development Permit Area 8 – Gorge Arm. Subdivision or zoning for further densification is not supported at this time.

### 6.4. Special Planning Area: Pioneer Housing Cooperative Block

In the event of a redevelopment of the Pioneer Housing Cooperative block which requires rezoning, the following principles should be considered:

- 6.4.1. All existing affordable housing units should be retained or replaced with a similar number and floor space of affordable housing units.
- 6.4.2. Ground-oriented housing including townhouses and stacked townhouses up to approximately three storeys may be supported. Some three-storey apartment buildings may also be included.
- 6.4.3. A density of up to 1.0 floor space ratio is supported on the site.
- 6.4.4. Redevelopment should create public connectivity for pedestrians and cyclists, between McCaskill Street and Hereward Road; and from the E&N Rail Trail to Sherk Street.
- 6.4.5. Buildings should be oriented towards and present friendly faces to public and private streets, open spaces, and pathways. Entries, windows, and spaces such as porches, patios and balconies should provide passive surveillance for these areas to create a sense of safety. This should include passive surveillance of the E&N rail Trail where possible from upper-floor windows and balconies. [New development should be designed, considering siting](#)

[and massing, to minimize impacts on access to sunlight and privacy for adjacent properties.](#)

6.4.6. [Community-serving uses accessory to the primary residential uses may be considered, e.g. child care, bicycle repair shop, a small café, event or gathering space, art gallery/studio, social enterprise, personal or medical services, co-working space\).](#) Such uses should [be limited in area and should not result in significant traffic impacts on local streets.](#)

6.4.7. Redevelopment to higher density should include a shared greenspace.

6.4.8. [Opportunities for urban food production are encouraged \(e.g. allotment garden, rooftop garden, edible landscape\).](#)

6.4.9. A variety of housing units is desired, with a focus on ground-oriented housing attractive to families with children. A mix of housing types which supports a multi-generational community is encouraged.

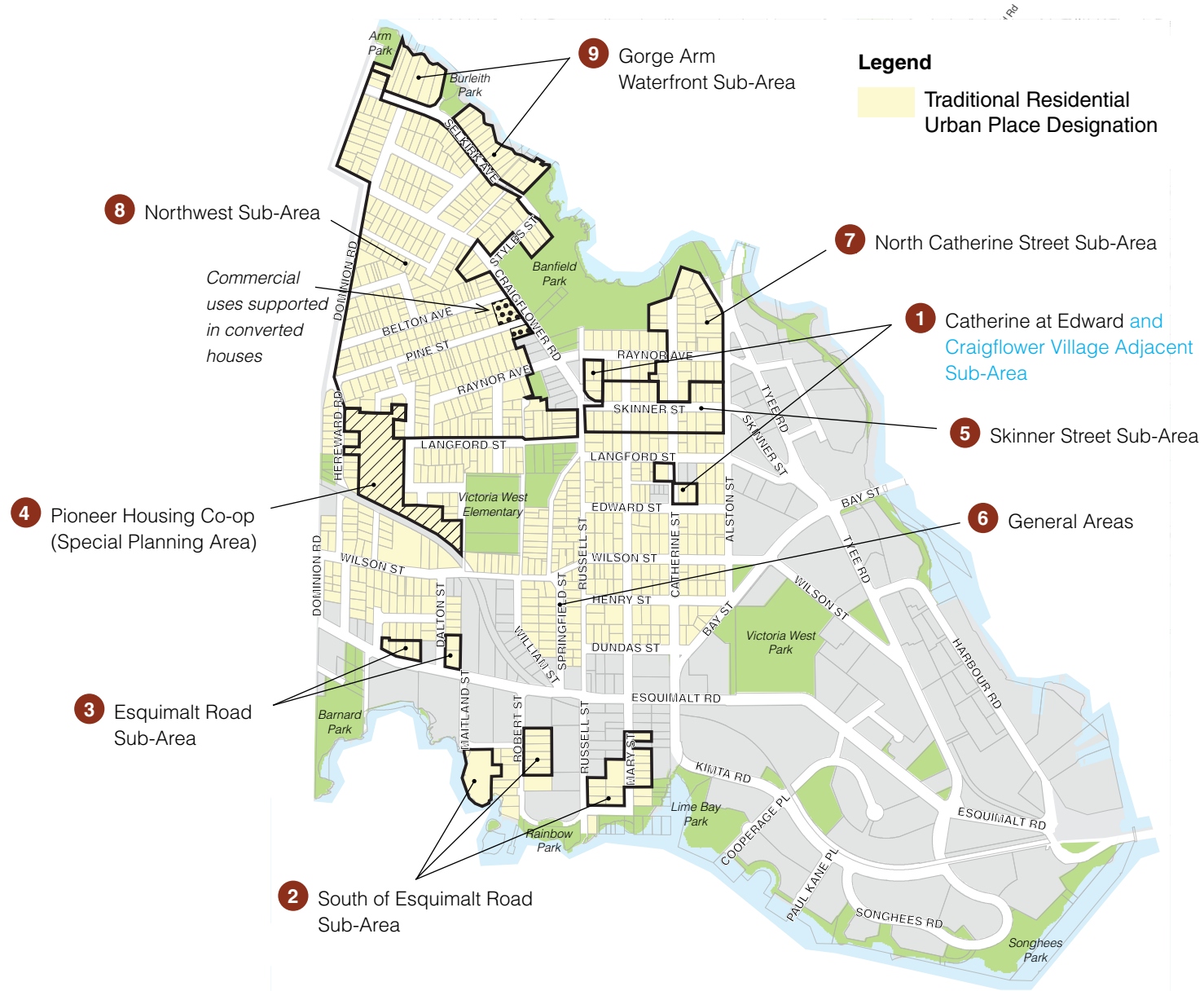
6.4.10. The impacts of surface parking should be minimized through strategies such as design, landscape, the use of previous pavement or on-site stormwater management, and the location of some or all parking in garages or underground.

6.4.11. Consider Transportation Demand Management strategies [if parking reductions are proposed](#). Strategies may include the provision of on-site car share services, and decoupled parking (parking spaces which are rented separately from the rent or ownership of housing units).



## Residential Areas

# Traditional Residential Sub-Areas, cont'd.



**Map 13. Traditional Residential Housing Sub-Areas**

## Residential Areas

# Traditional Residential Sub-Areas: Housing Types

### 6.5. Housing Types

Housing types supported within Traditional Residential Sub-Areas are outlined in Figure 13 and described in further detail in the subsequent sections. These sections are meant to guide redevelopment and zoning considerations, and are not meant to be a prescriptive or exhaustive list of development types that may be considered consistent with the desired characteristics of each area.

Fig. 13: Traditional Residential Areas – Envisioned Housing Types, Height and Density

Maximum density	Approx. maximum height in metres / storeys	Small Lot House	Single detached house with 2 suites <sup>b</sup>	Single-detached house <sup>a</sup>	Duplex with or without suites	House Conversion (house to multiple suites)	Houseplex	Townhouse in one row	Townhouse in more than one row	Small Apartment Building	Limited commercial uses	
1.5 FSR	10.7m (3 storeys)	✓	✓	✓	✓	✓	✓	✓	✓	✓		① Catherine at Edward and Craigflower Village Adjacent Sub-Area
1.2 FSR		✓	✓	✓	✓ <sup>c</sup>	✓	✓	✓	✓		② South of Esquimalt Road Sub-Area	
1.0 FSR		✓	✓	✓	✓ <sup>c</sup>	✓	✓	✓	✓		③ Esquimalt Road Sub-Area	
1.0 FSR		See section 6.4, Pioneer Housing Cooperative Special Planning Area										④ Pioneer Housing Cooperative Block
1.0 FSR	9m (2.5 storeys)	✓	✓	✓	✓ <sup>c</sup>	✓	✓	✓	✓			⑤ Skinner Street Sub-Area
*	7.6m – 8.2m (2 – 2.5 storeys)	✓ <sup>d</sup>	✓	✓	✓	✓	✓	✓	✓			⑥ General Areas
*		✓	✓	✓	✓	✓	✓	✓		✓		⑦ North Catherine Street Sub-Area
*		✓	✓	✓	✓	✓	✓ <sup>f</sup>	✓	✓		✓ <sup>e</sup>	⑧ Northwest Sub-Area
*			✓	✓			✓					⑨ Gorge Arm Waterfront Sub-Area

See policies 6.6 - 6.9 in the subsequent “Guidance for Traditional Residential Sub-Areas” section of this chapter for considerations regarding design and site layout, heights, setbacks, and heritage considerations.

See policies 6.10-6.16 in the subsequent “Traditional Residential Housing Types” section of the chapter, for specific policies for each housing type, including appropriate lot sizes.

- a. Single-detached houses may include a secondary suite or garden suite.
- b. A designated heritage house may include two suites, or a secondary suite and a garden suite.
- c. In these transit-friendly areas, duplexes are strongly encouraged to contain suites.
- d. Small lot subdivision is **discouraged** east of Russell Street, where other housing types are encouraged.
- e. Commercial uses are supported in converted houses fronting onto Craigflower Road between Belton Avenue and Craigflower Village
- f. Most places limited to 3 units; additional units supported along Craigflower Road
- \* Variable based on development type

Figure 13: Traditional Residential Areas - Envisioned Housing Types, Height and Density

## Residential Areas

# Guidance for Traditional Residential Sub-Areas

### 6.6. Considerations for Residential

#### Infill: Building Height

##### Intent:

Maintain building heights generally compatible with the surrounding neighbourhood, while providing new housing. Heights for any one location will be determined by zoning and may consider topography, setbacks and other design aspects.

6.6.1. The following building heights should guide consideration of rezoning in these areas:

- a) In sub-areas 1-4: Buildings up to approx. 10.7 metres (approx. 3 storeys).
- b) In sub-area 5, Skinner Street Corridor: Buildings of up to approx. 9 metres and not exceeding 2.5 storeys (see Fig. 14).
- c) In sub-areas 6-9:
  - i. For buildings fronting onto a public street, buildings of up to 2.5 storeys (up to approx. 7.6 - 8.2 metres) may be considered for infill housing (see Fig. 15).
- d) The above policies notwithstanding, see Policy 6.16 for building heights for small lot houses in all sub-areas.
- e) Other sections of this plan and associated Development Permit guidelines provide guidance for sensitive transitions to adjacent development, for buildings within the anticipated height ranges.

### 6.7. Building Siting

##### Intent:

Maintain setbacks and open site space which support the existing character of landscaped front and rear yards compatible with Vic West.

6.7.1. Support front setbacks consistent with the variety of modest front setbacks found in Vic West, with sufficient space for landscape and respect for existing patterns along the street (generally 3.5 - 6 metres)

6.7.2. Support side setbacks consistent with the rhythm of homes facing the street (generally 1.5 metres). For units that do not front onto a public street (e.g. a second row of townhouses), greater side setbacks are desired, depending on the height of the side elevation. For a second row of townhouses, where permitted, a minimum side setback of 4 metres is desired.

6.7.3. Support rear setbacks for all housing types which maintain a pattern of landscaped back yards as follows:

- a) For duplexes, townhouses in one row and houseplexes, a rear setback equal to 35% of the lot depth is desired.
- b) For townhouses in more than one row, a rear setback of at least 7.5 metres is desired.

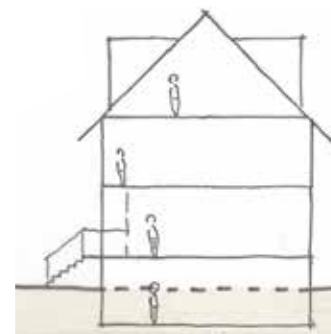


Figure 14. Illustration of 2.5 storey building above a basement. See policy 6.6.1 b).

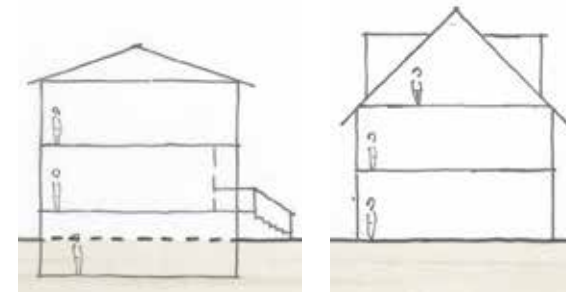


Figure 15. Different ways to achieve a 2.5 storey building (See policy 6.6.1 c-d).

6.7.4. For townhouses oriented to the side or flanking street (longer frontage) of a corner lot, (see Fig. 19) special considerations apply:

- a) The setback to the rear of units should provide for a usable rear yard and separation from adjacent buildings and back yards (6 metres is desired).
- b) The flanking street setbacks should provide for landscape while allowing units to be built closely to the street, thereby preserving backyard space. Front setbacks of 1.5 metres on Alston Street and 2.5 metres elsewhere may be considered where sufficient green space is provided in the boulevard to support at least medium-sized canopy street trees.

## 6.8. Heritage Considerations:

### Intent:

To achieve new housing supply that supports heritage conservation.

While this plan anticipates a number of potential infill housing types for various parts of Vic West, maintaining existing character is also a key objective for the community. Where a house of heritage merit exists and redevelopment is considered, retention of the existing building as part of the overall development is the preferred scenario.

The following policies apply:

6.8.1. Additions to protected Heritage Designated buildings may be considered and should be consistent with the National Standards and Guidelines for the Conservation of Historic Places in Canada.

6.8.2. Where a building is listed on the Heritage Register, retention and re-use of the existing building and its integration into any redevelopment is strongly encouraged.

6.8.3. Where a building is thought to have heritage merit, assessment for potential heritage value may be requested as part of rezoning proposals. Retention and re-use of buildings of heritage merit is encouraged.

6.8.4. Consider incentives to encourage Heritage Designation of eligible properties in the form of bonus density provisions or zoning variances.

6.8.5. See Section 10.3, Adaptive Reuse, for further information.



Figure 16. Illustrative example of heritage house conversion to multiple units.



## Residential Areas

# Guidance for Traditional Residential Sub-Areas, cont'd.

### 6.9. Form and Character Objectives for Traditional Residential Housing

#### Intent:

The following objectives should inform the creation of zoning and design guidelines for infill development of two or more units in Traditional Residential areas:

6.9.1. To achieve street-fronting buildings which present a friendly face to the street *with front entries visible from public spaces, design which creates visual interest for pedestrians, and landscape spaces consistent with the area context*

6.9.2. To support site design, location of infrastructure and drive aisle access which accommodates front yard landscape and boulevard planting of at least medium-sized canopy trees. Boulevards are one of the primary opportunities to maintain and enhance the urban forest in Vic West.

6.9.3. *To provide sensitive transitions to adjacent lower-scale development, considering massing, access to sunlight, appearance of buildings and landscape, and privacy.*

6.9.4. To site buildings in a manner which maintains Vic West's modest front yards, tree-lined streets, and landscaped back yards, with adequate separation between buildings and access to sunlight for living spaces and open spaces.

6.9.5. To encourage design and site planning which responds sensitively to topography.

6.9.6. To encourage design strategies that delineate private front-yard spaces from the public sidewalk while maintaining visibility of housing units.

6.9.7. To support livability and access to *usable outdoor space for individual living units*

6.9.8. To encourage site planning which results in rear yards *whose appearance* is dominated by landscape, not by parking, *and which accommodate tree planting space.*

6.9.9. To mitigate the impacts of surface parking through *design strategies which include* landscaping and screening, planting space, and attractively enclosed parking

6.9.10. Where front-accessed parking cannot be avoided, to nonetheless present a friendly face to the street, create a green landscaped front yard, accommodate boulevard tree planting, and minimize curb cuts.

6.9.11. To encourage the conservation and adaptive reuse of homes of heritage merit.

6.9.12. *To encourage building design which relates to existing context, with attention to streets or areas with a strong pre-existing character.*

6.9.13. *To encourage the use of quality materials.*

6.9.14. Where units are accessed from an internal drive aisle, create comfortable pedestrian access and a legible front door.

6.9.15. To support well-designed duplexes whether side-by-side, front-to-back or up-down, with legible front entries and access to usable open spaces for each unit.

6.9.16. *In Sub-Area 1 adjacent to Catherine at Edward Small Urban Village, to support residential buildings consistent with the desired form and character of the adjacent village commercial uses, creating a unified sense of place.*

6.9.17. *In Sub-Area 1 adjacent to Craigflower Village Building design should be sympathetic in form, materials and design to the existing character to the village. Design of buildings facing Raynor Avenue should establish sensitivity to context*

# Residential Areas

## Housing Types



Figure 17. Example of a house which has been converted to include commercial use.

### 6.10. Limited Commercial Uses

#### Intent:

Support the retention of existing houses adjacent to Craigflower Village while allowing for continued evolution which includes limited commercial uses.

**6.10.1. Along Craigflower Road between** Craigflower Village and Belton Avenue, commercial uses **may be** supported where an **existing** house is retained and adaptively re-used. This may include sensitive additions to the house.

**6.10.2. Accessory** community-serving uses may be supported at Pioneer Housing Co-op, maintaining **and supporting** the primary use of the site for housing (see 6.4.6.).



Figure 18. Example of a small apartment building.

### 6.11. Small Apartment Buildings

#### Intent:

To provide opportunities for additional multi-unit living along a transit corridor in close proximity to urban villages and park amenities.

#### 6.11.1. Locations:

- a) Sub-Areas 1, 2 and 3
- b) Sub-Area 4 where mixed with other Traditional Residential housing types

6.11.2. Density: 1.0 - 1.5 floor space ratio as identified in Fig. 13.

6.11.3. Height: Up to 10.7m (approx. three storeys)

#### 6.11.4. Specific Guidance:

- a) Within the Esquimalt Road Corridor, smaller buildings (which would generally fit on no more than **2 to 3 lots** assembled side-by-side) are encouraged to maintain a rhythm of buildings fronting the street.
- b) Buildings should establish a strong orientation to streets, with front yard landscape, street trees, individual front entries, patios or porches, and modulation of facades, and should respond to topography.

## Residential Areas

# Traditional Residential Housing Types, cont'd.

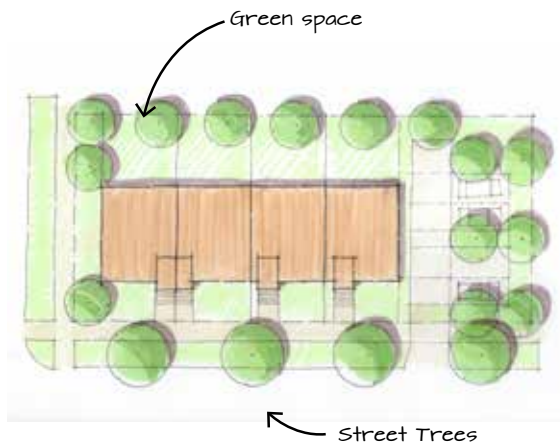


Figure 19. Illustrative example of townhouses in a single row. This example shows a possible arrangement on a corner lot measuring 18m x 40m.



Figure 20. Illustrative example of townhouses on two assembled lots. Shown with front-accessed parking arranged to minimize curb cuts and retain street trees.



Figure 21. Illustrative example of townhouses fronting the street.

### 6.12. Townhouses – Single Row

**Intent:** To provide more options for ground-oriented housing with access to on-site open space, as an alternative to single detached houses. Support townhouses on larger lots where the desired design qualities can be accommodated.

6.12.1. Locations: All sub-areas meeting site requirements.

6.12.2. Site Requirements:

- a) On corner lots, or lots with laneways, on lots of at least 18m (60 ft) in width.
- b) On lots interior to blocks, sufficient width (at least 23 metres [approx. 75 feet]) is required to provide for sufficient individual unit frontages and front yard space

to accommodate front-accessed parking while meeting design and open space objectives (see 6.9.2., 6.9.10.)

6.12.3. Density:

- a) For sub-areas 1-5, a maximum density as specified in Fig. 13
- b) For other sub-areas, up to 0.85 floor space ratio

6.12.4. Specific Guidance

- a) Front-accessed parking for individual units is discouraged where alternatives exist. Where it cannot be avoided, development should provide well-designed frontages and landscaped front yards which present a friendly face to the street, and curb cuts should be minimized to reduce interruptions to

pedestrians, retain planting space and on-street parking.

- b) Townhouses in a single row may contain lock-off suites.



Figure 22. Illustrative example of courtyard townhouses. Organized in two rows with one row fronting the street, with landscaped front and backyards.



Figure 23. Example of street-fronting townhouses.

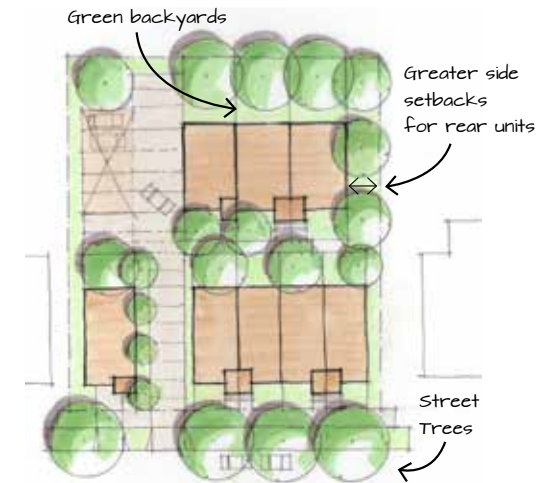


Figure 24. Illustrative example of townhouses on consolidated lots. Separating parking from units can improve their relationship to open space and reduce the amount of the site designed around car movement.

### 6.13. Townhouses – More than one row

#### Intent:

To provide more ground-oriented housing with access to on-site open space, as an alternative to single detached homes. Support more than one row of townhouses on larger lots where the desired design qualities can be accommodated.

6.13.1. Locations: Sub-Areas 1-6 (see Fig. 13)

6.13.2. Site Requirements:

- On lots with a minimum width of 30 metres (100 ft) and a minimum depth of 39.5 metres (130 feet).
- Variations which meet desired form and character objectives and transition sensitively to adjacent development may be supported on lots of varying dimensions.

6.13.3. Density:

- Up to 1.2 floor space ratio in sub-areas 1-2 (see Fig. 13)
- Up to 1.0 floor space ratio in sub-areas 3-5 (see Fig. 13)
- Up to 0.85 FSR in all other sub-areas

6.13.4. Specific Guidance

- Parking should be accessed from a flanking street or laneway where possible, where the two streets have the same road classification.
- Consider clustered parking instead of a parking stall associated with each unit, in order to support site layouts which reduce the impact of automobile circulation and parking on site and building design.

- Setbacks (side and rear) for units that do not front a street (units interior to a lot) should result in landscaped back yards and provide sufficient separation from adjacent buildings and yards.
- Townhouse units fronting a public street may contain lock-off suites.



## Residential Areas

# Traditional Residential Housing Types, cont'd.



Figure 25. Illustrative example of a front-to-back duplex

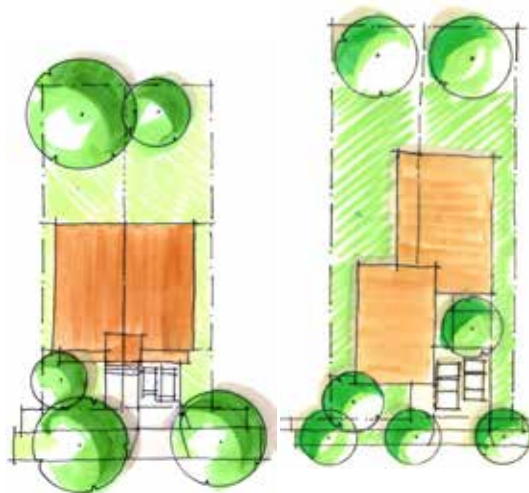


Figure 26. Examples of side-by-side (left) and front/back (right) duplexes with front yard parking, maintaining greenspace in the back yard.



Figure 27: Example of heritage house converted to a duplex

### 6.14. Duplexes

#### Intent:

Support more types of ground-oriented housing that fits into the existing character of the neighbourhood.

6.14.1. Locations: All **sub-areas**

6.14.2. Site requirements:

- a) Current neighbourliness guidelines for duplexes notwithstanding, duplexes without suites are supported on lots of at least 460m<sup>2</sup> (5000 sq ft) in area and 15 metres in width.
- b) Duplexes with suites **may be considered** in the following conditions:

- i. on corner lots or double-frontage lots of at least 460m<sup>2</sup> (5000 sq ft) in area and 15 metres (50 feet) in width

- ii. on interior block lots of at least 15m width and 555 m<sup>2</sup> (6000 sq ft) in area.

6.14.3. Density: Up to approximately 0.65:1 floor space ratio up to a maximum building size contained in zoning

6.14.4. Specific Guidance:

- a) Duplexes may be organized side-by-side (preferred), front-back or up-down.
- b) Development Permit Guidelines for duplexes will be updated to support the intent of this plan.



Figure 28. Illustrative example of houseplex featuring three units.



Figure 29. Example of houseplex featuring several units



Figure 30. Example of houseplex featuring four units

## 6.15. Houseplex - Three or more units

### Intent:

Support more intensive forms of housing that are compatible with the single detached character of the neighbourhood, through the creation of new units in buildings which are similar in appearance to, or architecturally compatible with, detached houses.

#### 6.15.1. Location and Site Requirements:

- On lots of at least 18 metres (60 feet) in width and 670m<sup>2</sup> (7,200 sq ft);
- On corner lots of at least 15m width and 555m<sup>2</sup> in area;

#### 6.15.2. Density:

- Up to 1.0 floor space ratio in sub-areas 1-5 (see Fig. 13)
- Up to approximately 0.75 floor space ratio in other sub-areas

#### 6.15.3. Specific Guidance

- Houseplexes generally limited to 3 units in the Northwest Sub-Area, except on larger lots along Craigflower Road or Styles Street, where houseplexes may contain up to 4 units.
- Houseplexes may contain up to 4 units in other sub-areas.

- Houseplexes with more units than specified in and b) may be considered, particularly in Sub-Area 5, Skinner Street, where additional residential density is envisioned. All houseplexes must meet the urban design objectives in Section 6.9, including for on-site landscaped open space. A parking reduction may be considered where the location and transportation demand management approaches are expected to result in lower rates of parking use.

## Residential Areas

# Traditional Residential Housing Types, cont'd.



Figure 31. Illustrative example of small lot house



Figure 32. Example of small lot houses in Vic West

### 6.16. Small Lot House

#### Intent:

Support small lot homes in parts of Vic West **compatible with** historical patterns and **existing small-lot** zoning, in order to add compatible **infill housing**, expand rental housing options and provide more diverse homeownership opportunities.

6.16.1. Small lot rezoning is discouraged east of Russell Street **on lots of sufficient size or depth to support other infill housing forms**, where a more orderly redevelopment pattern including townhouses and houseplexes is desired.

6.16.2. Small lot houses are supported consistent with the Small Lot House Rezoning Policy, with the following exceptions:

- a) Small lot houses **may be considered on lots smaller than 260m<sup>2</sup> (3,000 sq. ft.) where siting, location and design can meet the intent of the Small Lot Rezoning policy and associated Development Permit guidelines.**
- b) Rezoning for small lot houses is not supported where such rezoning and subsequent subdivision will result in the demolition of a house.
- c) Small lot houses may have a secondary suite.



# 7. Urban Villages

Urban villages provide walkable shops and services, encourage a neighbourhood social life, create a sense of place and provide different housing options. More housing near urban villages gives residents easy access to shops and services for daily living, and supports businesses with more customers nearby.



## Other Relevant Policies & Bylaws

- Official Community Plan
- Development Permit Area Designations and Guidelines

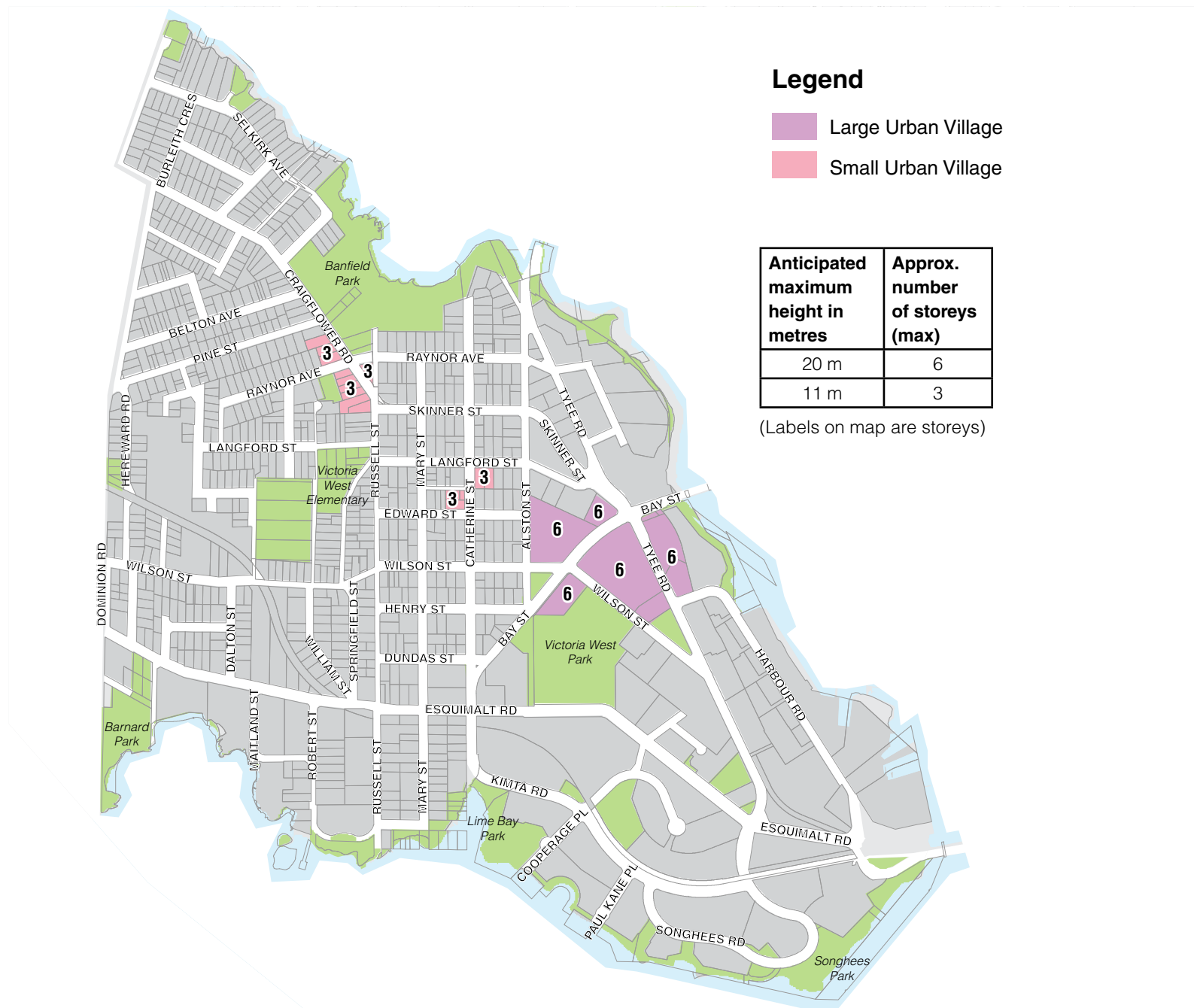
### Goals:

1. Strengthen and diversify the mix of businesses in urban villages
2. Improve the walkability, bikeability and public realm in and around urban villages
3. Support new housing within a five minute walk of urban villages

This plan aims to transform the area in and around Westside Village from a car-oriented commercial area to a vibrant, walkable heart of the neighbourhood. The plan also supports the viability of Craigflower Small Urban Village **with limited opportunities for housing, public space improvements and flexibility for more retail spaces.** It also designates a new Small Urban Village near Catherine Street and Edward Street to reflect existing uses.

The quality of design of buildings and their relationship with the existing urban form, in terms of appropriate density, massing, and height will be a key consideration in assessing development proposals in urban villages - as will good circulation, pedestrian and cycling linkages and a high quality public realm.





**Map 14. Urban Villages**

## Urban Villages

# Westside Large Urban Village



### Intent:

Strengthen Westside Village by supporting businesses with people living nearby, establishing a 'main street' for the Village, and making busy intersections more amenable to pedestrians.

### 7.1. Land Use and Urban Design Policies

7.1.1. Encourage a mix of uses such as residential, local commercial, retail, restaurant, employment, and community services, with residential uses located above commercial uses;

7.1.2. Development up to 20 metres in height (approximately 6 storeys) is supported throughout the Westside Urban Village.

7.1.3. In the areas designated Large Urban Village, densities of up to 1.5 floor space ratio are supported.

7.1.4. Additional density may be considered where public amenities or affordable housing are considered. The preferred contributions are:

- a) density up to approximately 2.5 floor space ratio may be considered where community facilities or public realm improvements are proposed, to be confirmed through the rezoning and Community Association Land Use Committee consultation process.
- b) For projects providing additional affordable housing benefit (e.g. deeper affordability, 3 bedroom units, additional on-site non-market units, etc.), additional density beyond 2.5 floor space ratio may be considered consistent with city-wide policies.

7.1.5. In the Urban Village area bounded by Bay Street, Wilson Street, Alston Street, Langford Street and Skinner Street, the following policies apply:

- a) At the corner of Skinner Street and Bay Street, development should be designed and oriented to create a gateway signalling entry to the Vic West neighbourhood.
- b) At the corner of Wilson Street and Bay Street, development is encouraged to accommodate large format ground-oriented retail in an urban mixed use format;
- c) Building frontages along the east side of Alston Street should be no more than 11 metres (approx. 3 storeys) in height to maintain a sensitive transition to the neighbourhood, taking advantage of the

## Urban Villages

# Westside Large Urban Village, cont'd.

grade changes from east to west. Buildings fronting Alston Street should establish a streetwall with modest setbacks, architectural design and features (such as windows and doors) which activate the public realm, and provide spaces for street tree planting and landscaping.

- d) New buildings should have active, store-front-type uses along Bay Street and Tyee Road in order to enhance animation and vibrancy in the Village.
- e) New development should support an overall vision of creating an internal public pedestrian or local street network (see figure 33).

7.1.6. On the Westside Shopping Centre site, the following policies apply:

- a) Emphasize Wilson Street as the primary street for gathering, with a series of smaller storefronts, active commercial uses at grade, space for patio seating, high quality pedestrian realm and amenities (e.g. lamps, seating, landscape and street trees). Active commercial uses are those that tend to generate foot traffic and pedestrian interest, and include retail stores, food and drink establishments, personal services, theaters and entertainment, and may include some professional services, medical services or personal services.

- b) Develop Tyee Road as a commercial main street with active uses and multiple storefronts at grade along Tyee Road. Consider a range of uses including retail but also food services, personal and medical services, and limited professional services.

- c) Enhance the pedestrian quality of the frontage along Bay Street, regardless of use
- d) Maintain and improve the pedestrian connection through the site from Wilson Street to Tyee Road.

- e) The overall site should maintain a 3-6 storey character with appropriately scaled street walls.

7.1.7. On the Da Vinci Centre site, support new development with commercial or community-serving uses on the ground floor, with the following objectives:

- a) Create an attractive interface with the park, establishing visual connectivity and, as appropriate, entrances or active uses facing the park.
- b) Encourage buildings sensitively designed to minimize shading on active use areas of Vic West Park, and on Wilson Street as a main street.
- c) Establish active uses along Wilson Street that reinforce its role as a commercial main street.

7.1.8. Explore partnership opportunities for a new community facility in or near Westside Village to promote neighbourhood development and meet the social, recreational or other programming needs of residents.

7.1.9. Consider features which support urban food production for residents, including edible landscaping, allotment or rooftop garden area.

## 7.2. Public Realm Policies

7.2.1. Include pedestrian-focused public realm improvements to Wilson Street to create a 'main street' environment for the Village. This may include elements such as large tree trees, additional street crossings representing natural places people desire to cross, wider sidewalks, special paving, on-street parking, benches and pedestrian-scale lighting.

7.2.2. Maintain on-street parking on Wilson Street, and manage it to provide short-term parking for businesses and park users.

7.2.3. Improve the Wilson Street-Bay Street intersection for pedestrians to encourage walkable connections between residential neighbourhood and Westside Village.

7.2.4. Include pedestrian-focused public realm improvements on Bay Street to better connect both sides of the street and improve pedestrian comfort.



# Concept diagram of Westside Village area

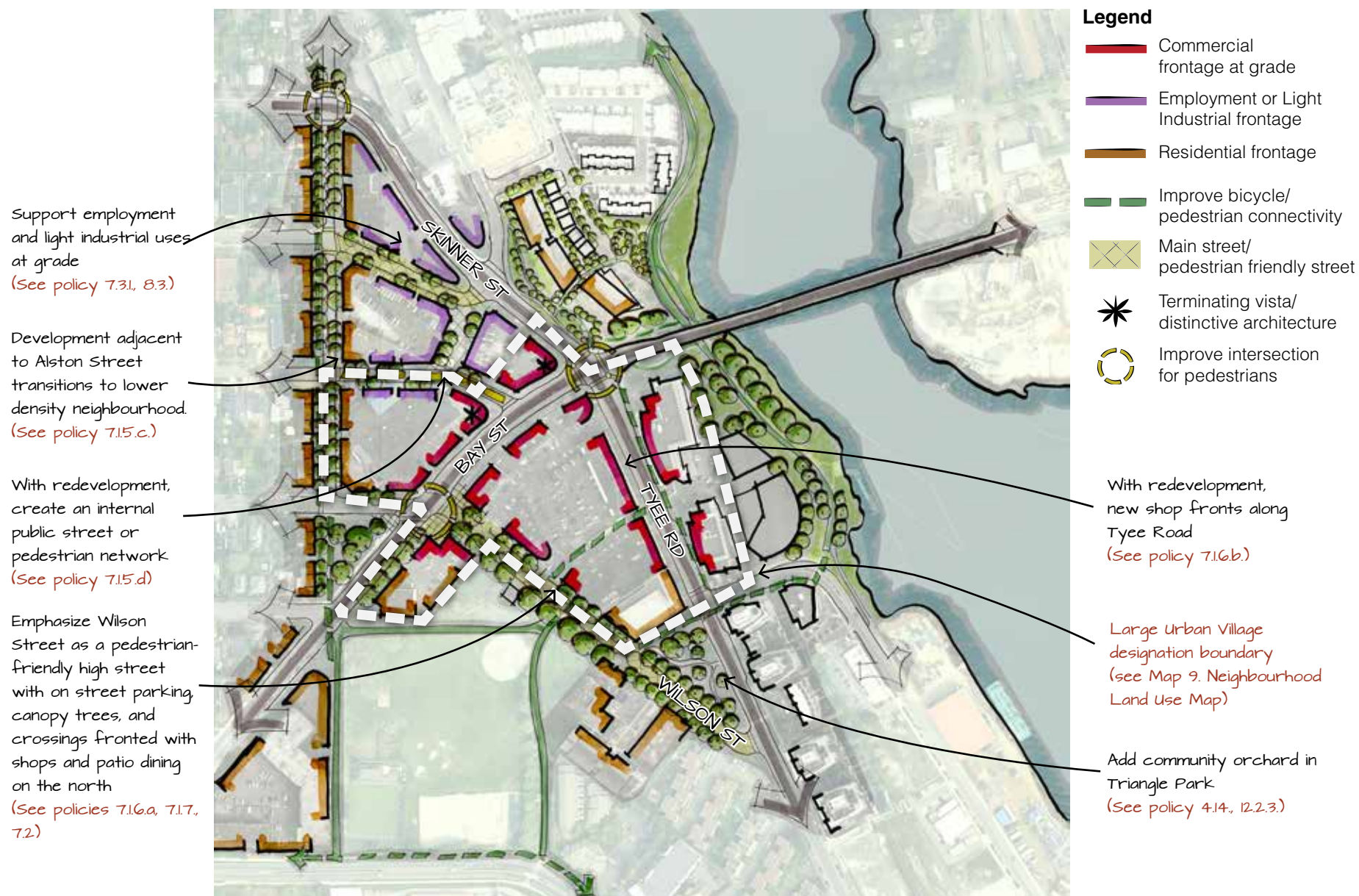


Figure 33. Concept diagram of Westside Village area



## Urban Villages

# Westside Large Urban Village (cont'd.)

### 7.3. Policies for Langford Street Employment Areas

7.3.1. Through redevelopment of sites surrounding Langford Street between Alston Street and Tye Road, support the development of an employment area with the potential for limited residential uses supporting and adding diversity to employment uses. Flexibility in the design of buildings and public realm is encouraged to accommodate a range of uses including light industrial, artisan or “maker” spaces, through the following strategies (see also section 7.2):

- a) At-grade spaces designed to accommodate a variety of uses, including light industrial or artisan employment uses at grade (e.g. with tall floor-to-ceiling heights, and loading bays) are encouraged along Langford Street, with general commercial and/or residential uses above.
- b) Frontages along Langford Street are encouraged to be designed with frequent entries, individual storefront modules, and features such as roll-up doors or loading bays which can accommodate a variety of uses. See Chapter 8, Employment Lands, for further detail.
- c) Where residential uses are included, they should occupy no more than half of the total floor space and are encouraged to be located in upper floors or along Alston Street to transition to the adjacent neighbourhood.

- d) Building frontages along the east side of Alston Street should be no more than 11 metres (approx. 3 storeys) in height to maintain a sensitive transition to the neighbourhood, taking advantage of the grade changes from east to west. Buildings fronting Alston Street should establish a streetwall with modest setbacks, architectural design and features (such as windows and doors) which activate the public realm, and provide spaces for street tree planting and landscaping.
- e) The Langford Street right-of-way between Alston Street and Skinner Street should be designed to allow typically “back-of-house” uses such as loading bay access to mix with customer-serving active uses, while creating a safe and interesting pedestrian environment. Alternative street improvements which separate pedestrians from parked cars with bollards may be considered in lieu of curb-and-gutter.



Figure 34. Example of light industrial frontage which creates an interesting and pedestrian friendly environment (see policy 7.3.1).

## Urban Villages

# Craigflower Small Urban Village

### Intent:

Support and expand Craigflower Village as a local-serving gathering place by adding more business and people nearby, and maintaining the village look and feel. Encourage a mix of affordable and market rate housing as feasible.

### 7.4. Land Use + Urban Design Policies

7.4.1. Support development up to 10.7 metres (approximately 3 storeys) in height and a density of up to 1.5:1 floor space ratio. (Note that some properties have existing zoning which allows for development up to 12 metres in height).

7.4.2. Encourage the retention and adaptive re-use of the existing mixed-use building at 414-424 Craigflower Road.

7.4.3. The following uses should occupy the ground floor fronting Craigflower Road:

- a) Properties at 405 and 414-452 Craigflower Road and 1004-1010 Russell Street should include active commercial uses on the ground floor fronting Craigflower Road (see Fig. 35). Active commercial uses are those that tend to generate foot traffic and pedestrian interest, and include retail stores, food and drink establishments, theaters and entertainment, and may include some professional services, personal services or medical services.
- b) Properties from 400-410 Craigflower Road and 510 Raynor Avenue may have commercial, residential or live-work uses on the ground floor fronting Craigflower Road (see Fig. 35).

- c) If a larger redevelopment (including consolidation of multiple lots) occurs between Raynor Avenue and Russell Street, a mix of commercial and residential ground floor uses is encouraged.

7.4.4. Upper floor uses should be residential to add housing to the village area.

7.4.5. If live-work units are included, the ground-floor “work” portion of the unit should be designed to be able to operate and be leased separately from the residential portion to allow for flexibility over time.

7.4.6. Commercial frontages should be located adjacent to the sidewalk and include frequent entries, small storefront modules, and individual commercial units with small footprints to support a diversity of businesses and pedestrian interest. Individual storefronts and spacing between entries is encouraged to be no more than 8-10 metres. Setbacks are appropriate where they accommodate patios or pedestrian spaces.

7.4.7. Residential frontages on Craigflower Road should also be established adjacent to or near the sidewalk, with patios or porches providing a transitional space from the public realm to front entries for individual units, to create a consistent urban form for the village.

7.4.8. Support business viability in Craigflower Small Urban Village by providing flexibility in on-site parking requirements for ground-floor commercial uses, where a change of use occurs in existing buildings (see also 3.7.2.).



# Concept diagram of Craigflower Village area

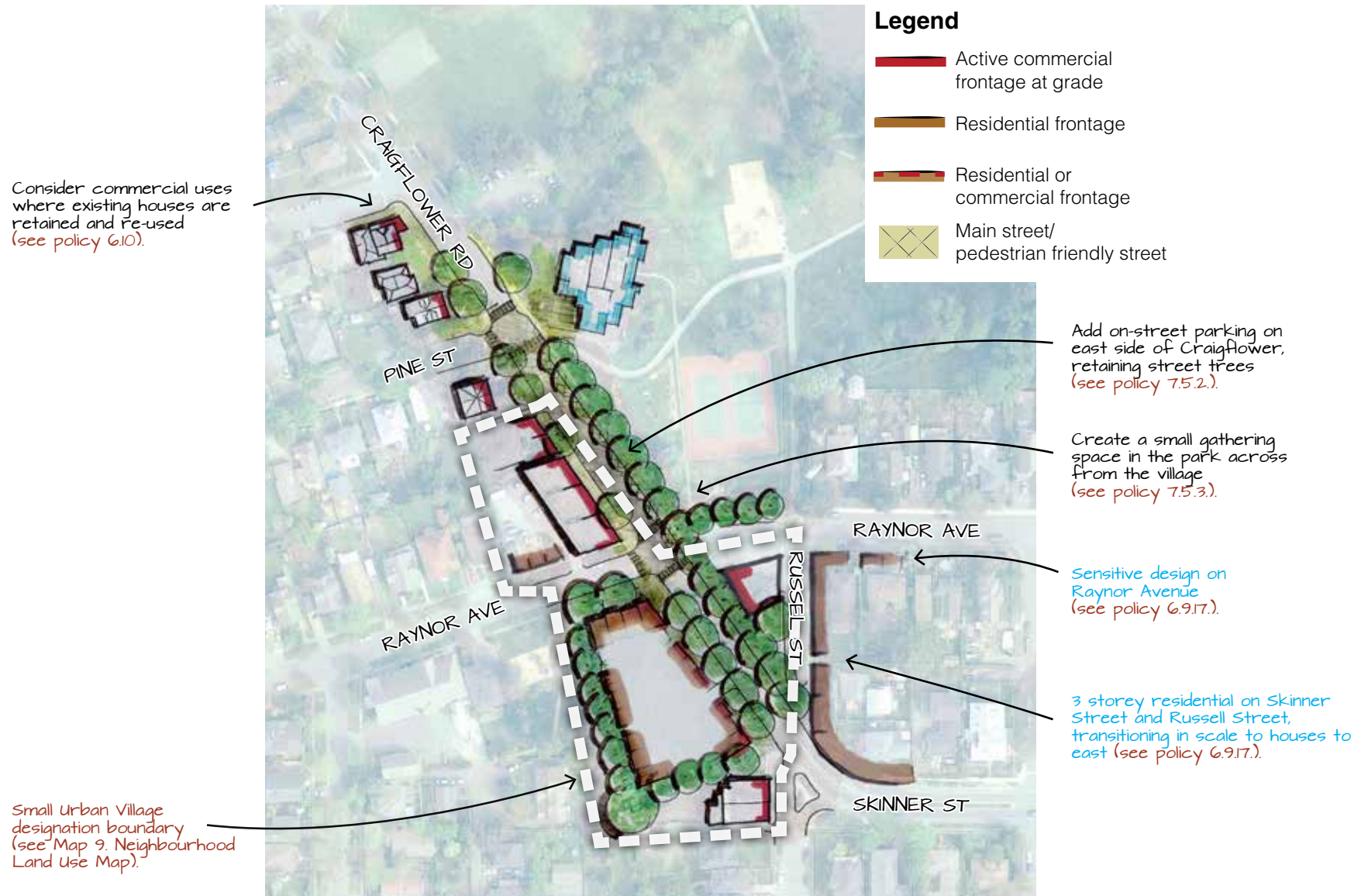


Figure 35. Concept diagram of Craigflower Village area

## Urban Villages

# Craigflower Small Urban Village (cont'd.)

7.4.9. Locate parking at the rear and/ or underground so as to not dominate the overall development. Location of circulation and parking should minimize impacts on the frontages facing Craigflower Road and Raynor Park.

7.4.10. Frontages facing Raynor Park should be residential and should include windows, patios, porches or balconies fronting onto the park, [while integrating landscape spaces and other features which clearly delineate private from public spaces.](#)

7.4.11. [Additional commercial uses are supported fronting Craigflower Road to the northwest of the village, if existing houses are retained and re-used. See policy 6.10 for further details.](#)

7.4.12. Consolidation of the property at 405 Craigflower Road with properties across Russell Street and with the City-owned Russell Street right of way may be considered, if:

- a) [all utilities connecting through Russell Street are maintained, with access for ongoing maintenance;](#)
- b) [the resulting development provides secured non-market housing whose value is equivalent to the value of the right of way; and,](#)
- c) [the resulting development does not exceed the anticipated building heights and densities in this plan.](#)

## 7.5. Public Realm Policies

7.5.1. Include pedestrian-focused public realm improvements through redevelopment

along Craigflower Road to encourage walkability. This may include wider sidewalks, new benches, lighting, landscaping, street trees, wayfinding and other features.

7.5.2. Consider the addition of on-street parking on the east side of Craigflower Road, in order to slow traffic, support businesses, and buffer pedestrians from traffic. Retain trees. Consider relocation of sidewalk to east side of trees on park land.

7.5.3. Establish picnic tables, benches and public art in Banfield Park directly across from the village to create an enhanced public gathering space which visually relates to the village.

7.5.4. Consider improvements to the crossing at Raynor Avenue and Pine Street, including distinctive and textured pavement, and a potential signalized crossing at Raynor if warranted.

7.5.5. [Encourage residents, businesses, and property owners to consider a Local Improvement Tax to accelerate these improvements.](#)



## Urban Villages

# Catherine Street at Edward Street Small Urban Village



### Intent:

Designate a Small Urban Village to retain existing uses and to encourage housing on upper floors in this historic “commercial corners” within the neighbourhood.

### Land Use and Urban Design Policies

7.5.6. Support mixed use buildings up to 3 storeys in height and 1.5 floor space ratio.

7.5.7. Ground floor uses should be commercial and feature storefront-type facades built at or near the edge of the sidewalk. Portions of buildings may be set back further to accommodate patios or pedestrian areas.

7.5.8. Upper floor uses should be residential.

7.5.9. Buildings should transition sensitively to neighbouring lower-scale development. This may be achieved through strategies including setbacks, landscape, varied building massing which responds to adjacent development and considers access to sunlight, and locating balconies and windows to respect privacy.

7.5.10. The design of buildings should activate Bella Street directly adjacent to the village, by including features such as entries, patios, balconies, and windows fronting the laneway.

### 7.6. Public Realm Policies

7.6.1. With new construction, support spaces between building frontages and curbs of sufficient width to accommodate canopy street trees.

7.6.2. An alternative road design that still achieves the required standards of safety and accessibility while enhancing sense of place on Bella Street can be explored with the community.

# 8. Employment Lands

Vic West has an important industrial history related to its location along the harbour and E&N railway. Point Hope shipyard remains an important employer in the region, and supports the key ocean and marine sector identified by the City's Economic Action Plan. Elsewhere in Vic West, pockets of light industry continue to evolve, supporting small businesses in the production, distribution and repair sectors, and providing spaces for artisans and emerging industries such as local breweries and technology companies engaged in manufacturing or prototyping. High demand is expected for light industrial spaces in the core cities of the region in coming years.

The Songhees Area is also an important location for tourism. More recently, office employment is locating in new developments primarily in the Core Songhees area, supporting the growth of the economy in Victoria.



## Other Relevant Policies & Bylaws

- Official Community Plan
- Making Victoria: Unleashing Potential - Economic Action Plan (2015)
- Harbour Road Industrial Waterfront Design Guidelines, 2008

### Goals:

1. Maintain the working harbour and adjacent industrial lands that support the ocean and marine sector
2. Protect and enhance light industry and other employment in Vic West
3. Celebrate the industrial heritage of the neighbourhood

# Employment Lands, cont'd.

## 8.1. Working Harbour

### Intent:

Continue to maintain the Upper Harbour as a working harbor supporting the region's ocean and marine sector.

8.1.1. Support marine industrial uses including primary processing, marine industrial, marine transportation, warehousing, shipping, bulk materials handling, and other industrial uses and accessory offices consistent with the Harbour Road Industrial Waterfront Guidelines and related zoning.

8.1.2. Through placemaking, celebrate the role of maritime industry as part of Vic West's history and identity, and an important component of our economy

- Continue to implement to goals of the Harbour Road Industrial Waterfront Design Guidelines (2008).
- Support views to the water and industrial activities, while prioritizing industrial use of industrially zoned areas.

Encourage the voluntary heritage designation of 420 William St (see policy 8.2.2)

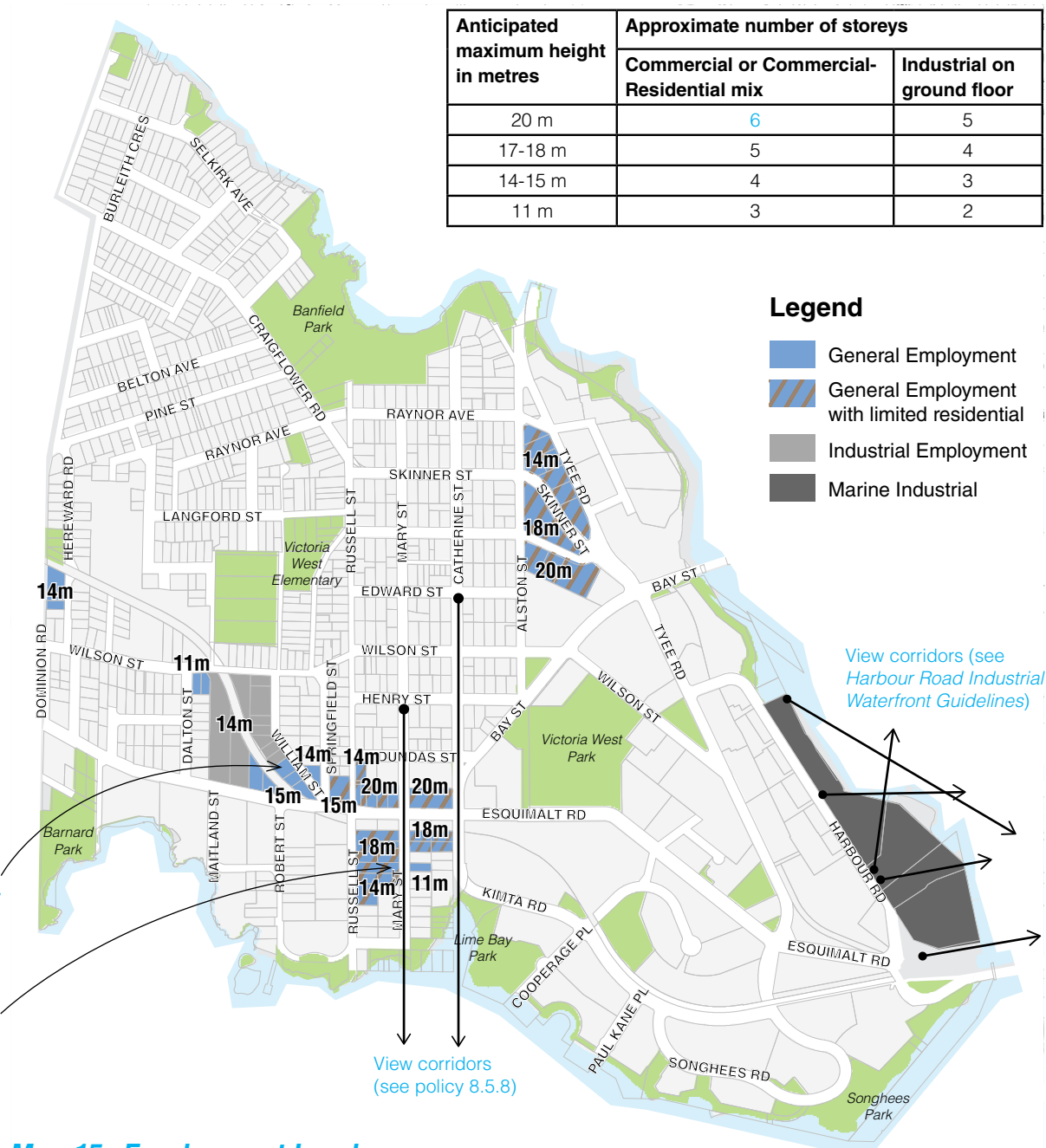
Encourage the voluntary heritage designation of the Ormond's Biscuit Factory Building (see policy 8.4.8)

View corridors (see policy 8.5.8)

Anticipated maximum height in metres	Approximate number of storeys	
	Commercial or Commercial-Residential mix	Industrial on ground floor
20 m	6	5
17-18 m	5	4
14-15 m	4	3
11 m	3	2

### Legend

- General Employment
- General Employment with limited residential
- Industrial Employment
- Marine Industrial



Map 15. Employment Lands

## 8.2. General Employment Areas

### Intent:

Maintain lands for a range of commercial uses appropriate to the neighbourhood context.

8.2.1. General Employment areas are envisioned to include a range of commercial or light industrial development up to four storeys and 2.0 floor space ratio.

8.2.2. Flexibility in use should be supported to encourage the heritage designation and adaptive reuse of the commercial block at 420 William Street.

8.2.3. Rezoning for light industrial uses that generate significant truck access off of primary truck routes should be avoided.

## 8.3. Industrial Employment Areas

### Intent:

Retain spaces for light industrial uses to support the neighbourhood and regional economy as well as new upper-floor commercial/office uses to support a business mix where producers, artisans, and professional offices can co-exist. Encourage design that is sensitive to adjacent residential uses.

8.3.1. In the Industrial Employment areas, consider buildings up to a maximum height identified in Map 15 and up to 2.0 floor space ratio.

- Commercial uses may comprise no more than 1.5 floor space ratio, where the ground floor is occupied by light industrial space (see Fig. 37).
- Ground floors should be designed to accommodate light industrial uses (e.g. with high floor-to-ceiling heights, and loading bay access).
- Alternative layouts mixing light industrial and commercial space may be considered, as long as an equivalent floor area in light industrial use is provided.
- The range of light industrial uses should be limited to those which are compatible to adjacent residential uses, as applicable, and do not generate significant impacts (e.g. dust, noise, odours, glare, or truck traffic outside of daytime work hours) on these adjacent uses.
- Ancillary sales of products produced on site is encouraged in a limited portion of the floor area.



Figure 36. Illustrative diagram of a mixed use development with light industrial uses at grade

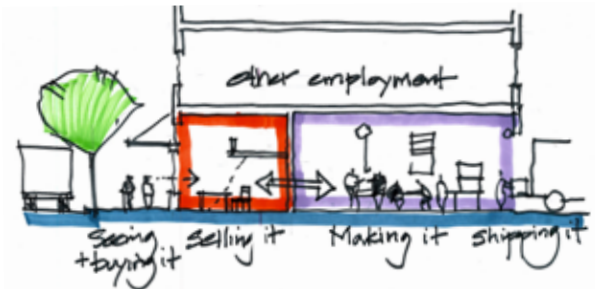


Figure 37. Conceptual examples of industrial employment building with neighbourhood-compatible light industrial uses on the ground floor and commercial uses (e.g. offices, professional services) on the upper floor(s)



## Employment Lands, cont'd.

### 8.4. Employment Residential Areas

#### Intent:

Retain space for employment uses while allowing for the expansion of housing or commerce in upper floors, compatible with the surrounding residential areas in Railyards and south of Esquimalt Road.

8.4.1. Buildings up a maximum height identified in Map 15 may be considered in these areas.

8.4.2. A maximum density of up to approx. 2.5 floor space ratio may be considered for sites between Skinner Street and Westside Large Urban Village, or along Esquimalt Road (including fronting the E&N Rail corridor directly adjacent to Esquimalt Road. Total densities up to approx. 2.0 floor space ratio may be considered elsewhere. These densities may be considered as follows:

- a) Residential floor area should be limited to one half of the total floor area
- b) Any residential density added through rezoning is encouraged to consider the provision of a public amenity, or affordable housing contribution to offset the impacts of density. The preferred contribution is secured non-market housing. Projects which include light industrial or artisan production spaces may not support a public amenity due to economic factors, but provide valuable employment space.

8.4.3. The ground floor should generally consist of employment uses including office or light industrial uses, wholesale, personal or professional services, medical services, community

services, food services. Residential or commercial uses are supported on upper floors. Other combinations of employment and residential spaces that achieve a mix of uses on site are also supportable.

8.4.4. Retail uses which are not ancillary to another primary use are discouraged.

8.4.5. In the Employment-Residential areas fronting Esquimalt Road (including the segment of the E&N Trail adjacent to Esquimalt Road) and Langford Street, flexibility in the design of buildings and public realm should be considered to accommodate a range of uses including light industrial, artisan or “maker” spaces through the following strategies (see also section 7.2):

- a) At-grade spaces designed to accommodate a variety of uses, including light industrial or artisan employment uses (e.g. with tall floor-to-ceiling heights, and loading bays) are encouraged, with general commercial and/or residential uses above.
- b) Where residential uses are included, they are encouraged to be located in upper floors, along Alston Street or adjacent to existing residential uses, to transition to the adjacent residential neighbourhood.

8.4.6. Any light industrial uses should be compatible with residential uses located on site, across the street and in adjacent properties.



8.4.7. Work-live uses **should be** designed so that the “work” portion of the development is located on the ground floor, and the “work” and “live” portions can be occupied or leased separately, allowing businesses to change over time.

8.4.8. Flexibility in use should be supported to encourage the designation and adaptive reuse of the Ormond’s **Biscuit** Factory building (242 Mary Street).

## 8.5. Urban Form, Character and Public Realm Guidance

8.5.1. Buildings facing Langford Street **should feature** pedestrian-friendly frontages with clear glazing and frequent entries, which may **include** light industrial design features. For example, large roll-up garage doors may be used **and spaces provided for loading**. (See Figure 34).

8.5.2. The Langford Street right-of-way between Alston Street and Skinner Street should be designed to allow “back-of-house” uses such as loading bay access to mix with customer-serving active uses, while creating a safe and interesting pedestrian environment. Street improvements which separate pedestrians from parked cars with bollards may be considered in lieu of curb-and-gutter.

8.5.3. Buildings should feature active frontages with storefront-type windows and entries at grade facing Esquimalt Road (**including the segment of the E&N Rail Trail directly adjacent to Esquimalt Road**).

8.5.4. On the south side of Esquimalt Road, **buildings** adjacent to the E&N Rail Trail right of way should support quality pedestrian friendly public realm through design of facades **with**

**glazing, front entries** and outdoor spaces fronting the right of way. **A mix of hardscape and landscape is desired adjacent to the future trail.**

8.5.5. Building frontages along the east side of Alston Street should be no more than 11 metres (approx. 3 storeys) in height to maintain a sensitive transition to the neighbourhood, taking advantage of the grade changes from east to west. **Buildings fronting Alston Street should establish a streetwall with modest setbacks, architectural design and features (such as windows and doors) which activate the public realm, and provide spaces for street tree planting and landscaping.**

8.5.6. A pedestrian connection is desired climbing the slope between Skinner Street and Tyee Road, with redevelopment, to increase permeability of the block to pedestrians.

8.5.7. Redevelopment may include angled parking on Tyee Road.

8.5.8. New development in the Esquimalt Road Corridor should respect the view corridors identified from Catherine Street at Edward Street and from Mary Street at Henry Street, by considering the location, siting and design of new development within the specified view corridor to maintain and frame views of the **Olympic Mountains**.

8.5.9. Buildings fronting onto the E&N Rail Trail north of Esquimalt Road should create “eyes on the trail” through the location of windows, patios, balconies and overlooks, and views through the site where appropriate. Entrances for customers and employees are encouraged where appropriate, and should be

designed to encourage natural surveillance.

- a) Buildings should feature active frontages with storefront-type windows and entries at grade facing Esquimalt Road.

8.5.10. The types of light industrial uses **considered** should be **appropriate to this** location within a residential neighbourhood. These uses should limit off-site impacts (noise, glare, dust and odours) and minimize noise and delivery impacts on residential areas outside of daytime business hours.

8.5.11. Outdoor storage and loading should be properly and attractively screened from residential areas.

8.5.12. Loading areas should be located to minimize truck traffic off of designated truck routes.

8.5.13. Buildings that mix light industrial and residential uses should be sensitively designed to reduce residential overlook of loading, storage or working areas and minimize impacts on the residential uses.

8.5.14. Where parking is located in a structure but not underground, avoid blank walls adjacent to the street

8.5.15. Development adjacent to lower-density residential uses should sensitively transition through massing, design, setbacks and landscape that minimizes shading and overlook and provides for building separation and privacy.

8.5.16. The height of street walls (that portion of a building closest to the street) to frame streets and provide definition and enclosure while maintaining sensitivity to and access to sunlight for any residential development across the street.



# 9. Core Songhees and Master Planned Areas

## Master Planned Areas

Part of Vic West's unique character is shaped by the recent master planned developments that ring the Inner and Outer Harbour. This area was the location of a Lekwungen village and later the Songhees Nation Reserve. After the 1911 relocation of the Songhees Reserve and sale of these lands, these lands were developed for harbour-related industry. With the decline of industry, planning began in the 1970s with the vision of re-purposing industrial lands along the waterfront.

Dockside Green, [Bayview Place \(including Roundhouse\)](#), Railyards and the Songhees Area feature medium and high-density multi-unit housing, public parks and open spaces, commercial areas, and pedestrian networks. The master planned areas help create diversity, vibrancy and density in the city's core while supporting Vic West retail, transportation choice and overall urban sustainability. They will accommodate a significant portion of new housing in Vic West and the city as a whole. Continued evolution is expected as they are built out over the life of this plan. Development in most of these areas is guided by existing Master Development Agreements, Development Permit Areas, and comprehensive development zones.

### Roundhouse

The Roundhouse Historic Railway Precinct is intended to create a mixed-use community centred on Turntable Plaza, a public open space north of the E&N Rail which will be surrounded by conserved heritage railroad buildings that feature

retail, restaurant, arts and commercial uses. Roundhouse is expected to be a destination for the surrounding community and visitors. The precinct envisions the following features:

- Enhanced connectivity through completion of the E&N Rail Trail through the site, pedestrian crossings, and connections to Vic West Park
- Lime Bay Mews, an important walking route and view corridor, framed with small retail and work spaces and connecting the waterfront and plaza
- Housing in townhouses and taller multi-unit buildings that step away from the waterfront and surround a network of semi-private courtyards and open spaces
- 20% of housing units containing lock-off suites to enhance the rental housing supply
- Enhancements to the adjacent Lime Bay Park, Sitkum Park and the Garry oak – camas meadow natural area southeast of Esquimalt Road and Kimta Road
- A community space or equivalent cash amenity contribution to the City of Victoria to provide improvements elsewhere in the neighbourhood
- Stormwater management features
- If passenger rail resumes on the E&N Rail, a station in this area may be considered; however, the primary station location for Vic West is at the west end of the Johnson Street Bridge



### Bayview Place

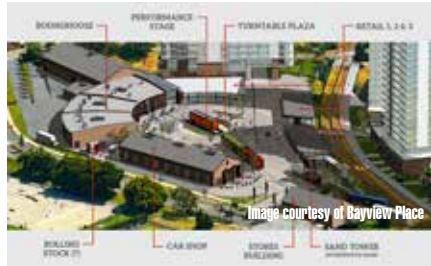
[Bayview Place](#) is a 20-acre neighbourhood development encompassing the Roundhouse and the Bayview Hillside Lands. Bayview includes high-density multi-unit residential buildings and a seniors' housing development within a network of open spaces and pedestrian pathways.

The Hillside Lands provide living space near the downtown and support activity and commerce in nearby areas such as Roundhouse. This development features:

- A network of pedestrian paths
- Seniors' housing
- Improvements to Songhees Hillside Park and the natural area along Esquimalt Road



## ROUNDHOUSE



## BAYVIEW PLACE



## RAILYARDS



## DOCKSIDE GREEN



**Map 16. Master Planned Areas - existing + future development**

## Railyards

Railyards creates a community of diverse housing opportunities in townhouses and multi-unit apartment buildings around a network of streets, open spaces, trails and natural stormwater features on the former CNR Rail Yard site, adjacent to the Galloping Goose Regional Trail. A small commercial component is included on Regatta Landing. The development plan features:

- Buildings reflecting a contemporary-industrial aesthetic
- A network of parks and open spaces including Bridges Park, with a children's play lot; and Regatta Point Park, both accessible from the Galloping Goose
- A bicycle and pedestrian connection from the Galloping Goose Regional Trail to the Bay Street – Tyee Road/Skinner Street intersection, where a small entry plaza will be developed
- Rain gardens for stormwater mitigation
- Water access at Central Spur pier and the Railyard pier and boardwalk
- Restored shoreline habitat

## Dockside Green

Dockside Green creates a sustainable, mixed-use community located between the working harbour, Tyee Road and downtown Victoria. It includes a mix of working and residential environments, retail services and restaurants, and buildings of varying heights organized around a network of green spaces, plazas, and pathways. When completed, the development will feature:

- Housing that steps down from Tyee Road to Harbour Road, where a mix of low-rise commercial or light industrial buildings transition to the working harbour

- A central, accessible public greenway along a linear water feature which limits stormwater runoff
- Additional pedestrian connections from Tyee Road to Harbour Road
- A number of publicly accessible parks and open spaces: centrally-located Playroom, a children's playground with natural features; Dockside Commons, with a large lawn area with formal and informal seating; and Dockside Landing, the primary plaza fronting onto Harbour Road
- Two areas for retail, restaurants and services: one at Dockside Landing, and one near the smaller Waterside Plaza where the Galloping Goose meets Harbour Road
- Sustainability features including on-site rainwater management, wastewater treatment facility, district energy plant, green building and LEED for Neighbourhood Design certification
- Provision of non-market rental and homeownership housing units
- Several public views preserved through the site
- A community space in the form of a sustainability centre, or an equivalent cash amenity contribution to the City to be used for improvements that benefit the Dockside lands

## Intent:

Encourage a diversity of medium and high density multi-unit housing in master planned sites at Dockside Green, [Bayview Place \(including Roundhouse\)](#), and other opportunity sites in the Core Songhees area. Continue to support the redevelopment of Master Planned Areas consistent with the existing Master Development Agreements, including all amenities and public improvements.

### 9.1. Master Planned Areas Policies

9.1.1. Support the continued implementation of the following:

- a) Railyards Master Development Agreement and Railyards Development Guidelines (2002, as amended)
- b) Dockside Master Development Agreement, Design Guidelines for the Dockside Area (2005, as amended), and Dockside Green Urban Design Guidelines (2016, as amended)
- c) Roundhouse Master Development Agreement and Roundhouse Design Guidelines (July 2008, as amended)
- d) Songhees Policy Plan and Design Guidelines (2008, as amended)

9.1.2. If any changes are proposed to existing MDAs, encourage consistency with the various policies in this plan, the City's Official Community Plan, and other policies including the [Victoria Housing Strategy](#). [Should rezoning and amendments to MDAs, which increase density, be proposed, consideration should be given for public amenities and/or affordable housing in support of community goals, to offset the impacts of density.](#)

# Special Planning Areas

Special Planning Areas are larger sites which have the potential to redevelop during the time frame of the plan. This section provides policy direction if significant redevelopment is proposed for Special Planning Areas shown on Map 17.

## 9.2. Special Planning Area: Tyee Cooperative

9.2.1. Any redevelopment of the Tyee Cooperative should maintain the existing number units and floor space of non-market housing.

9.2.2. Redevelopment should include a mix of housing units, including housing attractive to families with children.

9.2.3. Common facilities (e.g. open space, play lot, community garden) that serve a range of residents' needs. Rooftop open space may be considered.

9.2.4. Underground or structured parking is encouraged.

9.2.5. Redevelopment may include a mix of housing types such as townhouses, stacked townhouses, and apartment buildings, up to a density of 2.5 floor space ratio, with building heights generally compatible with the surrounding development and public open spaces.

9.2.6. The site may accommodate services and limited commercial uses (e.g. child care, medical services, educational services, personal services, coffee shop, co-working, incubator or affordable business spaces, and social enterprise) as an ancillary use to the primary residential use. These uses may be focused on Wilson Street and the Wilson-Tyee intersection to reinforce Wilson Street as a desired "main street" for the area.

9.2.7. A public pedestrian and bicycle connection should be established through the site, from Sitkum Road to Tyee Street or Wilson Street. This connection should be designed to provide visual connection to and casual surveillance from residences and commercial spaces on the site.

9.2.8. Parking reductions may be considered for affordable housing and the implementation of Transportation Demand strategies.

9.2.9. Opportunities for urban food production are encouraged (e.g. allotment garden, rooftop garden, edible landscape).



**Map 17. Special Planning Areas**



# 10. Heritage



1140 Arthur Currie Lane  
(Heritage designated)



Ormond's Biscuit Factory, 242 Mary Street



E&N Roundhouse, 253 Esquimalt Road (Heritage designated)

## Other Relevant Policies & Bylaws

- Heritage Tax Incentive Program (TIP)
- Victoria Register of Heritage Properties



420 William Street



740 Mary Street  
(Heritage designated)



225 Dundas Street

### Goals:

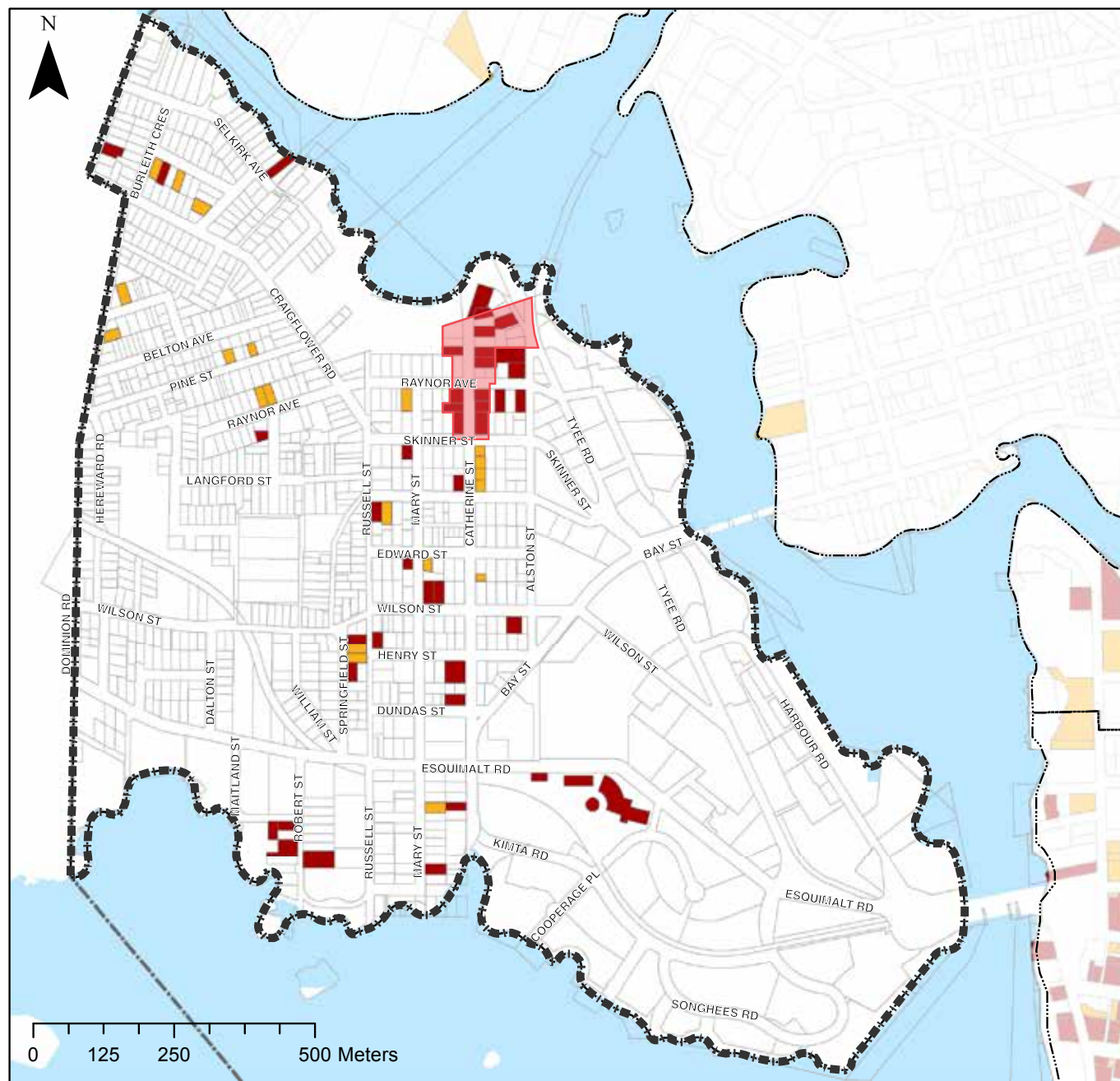
1. Protect the historic character of significant buildings and important sites
2. Celebrate and interpret the heritage of the neighbourhood

Vic West's lands, waterfront, buildings, streets and other special places shape the neighbourhood's identity and sense of place. Different places in the neighbourhood tell stories of Vic West's past, such as important Lekwungen fishing, hunting and

food gathering sites, ceremonial places and travel routes. The Songhees village was [relocated to the Vic West shoreline in 1853](#). [There](#), aboriginal people from across the region lived, traded and gathered until its residents were moved away again in 1911. Other places [in Vic West](#) tell of settler farms that supported Fort Victoria, shoreline that served as the industrial heart of Victoria, and thriving residential and industrial areas that flourished because of the streetcar, railway and walking connections to downtown. Elegant estates near the waterfront grew up alongside

Vic West's industrial and worker housing areas, and historic houses and industrial properties still dot neighbourhood streets today. Beginning in the 1970s, investments in [neighbourhood](#) parks, waterfront walkways and housing renewal set the stage for the gradual redevelopment of many of Vic West's former industrial places. Today, medium- and high-density housing are located alongside historic buildings and places, and contribute to the neighbourhood's evolving heritage and sense of place.





## Heritage Properties

Victoria West Boundary

### Heritage

- Designated
- Registered
- Existing Heritage Conservation Area 1 - Catherine Street

**Map 18. Heritage Areas**

## 10.1. Heritage Designated and Registered Properties

### Intent:

Protect the historic character of significant buildings and important sites.

10.1.1. Encourage landowners to consider the protection of heritage resources through the designation of properties listed on the City's Register of Heritage properties, identified on Map 18, or other buildings of heritage merit, including through the rezoning process.

10.1.2. Consider future additions of properties to the City's Register of Heritage Properties in consultation with property owners.

## 10.2. Heritage Conservation Areas

### Intent:

Conserve the historic character and integrity of special neighbourhood places.

10.2.1. Consider future citizen-initiated proposals for designation of Heritage Conservation Areas **where heritage merit exists**.

## 10.3. Adaptive Re-use of Buildings of Heritage Merit

### Intent:

Support heritage conservation **through** innovative site planning and design that supports the retention and adaptive re-use of heritage-eligible buildings while allowing the addition of housing or employment spaces.

For additional policies specific to Traditional Residential Areas, see also Section 6.8.

10.3.1. Support single detached homes with a suite and garden suite, or **the conversion of an existing house to contain two suites**, where the house is subject to heritage designation

10.3.2. Where redevelopment is proposed, consider flexibility in **site planning, building form and** massing that support the retention and adaptive reuse of buildings of heritage merit, and avoid demolition for new development.

10.3.3. With redevelopment of heritage properties, consider the relaxation of regulatory requirements (e.g. reduced parking requirements; variances to setbacks or suite coverage) while encouraging development that supports the overall objectives of this plan.

**10.3.4. Through the retention and adaptive re-use of existing houses:**

- a) Consider the addition of habitable area (e.g. through lifting the home or adding an appropriate addition) during the conversion of a character house into more than one unit.
- b) **Consider additional density (floor area or number of units) in a house conversion which is subject to heritage designation, as an incentive to encourage voluntary designation.**

10.3.5. Encourage the use of incentives for the rehabilitation or adaptive reuse of commercial or mixed use heritage properties.



Figure 38. Illustrative example of heritage property with a suite and a garden suite.



Figure 39. Example of adaptive re-use of heritage buildings on Amelia St, Victoria.



Figure 40. Example of added density around heritage buildings, Vancouver BC



# 11. Infrastructure and Green Development



## Goals:

1. Ensure sufficient infrastructure capacity to meet the future needs of residents and businesses
2. Use stormwater management to restore ecological processes.
3. Become a leader for sustainable buildings and infrastructure
4. Identify and address neighbourhood climate change impacts

Vic West is a neighbourhood with a strong history of activities, organizations, and development projects embracing sustainability and climate change mitigation and adaptation, with a strong local economy, some of the City's first passive house-certified homes, and internationally-renowned sustainable development projects. It is, and should continue to be, a leading neighbourhood in sustainable development in the city, region, and province.

This plan takes an integrated approach to sustainability, with sustainable development directions woven throughout the plan. This section identifies specific strategies to further support green buildings and infrastructure in Vic West.

## Other Relevant Policies & Bylaws

- Victoria Sustainability Framework
- City Climate Leadership Strategy and 100% Renewable Energy by 2050 Commitment
- Sustainability Checklist for New Construction (to be updated in 2018/2019)
- City-wide education and incentive programs
- Water System Master Plan
- Stormwater Master Plan
- Sanitary Sewer Master Plan (to be completed 2018)



# Infrastructure and Green Development, cont'd.

## 11.1. Infrastructure Upgrades

### Intent:

Ensure sufficient infrastructure capacity to meet the future needs of residents and businesses.

11.1.1. Consider the capacity of utility networks, including water distribution, sanitary sewer and storm drainage, in reviewing development applications and other land use changes.

11.1.2. Continue upgrading the underground infrastructure in the Victoria West neighbourhood as directed by City-wide master plans for water distribution, sanitary sewer and storm drainage upgrades.

## 11.2. Stormwater Management on Public Lands

### Intent:

Use infrastructure to mimic and restore ecological processes

11.2.1. Continue to monitor stormwater outfalls emptying in the Gorge Waterway.

11.2.2. Identify opportunities to incorporate green stormwater infrastructure or “green streets” as part of utility, active transportation and other street improvements. Potential locations include priority pedestrian and cycling routes and visible locations such as around urban villages.

11.2.3. Identify opportunities for stormwater management as part of public development projects or improvement on City-owned lands.

## 11.3. Stormwater Management on Private Lands

### Intent:

Promote stormwater management practices on private property.

11.3.1. Work with property owners and institutions to identify options for mitigating stormwater impacts on sites with high impervious cover, and thereby reducing the stormwater utility costs for these properties.

11.3.2. Encourage new developments to foster rainwater infiltration through the use of absorbent landscaping, swales, rain gardens, previous paving, green roofs, infiltration trenches, and other appropriate methods.

11.3.3. Encourage property owners to seek stormwater rebates through the City's rainwater rewards program.

11.3.4. With the redevelopment of large sites (e.g. special planning areas), encourage innovative on-site stormwater management integrated into the design.

## 11.4. Green Buildings

### Intent:

Encourage, promote, and facilitate the development of sustainable buildings and low carbon housing stock.

A number of City-wide initiatives are addressing this goal:

11.4.1. Through City-wide implementation of the BC Energy Step Code, require all new residential and commercial buildings to meet energy efficiency standards.

11.4.2. Through the city-wide Market Rental Revitalization Study, develop policies to encourage energy efficiency and support revitalization in existing rental apartment buildings in Vic West, while maintaining affordability and tenant stability.

11.4.3. Through the development process, support innovative sustainability features and practices in new multi-unit residential, commercial, and mixed use developments.

11.4.4. Through implementation of the City-wide Climate Leadership Plan, develop procedures to require new developments to identify all sustainability features and practices, such as energy efficiency, stormwater management and sustainable building materials.

11.4.5. Recognizing that Vic West has one of the highest proportions of houses heated by oil, eliminate the use of heating oil systems by 2030 through support programs such as BC Hydro's Home Renovation Program rebates, and the provincial Oil to Heat Pump program.

11.4.6. Encourage utility companies and the provincial government to adopt programs to incentivize solar and other renewable energy systems, through implementation of City-wide Climate Leadership Plan.

11.4.7. Review and streamline City processes to encourage broader adoption of solar and other renewable energy systems.

11.4.8. On public lands, use vegetation to shade impervious areas and buildings to reduce heat island effect.

## 11.5. Adapting to Climate Change

### Intent:

Identify and address neighbourhood climate change impacts

11.5.1. Identify plants and ecosystems vulnerable to climate change and development management strategies to mitigate impacts, through implementation of the City's Parks and Open Space Master Plan.

11.5.2. Adopt guidance for any development adjacent to the water, consistent with Provincial Guidance on planning for sea level rise (see also 4.1.6.).

11.5.3. Identify city infrastructure and facilities susceptible to impacts from sea level rise and develop strategies to adapt.



# 12. Neighbourhood Food Systems



## Other Relevant Policies & Bylaws

- Small-Scale Commercial Urban Food Production Regulations
- Community Gardens Policy
- Boulevard Gardening Guidelines

### Goals:

1. Create more places to get and grow food close-to-home
2. Integrate food production into public places

Vic West has a rich history of food production, beginning with the waters and lands where Lekwungen people fished, cultivated and gathered food for generations. After settlement, farms in Vic West supplied food for Fort Victoria. Over the last two decades, Vic West has emerged as a leader in creating strong neighbourhood food systems, with community allotment and commons gardens, a community orchard, boulevard gardens, home gardens, and a network of people involved in mentoring and building skills in urban food production and a variety of other community-led initiatives. These examples showcase how food can be incorporated into public and private land, and used as an important tool for community building. From the beginning of engagement on the neighbourhood plan, the neighbourhood food system was identified as an important value for Vic West. While many urban food

production policies are set at the City-wide level, the neighbourhood plan plays a role in identifying specific opportunities for Vic West.

## 12.1. Access to Neighbourhood Food

### Intent:

Create more places to get and grow food close-to-home

12.1.1. Support community-led efforts to establish a year-round farmers market in Victoria West.

12.1.2. When creating park improvement plans, consider opportunities to incorporate features that encourage the public gathering around food, such as picnic tables, barbecues, community ovens and barrier-free access to food gardens in public places.

## 12.2. Community Gardens and Food Production in Public Places

### Intent:

Integrate food production and community gardening into public places

12.2.1. Explore community-led opportunities for food gardens, pollinator gardens or native plant gardens on City lands at:

- a) Victoria West Park; and,
- b) at other suitable other locations in parks and public greens identified in partnership with the community. Suggestions from community consultation included Alston Green, Dundas Green and Mary Street Park.



## Neighbourhood Food Systems, cont'd.

12.2.2. Encourage and support community gardens and food projects on suitable public and private lands, in partnership with community organizations, such as the following initiatives supported through City-wide programs and policies:

- a) boulevard gardens
- b) community commons and allotment gardens
- c) community orchards
- d) stewardship of urban food trees
- e) keeping of bees and hens
- f) commercial urban food production.

12.2.3. Encourage applications for the establishment and/or enhancement of community orchards in locations of interest to the community, including:

- a) Hereward Park
- b) Triangle Park
- c) Banfield Park
- d) Other locations identified in partnership with the community.

### 12.3. Food Production on Private Lands

#### Intent:

Integrate food production into new multi-unit development where appropriate.

12.3.1. Encourage new multi-unit developments to incorporate food production and other food features, such as:

- a) rooftop or ground-level allotment gardens for private resident use in new multi-unit buildings.
- b) edible landscaping, such as trees, shrubs and other plants.
- c) potable water on balconies and outdoor spaces.
- d) rooftop greenhouses.
- e) beekeeping.

12.3.2. Encourage new allotment gardens at Tyee Co-op and Pioneer Co-op where redevelopment occurs which adds density to these sites.

# 13. Neighbourhood Well-Being



Photo: Jack Meredith

## Other Relevant Policies & Bylaws

- Great Neighbourhood Program
- City grant programs

### Goals:

1. Refresh indoor community facilities to support residents of all ages in a growing community
2. Support the creation of more childcare options to support families with children living in Vic West
3. Foster a safe and inclusive community for all residents

Vic West has a legacy of community-serving facilities and organizations, including the City's original hospitals, social halls, sports and community organizations. Today, Vic West contains several community-serving institutions including Victoria West Elementary School, Victoria West Community Centre, Da Vinci Centre and several places of worship. These facilities, along with other non-profit and private spaces, provide services to the neighbourhood and broader community, and are complemented by

facilities in other Victoria neighbourhoods, Esquimalt, and Saanich. This plan encourages programming and partnerships to help meet identified community needs such as recreation, inclusion and childcare. This plan also encourages the evolution of community facilities over time to meet the changing nature of Vic West.

# Neighbourhood Well-being, cont'd.

## 13.1. Community Facilities

### Intent:

Refresh indoor community facilities to support residents of all ages in a growing community

Support community facilities as part of existing or future Master Development Agreements

13.1.1. Continue to assist [Victoria West Community Association](#) to sustain and enhance community programming, services and facilities that meet the evolving needs of Vic West's growing community.

13.1.2. As part of the long-term development of Westside Large Urban Village, explore opportunities to establish a community facility to encourage gathering and reinforce the heart of the neighbourhood.

13.1.3. Work with School District to make school facilities more broadly available for community programming, sports, indoor and outdoor gathering.

13.1.4. Develop community space as described in the Roundhouse Master Development Agreement, or accept cash-in-lieu to be used for the creation of community facilities elsewhere in the neighbourhood.

13.1.5. Develop the Sustainability Centre, or an alternative public amenity that benefits the Dockside lands, as described in the Dockside Master Development Agreement, through planning that involves the community, the Neighbourhood Association, the City, and the developer.

## 13.2. Childcare

### Intent:

Support the creation of more childcare options to support families with children living in Vic West

13.2.1. Continue to support the [Victoria West Community Association](#) in the provision and potential expansion of child and youth care spaces at Vic West Community Centre and Victoria West Elementary School.

13.2.2. Support daycare and eldercare as a use throughout the neighbourhood in accordance with zoning, [suitable to the built scale of their immediate surroundings](#).

## 13.3. Neighbourhood Inclusion and Safety

### Intent:

Foster a safe and inclusive community for all residents

13.3.1. Support improvements to public spaces, such as Banfield Park and the Galloping Goose Trail, to improve the sense of safety at all times of day.

13.3.2. Continue to support a range of non-market and market housing options to support diverse and inclusive community.

13.3.3. Seek opportunities to promote social interaction between different generations through the design of public spaces, parks and public facilities.



# 14. Arts, Culture and Placemaking



## Other Relevant Policies & Bylaws

- Create Victoria Arts and Culture Master Plan (2017)

### Goals:

1. Encourage public art and placemaking that celebrate Vic West's identity
2. Support creative entrepreneurs

Vic West is a neighbourhood which has attracted a diverse population of artists. In addition to providing a range of opportunities for living, working, and selling works, a vibrant community weaves arts and culture into everyday life and helps create a strong sense of place. Opportunities for creative placemaking include temporary and permanent public art opportunities in parks and other public spaces. Public art can celebrate Victoria West's unique identity and reinforce the neighbourhood's human and natural heritage.



## Arts, Culture and Placemaking, cont'd.

### 14.1. Neighbourhood Public Art and Placemaking

#### Intent:

Encourage art and placemaking initiatives that celebrate Vic West's identity

#### Policies:

- 14.1.1. Introduce public art into small urban village areas as part of streetscape improvements.
- 14.1.2. Incorporate public art into the design of neighbourhood trails, connectors and parks spaces. Identify locations through active transportation and park improvement planning.
- 14.1.3. Partner with arts organizations to encourage art installations in neighbourhood public spaces, such as temporary pop-ups and artist-in-residence initiatives.
- 14.1.4. Engage Songhees and Esquimalt First Nations to determine opportunities for public art in public spaces.
- 14.1.5. **Suggested locations** for public art may include:
  - a) Craigflower Village and Banfield Park interface
  - b) Victoria West Park and Alston Green
  - c) E&N Rail Trail right of way adjacent to Esquimalt Road
  - d) E&N Rail Trail, other segments
  - e) Public spaces at Bayview Place (including Roundhouse) and Dockside Green

- f) Pedestrian connections within Westside Village (e.g. the stairwell between Tyee Road and the upper parking lot)

14.1.6. Through consultation, the community suggested various themes for public art. Suggestions for the community included: First Nations history and places; the ecology and natural history of the area including the Gorge waterway; the working / industrial history of the neighbourhood; the diversity of people calling Vic West home; urban sustainability; and the artistic creativity of the neighbourhood

14.1.7. Support community-led initiatives in partnership with the City and private development, to establish gateway signs at entrances to Vic West.

14.1.8. [Work with the Township of Esquimalt on appropriate gateway designs for the intersection of Esquimalt Road and Dominion Road \(see also 6.2.11.\).](#)

### 14.2. Support Creative Entrepreneurs

#### Intent:

Create/strengthen opportunities to showcase and feature neighbourhood artists and creators.

#### Policies:

- 14.2.1. Through the Arts and Culture Master Plan, engage the arts community and non-profit groups in identifying opportunities, incentives and partnerships to create an incubator space including affordable gallery, studio and creative incubation spaces.
- 14.2.2. Through the Arts and Culture Master Plan, explore ways to link potential creative entrepreneurs, home-based studios and self-employed individuals to available resources for business assistance, skills sharing and access to spaces to make and sell goods.

# 15. Neighbourhood Amenities and Improvements

This chapter highlights the neighbourhood's priorities for public improvements and amenities. This list captures, at a high level, key neighbourhood improvements identified through consultation. Many projects will require a combination of funding sources to be realized. This list should serve as a guide for future consideration of amenity contributions from development, prioritization of public improvements, seeking funding opportunities and building partnerships.

## 15.1. Priorities for Neighbourhood Improvement

Neighbourhood priorities are listed in no particular order:

**15.1.1. On-site non-market housing:** Affordable housing integrated within the neighbourhood was identified as desirable to support a diverse, sustainable and equitable neighbourhood into the future.

**15.1.2. Waterfront access improvements** (e.g. swim docks, small boat launches, and/or viewpoints), including at Banfield Park, are one of the key recreational opportunities desired in Vic West.

**15.1.3. Active transportation routes:** While Vic West is served by two major regional trails, consultation identified the need to travel between these trails and neighbourhood destinations more comfortably. Improvements to other active transportation routes in the neighbourhood, as identified in this plan, will help achieve this objective.

**15.1.4. Food production:** Opportunities for urban food production are important amenities to Vic West residents, and may include community and allotment gardens and community orchards.

**15.1.5. Placemaking and public space improvements in urban villages** is desired to encourage gathering and to strengthen urban villages as places not only for accessing shopping and services, but also for enhancing livability and building community.

**15.1.6. Additional community facility space** is desired to support expanded activities, programming and services for area residents. The Westside Village area was identified as an preferred place for additional facility space.

**15.1.7. "Complete street" improvements** will enhance mobility and connections within the neighbourhood and tie different parts of the neighbourhood together. Bay Street, Skinner Street, Tyee Road, and Esquimalt Road are seen as priorities with opportunities for streetscape improvements through future redevelopment, or other investment.

## Other Relevant Policies & Bylaws

- Density Bonus Policy
- Parks and Open Spaces Master Plan
- Bicycle Master Plan
- Pedestrian Master Plan
- Community Gardens Policy
- Great Neighbourhood Program
- City grant programs



# 16. Action Plan

The action plan provides a high-level list of actions identified in this draft plan. Implementation of this action plan must be balanced with available resources and other City priorities. The improvements may be accomplished through a combination of funding sources, including City capital programming, amenity contributions from development, senior government grants, and partnerships with other public, non-profit or private entities. Any future actions are to be considered through the City's budget process which occurs annually.

## 16.1. Plan Monitoring

The action plan is to be used as a working document and should be reviewed periodically (every 3-5 years) with the community as part of monitoring and adaptive management of the plan, in order to consider changing circumstances, desires and progress made.

## 16.2. Neighbourhood-based initiatives

Where City resources are limited, stakeholders are encouraged to seek other means of furthering priorities in this plan, including local improvement districts, partnerships, grant funding and other sources of funding to advance the objectives of this plan.

Topic	Short-term Actions (2018-2020)	Year	Lead	Funded?
<b>Transportation and Mobility</b>	Assess the Catherine Street at Bay Street lane designations for left and right turns (north side of intersection)	2018	Engineering and Public Works	√
	Share community feedback on pathways across Vic West School field with School District 61 (see Policy 3.3.9.)	2018	Sustainable Planning and Community Development	√
	Assess Langford Street at Russell Street for visibility and stop sign (see Policy 3.6.2.)	2018	Engineering and Public Works	√
	Complete pedestrian and bicycle connections from Vic West to downtown on the Johnson Street Bridge	2018	Engineering and Public Works	√
	Complete Point Ellice (Bay Street) Bridge rehabilitation	2018	Engineering and Public Works	√
	Replace the temporary barrier at Reno Street and Belton Avenue with a permanent barrier (see Policy 3.3.13.)	2018	Engineering and Public Works	√
	Add pylons or other physical separation for cycling along Skinner Street between Alston Street and Langford Street (see Policy 3.4.3.)	2019	Engineering and Public Works	√

Figure 41. Short-term Actions



Topic	Short-term Actions (2018-2020)	Year	Lead	Funded?
Transportation and Mobility (cont.)	<p>Assess transportation conditions at the following locations and update Neighbourhood Plan and Action Plan with suggested improvements as warranted:</p> <ul style="list-style-type: none"> <li>• Skinner Street, for improvements for pedestrian and cyclist comfort and safety, including visibility, crossings, vehicle speed and intersection improvements, from <a href="#">Russell Street</a> to <a href="#">Langford Street</a> (see Policy 3.4.3.).</li> <li>• <a href="#">Evaluate the crossing of Craigflower Road at Raynor Ave. to determine if improvement is needed</a> (see Policy 3.4.4.)</li> <li>• Mary Street, for speeding and cut-through traffic, and improvements for pedestrian safety, including intersection visibility and crossings at <a href="#">Wilson Street</a> (see Policy 3.6.3.).</li> <li>• <a href="#">Selkirk Street</a>, for vehicle speed and cut-through traffic from <a href="#">Banfield Park</a> to the <a href="#">Esquimalt border</a> (see Policy 3.6.4.).</li> <li>• <a href="#">Wilson Street</a> from <a href="#">Dominion Road/Viewfield Road</a> to <a href="#">Rothwell Street</a>, for pedestrian and cyclist safety, including visibility, crossings, vehicle speed and intersection improvement (see Policy 3.6.5.).</li> <li>• <a href="#">Rothwell Street</a> for vehicle speeds, commercial traffic volume and road designation and crossing at <a href="#">Esquimalt Road</a> (see Policy 3.6.6.).</li> <li>• <a href="#">Hereward Road</a> for vehicle speed and cut-through traffic (see Policy 3.6.6.).</li> <li>• <a href="#">Dalton Street</a> south of <a href="#">Wilson Street</a> for vehicle speed and cut-through traffic (see Policy 3.6.7.).</li> <li>• <a href="#">Bay Street</a> between <a href="#">Tyee Road</a> and <a href="#">Esquimalt Road</a>, for vehicle speed, visibility, crossings and other improvements for pedestrian safety to improve connections between residential neighbourhood and <a href="#">Vic West Park</a> and <a href="#">Westside Village</a> (see Policy 3.6.8.).</li> <li>• <a href="#">Assess E&amp;N Trail crossing at Wilson Street for safety and visibility</a> (see Policy 3.3.12.).</li> <li>• <a href="#">Evaluate opportunities for short-term sidewalk improvements on Langford Street between Alston Street and Tyee Road, and potential improvements to Alston Road, through update to pavement management plan</a> (see Policy 3.3.10.).</li> <li>• <a href="#">Monitor and review speed limit along Esquimalt Road following completion of new Johnson Street Bridge</a> (see Policy 3.3.20.).</li> <li>• <a href="#">Assess Craigflower Road, Tyee Road, Bay Street, Wilson Street and Esquimalt Road for consistency of speed limits along corridor and across municipal boundaries</a> (see Policy 3.6.9.).</li> </ul>	2019 (through 2018 budget planning)	Engineering and Public Works	
	Complete All Ages and Abilities connection on <a href="#">Harbour Road</a> (see Policy 3.2.1.).	2019	Engineering and Public Works	
	Complete pilot All Ages and Abilities route on <a href="#">Kimta Street</a> , including crossing at <a href="#">Esquimalt Road</a> and assessment of lighting on the pathway behind the <a href="#">Delta Hotel</a> (see Policy 3.3.16.).	2019	Engineering and Public Works	
	Complete the bicycle and pedestrian connection from the <a href="#">Galloping Goose</a> to the <a href="#">Bay-Tyee intersection</a> through the <a href="#">Railyards</a> (see Policy 4.13.2.).	Subject to development	Private Development	

Figure 41. Short-term Actions, cont.

Topic	Short-term Actions (2018-2020)	Year	Lead	Funded?
<b>Parks, Open Space and Waterfront</b>	<ul style="list-style-type: none"> <li>Implement the Vic West Park Improvement Plan (see Policy 4.15.1.)</li> <li>Assess vehicle drop-off/ parking conditions on Esquimalt Road for skate park (see Policy 4.15.2.).</li> </ul>	2017-2021	Parks, Recreation and Facilities; Engineering and Public Works	
	<ul style="list-style-type: none"> <li>Share community feedback on Galloping Goose with CRD, including improved lighting between Banfield Park and Harbour Road, Trans-Canada Trail pavilion intersection, visibility and speed of motorized vehicles; identify next steps (see Policy 4.11.1.).</li> </ul>	2018	CRD; Sustainable Planning and Community Development	
	Complete a habitat assessment to determine the feasibility of new or expanded public swim docks at Arm Street, Burleith and Banfield Parks (see Policy 4.6.1, 4.6.2, 4.8.1).	2018-2019	Parks, Recreation and Facilities	
<b>Housing, Urban Villages and Employment</b>	Update the Official Community Plan to reflect the land use directions in this plan.	2018 – concurrent with plan adoption	Sustainable Planning and Community Development	√
	Update Development Permit Area boundaries and guidelines for Urban Villages, Urban Residential and Employment Corridors and Townhouses to reflect area-specific policies in this plan.	2018 – concurrent with plan adoption	Sustainable Planning and Community Development	√
	Create zoning options for ground-oriented infill development including townhouses, duplexes, triplexes, fourplexes, and small apartments as described in this plan.	2019-2020	Sustainable Planning and Community Development	√
	Update the Neighbourliness Guidelines for Duplexes to reflect policies in this plan.	2019-2020	Sustainable Planning and Community Development	√
	Revise Small Lot House Rezoning policy to support secondary suites in small lot homes in Vic West and to consider smaller lot size as identified in this plan.	2019-2020	Sustainable Planning and Community Development	√
	Update zoning to reflect Industrial Employment areas policies in this plan.	2019-2020	Sustainable Planning and Community Development	√

Figure 41. Short-term Actions, cont.

Topic	Short-term Actions (2018-2020)	Year	Lead	Funded?
	Amend zoning to allow for a secondary suite and garden suite on heritage designated properties.	2019-2020	Sustainable Planning and Community Development	√
	Update the house conversion regulations, or develop new regulations, to support the retention and adaptive re-use of smaller heritage properties through the addition of habitable space and to create incentives for voluntary designation.	2019-2020	Sustainable Planning and Community Development	√
<b>Neighbourhood Food Systems</b>	Create voluntary guidelines for urban food production in mixed-use and multi-unit developments	2018-2020	Parks, Recreation and Facilities	√

Figure 41. Short-term Actions, cont.

Topic	Medium-term Actions (2021-2027)	Lead	Funded?
<b>Transportation and Mobility</b>	Complete the E&N Trail segment through the Roundhouse development (see Policy 3.2.3.).	Private Development	
	Establish a formal connection to the E&N Trail at Sherk Street (see Policy 3.3.11.).	Engineering and Public Works	
<b>Parks, Open Space and Waterfront</b>	Expand Songhees Park through improvements to the Johnson Street Bridge lands consistent with the Council-approved concept design (2017) (see Policy 4.20.1.).	Parks, Recreation and Facilities	
	Complete public spaces, including Bridges Park and the entry plaza, in the Railyards Master Development Agreement (see Policy 4.13.1.).	Developer	
	Complete public spaces, including Sitkum Park and Turntable Plaza, in the Roundhouse Master Development Agreement. (Timing dependent on development) (see Policy 4.9.2.).	Private Development	
	Complete a Park Improvement Plan for Banfield Park (see Policy 4.8.1.).	Parks, Recreation and Facilities	
<b>Infrastructure and Green Development</b>	Adopt guidance for any development adjacent to the water, consistent with Provincial Guidance on planning for sea level rise (see Policy 11.5.2.).	Sustainable Planning Community Development	
	Identify city infrastructure and facilities susceptible to impacts from sea level rise and develop strategies to adapt (see Policy 11.5.3.).	Sustainable Planning Community Development; Victoria Emergency Management Agency, Engineering Public Works	
<b>Art, Culture and Placemaking</b>	Concurrent with the Banfield Park improvement plan, develop a design for Craigflower Village streetscape, pedestrian environment, and Banfield park street frontage (see Policy 7.5.1.-7.5.4.).	Parks, Recreation and Facilities; Sustainable Planning & Comm. Development; Engineering and Public Works	

Figure 42. Medium-term Actions

Topic	Long-term Actions (2028+)	Lead	Funded?
<b>Transportation and Mobility</b>	Improve cycling and pedestrian facilities for Point Ellice Bridge as part of future bridge improvements (see Policy 3.4.1.).	Engineering and Public Works	
	Develop the Alston Street corridor as a north-south pedestrian and bicycle linkage between the Galloping Goose and the Westsong Walkway (see Policy 3.3.1., 3.3.10., 3.4.3.).	Private Development, Engineering and Public Work	
	Complete improvements to pedestrian and bicycle safety in Banfield Park (see Policy 3.3.3.).	Parks, Recreation and Facilities	
	Complete any outstanding segments of E&N Rail corridor (see Policy 3.2.3.).	Private Development; Engineering and Public Works	
	Complete All Ages and Abilities connections along William Street and areas adjacent to the Vic West Elementary School field (see Policy 3.3.2.).	Engineering and Public Works	
	Complete the All Ages and Abilities route along Selkirk Road from Banfield Park to Tillicum Road (see Policy 3.1.1.).	Engineering and Public Works	
	Work with CRD and BC Hydro to improve trail access to E&N Trail through Hereward Park (see Policy 3.3.6.).	CRD, Parks, Recreation and Facilities	
	Consider trail re-alignment through Triangle Park to reflect pedestrian travel patterns (see Policy 4.14.1.).	Parks, Recreation and Facilities	
<b>Parks, Open Space and Waterfront</b>	Complete public spaces, including Dockside Commons, the Greenway and the park adjacent to the Galloping Goose south of the Point Ellice Bridge in the Dockside Master Development Agreement (see Policy 4.10.1.).	Private Development	
	Complete a planning process for Arm Street Park and Burleith Park, which also includes further consideration a long-term goal of creating an environmentally sensitive waterfront pathway on all or part of the waterfront west of Banfield Park, considering land use policies and the potential for voluntary easements (see Policy 4.1.5., 4.1.6, 4.6.1., 4.6.2.).	Parks, Recreation and Facilities	
	Add wayfinding signage and other improvements along Westsong Walkway (see Policy 4.2.4., 4.17.1., 4.19.1., 4.20.1.).	Parks, Recreation and Facilities	
<b>Heritage</b>	Complete a reconnaissance survey of the neighbourhood to identify buildings of heritage merit (see Policy 10.1.2.).	Sustainable Planning & Community Development	

Figure 43. Long-term Actions



Topic	Ongoing Actions	Lead	Funded?
<b>Heritage</b>	Continue to work with owners to voluntarily designate buildings of heritage merit (see Policy 10.1.1.).	Sustainable Planning & Community Development	
	Consider citizen-initiated Heritage Conservation Areas, if proposed (see Policy 10.2.1.).	Sustainable Planning and Community Development	
<b>Arts, Culture and Place-making</b>	Implement the Arts and Culture Master Plan initiatives specific to Vic West (see Policy 14.2.).	Arts, Culture and Events	
	Support community-led placemaking initiatives (see Policy 14.1.).		
<b>Infrastructure and Green Development</b>	Continue underground infrastructure upgrades consistent with City master plans (see Policy 11.2.1.).	Engineering and Public Works	
	Identify opportunities for stormwater management on public lands and streets as part of road resurfacing, active transportation projects and other opportunities, as resources allow (see Policy 11.2.2., 11.2.3.).	Engineering and Public Works	
<b>Transportation and Mobility</b>	Periodically review parking needs upon request and explore new parking management strategies as required (see Policy 3.7.3.).	Engineering and Public Works	
	Continue to complete other neighbourhood-identified active transportation improvements as funding allow or through redevelopment where appropriate (see Policy 3.1.1.).	Engineering and Public Works	
<b>Parks, Open Spaces and Waterways</b>	Support community-led efforts, in partnership with the City, to create new public docks, wharves and other public access for neighbourhood swimming and/or watercraft at locations identified in this plan (see Policy 4.2.4.).	Parks, Recreation and Facilities	
	Collaborate with the Greater Victoria School District on park/open space improvements adjacent to school properties (see Policy 3.3.9.).	Parks, Recreation and Facilities	
	Continue to work with community, CRD, industry and provincial and federal partners to improve and restore water and environmental quality in the Gorge Waterway (see Policy 4.1.1.).	Parks, Recreation and Facilities	
	Support community efforts to establish a continuous marine trail linking View Royal, Esquimalt and Saanich (see Policy 4.2.3.).	Parks, Recreation and Facilities	
<b>Neighbourhood Food Systems</b>	Identify suitable locations for community orchards, community gardens and other food features on public and private lands, in response to community interest and partnership (see Policy 12.2.2.).	Parks, Recreation and Facilities	

Figure 44. Ongoing Actions

# 17. Glossary of Terms

**Affordable Housing:** Housing that falls within the financial means of a household living in either market or non-market dwellings. Total costs for rent or mortgage plus taxes (including a 10% down payment), insurance and utilities should equal 30 percent or less of a household's gross annual income. Housing affordability is influenced by household income, and cost and supply of housing.

**Apartment:** A dwelling located in a multi-story, multi-unit building that accesses the ground via shared corridors, entrances and exits.

**Attached Dwelling:** A building used or designed as three or more self-contained dwelling units, each having direct access to the outside at grade level, where no dwelling unit is wholly or partly above another dwelling unit.

**Attached Housing:** Any form of housing where more than two individual dwellings are structurally attached including duplexes, townhouses, row-houses, and apartments regardless of tenure.

**Complete Streets:** Transportation facilities that are designed and operated to enable safe access for all users – pedestrians, cyclists, public transit passengers and vehicles, commercial vehicles and automobiles. Users of all ages and abilities must be able to safely move along and across a Complete Street.

**Cohousing:** is an intentional community of private homes clustered around shared space. Each attached or single family home has traditional amenities, including a private kitchen. Shared spaces typically feature a common house, which may include a large kitchen and dining area, laundry, and recreational spaces.

**Cooperative Housing:** Housing owned by a non-profit cooperative association, where residents

have a share of ownership and often pay a monthly housing charge.

**Density:** The number of dwelling units on a site expressed in dwelling units per acre or units per hectare or Floor Space Ratio (FSR)

**Development Permit:** A document that includes approved site and building development plans illustrating land use, landscaping, built form, intensity of use, and appearance of the site and buildings, as well as conditions of development approval.

**Development Permit Area:** Under the Local Government Act (LGA), local governments may designate areas of land known as a development permit areas (DPA) for one or more purposes.

**District Energy System:** The provision of heating, cooling or electricity through the production of steam, hot water or chilled water at a central plant that is then distributed through piping to individual buildings in a larger neighbourhood or community.

**Duplex:** A two family dwelling.

**Dwelling Unit:** Any room or suite of rooms, intended for use by one household exclusively as a place of residence.

**Employment Uses:** A mix and range of office, industrial, warehousing, transportation and logistics, construction, communication, technology, commercial and financial services, retail and wholesale, institutional, research, education, public service and similar or related land uses and activities.

**Fee Simple:** Private ownership of property with no strata-title ownership or obligations.

**Floor Space Ratio:** The ratio of the total floor area of a building to the area of the lot on which it is situated.

**Frequent Transit:** Transit service that provides medium to high density land use corridors with a convenient, reliable, and frequent (15 minutes or better) transit service all day long. The goal of the Frequent Transit network is to allow people to spontaneously travel without having to consult a transit schedule and is characterized by transit priority, right-of-way improvements, a high level of transit stop amenities, and corridor branding.

**General Employment:** consists of primarily employment generating uses and accessory mixed-uses including light industrial, commercial, and institutional uses contained within a wide range of low to medium-rise building forms, including those with large floor plates on lots with space suitable for vehicle circulation, shipping and delivery.

**Ground-Oriented Dwelling:** A residential unit that has individual and direct access to the ground, whether detached or attached, including single-detached dwellings, duplexes and townhouses, as well as the principal unit and secondary suite in single-detached dwelling.

**Heritage Character:** Overall effect produced by traits or features which give heritage property or an area a distinctive appearance or quality.

**Heritage Conservation:** Includes, in relation to heritage, any activity undertaken to protect, preserve or enhance the heritage value or heritage character (including but not limited to character-defining elements) of heritage property or an area.

**Heritage Designation:** Bylaw to protect a heritage property that is formally recognized for its heritage value from exterior alterations, removal or demolition without the approval of City Council.

**Heritage Merit:** A building having heritage merit is one which is identified as having heritage value related to its architectural, historical, or cultural characteristics, but is not listed on the Heritage Register as “Heritage-Registered” or “Heritage-Designated.”

**Heritage Property:** A structure, building, group of buildings, district, landscape, archaeological site or other place in Canada that has been formally recognized for its heritage value.

**Heritage Register:** A list of property that is formally recognized by the local government to have heritage value or heritage character.

**Heritage Value:** The historic, cultural, aesthetic, scientific or educational worth or usefulness of (heritage) property or an area. The heritage value of a heritage resource is embodied in its heritage character.

**Houseplex:** A development of three or more units whose form and massing is compatible with traditional residential neighbourhoods. It often appears similar to a large detached house. Can be in stacked units or townhouse-style configurations.

**Intensification:** The development of a property, site or area at a higher density than currently exists through: a) redevelopment, b) the development of vacant and/or underutilized lots within previously developed areas; c) infill development; and, d) the expansion or conversion of existing buildings.

**Large Urban Village:** consists of low to mid-rise mixed-use buildings that accommodate ground-level commercial, offices, community services, visitor accommodation, and multi-unit residential apartments, with a public realm characterized by wide sidewalks, regularly spaced street tree planting and buildings set close to the street frontage, anchored by a full service grocery store or equivalent combination of food retail uses, serving either as a local, rapid or frequent transit service hub.

**Low-Rise Building:** A building four storeys or less in height.

**Mixed Use:** Different uses in relatively close proximity either in the same building (apartments above a store) or on the same or site or, when referring to an area or district, on an adjacent site (light industry adjacent to an office building).

**Multi-unit:** A building containing three or more dwelling units, also referred to as multi-family or a multiple dwelling.

**Non-market rental housing:** Housing that is rented at a price that is not set by market forces but set and controlled over time by some other means. Non-market rental housing is generally delivered by non-profit or public agencies. Social housing is a subset of non-market housing.

**Official Community Plan:** An Official Community Plan (OCP) can be developed by both municipalities and regional districts. The OCP provides the longer term vision for the community. Under the Local Government Act section 875, an OCP is a statement of objectives and policies to guide decisions on planning and land use management, within the area covered by the plan, respecting the purposes of local government.

**Open Space:** Land that provides outdoor space for unstructured or structured leisure activities, recreation, ecological habitat, cultural events or aesthetic enjoyment that is generally publicly-accessible, and that is not a designated City of Victoria park. Open space includes private lands, public lands and City-held property.

**Park:** Land managed by the City of Victoria that provides outdoor space for unstructured or structured leisure activities, recreation, ecological habitat, cultural events, or aesthetic enjoyment, not including planted areas within street rights of way.

**Placemaking:** A holistic and community-based approach to the development and revitalization of cities and neighbourhoods, that creates unique places of lasting value that are compact, mixed-use,

and pedestrian and transit-oriented with a strong civic character.

**Passive house:** A rigorous, voluntary standard for energy efficiency in a building, reducing its ecological footprint. It results in ultra-low energy buildings that require little energy for space heating or cooling.

**Priority Frequent Transit:** Transit service that provides medium to high density land use corridors with a convenient, reliable, and frequent (15 minutes or better) transit service all day long. Priority Frequent Transit differs from Frequent Transit, as it includes a semi-exclusive right of way on portions of the route for transit vehicles.

**Public art:** Works of art in any media that has been planned and executed with the specific intention of being sited or staged in the physical public domain, usually outside and accessible to all.

**Rapid Transit:** Transit service designed to move high volumes of passengers between major regional destinations along key transportation corridors. Services are very frequent (15 minutes or better) and stop less often than traditional transit services. To improve travel time and reliability Rapid Transit utilizes an exclusive or semi-exclusive right of way to eliminate or significantly reduce the impact of general traffic on transit vehicles. Rapid services use high capacity transit vehicle technologies such as light rail and bus rapid transit vehicles.

**Sense of Place:** The subjective experience of a place as having physical and social attributes that make it distinctive and memorable.

**Street Wall:** A generally continuous edge of building facades that collectively define streets and sidewalks, and include portions of building facades above the ground.

**Small Lot House:** A house whose lot size and design meets the requirements of the City’s Small Lot Rezoning Policy.

**Small Urban Village:** consists of a mix of commercial and community services primarily serving the surrounding residential area, in low-rise, ground-oriented multi-unit residential and mixed-use buildings.

**Streetscape:** All the elements that make up the physical environment of a street and define its character. This includes paving, trees, lighting, building type, style, setbacks, pedestrian amenities, street furniture, etc.

**Townhouse:** A variety of 3 or more residential buildings where individual houses lining a street share adjacent walls in common under a strata title, with each dwelling unit having a separate entrance and yard area. Stacked townhouses are stacked on top of each other, each with its own front door and private outdoor space.

**Traditional Residential:** consists primarily of residential and accessory uses in a wide range of primarily ground-oriented building forms including single, duplexes, townhouses and row-houses, house conversions, and low-rise multi-unit residential and mixed-use buildings.

**Transit-oriented development:** (TOD) is a mixed-use residential and commercial area designed to maximize access to public transport, and often incorporates features to encourage transit ridership while dissuading the ownership of automobiles.

**Triplex:** Three units sharing a dividing partition or common wall.

**Two Family Dwelling:** A building consisting of two self-contained dwelling units which share a common wall or an area that forms the floor of one unit and the ceiling of the other and are not linked by a trellis, deck, breezeway or similar connection.

**Urban Design:** Urban design is concerned with the human-made environment. It is a discipline that is dedicated to the relationships among the fields of urban planning, architecture and landscape architecture. The concerns of urban design range from a broad level, such as the layout of entire cities,

to particular aspects of designed environments such as architectural detailing, landscaping and street furniture.

**Urban Residential:** consists primarily of multi-unit residential in a wide range of detached and attached building forms, including townhouses and row-houses, low and mid-rise apartments, with a residential character public realm featuring landscaping and street tree planting, and mixed-uses located along arterial and secondary arterial roads.



A BYLAW OF THE CITY OF VICTORIA

The purpose of this Bylaw is to amend the Official Community Plan to make changes to Urban Place Designations and Development Permit Areas in order to implement the future land use, urban form and character directions identified in the Victoria West Neighbourhood Plan.

Under its statutory powers, including section 472 and 488 of the *Local Government Act*, the Council of the Corporation of the City of Victoria, in a public meeting, enacts as follows:

**Title**

- 1 This Bylaw may be cited as “OFFICIAL COMMUNITY PLAN BYLAW, 2012, AMENDMENT BYLAW (NO. 25)”.

**Definitions**

- 2 “OCP Bylaw” means the City’s Bylaw No. 12-013, the Official Community Plan Bylaw, 2012.

**Official Community Plan Bylaw**

- 3 The OCP Bylaw is amended in Schedule A, Table of Contents, under the heading “List of Map” by:
  - a) Replacing the text, “Map 31: Victoria West Strategic Directions” with the text “Map 31: Victoria West”.
  - b) Inserting the following text immediately after the text “Map 32: Composite Map of Development Permit Areas and Heritage Conservation Areas”:

“Map 32A: Composite Map of Attached Residential Development Permit Areas”;
  - c) Inserting the following text immediately after the text “Map 48B: DPA 6A: Small Urban Villages – Burnside at Harriet”:

“Map 48C: Small Urban Villages: Craigflower Village and Catherine at Edward Village”;
  - d) Inserting the following text immediately after the text “Map 56: DPA 7A: Corridors – Shelbourne Avenue”:

“Map 56A: Corridors – Esquimalt Road”
  - e) Inserting the following text immediately after the text “Map 72: DPA 15C: Intensive Residential – Rockland”:

“Map 72A: DPA 15F: Intensive Residential – Attached Residential Development”;
  - f) Renumbering the page number references accordingly.
- 4 The OCP Bylaw is amended in Schedule A, Section 6: Land Management and Development, as follows:
  - a) By repealing Map 2: Urban Place Designations and replacing it with a new Map 2: Urban Place Designations, attached to this Bylaw in Schedule 1.

- b) In Figure 8: Urban Place Guidelines, in the Designation “Traditional Residential,” under the category “Built Form,” by adding the following words immediately after the phrase “Ground-oriented buildings up to two storeys.”:

“Ground-oriented buildings up to two and one-half storeys may be considered in the Victoria West neighbourhood for certain infill housing types, as indicated in the Victoria West Neighbourhood Plan”.

- c) In Figure 8: Urban Place Guidelines, in the Designation “Traditional Residential,” under the category “Built Form,” by adding the following words immediately after the phrase “Multi-unit buildings up to three storeys, including attached residential and apartments on arterial and secondary arterial roads”:

“, and in other locations indicated in the Victoria West Neighbourhood Plan.”

- d) In Figure 8: Urban Place Guidelines, in the Designation “Traditional Residential,” under the category “Uses,” by adding the following language:

“Limited local-serving commercial uses in specific locations identified in the Victoria West Neighbourhood Plan.”

- e) In Figure 8: Urban Place Guidelines, in the Designation “Traditional Residential,” under the category “Density,” by adding the following language:

“Additional density may be considered where appropriate in certain limited areas in the Victoria West Neighbourhood up to a maximum indicated in the Victoria West Neighbourhood Plan.”

- 5** The OCP Bylaw is amended in Schedule A, Section 14: Economy, by repealing Map 14: Employment Lands and replacing it with a new Map 14: Employment Lands, attached to this Bylaw in Schedule 1.

- 6** The OCP Bylaw is amended in Schedule A, Section 21: Neighbourhood Directions, as follows:

- a) By repealing Map 24: Harbour Strategic Directions, and replacing it with a new Map 24: Harbour Strategic Directions, attached to this bylaw in Schedule 1.
- b) By repealing Map 31: Victoria West Neighbourhood Directions and replacing it with a new Map 31: Victoria West Neighbourhood, attached to this Bylaw in Schedule 1.
- c) In section 21.26.4., by replacing the words “Develop a local area plan” with the words “Implement, monitor and regularly update the local area plan”.
- d) By deleting Policies 21.26.5. and 21.26.7., and renumbering remaining policies accordingly.

- 7** The OCP Bylaw is amended in Schedule A, Appendix A: Development Permit Areas and Heritage Conservation Areas by:

- a) Repealing Map 32: Composite Map of Development Permit Areas and Heritage Conservation Areas and replacing it with a new Map 32: Composite Map of Development Permit Areas and Heritage Conservation Areas, attached to this Bylaw in Schedule 1.
- b) Inserting immediately after Map 32: Composite Map of Development Permit Areas and Heritage Conservation Areas, Map 32A: Composite Map of Attached Residential Development Permit Areas, attached to this Bylaw in Schedule 1.

- 8** The OCP Bylaw is amended in Schedule A, Appendix A, DPA 5: Large Urban Villages by:
- a) Adding the following guidelines in section 5(b)(viii) after the words “Policy Plan and Design Guidelines for the Songhees Area of Victoria West (2008).”:
    - › Design Guidelines for: Multi-Unit Residential, Commercial and Industrial (2012).
    - › Revitalization Guidelines for Corridors, Villages and Town Centres (2017).”
  - c) Repealing Map 39: DPA 5: Large Urban Villages, and replacing it with a new Map 39: DPA 5: Large Urban Villages, attached to this Bylaw in Schedule 1.
  - d) Repealing Map 41: DPA 5: Large Urban Villages – Victoria West Village and replacing it with a new Map 41: DPA 5: Large Urban Villages – Westside Village, attached to this Bylaw in Schedule 1.
- 9** The OCP Bylaw is amended in Schedule A, Appendix A, DPA 6A: Small Urban Villages, as follows:
- a) In section 1, by deleting the words “Maps 48A and 48B” and replacing them with the words “Maps 48A, 48B and 48C”.
  - e) By adding the following sections immediately after section 5(b)(ii):
    - “(iii) to Craigflower Village:
      - › Design Guidelines for: Multi-Unit Residential, Commercial and Industrial (2012).
      - › Revitalization Guidelines for Corridors, Villages and Town Centres (2017).
    - (iv) to Catherine at Edward Village:
      - › Design Guidelines for: Multi-Unit Residential, Commercial and Industrial (2012).
      - › Revitalization Guidelines for Corridors, Villages and Town Centres (2017).”
  - f) By repealing Map 48: DPA 6A: Small Urban Villages and replacing it with a new Map 48: DPA 6A: Small Urban Villages, attached to this Bylaw in Schedule 1.
  - g) By inserting, immediately after Map 48B, a new map 48C: DPA 6A: Small Urban Villages – Craigflower Village and Catherine at Edward Village, attached to this Bylaw in Schedule 1.
- 10** The OCP Amendment Bylaw is amended in Schedule A, Appendix A, DPA 7A: Corridors, as follows:
- a) In section 1, by deleting the words “and 56” and replacing them with the words “, 56 and 56A”.
  - b) In section 4(c), by adding the phrase “, Esquimalt Road” immediately after the words “Oak Bay Avenue”.
  - c) By adding the following section immediately after section 5(b)(iii):
    - “(iv) Esquimalt Road corridor:
      - › Design Guidelines for: Multi-Unit Residential, Commercial and Industrial (2012).
      - › Revitalization Guidelines for Corridors, Villages and Town Centres (2017).”
  - d) By repealing Map 50: DPA 7A: Corridors and replacing it with a new Map 50: DPA 7A: Corridors, attached to this Bylaw in Schedule 1.

- |                            |        |      |
|----------------------------|--------|------|
| READ A FIRST TIME the      | day of | 2018 |
| READ A SECOND TIME the     | day of | 2018 |
| Public hearing held on the | day of | 2018 |
| READ A THIRD TIME the      | day of | 2018 |
| ADOPTED on the             | day of | 2018 |

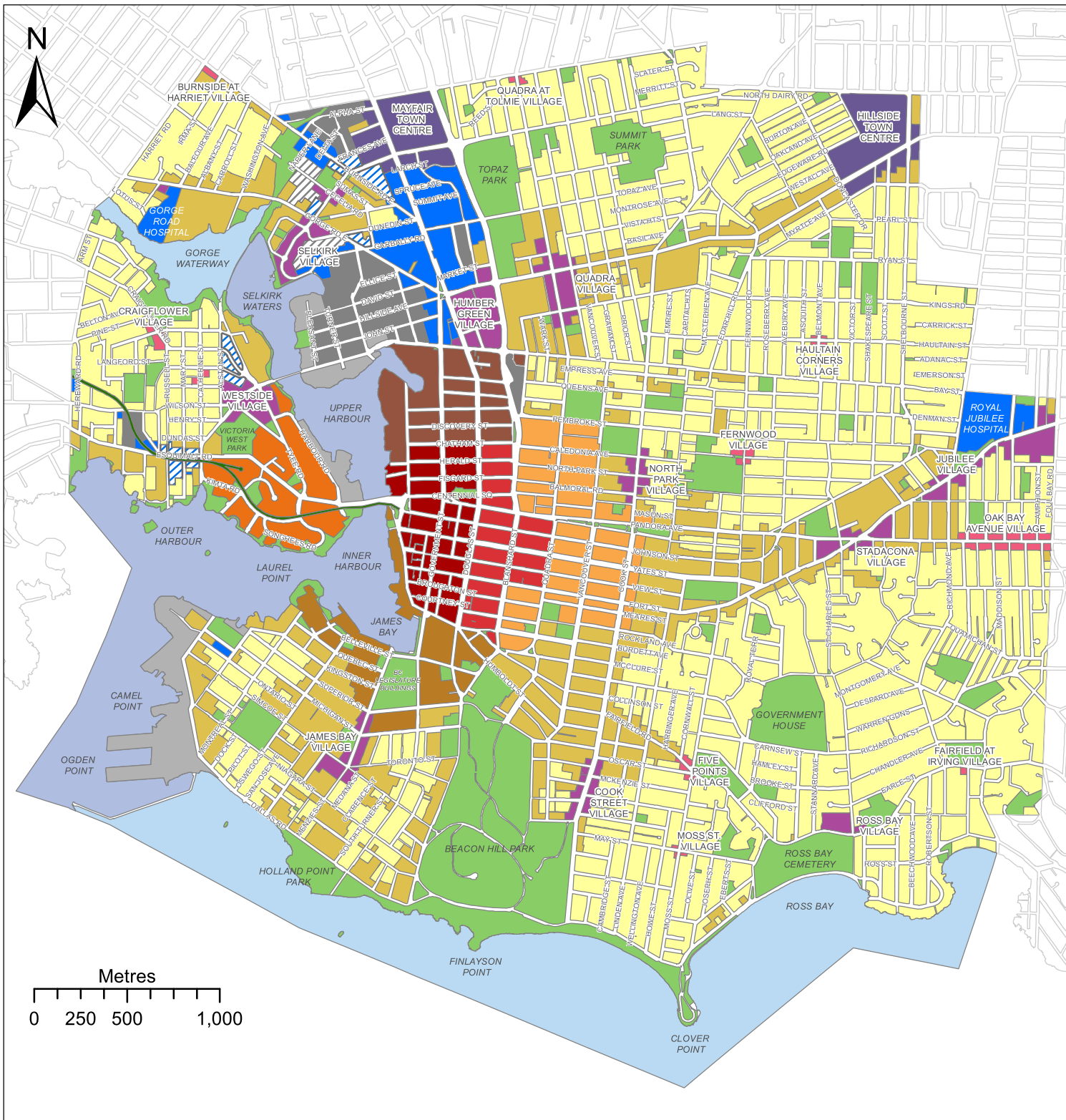
MAYOR



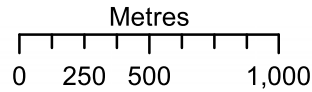
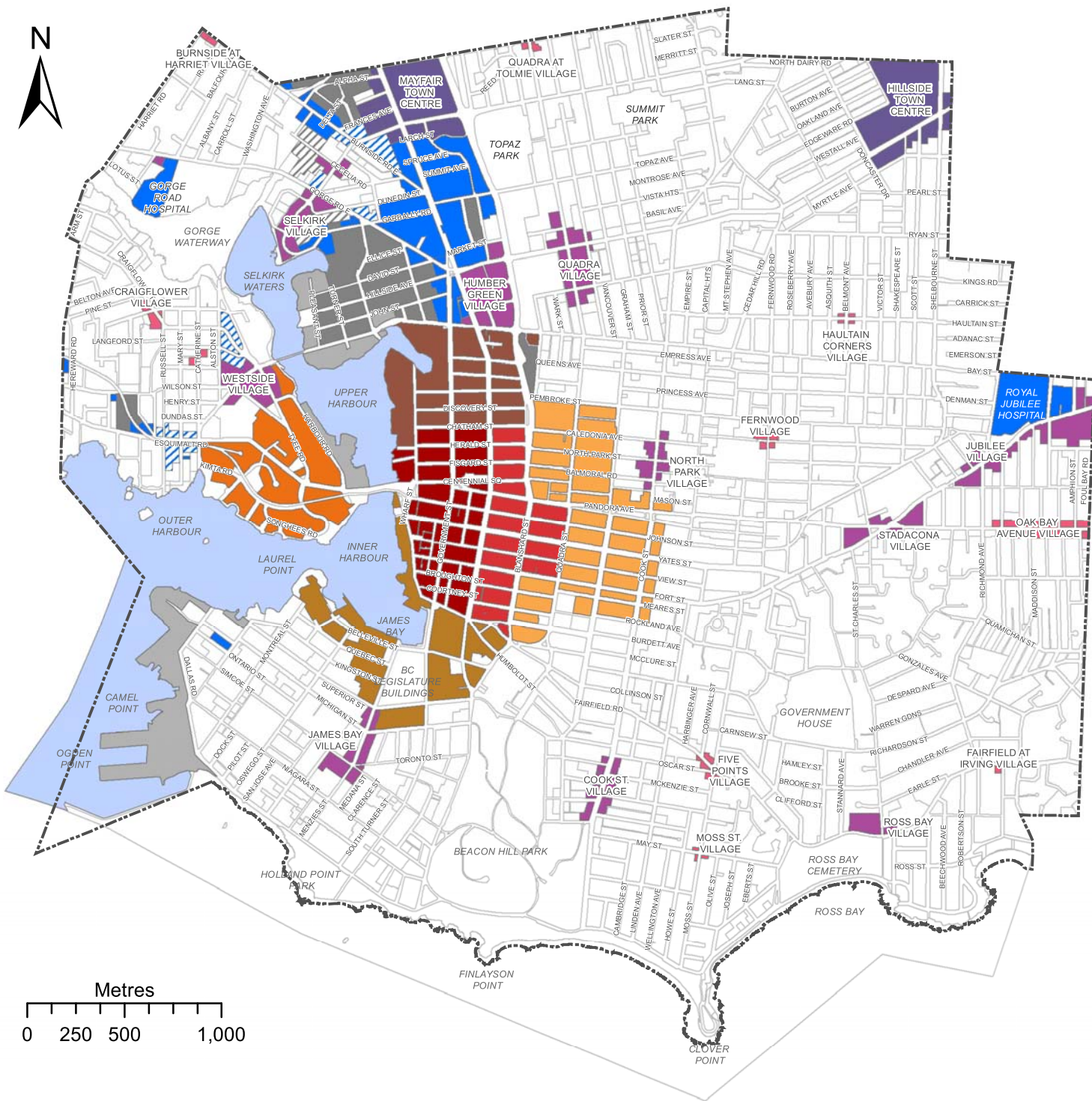
# MAP 2

## Urban Place Designations

- Core Historic
- Core Business
- Core Employment
- Core Inner Harbour/Legislative
- Core Songhees
- Core Residential
- General Employment
- Employment-Residential
- Industrial Employment
- Industrial Employment-Residential
- Marine Industrial
- Town Centre
- Large Urban Village
- Small Urban Village
- Urban Residential
- Traditional Residential
- Public Facilities, Institutions, Parks and Open Space
- Rail Corridor
- Working Harbour
- Marine



Urban Place Designations extend to the centerlines of adjacent streets.



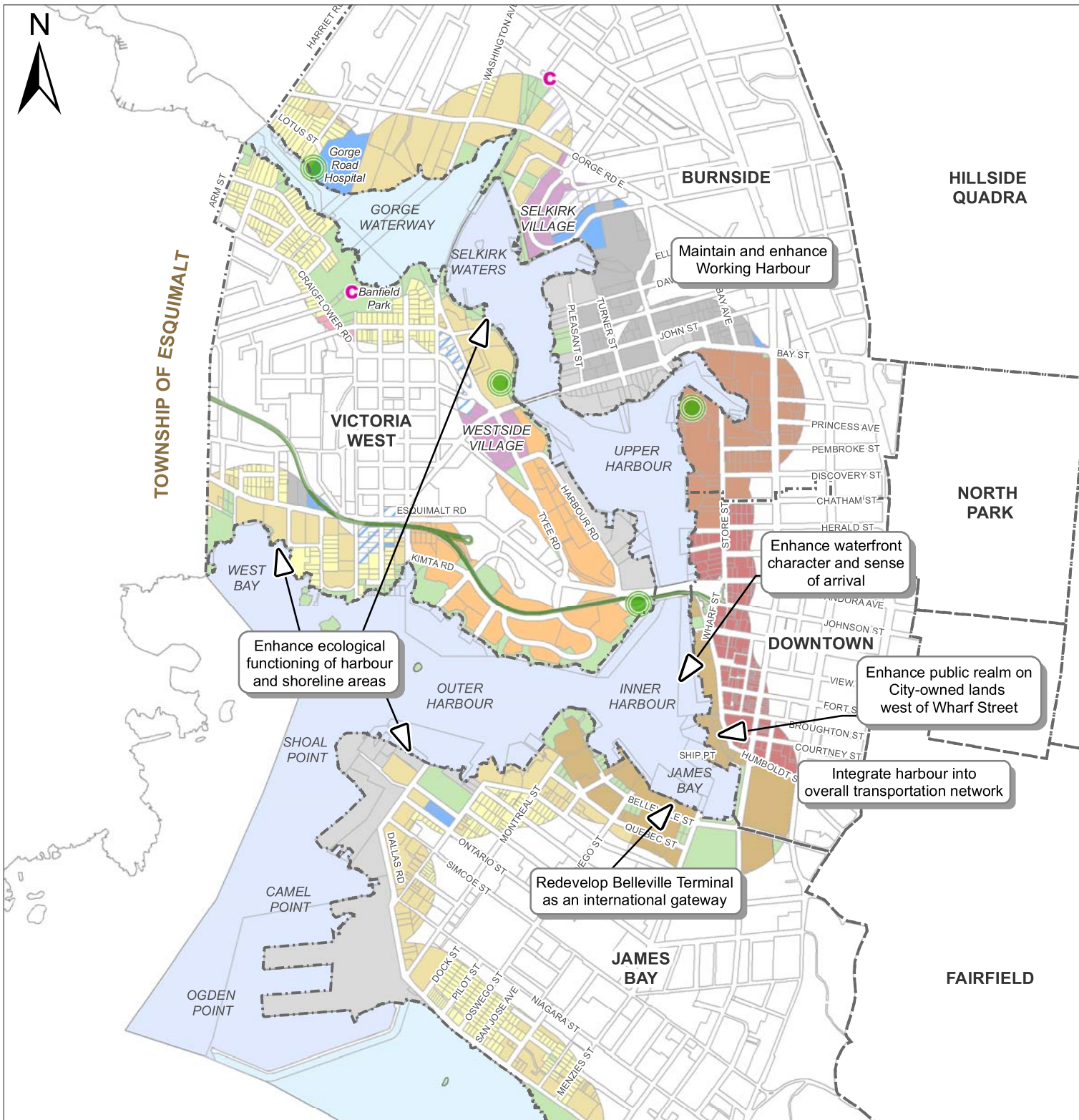
# MAP 14

## Employment Lands

- Core Historic
- Core Business
- Core Employment
- Core Inner Harbour/Legislative
- Core Songhees
- Core Residential
- General Employment
- Industrial Employment
- Marine Industrial
- Town Centre
- Large Urban Village
- Small Urban Village
- Marine - Harbour
- Employment-Residential
- Industrial Employment-Residential

Urban Place Designations are provided for information purposes only. Please refer to Map2 and Figure 8 for designation information.





\*Urban Place Designations are provided for information purposes only. Please refer to Map 2 and Figure 8 for designation information.



TOWNSHIP OF ESQUIMALT



DOWNTOWN

# MAP 31

## Victoria West Neighbourhood

### Urban Place Designations\*

- Core Songhees
- General Employment
- Marine Industrial
- Large Urban Village
- Small Urban Village
- Urban Residential
- Traditional Residential
- Public Facilities, Institutions, Parks and Open Space
- Rail Corridor
- Working Harbour
- Marine
- Employment-Residential
- Industrial Employment

### Public Facilities

- Existing Public School
- Proposed Park (approximate location)
- Community Centre

\*Urban Place Designations are provided for information purposes only. Please refer to Map 2 and Figure 8 for designation information.



# MAP 32

## COMPOSITE MAP OF DEVELOPMENT PERMIT AREAS AND HERITAGE CONSERVATION AREAS

- DPA 1 (HC): Core Historic
- DPA 2 (HC): Core Business
- DPA 3 (HC): Core Mixed-Use Residential
- DPA 4: Town Centres
- DPA 5: Large Urban Villages
- DPA 6A: Small Urban Villages
- DPA 6B (HC): Small Urban Villages Heritage
- DPA 7A: Corridors
- DPA 7B (HC): Corridors Heritage
- DPA 8: Victoria Arm Gorge Waterway
- DPA 9 (HC): Inner Harbour
- DPA 10A: Rock Bay
- DPA 10B (HC): Rock Bay Heritage
- DPA 11: James Bay and Outer Harbour
- DPA 12 (HC): Legislative Precinct
- DPA 13: Core Songhees
- DPA 14: Cathedral Hill Precinct
- HCA 1: Traditional Residential
- DPA 15C: Intensive Residential Rockland

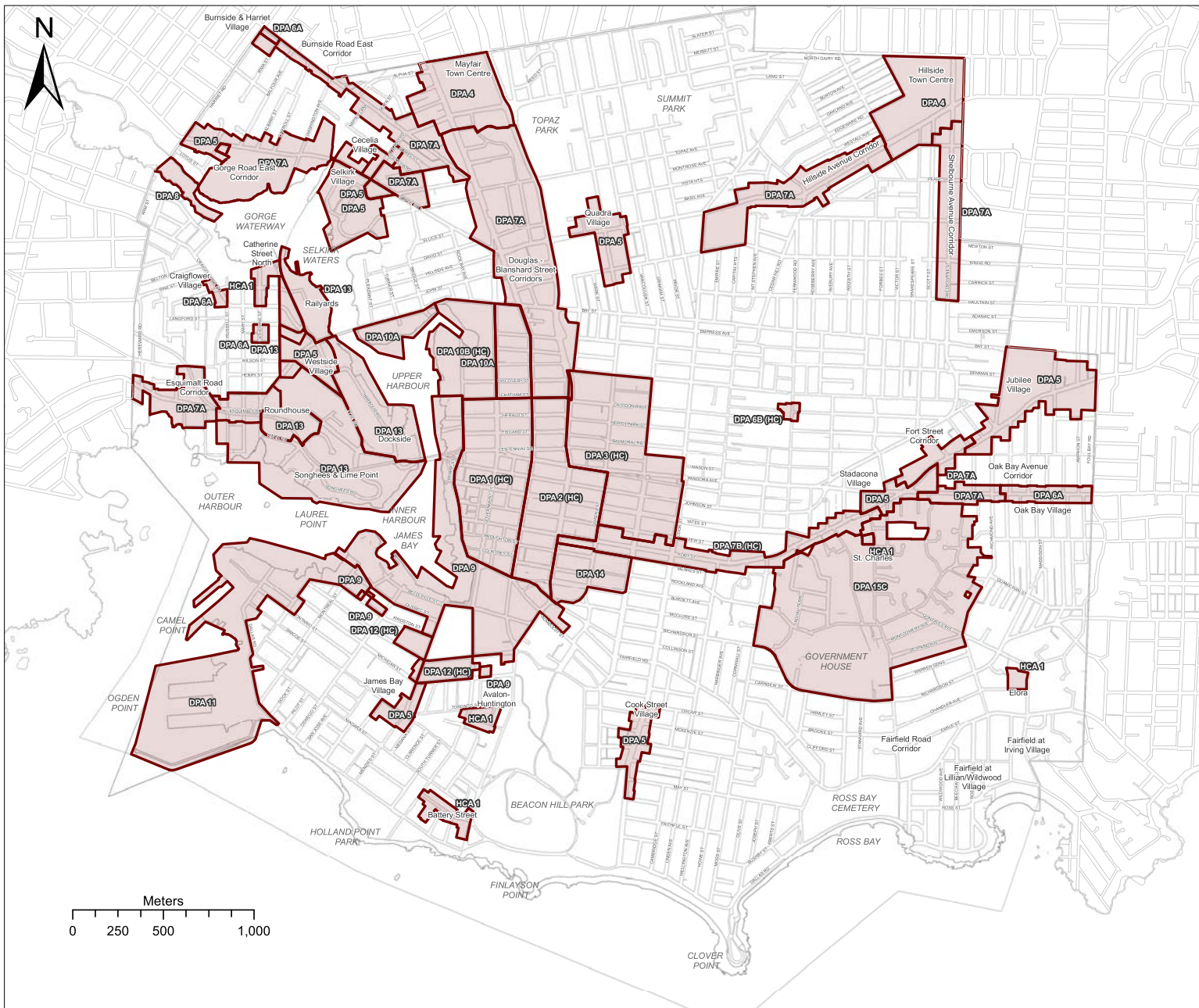
See Map 32A for the following Intensive Residential Development Permit Areas:

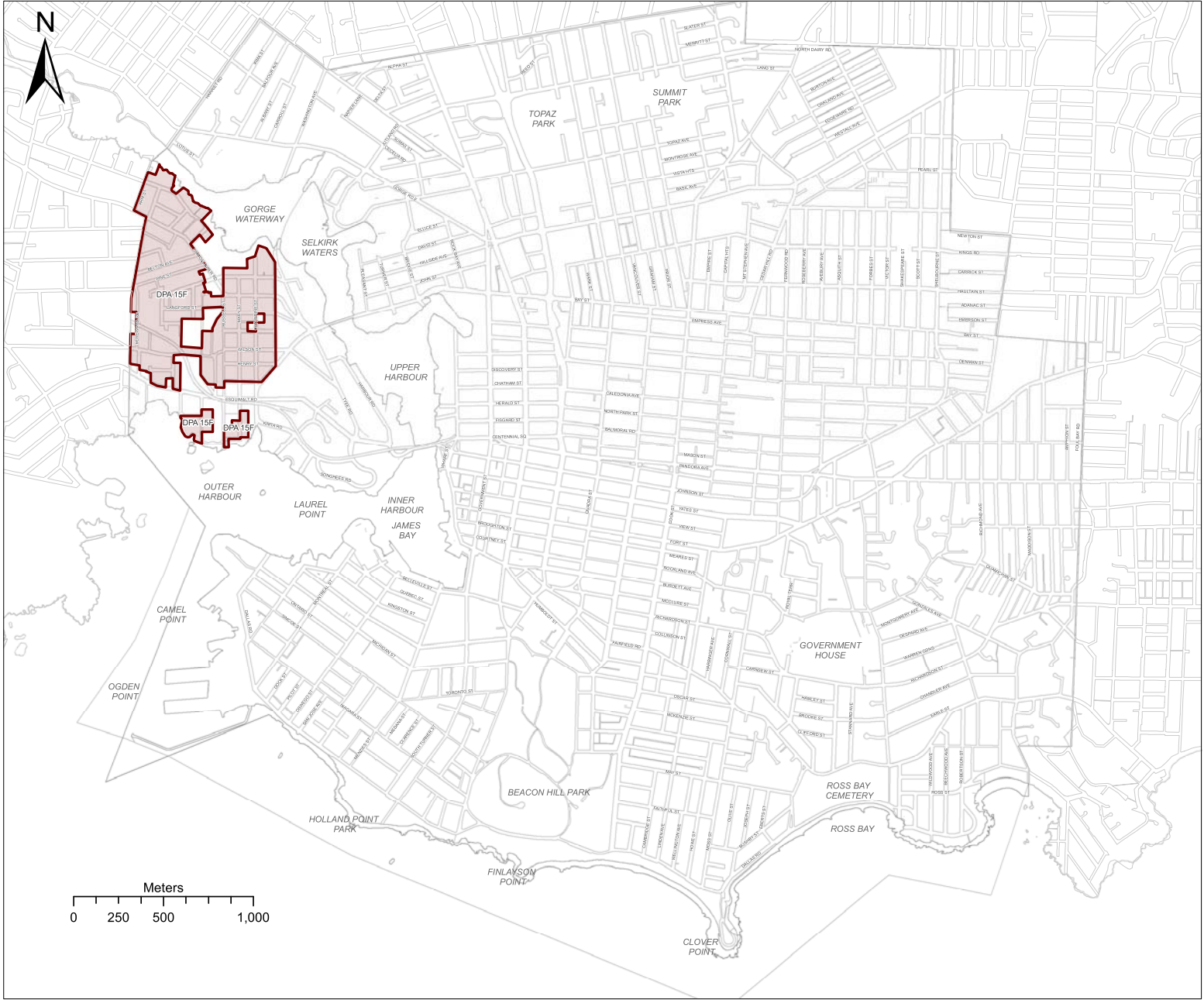
DPA 15F: Intensive Residential - Townhouse and Rowhouse

The following designations apply to all areas within the City of Victoria and are not shown on this map:

- DPA 15A: Intensive Residential Small Lot
- DPA 15B: Intensive Residential Panhandle Lot
- DPA 15D: Intensive Residential Duplex
- DPA 15E: Intensive Residential Garden Suites
- DPA 16: General Form and Character

This composite map is provided for reference only. Please see the map and provisions for each designated DPA and HCA for legal information.





# MAP 32A

## COMPOSITE MAP OF ATTACHED RESIDENTIAL DEVELOPMENT PERMIT AREAS

DPA 15F: Intensive Residential -  
Attached Residential Development



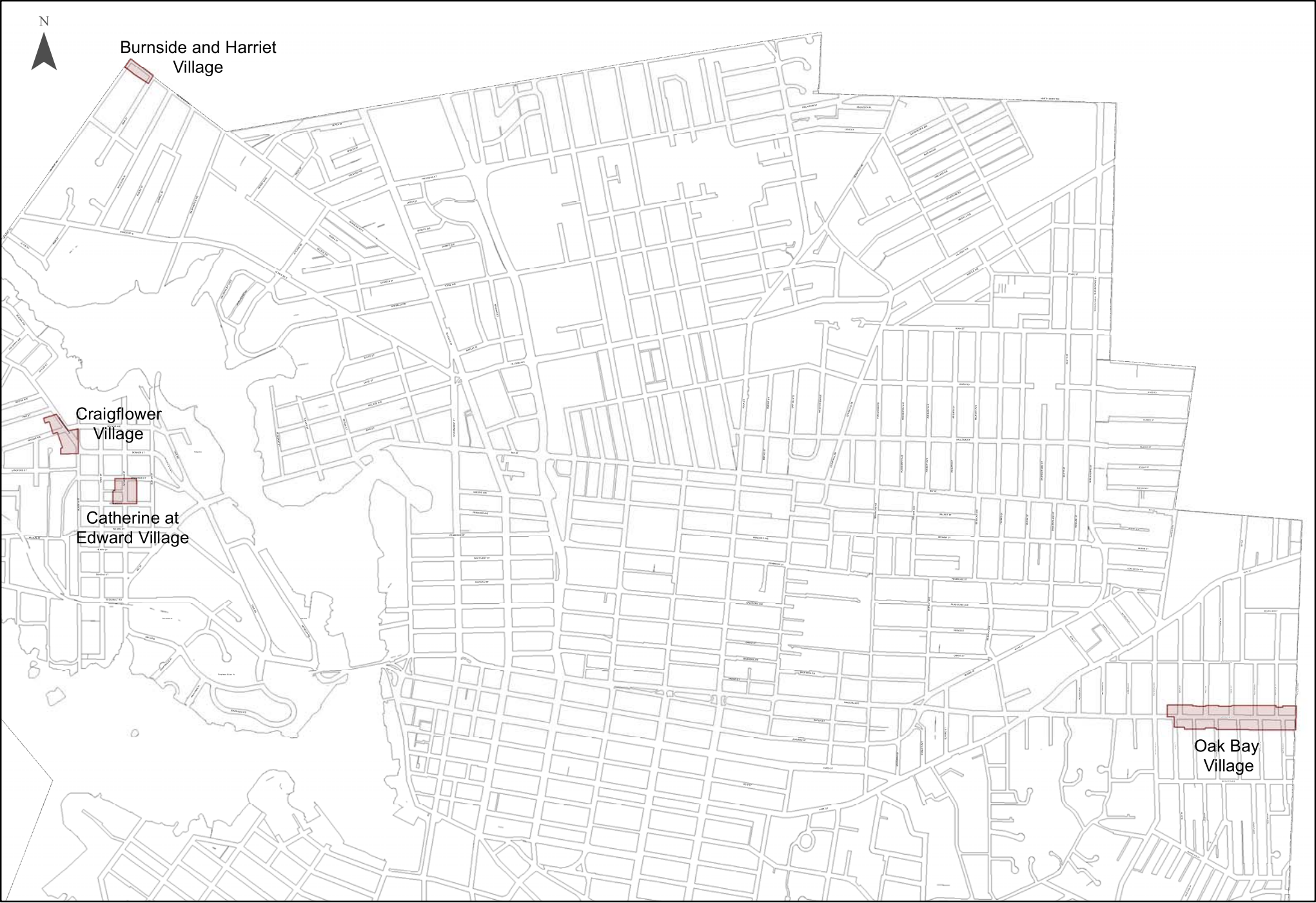
Map 39: DPA 5: Large Urban Villages







**Map 48: DPA 6A: Small Urban Villages**

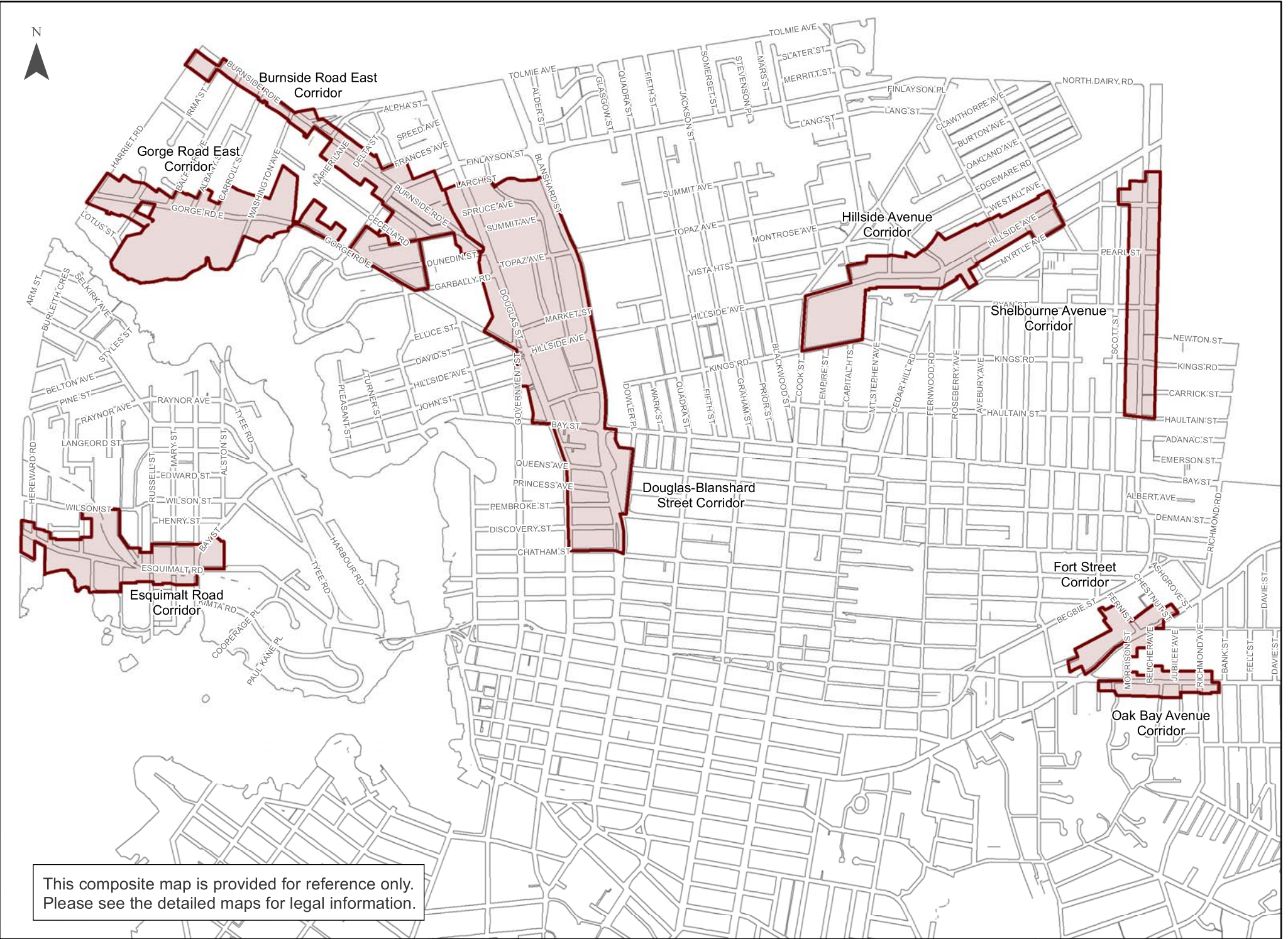


This composite map is provided for reference only.  
Please see the detailed maps for legal information.

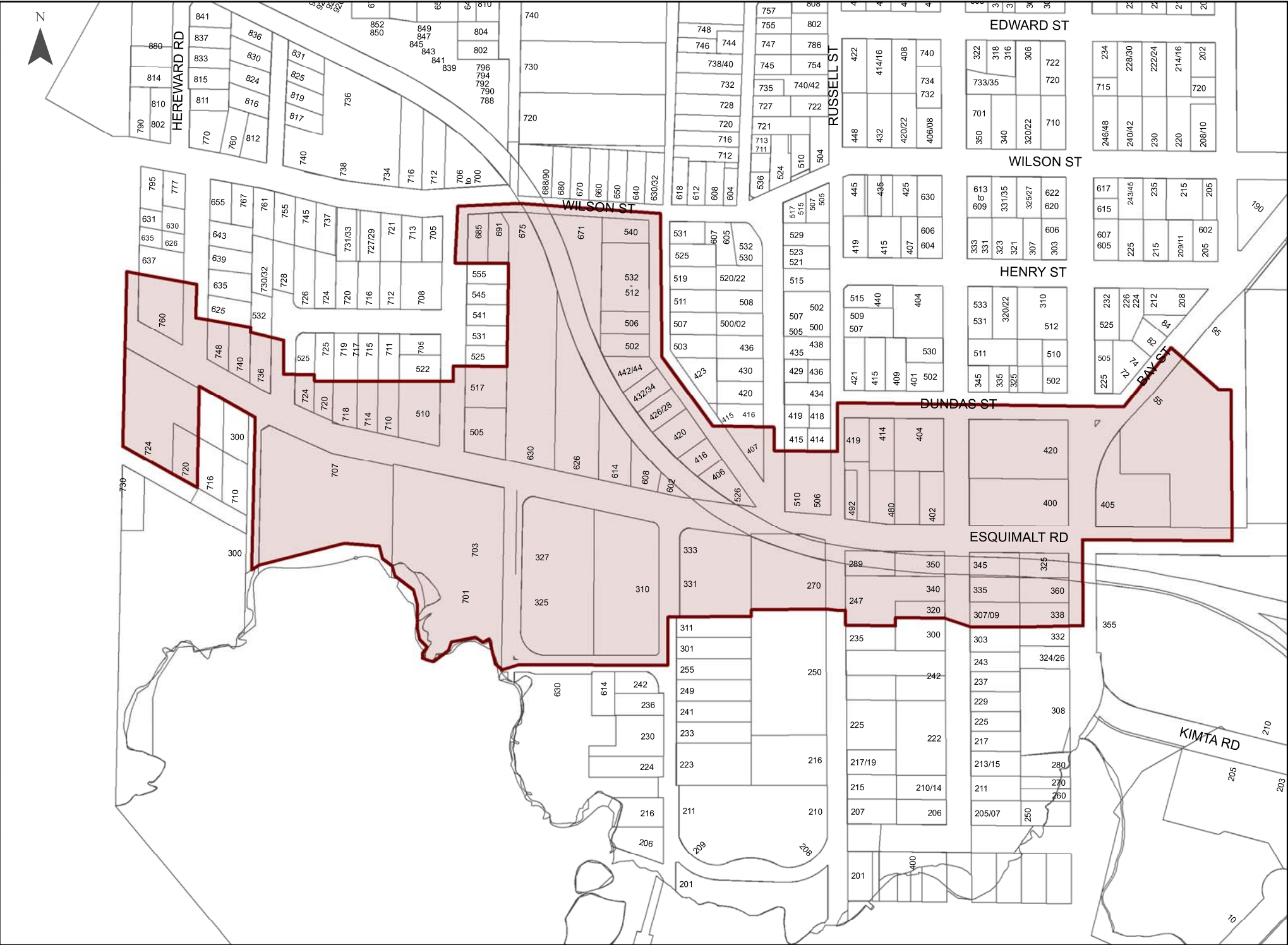




Map 50: DPA 7A: Corridors

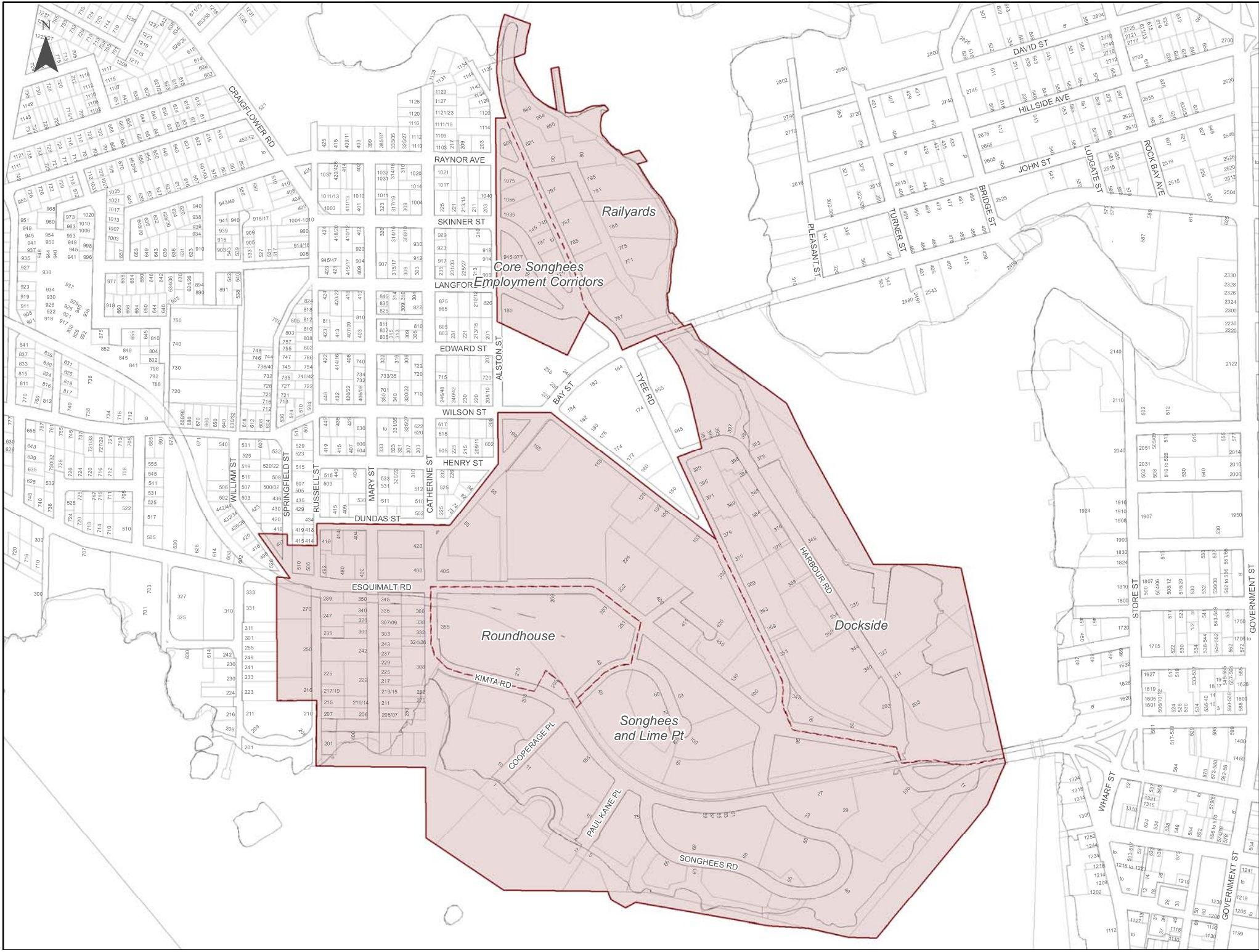


Map 56A: DPA 7A: Corridors - Esquimalt Road



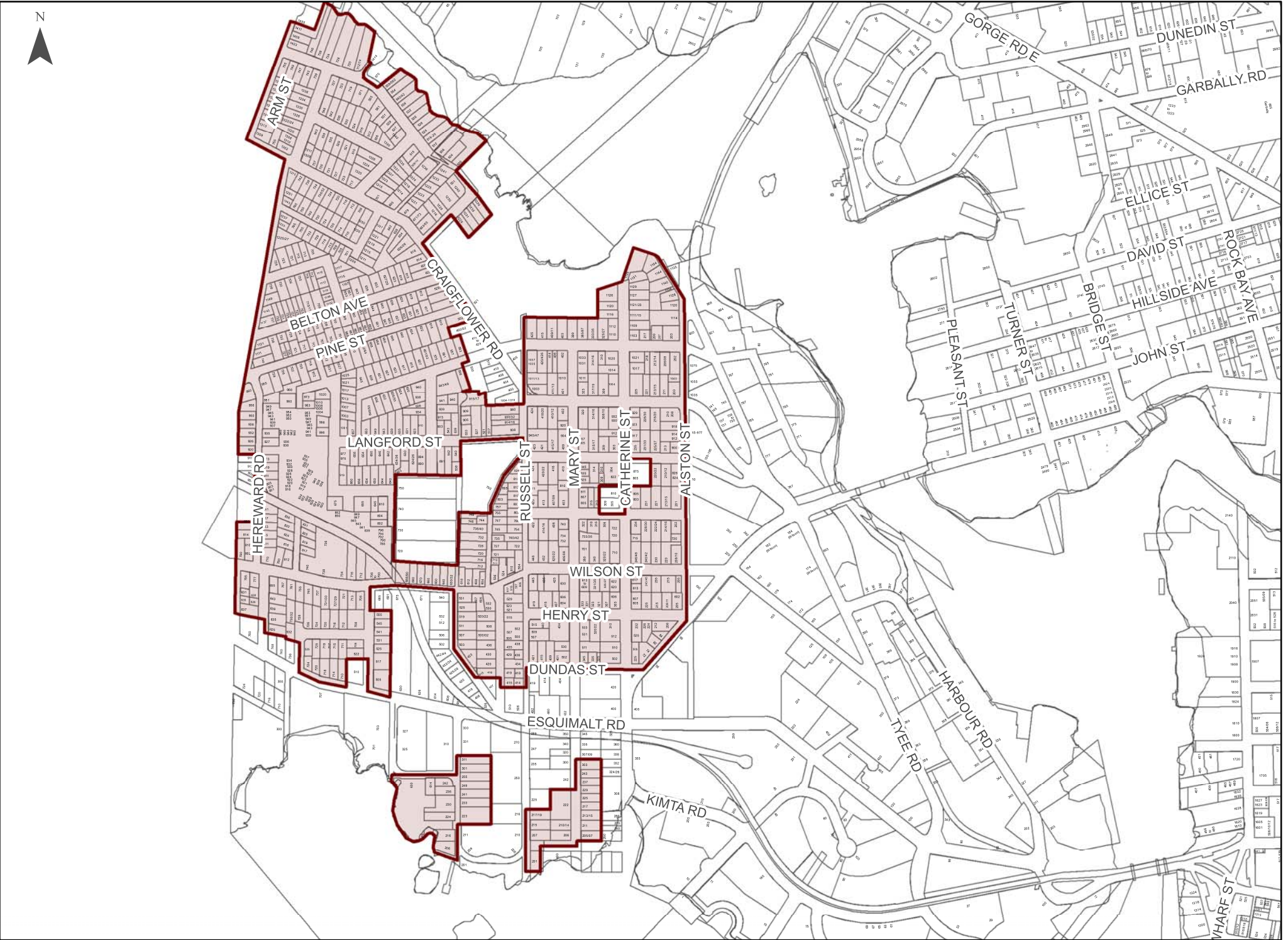


Map 64: DPA 13: Core Songhees





Map 72A: DPA 15F: Intensive Residential - Attached Residential Development



## DPA 15F: INTENSIVE RESIDENTIAL – ATTACHED RESIDENTIAL DEVELOPMENT

1. Pursuant to Section 488 (1) (e) of the Local Government Act, the area that is shaded and circumscribed by solid lines on Map72A is designated as Development Permit Area DPA 15F, Intensive Residential – Attached Residential Development, for the purpose of establishing objectives for the form and character of intensive residential development.

2. Application and Exemptions:

(a) In this area:

“Intensive Residential – Attached Residential Development” means construction of, addition to or alteration of a building, accessory structure or landscape on a lot which includes three or more attached, ground-oriented housing units.

(b) Development Permits are required for Intensive Residential – Attached Residential Development in accordance with the Local Government Act, subject only to the General Exemptions identified in the “Overview” section of this Appendix and the following exemptions.

(c) Specific Exemptions for DPA 15F, Intensive Residential – Attached Residential Development:

(i) A Development Permit is not required for:

- (1) development that is not Intensive Residential – Attached Residential Development;
- (2) residential single-family dwellings and their accessory buildings and structures unless the single-family dwelling is located on a lot that also includes Intensive Residential – Attached Residential Development;
- (3) duplex development and their accessory buildings and structures unless the duplex is located on a lot that also includes Intensive Residential – Attached Development;
- (4) house conversion;
- (5) commercial or industrial development;
- (6) alterations to landscaped areas which neither expand the extent of paved or impervious areas nor remove trees or shrubs which are shown in a previously approved Development Permit.

3. The special conditions that justify this designation include:

- (a) Victoria’s Traditional Residential areas are primarily characterized by low density, single-family dwellings.
- (b) These neighbourhoods each have a unique sense of place, traditional scale and character.
- (c) Attached residential development provides an opportunity to integrate other types of ground-oriented housing forms into neighbourhoods to provide more housing diversity.

4. The objectives that justify this designation are:

- (a) To accommodate 10% of Victoria’s anticipated population growth and associated housing growth in Small Urban Villages, and residential areas, to encourage and support future and existing commercial and community services.

- (b) To integrate more intensive residential development in the form of ground-oriented attached residential development within Traditional Residential areas in a manner that respects the established character of the neighbourhoods.
- (c) To accommodate housing growth in Traditional Residential areas in a manner that is gradual, of a compatible scale and adaptive to the local contexts.
- (d) To achieve a high quality of architecture, landscape and urban design to enhance neighbourhoods.
- (e) To integrate infill development in Traditional Residential areas that is compatible with existing neighbourhoods through considerations for site planning and building designs that promote livability and provide sensitive transitions to adjacent developments.

#### 5. Guidelines:

These Guidelines are to be considered and applied for Development Permits:

- (a) Design Guidelines for Attached Residential Development (2018)
- (b) Guidelines for Fences, Gates and Shutters (2010)





# Revitalization Guidelines for Corridors, Villages and Town Centres

**July 2017**  
Updated May, 2018



# Revitalization Guidelines for Corridors, Villages and Town Centres

## Preamble:

These guidelines apply to designated Corridors, Villages and Town Centres and are intended to supplement the *Design Guidelines For: Multi-Unit Residential, Commercial and Industrial, July 2012* which address form and character of developments across the city.

It is intended that both guideline documents will be considered together in conjunction with other applicable guidelines noted in each designated development permit area as detailed in the *Official Community Plan*. Collectively, the guidelines are intended to guide applicants in achieving new development and additions to existing buildings that result in design excellence, livability, and high-quality pedestrian environments. This is intended to contribute to sense of place and urbanism that is responsive to Victoria's context, while enabling flexibility and fostering creativity.

All visuals in this document are provided for illustrative purposes only to support description of the guidelines.

## General Guidelines

### 1) Context and Streetscapes:

- a. Buildings flanking streets should create a sense of enclosure and human scale. To achieve this, buildings fronting streets should provide a “street wall” that is at a height approximately 1/2 to 1/3 the width of the flanking street. This can be expressed as a street-wall-to-street-width ratio range of approximately 1:2 to 1:3. For buildings located on corner sites, this principle should be applied to the facades facing both streets where possible.

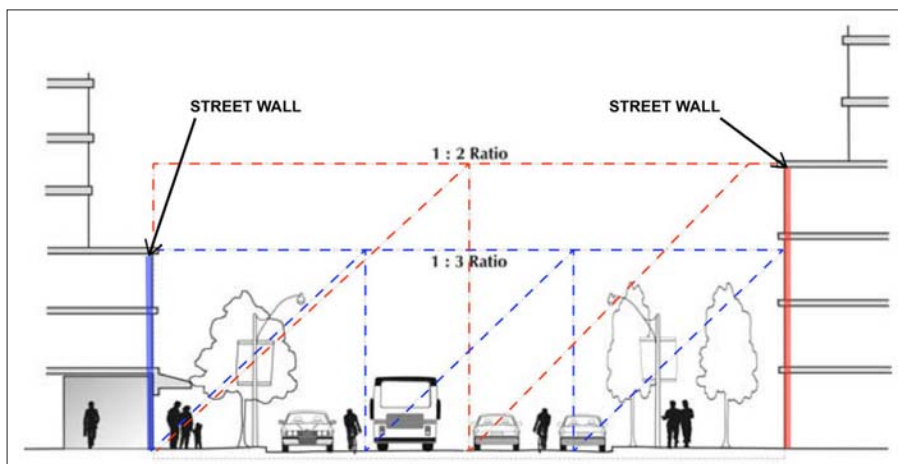


Figure 1: A building height-to-street-width ratio of between approximately 1:3 and 1:2 is recommended to frame streets and provide human scale. Portions of buildings above the street wall are encouraged to step back.

- b. To mitigate the visual impact of building height and to maximize sunlight exposure to the street, the upper portions of buildings above the street wall should be set back by at least two metres.
- c. Where an established pedestrian-friendly street wall exists, the front facade of new buildings should be generally aligned with adjacent buildings to create visual continuity along the streetscape.
- d. Buildings with commercial uses at grade should generally be built up to the sidewalk. Portions of the front facade may be set back from the front property line to accommodate features such as patios, courtyards or seating areas.
- e. Buildings should create “eyes on the street” and public spaces by orienting doorways, windows and balconies to overlook sidewalks, walkways, parks and other open spaces.
- f. Consider unique rooflines for taller buildings that have a visually prominent location (e.g. at corners, or at terminating vistas of streets) in order to create a distinct landmark.

## 2) Building Design:

- a. Building facades, especially those facing streets, should be well-designed and articulated with human-scale architectural features that create visual interest for pedestrians. Facade designs should consider the rhythm and pattern of existing building facades and architectural elements in the surrounding context, such as building articulation, rooflines, window placement, entryways, canopies and cornice lines.
- b. Large expanses of blank walls should be avoided. Where this is not possible, design treatments such as vertical plant materials, landscaping, art (e.g. mosaic, mural or relief) or the use of other building materials and building elements are encouraged to add visual interest.
- c. Weather protection for pedestrians should be provided in the following manner:
  - a) Individual canopies or awnings of sufficient depth should be provided to protect pedestrians from inclement weather, especially at building entrances.
  - b) The underside of canopies should be illuminated.
  - c) Canopies with translucent or frosted glazing are encouraged to maximize winter sunlight, particularly for north-facing facades.
- d. For buildings located on a corner, the corner design should include an architectural feature that addresses and emphasizes the corner. Strategies to achieve this include but are not limited to a chamfered or setback corner, prominent glazing, or a primary building entrance oriented to the corner.
- e. The first storey of a mixed-use or commercial building should be designed with a minimum floor-to-ceiling height of at least 4m and a minimum depth of approximately 10 metres to accommodate a range of commercial uses.
- f. Buildings with commercial uses at grade should be designed with a series of modulated storefronts and entrances, with transparent glazing. This design strategy is encouraged even where the building has a single tenant or use.



*Figure 2: Modulated, transparent storefronts create interest for pedestrians and encourage activity along the street.*

- g. Buildings that extend along sloping sites should be designed to follow and respond to the natural topography while maintaining a strong relationship of facades and building entrances to the street. Where retaining walls are unavoidable, they should be designed to ensure that they do not negatively impact the pedestrian experience along adjacent sidewalks.

## 3) Parking:

- a. Parking should be located underground or to the rear of buildings to provide human scale pedestrian environments. Where rear yard surface parking is proposed, building designs and landscaping interventions should be employed so that parking is integrated into sites in a manner that results in an attractive and safe environment.

#### 4) Livability:

- a. Where two or more buildings are located on a single site, or where a single structure contains two or more building elements above a common base or podium, a comfortable separation space should be provided for residential units, with consideration for window placement, sunlight penetration to residential units, and adequate spaces for landscaping.

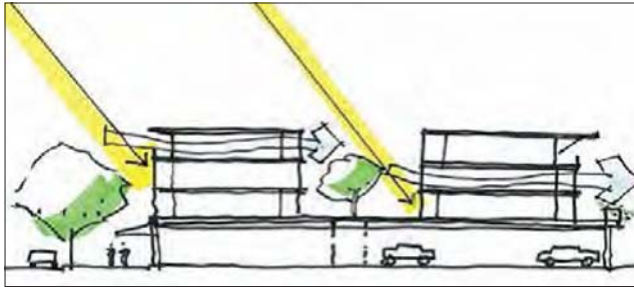


Figure 3: Comfortable separation space allows for sunlight access to individual units and outdoor spaces.

- b. Multi-unit buildings should be designed to provide a sensitive transition in scale to adjacent, smaller developments through considerations for massing and other design features. Strategies to achieve this may include but are not limited to setting upper storeys back, varying roof lines, siting or scaling buildings to reduce shading, etc.
- c. Residential building designs are strongly encouraged to include common outdoor space such as landscaped courtyards or rooftops, where possible.
- d. Buildings with residential use should be designed so that units receive daylight and natural ventilation from at least two sides of the building, or from one side and a roof. Where possible, provide dwelling units with a choice of aspect: front and back, or on two sides (for corner units).
- e. Residential buildings located along busy arterial streets should incorporate design features that minimize noise and pollution impacts (e.g. triple-pane glazing, residential units oriented towards courtyards, design of residential units with multiple orientations or side orientations, and building air intakes located away from the road).
- f. As a means to improve privacy between adjacent buildings, consider design solutions such as window size, window height, window placement and orientation, exterior landscaping, privacy screens or the use of frosted glazing on balconies.
- g. Pedestrian walkways that connect the primary entrance of multi-unit residential or commercial buildings with the adjacent public sidewalk should be a minimum of 2 m wide and distinguishable from driving surfaces by using varied paving treatments.

#### 5) Materials and Finishes:

- a. Exterior materials that are high quality, durable and capable of withstanding a range of environmental conditions throughout the year are strongly encouraged, particularly on lower portions of buildings that are more closely experienced by pedestrians. High quality building materials include but are not limited to:
  - Natural wood
  - Composite materials
  - Brick masonry
  - Glazed tile
  - Stone
  - Concrete
  - Flat profile “slate” concrete tiles
  - Glass and wood for window assemblies
  - Standing seam metal roofing
- b. Light-coloured, heat reflective and permeable paving materials are encouraged for hard surfaces such as parking areas, walkways, patios and courtyards as a means to reduce storm water run-off and reduce heat-island effects. Light-coloured or heat reflective materials are also encouraged for rooftops to reduce heat island effects.



## 6) Landscaping and Open Space:

- a. Buildings that include residential units should include private open space (e.g. balconies, porches) or easily accessed shared open space in the form of courtyards, green spaces, terraces, yards, play areas or rooftop gardens.
- b. The rear yard of multi-unit or mixed-use buildings adjacent to lower scale residential development should provide landscaping and trees that mitigate the appearance of massing and contribute to a transition in scale.
- c. Landscape design should consider the local climate and water efficiency through species selection, including selection of draught-tolerant species, efficient irrigation systems or design of unirrigated landscapes, use of run-off for irrigation, presence of rain gardens and other approaches.
- d. Consider features in landscaping or open space that add to sociability, such as shared areas to sit, garden plots, play areas, balconies fronting courts, etc.

## Area-Specific Guidelines:

In addition to the General Guidelines, the following guidelines apply to each specific designated area.

### 1) Town Centres

#### a. Mayfair Town Centre

- i. Taller buildings should generally be focused in the western part of the site, near Douglas Street.
- ii. Design taller buildings to have a clear architectural distinction between the base (podium or street wall portion), middle and upper portion of the building.
- iii. The podium base or street wall portion of buildings are encouraged to be three to five storeys (approximately 10–15 m) in height.
- iv. Major redevelopment of the Mayfair Shopping Centre should incorporate an internal network of pedestrian-friendly streets and connections between Speed Street, Nanaimo Street and Oak Street in order to create a structure of city blocks and to support permeability for pedestrians, cyclists and vehicles.

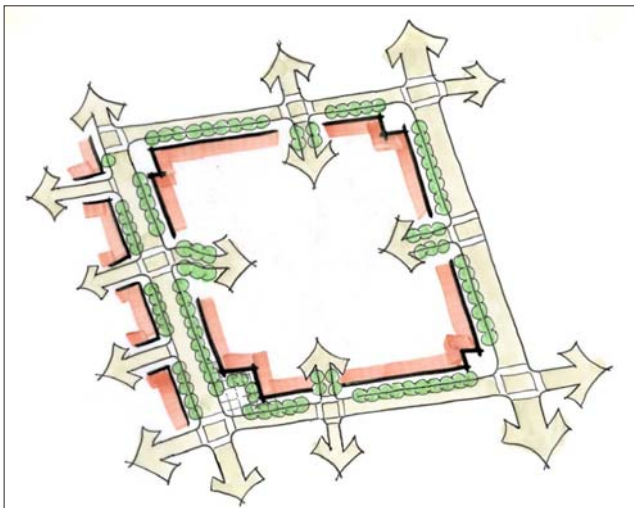


Figure 5: Major redevelopment of the Mayfair Shopping Centre site should establish an internal block structure connecting to adjacent streets.

- v. Building design should emphasize Douglas Street as the primary retail street of the Mayfair Town Centre. However, building designs should not “turn their back” on adjacent streets. Instead, provide facades that address all street frontages and are consistent with the General Guidelines for Building Designs (SECTION 2).
- vi. Building design that results in a landmark expression is encouraged at the intersection of Douglas and Finlayson Streets.
- vii. The tower portions of buildings above six storeys in height should generally be sited and designed to maintain access to sunlight, with a sufficient face-to-face separation distance between towers on the same site, and a sufficient clear distance to lot lines abutting other developable parcels. A desired face-to-face separation distance for towers at the Mayfair Shopping Centre site (the area bounded by Douglas Street, Nanaimo Street, Blanshard Street and Tolmie Avenue) is 25 metres.

## 2) Villages

### a. Gorge at Irma Village

- i. Development within this village should create multiple smaller storefronts facing Gorge Road and turning the corner onto Irma Street to support a variety of neighbourhood-oriented commercial uses.

### b. Craigflower Village

- i. Craigflower Village is envisioned as a neighbourhood-serving area with smaller storefronts, facades activating street or park edges regardless of use, and with scale transitioning to the adjacent neighbourhood. To achieve this:
  - 1. Buildings with commercial uses at grade should feature frequent entries and smaller storefront modules. A maximum distance of 10 metres for each module should be maintained.
  - 2. Between Raynor Street and Russell Street, new buildings should be sited close to the sidewalk's edge, regardless of use. Greater setbacks are encouraged where they accommodate residential or commercial patios or porches.

### c. Catherine at Edward Village

- i. Buildings fronting along Catherine Street between Edward Street and Langford Street are encouraged to establish a consistent streetwall close to the sidewalk's edge, regardless of use, with individual entries on the ground floor. Greater setbacks are encouraged where they accommodate residential or commercial patios or porches.
- ii. Features which activate the frontage (e.g. glazing along the ground floor, entries, patios or porches, and windows or balconies providing for surveillance of the public realm) should wrap around the corner to activate the laneway (Bella Street) as a pedestrian or gathering space and provide for surveillance.
- iii. Establish sensitive transitions to adjacent lower-scale residential development, considering building massing, access to sunlight, privacy, and landscape.

### d. Westside Village

- i. The section of Wilson Street stretching from its intersection at Bay Street and running east along the edge of Vic West Park is envisioned as an important gathering area. In addition to the General Guidelines, development in this area should provide frequent entries and be set back to provide sufficient space for pedestrian circulation, patio dining and street furniture along the Wilson Street frontage.
- ii. With new development, maintain or enhance the pedestrian connection that links Tyee Road to Wilson Street through the site as illustrated conceptually in Figure 6.



Figure 6: Conceptual illustration for Westside Large Urban Village

- iii. Buildings fronting on the Wilson Street – Bay Street intersection should engage the intersection with corner entries, enhanced pedestrian treatments or small plazas.
- iv. Buildings at the two identified terminating vistas on the west side of Bay Street, identified in Figure 6, should create a distinct landmark consistent with the General Guidelines.
- v. With redevelopment of the block west of Bay Street, new development is encouraged to create an internal pedestrian and/or road network linking the neighbourhood to the west, Bay Street, and Langford Street. (See, for example, conceptual illustration in Figure 6.)
- vi. In order to transition sensitively to the neighbourhood to the west, buildings fronting Alston Street should establish a streetwall of no more than 3 storeys as viewed from Alston Street, with modest landscape setbacks, architectural design and features (such as windows and doors) which activate the public realm.
- vii. Langford Street between Alston Street and Tyee Road is envisioned as an opportunity to establish a physical environment accommodating a mix of employment uses, possibilities for artisan, production, and similar uses mixed with pedestrian activity. Buildings fronting Langford Street are encouraged to be designed with frequent entries, individual storefront modules, and features such as roll-up doors or loading bays which can accommodate a variety of uses.
- viii. See also Supplementary Guidelines for Light Industrial Uses in Victoria West Corridors and Villages (section 11 of these Guidelines), if such uses are anticipated.

### 3) Corridors

#### a. Douglas-Blanshard Corridor

- i. In the Humber Green area between Douglas and Blanshard Streets, residential units are encouraged to be oriented to inner courtyards or quieter interior streets to mitigate noise impacts from adjacent arterial traffic. However, building designs should not “turn their backs” to Douglas and Blanshard Streets. Instead, provide facades that address all street frontages and are consistent with the General Guidelines for Building Designs (SECTION 2).

#### b. Gorge Road East Corridor

- i. Redevelopment along Gorge Road East should consider site planning and building massing to preserve and enhance view corridors looking south from Balfour Street and Carroll Street toward the Olympic Mountains.

#### c. Core Songhees Employment Corridors

The following guidelines apply to the area bounded by Alston Street, Tyee Road, and Langford Street, as well as lands fronting directly onto Langford Street between Alston Street and Tyee Road. These areas contain existing employment uses and are intended to accommodate future employment uses mixed with, or transitioning to, residential uses.

- i. In order to sensitively transition to the lower-scale residential areas to the west, buildings fronting Alston Street should establish a streetwall of no more than 3 storeys as viewed from Alston Street, with modest landscape setbacks, architectural design and features (such as windows and doors) which activate the public realm.
- ii. The design of buildings fronting onto the south side of Tyee Road should mitigate impacts on residential development across the street, considering massing and access to sunlight, given changes in elevation on these sites.
- iii. Langford Street between Alston Street and Tyee Road is envisioned as an opportunity to establish a physical environment accommodating a mix of employment uses, possibilities for artisan, production, and similar uses mixed with pedestrian activity. Buildings fronting Langford Street are encouraged to be designed with frequent entries, individual storefront modules, and features such as roll-up doors or loading bays which can accommodate a variety of uses.
- iv. See also Supplementary Guidelines for Light Industrial Uses in Victoria West Corridors and Villages (section 11 of these Guidelines), if such uses are included.

#### d. Esquimalt Road Corridor

- i. New development within the corridor and located between Mary Street and Victoria West Park should be designed to respect the public view corridors identified from Catherine Street at Edward Street, and from Mary Street at Henry Street, looking south to the Olympic Mountains, by considering the location, siting, massing and design of new development.
- ii. Uses along the south side of Esquimalt Road which are adjacent to the (future) E&N Rail Trail should provide for active frontages facing the trail, with entries, transparent glazing, and upper floor windows or balconies which provide for “eyes on the trail.” Pedestrian areas with a mix of hard and soft landscaping should be provided adjacent to the trail and delineated from the portion of the trail which is for active movement.
- iii. Development adjacent to the intersection of Esquimalt Road and Dominion Road should help create a gateway recognizing the transition between Esquimalt and Victoria.
- iv. Buildings fronting onto Dundas Street, or other streets where they are located across from Traditional Residential development, should transition sensitively across the street. Strategies to achieve this include emphasizing front yard landscaping, individual unit entries, and streetwall height.
- v. Buildings located adjacent to lower-scale residential development should transition sensitively to that development, considering building massing, access to sunlight, privacy, and landscape.

#### 4) Supplementary Guidelines:

##### a. Light Industrial Development in Victoria West Corridors and Villages

Context and Intent: The Victoria West neighbourhood is a mixed neighbourhood of largely residential, commercial and mixed uses with pockets of light industrial uses in specific corridors. These light industrial areas accommodate employees and customers, interface with public spaces such as streets and trails, are found adjacent to or across the street from residential or commercial areas, and may contain a mix of uses. Therefore, it is important that care be taken in designing light industrial development to provide a positive environment for users, to minimize impacts on adjacent non-industrial development, and to support the pedestrian environment and public realm.

In addition to the General Guidelines, the following additional guidelines apply to any development which permits a light industrial use:

- i. Where development with an industrial use is located adjacent to or across the street from residential development, ensure a sensitive transition by:
  1. Screening any outdoor storage areas, work areas or loading areas, incorporating generous landscape where adjacent to residential uses. Where light industrial uses or loading are likely to generate noise, screening may include more substantial features (e.g. walls) which provide noise attenuation.
  2. Locating outdoor loading, work and storage areas away from adjacent residential uses.
  3. Providing sufficient building separation between light industrial uses and adjacent residential uses.

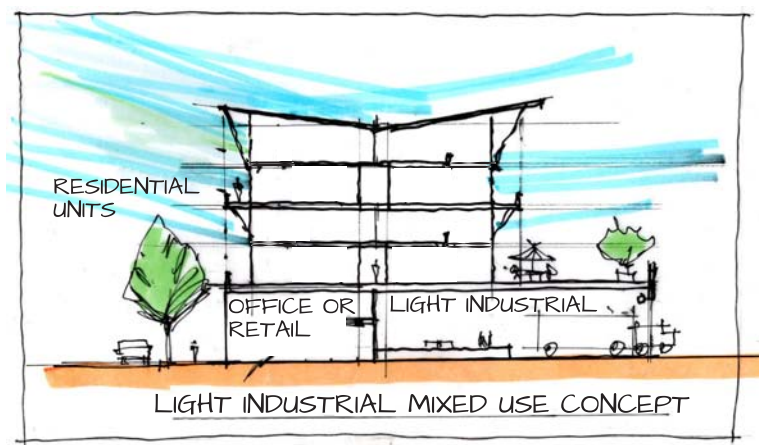


Figure 7: Example of a light industrial and residential mixed use building, showing how residential units might be set back from the loading areas.



4. Locating and screening ventilation and other equipment so as to minimize noise and visual impacts on residential uses.
- ii. In development which mixes light industrial uses with other uses (including commercial and residential uses), design should mitigate impacts of industrial uses on non-industrial uses. These impacts include but are not limited to noise, odours, glare and visual impacts of outdoor storage and activity. Strategies to achieve this include but are not limited to:
  1. Avoid residential overlook of loading areas or outdoor storage areas, through organization of building massing and orientation of windows and balconies.
  2. Incorporate methods for noise attenuation (e.g. triple-glazed windows; organization of building massing).
  3. Locate air intakes away from loading, circulation or work areas.



# Design Guidelines for Attached Residential Development

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# Design Guidelines for Attached Residential Development

## Introduction

The purpose of these guidelines is to encourage high quality design that enhances neighbourliness and social vitality and creates a good fit with the existing neighbourhood.

These guidelines apply to residential developments of three or more self-contained units on a site, each having direct access to the outside at grade level, and at least three of which share common walls. Attached Residential Developments can be designed in different forms (e.g. townhouses, rowhouses, etc.). They can also be designed in different configurations, and may involve more than one building complex on a site, which may be organized in more than one row where supported in plan policies and permitted by zoning.

## 1) Site Planning

**Objectives:** *To site buildings in a manner that considers and maintains the pattern of landscaped front and back yards, that makes a positive contribution to the streetscape and that achieves a more compact residential building form, while maintaining livability.*

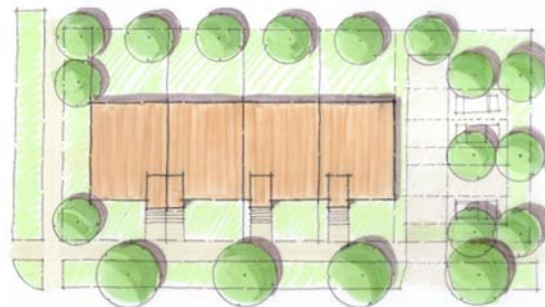
- a. Building Placement
  - i. Attached residential buildings should be designed parallel to the street with unit entrances oriented to, and directly accessed from, the fronting street. Both front and rear yards should be provided.
  - ii. For properties that include buildings of heritage value (Heritage Designated or listed on the City's Heritage Register) alternative siting of new buildings or additions may be considered to facilitate heritage conservation.



*Townhouses can contribute positively to streetscapes and the overall neighbourhood, supporting walking and creating greater housing diversity within existing neighbourhoods.*



*Illustrative examples of how townhouses along a street might be arranged with well-articulated street frontages (above and below).*



*Illustrative example of how townhouses might be organized on a corner lot to minimize curb cuts and provide for on-site open space.*



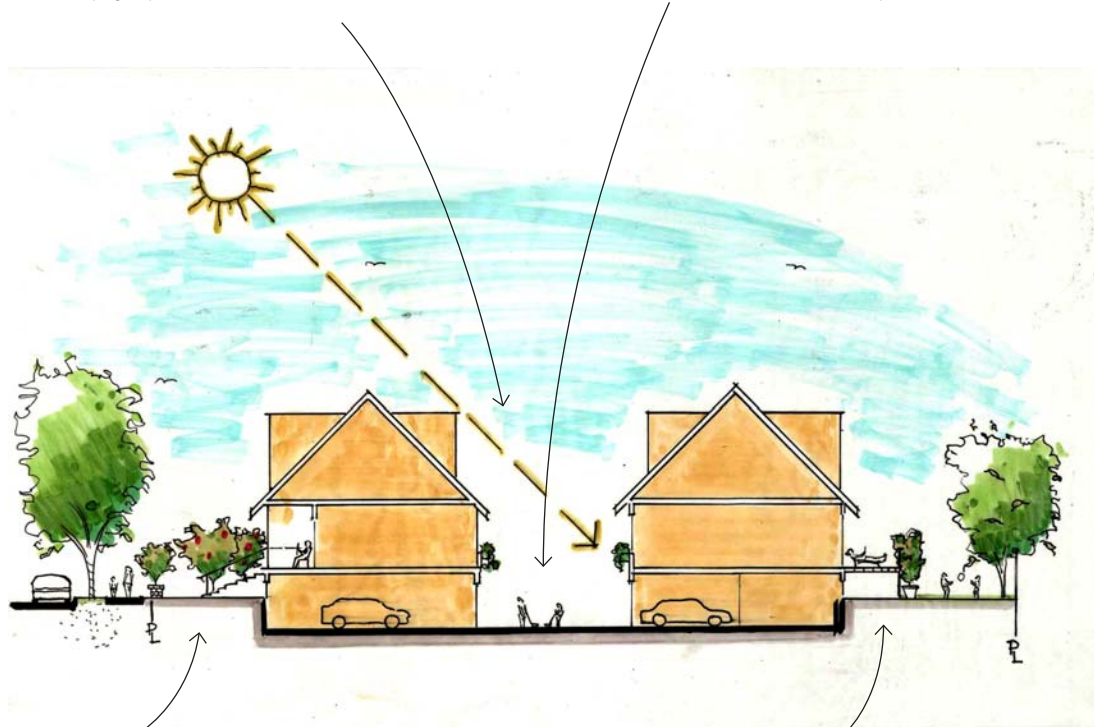
- iii. For properties that include significant natural features (e.g. significant trees, topography, rocky outcrops), buildings and landscape should be sited and designed to respond to natural topography and protect significant natural features wherever possible. Strategies to achieve this include but are not limited to alternative siting or clustering of buildings to avoid disturbance of natural features, and clustering of parking to reduce pavement on the site. (See also Section 4)
- iv. Some locations and lot sizes, as noted in local area or neighbourhood plan policies, may permit developments sited in more than one building complex on a site (i.e. more than one row). For these developments, the following should be achieved:
  1. Site planning should ensure that dwelling units face the street;
  2. Units located in the interior of lots should be designed with adequate separation from other buildings and have access to open space;



3. Vehicle access, parking and circulation should be integrated sensitively so it is not the dominant aspect of the development. See Section 1. vi. for further guidance.
  4. Dwelling units located in the interior of a site should have rear yard and side yard setbacks sufficient to support landscape and sensitive transitions to adjacent existing development and open spaces.
  5. Sufficient building separation should be provided between buildings to maximize daylight and minimize shadowing and overlook.
  6. Buildings which do not front onto the public street should be sited to provide sufficient separation from shared property lines and adjacent development in order to reduce overlook and shading, protect privacy for residents and neighbours, and provide space for landscaping.
- v. “Galley-style” developments, where building complexes are sited perpendicular to streets with residential unit entries oriented internally, are strongly discouraged. This layout is discouraged because it does not orient as many residential units towards the street, and typically provides less landscaped open space.

*Ensure adequate building separation to enable natural daylight penetration (8 metres is desired)*

*Design driveways and parking access as flex-use shared spaces*

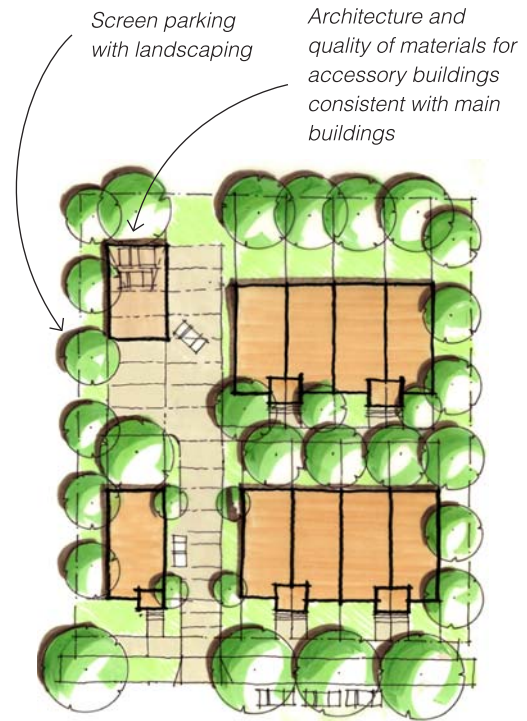


*Orient and animate entry ways towards public streets*

*Incorporate semi-private and private usable outdoor amenity spaces for residents*

vi. Vehicular access, circulation, garage doors and parking should not be the dominant aspect of developments and should be integrated to minimize impacts on fronting streets and adjacent public and private open spaces. Design strategies should be employed to minimize the impact of accommodating vehicles on site, including but not limited to the following:

1. Integrate parking in a manner that provides substantial landscaped areas in rear yards;
2. Locate and consolidate off-street parking areas to minimize extent of driveways and eliminate need for driveway access to individual units (refer to site plan showing shared/clustered parking);
3. Consider grouping driveway access points to minimize the number of driveway cuts and maximize space for landscaping and on-street parking;
4. Location of driveway access should strive to preserve existing canopy trees or provide opportunities for new canopy trees within the boulevard by providing enough planting space. See Section 4 Open Space Design for further guidance;
5. Front-accessed parking may be appropriate in some areas in order to avoid excessive pavement in rear yard areas. In these cases, attention to design is required to emphasize front yard landscape, provide tree planting space, and ensure a pedestrian-friendly building façade.
6. Minimize the impact of garage doors and vehicular entries by recessing them from the facade to emphasize residential unit entries.
7. Use high quality and, where appropriate, permeable paving materials for driveways;
8. Use attractive, high quality materials and consider incorporating glazing in garage doors;
9. See Section 4, Open Space Design for additional design guidelines related to landscaping and screening.



*Example of a site layout which clusters parking in order to minimize the area of the site dedicated to vehicle circulation, and enhances the relationship of individual units to open spaces.*



## 2) Orientation and Interface - A Friendly Face

**Objectives:** To ensure new development is oriented and designed to enhance public streets and open spaces and encourage street vitality and safety through increased “eyes on the street.”

- a. Residential buildings should be sited and oriented to overlook public streets, parks, walkways and open spaces balanced with privacy considerations.
- b. Developments should maintain a street-fronting orientation, parallel to the street.
- c. All residential units facing streets should have entries oriented towards, and be clearly accessible and visible, from the street.
- d. Where some units do not front onto a public street, a clear, legible and welcoming pedestrian pathway from the public street should be established.
- e. For developments that have interior-facing units, ensure unit entries are legible and emphasized through design features.
- f. Consider design strategies to delineate private front yard spaces, porches or patios from the public realm, while maintaining visibility of unit entrances. Design strategies may include but are not limited to:
  - i. elevating the front entryway or patio slightly above the fronting sidewalk level; or
  - ii. where a change in grade is not desired to provide accessibility, delineate the space through other means such as landscaping features, low fencing or planters.
- g. The design and placement of buildings and landscape should establish a sensitive transition to adjacent parks, trails, open spaces, and natural areas, considering a landscaped edge; respect the root zones of adjacent trees; and minimize impacts on ecologically sensitive areas and natural features.
- h. For new development adjacent to parks and larger public outdoor open spaces, design should clearly delineate private from public spaces, to avoid “privatizing” of public space.
- i. The location of blank walls or extensive parking areas adjacent to parks, trails and natural areas is strongly discouraged.

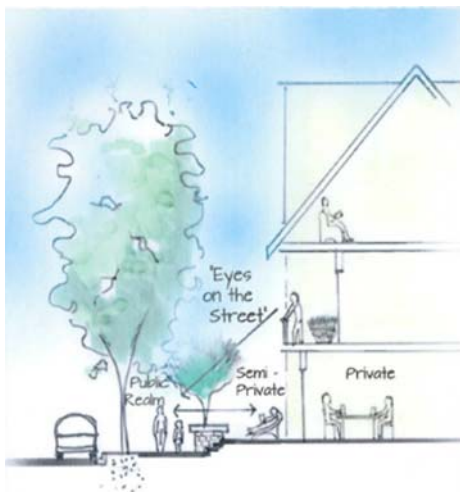


Illustration of interface with street.



Example of delineation of private front yard and visible entrance.



### 3) Building Form, Features and Context

#### 1) Building Form and Design

**Objectives:** *To achieve buildings of high architectural quality and interest with human-scale building proportions that are oriented towards and are compatible with the established streetscape character and pattern. Human scale refers to the use of architectural features, details and site design elements that are human proportioned and clearly oriented towards pedestrian activity. Building articulation refers to the many street frontage design elements, both horizontal and vertical, that help create an interesting and welcoming streetscape.*

- a. Building design elements, details, and materials should create a well-proportioned and cohesive building design and exhibit an overall architectural concept.
- b. Incorporate a range of architectural features and design details into building facades that are rich and varied in detail to create visual interest when approached by pedestrians. Examples of architectural features include:
  - i. building height, massing, articulation and modulation
  - ii. bay windows and balconies
  - iii. fenestration pattern (proportions and placement of windows and entry ways)
- c. Modulation in facades and roof forms are encouraged to break up building mass, differentiate individual units within attached residential developments, and to provide architectural interest and variation along the street.

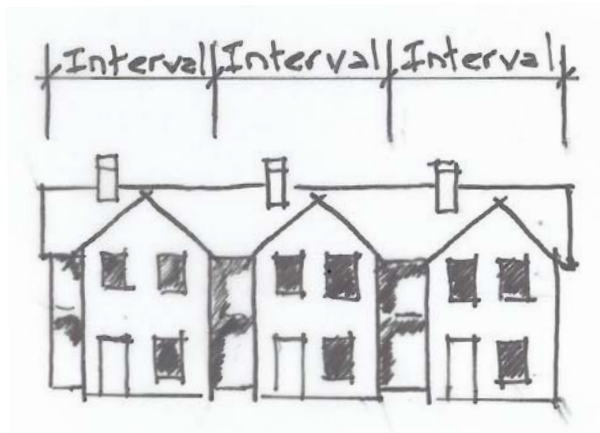


*Development which exhibits a cohesive architectural expression, with variation in units, clear front entries, and architectural interest for pedestrians.*



*Historic traditional townhouses (left) demonstrate human scale architecture, relationship to the public street, and cohesiveness of architectural expression. These same principles should guide the design of more modern developments which may be expressed in varied architectural styles (example at right).*

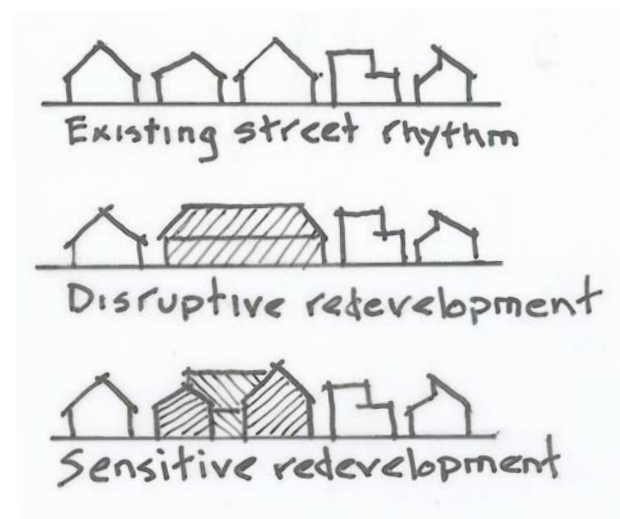
- d. Entrances should be located and designed to create building identity, to distinguish between individual units, and generally create visual interest for pedestrians. Well-considered use of architectural detail and, where appropriate, landscape treatment, should be used to emphasize primary entrances, and to provide “punctuation” in the overall street-scape treatment.
- e. Upper floor areas should be integrated into roof forms to help further mitigate the scale of new developments.
- f. Balconies should be designed as integral to the building. Overly enclosed balconies should be avoided, as these limit views and sunlight access.
- g. Building sidewalls should be designed to be attractive and interesting when viewed from adjacent buildings, street, and sidewalks through the use of materials, colours, textures, articulation, fenestration, and/or plant material.
- h. Creative use of landscaping or other screening should be used to reduce the perceived scale of adjacent development without compromising surveillance of public areas.
- i. Accessory structures should be compatible in architectural expression and quality of materials to main structures.



## 2) Neighbourliness/Compatibility

**Objectives:** To respond to the established form and architectural characteristics of surrounding buildings in order to achieve new buildings which are compatible with their context and minimize impacts on neighbours..

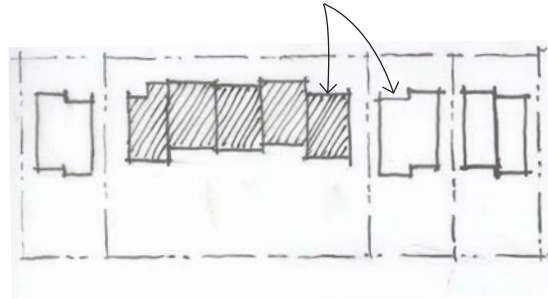
- a. New development should ensure a good fit with existing development by incorporating architectural features, details and building proportions that complement and respond to the existing architectural context, and by referring to distinctive and desirable architectural qualities of existing adjacent buildings in new development. Consideration should be given to the following aspect of development:
  - i. building articulation, scale and proportions
  - ii. similar or complementary roof forms
  - iii. building details and fenestration patterns
  - iv. materials and colour





- b. In some cases where a contextual architectural form and pattern does not exist, architectural character may be created rather than reflecting contextual precedent. In such cases, a well designed, new project can become a contribution to the context that may inform future development considerations.
- c. New townhouse development should transition in scale to existing residential buildings. Strategies to achieve this include but are not limited to the following:
  - i. A maximum one storey height difference between the end units of new street fronting developments and adjacent existing development should be achieved.
  - ii. The end units of new street fronting townhouse developments should be sited to match or transition to the front yard set back of adjacent existing residential buildings.
- d. The views from upper stories of new buildings should minimize overlook into adjacent private yards, especially in less intensive areas. Strategies to achieve this include but are not limited to the following:
  - i. Increased setback.
  - ii. Stagger windows to not align with adjacent, facing windows.
  - iii. Primary windows into habitable spaces, and also decks and balconies, should not face or be oriented to interior side-yards
  - iv. Locate and screen upper level windows, decks, and balconies to minimize overlook.
  - v. Use of skylights, translucent windows and clerestory windows are encouraged to minimize overlook of side yards.
  - vi. Landscape screening.
- e. Site, orient and design buildings to minimize shadowing impacts on adjacent properties.

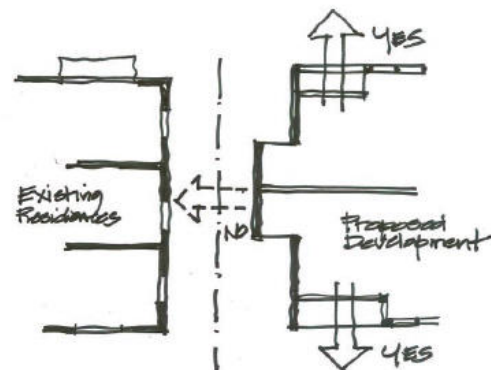
*End units should be set back to match or transition to existing neighbouring houses*



### 3) Mechanical equipment and service areas

**Objective:** To site and screen mechanical equipment and service areas to minimize impacts on neighbours and the public realm.

- a. Mechanical equipment, vents and service areas (e.g. for the collection of garbage or recycling) should be integrated with architectural treatment of the building, and screened with high quality, durable finishes compatible with building design.
- b. Mechanical equipment, vents and service areas should be located to minimize impacts on adjacent development by avoiding proximity to windows, doors and usable outdoor spaces.
- c. Location and installation of gas and electrical meters and their utility cabinets, as well as other mechanical or service apparatus should be carefully integrated into building and site design. Gas and electrical metres and utility cabinets on building frontages should be screened.



*Orientation and placement of windows, balconies and porches to respect privacy of adjacent development*

*New developments should transition in height to existing adjacent residential buildings*





*High quality materials in buildings and landscape elements*

#### 4) Materials

**Objective:** *To use materials which are high quality, weather gracefully, and contribute to the overall neighbourhood image.*

- a. An integrated, consistent range of materials and colours should be used, and variety between buildings and building frontages should be provided that is consistent with the overall streetscape.
- b. In general, new buildings should incorporate substantial, durable and natural materials into their facade to avoid a 'thin veneer' look and encourage graceful weathering of materials over time. Materials such as masonry, stone, natural wood, etc. are encouraged. Vinyl siding and large areas of stucco are discouraged and should generally be avoided.

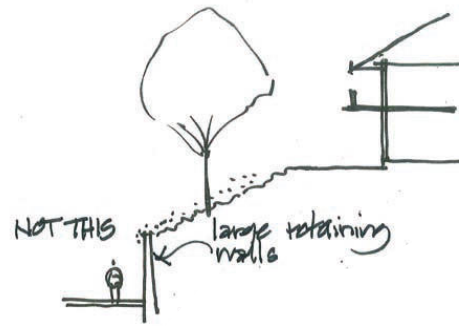
#### 4) Open Space Design

**Objective:** *To enhance the quality of open space, support the urban forest, provide privacy where needed, emphasize unit entrances and pedestrian accesses, reduce storm water runoff, and to ensure that front and rear yards are not dominated by parking.*

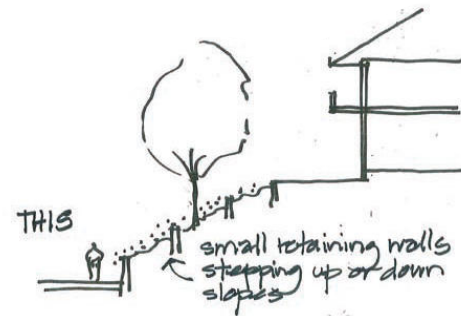
- a. Landscape treatments including use of front patios, accented paving treatments, fence and gate details, and other approaches are encouraged to help call out a residential entry and add interest along the street and sidewalk
- b. Areas within setbacks should incorporate plantings to create a green interface between buildings and streets
- c. Topographic conditions should be treated to minimize impacts on neighbouring development, for example by using terraced retaining walls of natural materials or by stepping a project to match the slope.
- d. Development should avoid significant reworking of existing natural grade.
- e. Where a building's ground floor is elevated above a pedestrian's eye level when on the sidewalk, landscaping should be used to help make the transition between grades. Some techniques for achieving this guideline include:



- i. rockeries with floral displays, live ground cover or shrubs.
- ii. terraces with floral displays, live ground cover or shrubs.
- iii. low retaining walls with raised planting strips
- iv. stone or brick masonry walls with vines or shrubs.
- f. Accessibility should be provided, where possible, in open space design.
- g. Landscape areas are encouraged to include a mixture of tree sizes and types
- h. Landscape on sites with significant natural features (e.g. significant trees, topography, rocky outcrops) should be located and designed to be sympathetic to the natural landscape.
- i. Consider planting tree species and other landscape plants that will tolerate a degree of drought and will survive the summer water restrictions and dry conditions of southern Vancouver Island.
- j. In considering tree placement along boulevards or in the front yard setback adjacent to street rights-of-way, consider tree sizes and spacing indicated by the City's specifications and policies for street trees.
- k. Landscaped screening along circulation and parking areas which abut lot lines is strongly encouraged, while maintaining site lines and enabling casual surveillance. Other surface parking areas should be screened with landscaping.
- l. Integration of landscaping to soften hardscape areas associated with vehicle circulation and parking is encouraged.
- m. Site design should integrate features to mitigate surface runoff of stormwater. This may include a variety of treatments (e.g. permeable paving for driveways and parking areas, landscape features designed for rainwater management, cisterns or green roofs, and/or other approaches) which are consistent with approved engineering practices and other city policies.
- n. Non-glare lighting should be provided at residential unit entrances, along pedestrian paths and common areas to contribute to safety. Lighting strategies that mitigate undue spill-over for adjacent residential units are strongly encouraged.



*Avoid large retaining walls and significant reworking of natural grade.*



*Transition sensitively between grades.*

### Attachment 3: Summary of Feedback and Revisions to Draft Victoria West Neighbourhood Plan (January 2018)

The following table presents a summary of feedback received from the community and Council, and describes any corresponding changes made to the revised version of the plan (Proposed Plan, January 2018). Comments and proposed revisions are grouped in the following order:

General Comments

Introduction

Transportation – General

Transportation – Specific Locations

Trails and Pathways

Parking

Parks

Urban Forest

Gorge and Harbour

Environmental Quality

Future Land Use Map

Future population growth/density – general

Amenities in new development – general

Noise – general

Traditional Residential Areas

Urban Residential Areas

Special Planning and Master Planned Areas

Urban Villages

Employment Lands

Heritage

Infrastructure and Green Buildings

Neighbourhood Food Systems

Neighbourhood Well-Being

Arts, Culture and Placemaking

Action Plan

General/Other Comments

Comments	Proposed Revisions & Rationale
<b>General comments</b>	
Refer to Roundhouse, Bayview consistently throughout	Changes made
Review for consistency: 2.5 storeys vs 2 ½, metres vs m , Catherine at Edward Street Village	Changes made
Lime Point refers to the area east of Lime Bay, not west of Lime Bay	Change all references to “South of Esquimalt Road”, from “Lime Point Area”
Change cover to “proposed Plan”	Changes made
Refer to “Lekwungen People” when referencing earlier Aboriginal use and occupation The historic Songhees Village is a major part of our history - anything to honour that would be good	Revisit references, history in plan and revise to give more prominence, recognition.
Refer plan to Township of Esquimalt	Revised draft plan to be formally referred to Esquimalt, with invitation to meet, once community feedback is incorporated. No text changes recommended.
Add titles and preamble to maps to make them more clear	Revised titles of maps to make them more prominent Added preambles to Land Use Map and other urban place designation maps to better explain the intent.

Numbering is challenging to follow. Consider consistent numbering between goals and sections and action plan in order to see which sections are related.	Goals refer to more than one policy; no change to numbering recommended. Add policy cross-reference to Action Plan
Ensure goals in introduction match goals in individual chapters	Revised
Clearly indicate desired amenities and relative priority for Vic West	(NEW) Add new Neighbourhood Amenities and Improvements (Chapter 15)
Order of land use chapters could be re-organized to help with user-friendliness	Re-order chapters and add overview to Future Land Use section to help with navigation.
Concern that there is a mismatch between what we heard in early engagement and what the plan addresses	General comment.
<b>Introduction</b>	
More description of the history of the neighbourhood and its role in the development of the city and the placemaking in Vic West. Exceptional natural (and cultural) heritage, overall, needs to be better identified	Revise pages 13, 19, 20, history timeline, Chapter 10 (Heritage)
Acknowledge and include the role of Core Songhees and master planned areas in neighbourhood plan, and the theme of knitting the areas together. Add more detail about the vision for each area, and major elements Important for reader to understand full picture of future development Integrate master planned area features into individual chapter topics (transportation, parks, etc) Update Big Moves map to better integrate master planned areas	(NEW) Update Big Moves map to better integrate master planned/ Songhees area. Revise Big Moves to add a new move- Knit together the old and new (p. 9) Add additional content to Master Planned Areas (p.87) and images throughout plan.
Provide overall neighbourhood targets to be achieved, such as number of added residential units; overall population; number of added commercial and industrial units; etc.  Add targets for other goals in neighbourhood plan.	No change recommended. Population projections, and actual number of additional residential, commercial and industrial units depend on many variables and are difficult to predict accurately.  Other targets monitored at City-wide level rather than neighbourhood level. No further changes recommended.
Add more First Nations history and context Review language used in heritage timeline to communicate history of First Nations people in Vic West (especially with respect to Songhees Reserve) Add more neighbourhood historical features to timeline	<ul style="list-style-type: none"> <li>• Revise heritage timeline (p. 222-23) to add more points in time and emphasise pre-European settlement history</li> <li>• Reviewed language used</li> <li>• Heritage timeline remains brief for overall readability</li> </ul>
<b>Transportation - General</b>	
Add map summarizing all active transportation improvements	(NEW) Add new Map 4 to summarize transportation improvements
Add Transit Map	(NEW) Add transit network map in Section 3.11
Goals missing from Transportation section.	Revise to match with goals in introduction.
Make it more clear that plan encourages development that supports a range of transit options	(NEW) Add sub-section 3.11 to encourage development (housing, commercial uses) that support a range of transit options. (NEW) Add goal to transportation section: Connect Vic West to the rest of the city and region by transit, rail and water. Create additional opportunities for housing to be located near frequent transit routes

Photo of bus on p. 27 poor quality	To be replaced in final document
For busy roads, desire for stronger language for transportation improvements, rather than “encourage”, “consider”, since many improvements have been suggested in past Make taming major roads a more central goal Reduce speeds to 30 km h on all major roads	Many of these improvements will be achieved through redevelopment, so difficult to assign a timeline. Added cross-referencing to Map 4 to refer to specific actions/ initiatives on major roads. 30 km/h speed limits have been considered in past; would increase risk of congestion and have impacts on adjacent streets. No further changes recommended.
Need more covered and secure bike lock up areas	No change recommended Locations to be explored through individual developments, street improvements and park improvement planning.
Concern regarding E&N Rail going to the new bridge	No change recommended The Plan reflects current City policy regarding assessing the re-establishment of E&N train service, and maintenance of corridor for transportation purposes (see 3.11.3)
Coordinate bike routes with Esquimalt (e.g. off-Esquimalt Rd route, to Tillicum)	Revised Map 3: Neighbourhood Active Transportation Network to show continuation of routes into Esquimalt on map Revised plan will be referred to Township of Esquimalt for review.
Include more north-south and east-west AAA facilities.	No change recommended All Ages and Abilities network determined at City-wide level. Local streets to be improved over time.
Facilitate walking and cycling between Vic West Elementary and residents in Esquimalt around Esquimalt High.	No change recommended Plan identifies several initiatives to improve walking/cycling, including connections between Esquimalt and Vic West: Pine Street, E&N improvements, intersection of Hereward St and Wilson Street.
Traffic slow down/calming is needed for Craigflower, Wilson, Hereward, and Catherine. Many drivers drive twice the speed limit on these streets.	No change recommended Plan identifies locations to be assessed for safety/comfort on each of these corridors (see summary Map Several initiatives to slow down traffic identified for Craigflower Village through urban design, on-street parking (7.1, 7.2)
Add Roundhouse as key destination	Revise Map 3 to add Roundhouse, Dockside Commercial Areas, Westside Plaza.
Consider pairing traffic calming measures with cycling routes	No change recommended Locations will be assessed for needed pedestrian/cycling improvements; this could include traffic calming depending on the conditions of the particular location.
Improve sidewalks in the area between Russell and Catherine, Esquimalt and Wilson.	No change recommended Relative priority of sidewalk improvements identified in Pedestrian Master Plan. No change recommended.
Consider a light at the crosswalk at Turner St. and Bay St.	In Burnside Neighbourhood (out of Vic West neighbourhood plan). Comments forwarded to Engineering and Public Works
Concern that roads / infrastructure not able to address additional traffic impacts from new development/ commuters	No change recommended 2004 Transportation Study confirmed sufficient road/bridge capacity for vehicle traffic from future Victoria West and background regional development, with accompanying traffic demand management in new developments.
<b>Transportation – Specific Locations</b>	
<b>Alston Street:</b> <ul style="list-style-type: none"> <li>Several comments on need for safe crossings at Alston/Skinner, Alston/Henry/Bay. Alston is a Greenway and should be safe to walk.</li> <li>More signage and lighting to support Alston as future pedestrian/bike connection. Add garbage receptacles along route</li> </ul>	Include a new Map 4: Neighbourhood-Identified Priority Transportation Improvements for clarify/ ease of reading (NEW) Map 4 initiative #1 identifies Alston Street as pedestrian-oriented corridor. Added cross-referencing to 3.3.10. and 3.4.3. 3.3.10 identifies Alston Street as a location for sidewalk network improvements through redevelopment. Catherine Street would remain as designated cycling route; Alston Street is more direct for pedestrians.



<ul style="list-style-type: none"> <li>Alston road is not a good choice for active transportation corridor - too steep and off the main. Make Catherine and Bay transportation corridors.</li> </ul>	
<b>Bay Street:</b> <ul style="list-style-type: none"> <li>Assess pedestrian crossing safety and wheelchair accessibility at Bay and Tyee (particularly pedestrians coming from stairs and crossing on east side of Tyee)</li> </ul>	Crossing recently upgraded. No further changes at this time.
<b>Bay St Bridge:</b> <ul style="list-style-type: none"> <li>Bay St. Bridge needs upgrading, and improved pedestrian and bicycle accessibility.</li> <li>Concern about Bridge's ability to deal with future population, commuter traffic</li> <li>Do upgrades to bridge before any more development</li> </ul>	<p>No change recommended</p> <p>2004 Transportation Study confirmed sufficient road/bridge capacity for vehicle traffic from future Victoria West development, with accompanying traffic demand measures in new developments. Bridge to be resurfaced and painted (2018); long-term plan to widen road deck for cycling facilities, add new sidewalk, extend turning lane. Region's long-term focus of on walking, cycling and transit improvements aims to reduce commuter traffic over time.</p>
<b>Craigflower Road:</b> <ul style="list-style-type: none"> <li>Enforce 30km/h speed limit on Craigflower in front of "Village"</li> <li>Concerns about speeding through Craigflower – would love to see more traffic calming</li> <li>Add light to pedestrian crossing at Raynor/ Craigflower</li> <li>Add Pedestrian crossing at Styles St.</li> <li>Bus stop at Craigflower and Raynor</li> <li>Questioning need to traffic calm Craigflower side streets</li> </ul>	<p>No change recommended</p> <p>Speeding concerns to be referred to Victoria Police – operational issue.</p> <p>Additional crossing improvements were previously evaluated – no additional changes recommended at this time.</p> <p>Poor visibility, topography and traffic would make it difficult to have another bus stop at Craigflower and Raynor.</p>
<b>Edward Street:</b> <ul style="list-style-type: none"> <li>Close west side of Edward at Catherine to cars</li> </ul>	<p>No change recommended.</p> <p>Would shift traffic impacts to adjacent streets.</p>
<b>Esquimalt Road</b> <ul style="list-style-type: none"> <li>Reduce speed on Esquimalt Rd. to 30km/h all along to Esquimalt</li> <li>Need more effective traffic calming and beautification along Esquimalt Road in Victoria, like in Esquimalt.</li> <li>Add parking for businesses along Esquimalt.</li> <li>Add pull-out in front of Skate Park so parents can pick up children (currently stopping in traffic).</li> <li>Add new crosswalk at Southwest corner of Vic West park to get to bus stop</li> <li>Change light at Robert St./Esquimalt Rd. to "on demand" so light turns green when car on Robert exits or enters Robert St.</li> <li>Turning lights may no longer be needed at Maitland Street and Esquimalt Road</li> <li>Lighting along Esquimalt to Wilson cycle/pedestrian path needed for safety (location unclear)</li> <li>Re-design the E&amp;N/ bike lane crossing at Esquimalt and William St for safety</li> </ul>	<ul style="list-style-type: none"> <li>Road design can accommodate 50 km/h traffic. Future development along Esquimalt Road will provide a narrowed feeling along the corridor, and help reduce speeds.</li> <li>(NEW) 3.3.20. Monitor and review speed limit along Esquimalt Road following completion of new Johnson Street Bridge. Also added to Action Plan.</li> <li>Parking on Esquimalt Road: where new development occurs, City's street cross-sections apply and may include on-street parking (e.g. on Esquimalt Road).</li> <li>(NEW) Add "Assess vehicle drop-off/parking for skate park" to 4.15.1. and Action Plan short-term Parks, Open Space and Waterfront items (2018-2019)</li> <li>Traffic lane changing from Maitland Street to Robert Street previously evaluated, but would have removed the signalized crossing to the transit stops.</li> <li>E&amp;N Trail currently identified as long-term (2027+ initiative). Action Plan will be adjusted if timelines change.</li> </ul>
<b>Harbour Road:</b>	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>Concerns should be addressed when new cycling facilities associated with Johnson Street Bridge are ready</li> </ul>

<ul style="list-style-type: none"> <li>Concern about bike/pedestrian/vehicle congestion from Harbour Road onto Esquimalt Road. Suggestions include bike-priority signals; discourage car traffic on Harbour Road; eliminate vehicle turns on/off Harbour Road</li> <li>protected two way bikeway on Harbour between Esquimalt and Goose</li> </ul>	<ul style="list-style-type: none"> <li>Suggestion already supported by City-wide AAA network plans. See 3.2.1. regarding two-way protected facility planned for Harbour Road.</li> </ul>
<b>Hereward Street:</b> <ul style="list-style-type: none"> <li>Sidewalk extension and a crosswalk on Hereward at Hereward Orchard would greatly improve access to the E &amp; N Walkway and slow/calm traffic</li> <li>Narrow, dangerous road. Concern about bikes here in future.</li> </ul>	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>Plan identifies Hereward Street to be assessed for cycling and pedestrian improvements (3.3.7) in short-term.</li> </ul>
<b>Hereward St/ Wilson St/ Rothwell St and Hereward St/ Pine St/ Dominion St:</b> <ul style="list-style-type: none"> <li>Many comments on need to assess and improve safety at Hereward/Wilson/Rothwell and Hereward/Pine/Dominion.</li> <li>Include a 'blue line' from Barnard Park north along Rothwell, Hereward to Dominion and Arm to Arm Park. This is a 'water to water' pedestrian/cycle route.</li> <li>Concern that Rothwell is not identified as connected to the adjacent priority pedestrian and/or cycling routes highlighted for Hereward and Esquimalt roads</li> <li>Connect Selkirk and Barnard/West Song by enhancing pedestrian and cyclist connectivity along Hereward/Rothwell.</li> </ul>	<ul style="list-style-type: none"> <li>Revise Map 3, Map 4 to include active transportation improvements on Rothwell St to link Hereward St to Barnard Park.</li> <li>Plan already identifies Wilson Street at Hereward St crossing 3.3.14; Hereward Street to be assessed for cycling and pedestrian improvements 3.3.14; Action Plan: Hereward/Wilson/Rothwell and Hereward/Pine/Dominion to be assessed for future improvements. No additional changes recommended.</li> </ul>
<b>Kimta Road:</b> <ul style="list-style-type: none"> <li>Need stop sign at the corner of Kimta and Tyee, have had problems with cars not stopping</li> </ul>	<p>No change recommended</p> <ul style="list-style-type: none"> <li>To be reviewed in conjunction with development of Kimta Street AAA project (2019)</li> </ul>
<b>Langford Street:</b> <ul style="list-style-type: none"> <li>Improve pedestrian conditions on Langford St between Alston and Tyee – prevent cars from parking on "sidewalk;"</li> <li>Others support retaining parking in Alston/Langford/Tyee commercial area, even as pedestrian improvements are made.</li> </ul>	<ul style="list-style-type: none"> <li>(NEW) Referred for evaluation for short-term improvements as part of update to Pavement Management Plan (3.3.10).</li> <li>(NEW) Add evaluation for short-term improvements as part of update to Pavement Management Plan (2018) to Action Plan.</li> <li>Long-term: Sidewalks identified as part of Pedestrian Master Plan ; to occur through redevelopment (3.3.10)</li> </ul>
<b>Lime Point Area:</b> <ul style="list-style-type: none"> <li>Designate more residential parking south of Esquimalt Rd. (e.g. Robert St., Maitland, etc.)</li> </ul>	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>Residents can request Residential-Only Parking areas through existing City program.</li> <li>Any rezoning on Robert Street would require the dedication of right of way or easements to meet current standards for on-street parking and sidewalks on both sides of the street.</li> </ul>
<b>Mary Street:</b> <ul style="list-style-type: none"> <li>Wilson and Mary intersection is dangerous</li> <li>Mary St. cut through traffic</li> </ul>	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>3.6.3. already identifies need for evaluation of road conditions at Mary Street and Wilson Street.</li> </ul>
<b>Pine Street:</b> <ul style="list-style-type: none"> <li>Improve traffic control at Pine Street and Craigflower Road in light of being main connection point for cyclists to the Galloping Goose</li> </ul>	<p>No change recommended</p> <ul style="list-style-type: none"> <li>Neighbourhood plan identifies problem areas; detailed assessment and any improvements, including detailed design, would be determined after neighbourhood plan is adopted.</li> </ul>

<ul style="list-style-type: none"> <li>Concerns about cycling/pedestrian route on Pine St impacting parking</li> <li>Pine Street traffic safety concerns - particularly Pine &amp; Craigflower - there isn't enough detail or commitment in the plan for me to trust that "taming" traffic or community-led initiatives would be successful.</li> </ul>	<ul style="list-style-type: none"> <li>Pine Street is identified as part of the City's long-term All Ages and Abilities network. To be designed as shared use facility, which shares road with cycling, on-street parking and vehicles.</li> </ul>
<b>Reno Street:</b> <ul style="list-style-type: none"> <li>Installing permanent barriers at Belton/Reno with good access for cyclists to pass through</li> </ul>	No change recommended <ul style="list-style-type: none"> <li>Barrier to be replaced in 2018 (reflected in Action Plan). Pedestrian and cycling needs to be considered as part of detailed design.</li> </ul>
<b>Rothwell Street:</b> <ul style="list-style-type: none"> <li>Pedestrian crossing time is too short at Rothwell Street and Esquimalt Road. Red light is not respected if cars don't see pedestrians.</li> <li>Reconsider need for cycling connection to Barnard Park, given that you can't cycle in/through there (but pedestrian connection can be improved)</li> </ul>	<ul style="list-style-type: none"> <li>Revise 3.6.6 to include assessment of crossing at Esquimalt Road.</li> <li>Cycling connections to parks encouraged through City's Parks and Open Space Master Plan. No further changes recommended.</li> </ul>
<b>Selkirk Street:</b> <ul style="list-style-type: none"> <li>Concern regarding traffic speed on Selkirk – can Selkirk on Victoria side be traffic calmed?</li> </ul>	No change recommended. <ul style="list-style-type: none"> <li>Policy 3.6.4. already identifies Selkirk Street for assessment of cut through traffic and speed.</li> </ul>
<b>Sitkum Street:</b> <ul style="list-style-type: none"> <li>Traffic lights needed at Sitkum &amp; Esquimalt</li> </ul>	No change recommended. <ul style="list-style-type: none"> <li>Identified as future improvement in plan when required by redevelopment (3.3.15).</li> </ul>
<b>Skinner Street:</b> <ul style="list-style-type: none"> <li>Concern about sightlines at intersections in Skinner St/Langford St. etc.</li> <li>Evaluate pedestrian crossing safety at Skinner at Catherine (esp. for cars turning left off Catherine onto Skinner)</li> </ul>	Staff will monitor. No further action recommended at this time.
<b>Suffolk Street:</b> <ul style="list-style-type: none"> <li>Concerns about short cutting on Suffolk Street for drivers trying to avoid the E &amp; N stoplights and there are several young families here.</li> </ul>	(NEW 3.6.7) Assess Dalton Street south of Wilson Street for short-cutting and speed. (NEW ) Action Plan 2019: Assess Dalton Street south of Wilson Street for short-cutting and speed.
<b>William Street:</b> <ul style="list-style-type: none"> <li>500 block William Street not good for bike lane, very narrow, lots of delivery trucks, etc. Suggestion – lane go down Springfield or stay on dedicated bike lane to Wilson Street</li> </ul>	No change recommended <ul style="list-style-type: none"> <li>William Street greenway would be developed as shared use route due to low traffic volumes and speed; no bicycle lane planned.</li> </ul>
<b>Wilson Street (between Bay Street And Hereward St)</b> <ul style="list-style-type: none"> <li>Cars don't stop at mid-block pedestrian crossing on Wilson</li> <li>Signage and formal crossing needed at intersection of E&amp;N with Wilson – poor visibility, grade, fast traffic, trucks</li> <li>Concern with Wilson St. blind corner and speed change - too much speeding on this street</li> <li>Desire to improve foot traffic safety on Wilson Street, particularly where there is a need to cross from Rothwell.</li> </ul>	<ul style="list-style-type: none"> <li>(NEW 3.3.12) Assess E&amp;N Trail on Wilson Street crossing for safety and visibility.</li> <li>(NEW) Action Plan 2019 Assess E&amp;N Trail on Wilson Street crossing for safety and visibility.</li> </ul>

## Trails and Pathways

Connect to trails in other communities (e.g. to waterfront trail in Esquimalt to Gorge park)	<ul style="list-style-type: none"> <li>• Revise 3.1.1. to add “and adjacent neighbourhoods and municipalities”</li> <li>• Proposed active transportation routes and waterfront pedestrian routes designed to connect across municipal boundaries.</li> <li>• Extend trails across municipal boundaries (Map 3 and 4)</li> <li>• Show possible future opportunity to be explored linking to Gorge waterfront in Esquimalt (Map 4)</li> </ul>
Mixed opinions on whether or not lighting should be added to trails in parks	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>• Concerns regarding Galloping Goose Regional Trail to be referred to CRD for future trail improvement planning (Action Plan for 2018).</li> <li>• Lighting through Banfield Park to be addressed through future park improvement planning (see Action Plan)</li> <li>• Safety goals need to be balanced with potential ecological impacts.</li> </ul>
<b>Banfield Park:</b> <ul style="list-style-type: none"> <li>• Safety concerns with shared cycling/pedestrian path.</li> <li>• Separate so that cycling paths and pedestrian paths are clearly separate (e.g. reflective line).</li> <li>• Mixed opinions on paving trails. Some fear paving will increase speeds or have ecological impacts. Others feel that existing flooding and gravel is dangerous.</li> </ul>	<ul style="list-style-type: none"> <li>• Cycling safety identified as key concern in community engagement.</li> <li>• 3.3.3., 3.3.5. already identify future assessment of trail in Banfield Park for safety and visibility.</li> <li>• Trail conditions and possible improvements to be addressed through future Park Improvement Plan for Banfield Park. Action Plan changed from “Long Term” to “Medium Term” to reflect adjusted timeframes.</li> <li>• No further change recommended.</li> </ul>
<b>Delta Hotel connection:</b> <ul style="list-style-type: none"> <li>• Sign E &amp; N Trail better from the path behind the Delta Hotel</li> </ul>	<ul style="list-style-type: none"> <li>• Revise 3.3.16 to include wayfinding as part of Kimta Road Pilot Project.</li> </ul>
<b>E&amp;N Trail</b> <ul style="list-style-type: none"> <li>• Sherk Street E&amp;N connection should be north-south, not east west.</li> <li>• Provide details of when Sherk Street connection to E&amp;N Trail will be improved</li> <li>• The rail bed between the Round House and the bridge could host special events similar to street markets - add power outlets and places for stalls and good lighting</li> <li>• Lighting for walking/cycle path from Esquimalt Rd. to Wilson St. needed soon</li> </ul>	<ul style="list-style-type: none"> <li>• Revise Map 3 to show correct alignment of E&amp;N Trail connection at Sherk Street.</li> <li>• (NEW) Add connection to Sherk Street to Action Plan as medium term item</li> <li>• Community-led placemaking projects encouraged in plan (14.1) and existing City programs, with support of land owners.</li> <li>• Refer to CRD.</li> </ul>
<b>Galloping Goose:</b> <ul style="list-style-type: none"> <li>• Please add signage to the trails, especially where people enter the trail to warn about bicycles passing (e.g. bottom of stairs near Bay St Bridge)</li> <li>• Speed control for cyclists</li> <li>• Put ramp on/replace stairs down to Goose/Trestle from Arthur Currie Lane</li> <li>• Upgrade Trestle Bridge.</li> <li>• Widen trail.</li> <li>• Add a speed bump or other indication for cyclists to slow down at bottom of Bay Street stairs for pedestrian safety</li> <li>• Add garbage cans.</li> </ul>	<ul style="list-style-type: none"> <li>• Operational concerns regarding Galloping Goose Regional Trail to Parks, Recreation and Culture CRD for future trail improvement planning</li> <li>• Maps 3 and 4 revised to show likely connection from Raynor Avenue to the Galloping Goose via Tyee Road rather than the existing stairs.</li> <li>• Staff reviewed garbage can placement in response to suggestion. Supply and placement is adequate.</li> </ul>



<b>Hereward Park:</b> <ul style="list-style-type: none"> <li>Connecting Hereward to E&amp;N Rail Trail on the bike is very awkward. A better path up that park would be a great help.</li> <li>Access from Hereward onto E&amp;N needs improvement shortly, not in 10-15 years.</li> </ul>	<ul style="list-style-type: none"> <li>3.3.6. Hereward Street to be assessed in future for cycling and pedestrian improvements, to include crossings.</li> <li>Informal path in Hereward Park is on BC Hydro and Township of Esquimalt property. Neither have indicated plans to upgrade path.</li> </ul>
<b>Pioneer Co-op:</b> <ul style="list-style-type: none"> <li>Consider formalizing mid-block connectivity through Pioneer Co-op</li> </ul>	<ul style="list-style-type: none"> <li>No change recommended.</li> </ul> <p>Formal connection identified as desirable in plan (6.4.4.); to be secured through redevelopment. Decision to make access public in meantime would be up to Co-op.</p>
<b>Selkirk Trail (future)</b> <ul style="list-style-type: none"> <li>Support for future trail on waterfront instead of Selkirk Ave.</li> </ul>	<ul style="list-style-type: none"> <li>No change recommended.</li> </ul> <p>Potential for waterfront trail to be explored through future planning for Arm Street and Burleith Parks (4.2.2). Additional work needed for habitat assessment, consultation and comprehensive planning for this area.</p>
<b>Tyee Co-op:</b> <ul style="list-style-type: none"> <li>Concern that Tyee Cooperative pathway is not public, although plan shows it as a connection. Some recent near-miss incidents.</li> </ul>	<ul style="list-style-type: none"> <li>Re-word 3.3.15 to clarify that Tyee pathway is not currently public path but that a future public path is desired there through any redevelopment of the site.</li> <li>Change transportation map improvement for Tyee Co-op 3.3.15 to show as “potential improvement”.</li> </ul>
<b>Vic West Park:</b> <ul style="list-style-type: none"> <li>Path between apartment at 55 Bay Street and Vic West Lawn Bowling need lights and better drainage</li> <li>Ensure lighting for safety along Wilson St. at Westside Village</li> <li>Show trail in Vic West park as greenway</li> </ul>	<p>No change recommended</p> <ul style="list-style-type: none"> <li>Staff to assess for lighting needs (operational issue)</li> <li>Plan shows greenways, cycling routes and AAA routes collectively as “active transportation routes”</li> </ul>
<b>Vic West School Trails:</b> <ul style="list-style-type: none"> <li>Concern with Vic West Elementary trails across playing field.</li> <li>Concern with public access during school hour</li> <li></li> </ul>	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>Many existing examples of public using trails in other school yards, with management options (e.g. after hours only; fencing).</li> </ul>
<b>Westsong Way:</b> <ul style="list-style-type: none"> <li>Concern about people biking on Westsong walkway from the bridge to head St. Refresh the signage.</li> <li>Allow cycling over long term on Westsong Walkway</li> <li>Do not allow cycling on Westsong Walkway</li> </ul>	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>Signage concern referred to Engineering, Parks and Public Works – operational issue.</li> <li>Expanding walkway to accommodate cyclists would be challenging due to ecological concerns and narrow space. Parallel routes proposed along Kimta/Esquimalt Road.</li> </ul>
<b>Water Lots along Westsong Way:</b> <ul style="list-style-type: none"> <li>Concern about future development of existing privately held water lots</li> <li>Concern about maintaining secured waterfront public access at the Victoria International Marina</li> </ul>	<ul style="list-style-type: none"> <li>Added Section 4.3 providing principles for consideration by the Harbourmaster in the case of an application for works on a water lot and/or by the City in case of a request for access to a water lot across City lands.</li> <li>Recommend revision of the Victoria Harbour Plan concurrent with the Victoria West Neighbourhood Plan in order to reflect these additional principles. The Harbour Plan did not recognize the existence of water lots west of the Songhees area.</li> <li>Added existing waterfront access to Map 3: Existing and future waterfront pedestrian routes.</li> </ul>
<b>Parking</b>	
<ul style="list-style-type: none"> <li>Designate residential only parking on Catherine near Skinner</li> <li>Change parking to residential in the area between Russell and Catherine, Esquimalt, Lime Point</li> <li>Suffolk St. parking is a concern</li> <li>Bowlsley St. has no parking; those residents park on Suffolk</li> </ul>	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>Residents can request Residential-Only Parking areas through existing City program.</li> </ul>

<p>Different perspectives on parking (e.g.):</p> <ul style="list-style-type: none"> <li>• Numerous concerns regarding traffic impacts that may come from new development</li> <li>• Ensure adequate parking is offered for homes and businesses. Even if people are biking more and more, most households still keep a car for grocery shopping, ferrying the kids, or going out of town.</li> <li>• Some feel parking requirements for new developments should be reduced. Car ownership is falling and will continue to decline.</li> </ul>	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>• Plan tries to strike balance between current demand and shifting to less car ownership/ more transportation alternatives in the future.</li> <li>• Parking requirements regulated by zoning bylaw which has been updated in 2017 based on recent parking occupancy study; owners can apply for variances, which are considered on case-by-case basis.</li> </ul>
<b>Parks</b>	
<b>General</b>	
Photos – add photos with people, add photo of Gorge Waterway (Arm Street Park photo is in Esquimalt)	<ul style="list-style-type: none"> <li>• Add or replace photos in Chapter 4: Parks to show people</li> <li>• New photo to show Gorge Waterway.</li> </ul>
Add introduction to parks section and urban forest sections Recognize urban forest master plan	<ul style="list-style-type: none"> <li>• (NEW) preamble added to Parks, Open Space and Waterways section (Chapter 4) and Urban Forest Sub-Section</li> </ul>
Incorporate accessibility in parks for persons with disabilities. Ask for input from people with disabilities	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>• Accessibility is a guiding principle in the Parks and Open Space Master Plan and will be an important consideration for future park improvements. Specific details to be developed through individual park improvement plans.</li> </ul>
Suggestions for any park space in neighbourhood include water park, interactive art installations	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>• 4.5.6 and 14.1.2. propose public and interactive art in parks.</li> <li>• Parks and Open Space Master Plan suggests water features are a higher priority in non-waterfront neighbourhoods.</li> </ul>
Continue to remove invasive species and plant native species in Banfield, Burleigh, and other parks	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>• Operational issue – referred to Parks, Recreation and Facilities</li> </ul>
Need more/improved play places for families with kids including playground improvements in Barnard Park.	<ul style="list-style-type: none"> <li>• Need and timing for playground upgrades identified outside neighbourhood plan, through capital planning.</li> </ul>
Desire for park improvement plans for Arm Street, Burleigh and Banfield Park to be done in short-term	<ul style="list-style-type: none"> <li>• Banfield Park improvement plan moved to medium term to reflect adjusted timeframes</li> <li>• Park planning timelines are established with consideration for park planning needs and priorities across the city. No further change recommended.</li> </ul>
Need summary of Bayview parks Add images to show planned parks for Railyards and Dockside.	<ul style="list-style-type: none"> <li>• (NEW) added Map 12 to show parks planned for new areas, with details p 87- 89.</li> <li>• Reorganized Bayview Lands and Dockside parks policies to be consecutive (4.9 and 4.10).</li> </ul>
Public washrooms requested for Songhees walkway and Banfield park	<p>No change recommended</p> <ul style="list-style-type: none"> <li>• Visitor facilities to be identified through individual park planning and/or Parks and Open Spaces Master Plan implementation.</li> </ul>
Order is confusing for park improvements	Re-order 4.6 – 4.16. to be more coherent
<b>Alston Green</b>	
Establish new community garden space at Alston Green.	(NEW) Identified as possible location for community gardens in 12.2.1.b. Community can approach City with proposal, under Community Gardens policy.

<b>Banfield Park</b>	
Fence in the green space around the playground in Banfield park. Stop kids from running onto the bike path or towards the bluff edge.	<p>No change recommended</p> <ul style="list-style-type: none"> <li>• Suggestions referred to Parks, Recreation and Facilities for future consideration. Specific park features to be determined through future park improvement planning for Banfield Park.</li> </ul>
Mixed opinions on paving trails in Banfield Park. Mode separation desired given common concern re: safety problems with cyclists and pedestrians sharing the path and potential for pavement to increase speeds.	<p>No change recommended</p> <ul style="list-style-type: none"> <li>• Trail improvements to be evaluated as part of future park planning for Banfield Park. Improvements need to balance safety with ecological goals.</li> </ul>
<ul style="list-style-type: none"> <li>• Desire for a beach that provides a more accessible (kids and seniors) and swimmable shoreline. Some suggested this for Banfield park.</li> <li>• Better beach access identified as number one priority at swim event, followed by expanded dock</li> </ul>	<ul style="list-style-type: none"> <li>• Revised Banfield Park Policy 4.8.b. (new numbering) to explore opportunity for improved beach access.</li> </ul>
<b>Barnard Park</b>	
Consider putting a bathroom in Barnard park, since it is already being used for that even without a facility	<p>No change recommended</p> <ul style="list-style-type: none"> <li>• No plans for additional washroom at this time – staff to monitor</li> </ul>
Need a doggy bag dispenser in Barnard park.	<p>No change recommended</p> <ul style="list-style-type: none"> <li>• Dispenser recently installed</li> </ul>
<b>Bayview Place and Roundhouse</b>	
Include reference to Turntable Plaza or more generally to new open space and Lime Bay Park improvements at Roundhouse	<ul style="list-style-type: none"> <li>• Revised under newly numbered policy 4.9 Bayview Place and Roundhouse</li> </ul>
<b>Docks</b>	
Strong support in survey and events for more swimming docks Concern that plan puts onus on community to establish docks – reword as a partnership	<ul style="list-style-type: none"> <li>• Revise policy 4.2.4 to “support community-led efforts in partnership with the City”</li> <li>• Dock may also be considered in more detail through park improvement planning for individual parks.</li> </ul>
Suggestions that the City fund swim docks like playground infrastructure, or funded by developers.	<ul style="list-style-type: none"> <li>• Other funding sources are encouraged.</li> <li>• (NEW) Chapter on Neighbourhood Improvements and Amenities identifies waterfront access as key neighbourhood priority (15.1).</li> </ul>
Many suggested small (non-motorized) boat launches, docks, and storage facilities for many of the waterfront parks  Support for establishing swim docks for many of the waterfront parks	<ul style="list-style-type: none"> <li>• 4.2.4. already identifies potential locations for future docks, launches.</li> <li>• (NEW) section 4.3 identifies possible water lot uses which could support non-motorized recreation.</li> </ul>
<b>Island at End of Robert Street</b>	
Please mark Island at south end of Robert Street as a “no go” space out of respect for First Nation use and history Protect islands as both a burial island and a sensitive ecological area. On-going erosion from human use.	No change recommended at this time. Staff will monitor.
<b>Commercial uses</b>	

Allow some trail-side stands to sell produce Some suggest allowing commercial vendors along trails. Others cautioned not to.	No change recommended. <ul style="list-style-type: none"> <li>To be addressed through implementation of mobile vending bylaw, which identifies exploring limited commercial uses in some locations.</li> </ul>
<b>Dogs in Parks</b>	
Opinions mixed on whether or not there should be an off-leash dog area in Banfield Park	No change recommended. <ul style="list-style-type: none"> <li>Upcoming City-wide Dogs in Parks Strategy will review off-leash areas and needed changes. No change recommended.</li> </ul>
Fence in the grassy off leash dog park at Vic West park fenced in. Grass is more accessible than gravel for physically impaired pet owners.	No change recommended. <ul style="list-style-type: none"> <li>Split rail fence recently installed. Staff are monitoring.</li> </ul>
<b>E&amp;N Rail Trail</b>	
Get rid of East and North rails and create a linear park Support the revitalization of the train for commuting and tourism	No change recommended. <ul style="list-style-type: none"> <li>E&amp;N Rail corridor identified as future transportation corridor (3.11.3). Stakeholder groups/ businesses currently exploring options.</li> </ul>
<b>Hereward Park</b>	
Access from Hereward onto E & N needs improvement shortly, not in 10-15 years.	<ul style="list-style-type: none"> <li>Access would be a partnership with BC Hydro and CRD. To be explored as opportunities arise.</li> </ul>
Improve Hereward Orchard by planting young trees, added benches, etc. and adding another waste receptacle.	No change recommended. <ul style="list-style-type: none"> <li>Already identified in draft plan as potential community orchard site (12.2.2)</li> <li>Community can approach City with proposal for community gardens or orchard improvement under Community Gardens Policy.</li> </ul>
<b>Railyards Park</b>	
Animate parks by Railyards – beautiful space with very little going on, not many people using it.	No change recommended <ul style="list-style-type: none"> <li>Neighbourhood plan encourages use of parks for events, placemaking and other community-led initiatives ( 4.5.6)</li> <li>Completion of the Railyards development will include additional housing, an additional park/playlot and pedestrian and bicycle connections from the Skinner/Bay Tyee intersection to the Galloping Goose, encouraging use of open spaces here.</li> </ul>
<b>Rainbow Park</b>	
Designate Rainbow Park permanently (prior plans were for road to connect Robert Street to Russell Street) Questions regarding plan to buy or renew lease of the Park? Please clarify in plan.	<ul style="list-style-type: none"> <li>Various maps have been updated to show the entire park, while also showing the existing right of way.</li> <li>Any changes to Rainbow Park Right of Way may have implications for future planning of E&amp;N trail. Future planning process for design of the E&amp;N Rail Trail from William Street to the Johnson Street Bridge to be undertaken in partnership with CRD.</li> </ul>
Picnic tables and community garden in Rainbow Park – better maintenance of park	No change recommended <ul style="list-style-type: none"> <li>Community can approach City with proposal for community gardens under Community Gardens Policy.</li> <li>Request for picnic tables referred to Parks, Recreation and Facilities (operational)</li> </ul>
<b>Milne Street green space</b>	



Develop pathway on Milne Street right of way to connect Catherine Street with Mary Street. Overgrown. Boulder is part of public land.	No change recommended <ul style="list-style-type: none"> <li>Boulder is on public right of way. Grades are not suitable for formal public walkway.</li> <li>Lawn and vegetation control referred to Parks, Recreation and Facilities (operational issue).</li> </ul>
<b>Russell Street green space</b>	
Suggestion to protect green space on the block of Russell St. running north-south, between Henry St. and Wilson St.	<ul style="list-style-type: none"> <li>Referred to Parks, Recreation and Facilities staff for further assessment and consideration.</li> </ul>
<b>Triangle Park</b>	
Don't want orchard/garden in Triangle Park. Park is too small - a nice oasis for relaxing.	No change recommended <ul style="list-style-type: none"> <li>Community support on walk about for additional features to animate the park. More trees/gardens still compatible with opportunity to relax. No change recommended.</li> </ul>
<b>Vic West Park</b>	
Pave walkways through park from Esquimalt to Save On so we can skate to the park Repair cracked sidewalks Lights at the skate park	No change recommended <ul style="list-style-type: none"> <li>Lighting and pathway improvements coming (2017 – 2019) through implementation of Vic West Park management plan.</li> </ul>
Add water fountain	No change recommended <ul style="list-style-type: none"> <li>New water fountain at Skate Park.</li> </ul>
Concern regarding cars stopping on Esquimalt Road to pick up people using Skate Park Improve signage to Vic West Park access/drop-off points	<ul style="list-style-type: none"> <li>Revised 4.15.1 to add assessment of vehicle drop-off/parking conditions for Skate Park.</li> <li>(NEW) Add short-term action: Staff to assess need for pull-out to service Vic West Skate Park.</li> </ul>
Take down some trees to make the space more open and inviting for park and parking lot	No change recommended <ul style="list-style-type: none"> <li>Goal is to preserve trees in parks unless there is a safety or infrastructure concern.</li> </ul>
<b>Urban Forest</b>	
Require large street trees in new developments. Desire for more trees and shrubs Concern about boulevards that impede the view of drivers, cyclists, and pedestrians	<ul style="list-style-type: none"> <li>Revised 4.18.4 to supports street tree in other multi-unit development.</li> <li>(NEW) Proposed Design Guidelines for Townhouses and Attached Housing include consideration of planting spaces and a variety of trees on-site, and support for canopy street trees.</li> </ul>
Develop a Urban Forest/Street Tree sub Plan for Vic West to propose strategy for overall tree canopy in the neighbourhood while identifying which sub-neighbourhoods can be improved with more consistently laid-out trees. The city still needs a comprehensive Boulevard plan for all.	No change recommended. <ul style="list-style-type: none"> <li>Additional urban forest initiatives to be undertaken at city-wide level, through implementation of Urban Forest Master Plan, including canopy targets for different parts of city, street tree programs, etc.</li> </ul>
<b>Gorge and Harbour</b>	
Identify clearly the historic Victoria Harbour Migratory Bird Sanctuary and provide background Identify significant species and invasive threats	<ul style="list-style-type: none"> <li>(NEW) Add Migratory Bird Sanctuary to Map 7 (Parks and Open Space Network)</li> </ul>
<ul style="list-style-type: none"> <li>Strong but mixed opinions about removing, engaging with, or supporting those living on boats moored in Gorge</li> <li>Remove derelict boats</li> </ul>	No change recommended. <ul style="list-style-type: none"> <li>Outside scope of neighbourhood plan. Being addressed through other City initiatives.</li> <li>Future moorage not contemplated in Gorge due to environmental concerns.</li> </ul>

<ul style="list-style-type: none"> <li>Consider putting boat buoys in the Gorge and renting them out to generate revenue and clean up the current situation.</li> </ul>	
<p>Consider Green Shores guidelines and certification for waterfront development</p> <p>Concern about sea level rise in Vic West</p>	<ul style="list-style-type: none"> <li>(NEW) Policy 4.1.5. Investigate incorporating Green Shores principles for ecologically-sensitive shoreline development in waterfront parks.</li> </ul>
<p>Remove smelly seaweed to eliminate the foul odours, particularly between Maitland Road and Esquimalt boundary.</p>	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>Seaweed is important part of shoreline ecosystem. No further action recommended.</li> </ul>
<p>The bank area at Burleith park should be reinforced to stop the bank erosion.</p>	<ul style="list-style-type: none"> <li>(NEW) Added direction to look at shoreline ecosystem health through park planning for Burleith Park (4.6.1.)</li> <li>Management strategies to be addressed through future park improvement planning for Burleith Park (long term action).</li> </ul>
<ul style="list-style-type: none"> <li>Lighting and signage to feature wildlife and history- foot of Mary Street it's possible to build tide pools - there are a few natural ones there and our daughter loved watching the sea life in them</li> <li>Need more thorough acknowledgement of "naturehood," bird sanctuary and migration routes in Vic West</li> <li>Recognize eel grass and other sensitive Gorge ecosystems</li> <li>Identify significant species and invasive threats</li> </ul>	<ul style="list-style-type: none"> <li>(NEW) Add Migratory Bird Sanctuary to Map 7 (Parks and Open Space Network)</li> <li>(NEW) Added direction to look at shoreline ecosystem health through park planning for Burleith Park (4.6.1)</li> <li>Refer to Parks, Recreation and Facilities – interpretive signage can be addressed through individual project proposals or operations. No further change recommended.</li> </ul>
<p>Add DPA 8 Gorge Waterway to map</p>	<p>(NEW) Update Map 7 (Existing Parks) to indicate existing Environmental Development Permit Area 8.</p>
<p>Suggestion to add floating trash cans in the Gorge, as there are on Cowichan River.</p>	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>Discussed with Parks, Recreation and Facilities staff. Would create challenges for trash collection, as City does not have boat.</li> </ul>
<b>Environmental quality</b>	
<p>Ensure that improvements do not compromise ecological integrity</p>	<p>General comment. No changes.</p>
<p>Alston boulevard needs restoration</p>	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>Identified for future improvement through redevelopment, and Pedestrian Master Plan.</li> <li>(NEW) Figure 36 for Alston-Bay-Tyee development indicates desired cross-section with boulevards, street trees and sidewalks</li> </ul>
<p>Invasive vegetation has taken over from Rainbow park to Esquimalt</p>	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>Operational issue – referred to Parks, Recreation and Facilities.</li> </ul>
<b>Future Land Use map</b>	
<p>Future land Use Map – needs introduction/ more context</p>	<ul style="list-style-type: none"> <li>Add new introduction to Future Land Use Map, describing vision.</li> </ul>
<p>Throughout chapters 5-11: Avoid the use of the words “is/are supported” because the implementation of some policies on any site would be considered through rezoning process which considers a number of factors and site context.</p>	<ul style="list-style-type: none"> <li>Replaced “supported” in some places with “may be considered” or similar language</li> </ul>
<b>Future population growth/ density - general</b>	

<ul style="list-style-type: none"> <li>• General support in survey/ open houses for plan's direction for where and how to accommodate future growth.</li> <li>• Others had concerns: <ul style="list-style-type: none"> <li>○ Concern that too much new housing being added to neighbourhood overall and/or traditional residential areas, given OCP intent to protect character of traditional residential areas and to direct 90% of pop increase in core areas and large urban villages and 10% in traditional residential areas.</li> <li>○ Concerns expressed about too much change, too much height and density. Concern about impacts on future traffic (particularly at urban villages)</li> <li>○ Concern that there is a mismatch between what we heard in early engagement and what the plan addresses</li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>• Revisions made to plan to reduce envisioned density in certain areas in response to public input. These changes are detailed in the Traditional Residential Areas section, below. <ul style="list-style-type: none"> <li>○ reducing the area proposed for Urban Residential development south of Esquimalt Road;</li> <li>○ removing Skinner Street and Suffolk Street as areas where 3-storey apartment buildings may be considered;</li> <li>○ further reducing the areas where townhouses in two rows are supported, and increasing the minimum lot area suitable for this housing type;</li> <li>○ increasing the minimum lot size for a duplex with suites from 500m<sup>2</sup> to 555m<sup>2</sup></li> <li>○ increasing the minimum lot size considered for small lot house zoning</li> <li>○ clarifying policies encouraging the retention and adaptive re-use of heritage buildings rather than new development</li> </ul> </li> <li>• The proposed policies are meant to provide opportunities for incremental development over 25 years considering: <ul style="list-style-type: none"> <li>○ Community engagement showed strong support for more housing diversity and for most proposed housing policies.</li> <li>○ New development within Traditional Residential Areas is focused in part within close walking distance of large urban villages - areas where the OCP anticipated 40% of residential growth - and along frequent transit routes, with less development envisioned in certain areas (e.g. "Northwest sub-area")</li> <li>○ New housing types proposed in plan unlikely to result in rapid population growth in traditional residential or urban residential areas, due to land values and limits on height/lot sizes. New development will be monitored, and policies can be adjusted if growth rate is faster than anticipated.</li> <li>○ Draft Plan reduces potential for certain types of housing (e.g. townhouses) by limiting areas and density (the Official Community Plan currently supports them throughout Vic West)</li> </ul> </li> </ul>
<p>Other areas/neighbourhoods could accommodate more growth. Recent development in Vic West has accommodated enough growth for this area. Slow development to let the neighbourhood integrate new development and settle.</p>	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>• All neighbourhoods are projected to increase in population over next 20-25 years. Increase in housing in traditional residential/urban residential areas expected to be incremental due to land values and restrictions on lot sizes/ heights/ location of types of development.</li> </ul>
<p>Some people expressed desire for more quantitative information:</p> <ul style="list-style-type: none"> <li>• Want to see more clear visualization of what full buildout could look like (both of villages and neighbourhood as whole)</li> <li>• Add population projections for Vic West to plan</li> <li>• Desire for capacity analysis</li> <li>• Add specific growth projections for different land uses</li> <li>• Add numbers anticipated for different Master Planned areas</li> <li>• Add targets for population and housing types into overview</li> </ul>	<ul style="list-style-type: none"> <li>• A visualization model is more suitable to master planned sites or greenfield development. It is difficult to accurately produce a model for traditional/ urban residential areas, as redevelopment of individual properties depends on many different variables.</li> <li>• Information on population projections/master planned areas/ land uses provided as background information for plan. Difficult to make accurate predictions.</li> <li>• Chapter 5: New introductory sentences to overview describing where future population will be concentrated (along corridors, villages).</li> </ul>
<p>Address overcapacity school in light of potentially more people here.</p>	<p>Revised plan to be referred to School District 61 for review. Population growth under age 18 expected to be incremental</p>
<ul style="list-style-type: none"> <li>• Concern about ability of infrastructure, particularly bridges and roads, to handle future population increase.</li> <li>• Suggestion that infrastructure improvements should be completed (e.g. Bay Street Bridge) before new development</li> </ul>	<p>2004 transportation studies/modelling indicate sufficient road capacity based on full build out. Bay Street upgrades for pedestrian/cycling improvements planned for medium term (5 – 10 years).</p>

### Amenities in new development – General

Require accessibility (e.g. wheelchair accessibility) in portion of new units.	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>Individual property owners/ developers encouraged to add features to developments that meet the needs of future residents. Accessibility considerations now highlighted in all reports to City Council, including for new developments.</li> <li>Provincial building code addresses accessibility.</li> <li>Proposed guidelines for Townhouses and Attached Units include guideline 5.a. for accessibility.</li> </ul>
Ensure more tax revenues from new development in Vic West (e.g. Roundhouse) stays in Vic West	<ul style="list-style-type: none"> <li>Current City approach is that all tax revenue goes to general revenues, to be allocated across City as needed.</li> <li>Public amenity contributions from development would directed to Vic West.</li> <li>(NEW) The addition of Chapter 15, Neighbourhood Amenities and Improvements, provides an overview of the preferred improvements.</li> </ul>
<b>Noise- General</b>	
<ul style="list-style-type: none"> <li>Suggestion of a noise curfew if waterfront infrastructure is added</li> <li>Assess and mitigate noise from airport and other waterfront industries</li> <li>Should not build at Lime Point until airport situation fully declared with noise exposure forecast</li> <li>Concern about potential noise if train service is very frequent</li> </ul>	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>Hours of operation from industrial/commercial properties regulated by business licences.</li> <li>New development in close proximity to the working harbour are encouraged to consider noise attenuation measures, and ensure that new residents are aware of potential noise impacts.</li> </ul>
<b>Traditional Residential Areas</b>	
<b>Overall density, growth and development</b>	
<ul style="list-style-type: none"> <li>Concern with developer driven assembly and redevelopment incented by townhouses in traditional residential areas</li> <li>Mixed opinions on need for increasing density of housing in traditional residential area</li> <li>Some concerned these housing types would lead to densely packed housing in the area</li> <li>Others feel the small lot houses and more suites is a good way to add invisible density</li> <li>Community engagement showed strong support for more housing diversity. Strong support for most housing policies.</li> </ul>	<p>Some changes to this section of the plan, which are detailed in subsequent comments below. and summarized here:</p> <ul style="list-style-type: none"> <li>Revised Maps 10, 11 Maintain existing Traditional Residential designation on Robert Street (draft plan had proposed Urban Residential development here) and limits to 3 storeys.</li> <li>(NEW) New policy 6.5 removes 3-storey apartment buildings as an option on Skinner Street and Suffolk Street</li> <li>(NEW) New policy 6.5 reduces the area where townhouses in 2 rows are supported (by excluding from the North Catherine Street area) and revised policy 6.13 (new numbering) makes it harder to build this housing type by requiring a larger lot size</li> <li>Revised policy 6.14 (new numbering) increases the size of lot considered for duplexes with suites slightly from 500m<sup>2</sup> (5,500 sq. ft.) to 6,000 sq. ft. (555m<sup>2</sup>)</li> <li>Revised Policy 6.15 (renumbered) to remove consideration</li> <li>Revised Policy 6.16 (new numbering) proposes a minimum lot size of 220m<sup>2</sup> (approx. 2,400 sq. ft.) for consideration of small lot rezoning. (Draft Plan proposed 180m<sup>2</sup> (2,000 sq. ft.))</li> <li>Revised Policy 6.16. softens the language discouraging small lot rezoning as an option east of Russell Street, while still maintaining a preference for other housing types.</li> <li>Revised Section 6.8. clarifying policies encouraging the retention and adaptive re-use of heritage buildings rather than new development</li> </ul> <p>Plan maintains a range of options for infill housing based on the following rationale:</p> <ul style="list-style-type: none"> <li>Community engagement showed strong support for more housing diversity. Strong support for most housing policies.</li> <li>Development pressure likely to continue in all City neighbourhoods.</li> </ul>



	<ul style="list-style-type: none"> <li>Plan's housing policies aim for incremental change in the residential areas. New housing types proposed in plan unlikely to result in rapid population growth in traditional residential or urban residential areas, due to land values and limits on height/lot sizes.</li> <li>New development will be monitored, and policies can be adjusted if growth rate is faster than anticipated.</li> </ul>
Why does more population need to be accommodated in Vic West / especially in Traditional Residential Areas? Noting, OCP intent to protect character of traditional residential areas and to direct 90% of pop increase in core areas and large urban villages and 10% in traditional residential areas.	<ul style="list-style-type: none"> <li>Development pressure likely to continue in all City neighbourhoods. Role of neighbourhood plan is to identify what kind of development is desirable, if a property owner chooses to redevelop their site.</li> <li>Neighbourhood plan policies direct majority of growth core area, corridors and within walking distance of large urban villages. Due to new limits on lot sizes, density and geographic distribution, new neighbourhood plan policies direct proportionally less growth traditional residential areas than in current OCP.</li> <li>Plan's housing policies aim for incremental change in the residential areas. New housing types proposed in plan unlikely to result in rapid population growth in traditional residential or urban residential areas, due to land values and limits on height/lot sizes. New development will be monitored, and policies can be adjusted if growth rate is faster than anticipated.</li> </ul>
Allow incremental infill (multiplex, duplexes), but don't want to see big changes all at once.	<ul style="list-style-type: none"> <li>Pace of change in traditional residential areas expected to be gradual due to land values and restrictions on heights/ lot sizes/ boundaries of different housing types.</li> <li>Development will be monitored, and policies can be adjusted in case of rapid changes.</li> </ul>
<b>Clarity</b>	
Difficult to understand which housing types are appropriate for which area	<p>The Traditional Residential Areas section has been revised for added clarity, as follows:</p> <ul style="list-style-type: none"> <li>Inclusion of a revised Map 11 (formerly Map 8) and the addition of a table of housing types, density and building height which may be considered.</li> <li>A greater number of named and numbered sub-areas to reflect the different contexts and policies in the Traditional Residential Areas</li> <li>Key provisions of each sub-area (housing types considered, density, and height) remain the same, excepting changes identified in this document.</li> </ul>
Avoid rounding metric measurements where it makes policies out of line with existing zoning or lot sizes found in Victoria (lots were platted in Imperial measures.) Provide lot dimensions in both metric and feet (many people more easily identify lot width/size by feet/square feet)	<p>Minor changes to lot requirements as follows:</p> <ul style="list-style-type: none"> <li>Houseplexes on corner lots: change from 550m2 to 555m2 (6,000 sq. ft.)</li> <li>Lot depth for townhouses in two rows: change from 40m to 39.5m (130 ft.)</li> </ul>
Update images to better reflect policies, remove "placeholders"	<ul style="list-style-type: none"> <li>Added new Fig. 15 showing an example of a house converted to commercial use</li> <li>Removed Figure 16 (as numbered in Draft Plan) which shows townhouses in two rows on a single lot</li> <li>Added new Fig. 19 showing a townhouse option with no front-accessed parking</li> <li>Added Fig. 24 (new numbering) showing conceptual site organization and parking options for duplexes; removed Fig. 18, 19 (Draft plan numbering) which were photographs of duplexes. (Fig. 18 was redundant with Fig. 17).</li> <li>Added Fig. 30 (new numbering) showing actual small-lot houses in Vic West on lots aligning with the revised policy 6.16.</li> </ul>
<b>Boundaries of Traditional Residential Areas</b>	
<ul style="list-style-type: none"> <li>Please consider Area 2 to continue west along Craigflower to Alderman – one lot deep. All are large lots with duplexes near the end of their lifecycles</li> <li>Extend Trad Res Area 1 to Craigflower between Russell and Alderman.</li> </ul>	<p>Limited changes recommended:</p> <ul style="list-style-type: none"> <li>Three largest lots between Belton Avenue and Alderman Road have been included on revised Map 11 (formerly Map 8) in the "General Areas" where townhouses in two rows may be considered, as these lots exceed the dimensions for this housing type, and topography and lot pattern provide transition to adjacent lots.</li> </ul>

	<ul style="list-style-type: none"> <li>Other parts of Craigflower Road remain in the Northwest Sub-area, but new Policy 6.5 and revised Policy 6.15 supports houseplexes of up to 4 units along Craigflower Road and nearby and Styles Street, rather 3 where lots are of sufficient size (currently few lots meet this requirement).</li> </ul> <p>Rationale:</p> <ul style="list-style-type: none"> <li>Community engagement indicated support for maintaining traditional residential character of this area.</li> </ul>
Do not agree with breaking up Langford St. – one side Area [3], the other side Area 2. Leave the remainder of Langford Area [3].	<p>No change recommended</p> <ul style="list-style-type: none"> <li>Map 11 (formerly Map 8) and new policy 6.5. rename these areas as “Northwest Sub-area” (north of Langford Street) and “General Areas” (South of Langford Street).</li> <li>The north and south sides of the street have different characteristics, with more character houses located on the north side of the street.</li> <li>For lots in their current configuration, there are minimal policy differences between these two sub-areas.</li> </ul>
Difficult to understand which housing types are appropriate for which area	<p>The Traditional Residential Areas section has been revised for added clarity, as follows:</p> <ul style="list-style-type: none"> <li>Inclusion of a revised Map 11 (formerly Map 8) and the addition of a table of housing types, density and building height which may be considered.</li> <li>A greater number of named and numbered sub-areas to reflect the different contexts and policies in the Traditional Residential Areas</li> <li>Key provisions of each sub-area (housing types considered, density, and height) remain the same, excepting changes identified in this document.</li> </ul>
<ul style="list-style-type: none"> <li>Suggestion to restrict heights of new buildings to what is there now.</li> <li>Concern that accommodating infill development and conserving green space is better supported with 3 storeys of living space.</li> <li>Differing opinions on preference for peaked roofs.</li> </ul>	<ul style="list-style-type: none"> <li>Where 3 storey buildings remain supported (Esquimalt Road, south of Esquimalt Road, adjacent to Catherine Street at Edward Small Urban Village), strengthen language that buildings should transition sensitively to adjacent lower-scale development (Policy 6.6.1.b., was 6.15.1.)</li> <li>Revised policy 6.6.1. (renumbered) reduces the height supported along Skinner Street to 9 metres and 2.5 storeys (with or without basement) rather than the 3 storeys supported in the Draft Plan, and encourages more sensitive transitions to adjacent properties and peaked roof forms. (Newly numbered Policy 6.6.1, was 6.1.5.1)</li> <li>Revised policy 6.6.1. (renumbered3d) clarifies that buildings of 2 – 2.5 storeys are supported elsewhere in Traditional Residential Areas, with 2.5 storeys and additional measured height supported only where a half-storey is contained in a peaked roof.</li> </ul>
<b>Townhouses, Houseplexes, Duplexes, Triplexes, Small Lots Houses</b>	
<ul style="list-style-type: none"> <li>Concerns expressed about townhouses, houseplexes, and small lot homes having potential to incentivize lot assembly and large scale demolition and redevelopment.</li> <li>Desire to avoid lot assembly, larger scale demolition and redevelopment. Smaller/lot scale redevelopment preferred.]</li> <li>Concern that affordability will decline as single family lots are marketed for development potential</li> </ul>	<ul style="list-style-type: none"> <li>Remove townhouses in two rows as an option in the newly identified North Catherine Street area (Policy 6.5, Policy 6.12)</li> <li>Increase the minimum size of lot considered for small lot rezoning (Policy 6.16)</li> <li>In some cases (townhouses in 2 rows, in those areas where location and lot depth permit), requiring a minimum lot size supports a sensitive design, on-site open space and transitions to neighbouring properties, while making this development type more difficult, and therefore encouraging a slower pace of changeThe OCP and previous Vic West Plan (1998) supported townhouses throughout Vic West, but little land assembly.</li> <li>Economic analysis for other neighbourhoods has shown that development of townhouses is about as economically viable as other uses, including continued use as single detached houses.</li> <li>The proposed plan reduces the density for townhouses in most areas, as compared to the Official Community Plan.</li> <li>Plain aims to support incremental, not rapid, redevelopment. If trends show unexpected rate of change, policies can be revisited.</li> <li>Enhanced policies to encourage the retention and reuse of houses of heritage merit (policies sections 6.8 [renumbered], 10.3).</li> </ul>

Desire expressed to avoid tracts of newly built houses - prefer new housing construction to appear more piecemeal and incremental.	<p>No change recommended</p> <ul style="list-style-type: none"> <li>Design guidelines would apply to all duplexes, triplexes, houseplexes and other new multi-unit housing required to following design guidelines with focus on new housing that fits with surroundings, and minimizing impacts on adjacent properties. Variability in design encouraged.</li> <li>Land values in Vic West unlikely to support rapid or large-scale redevelopment</li> <li>New development will be monitored, and if trends show unexpected rate of change, policies can be revisited</li> </ul>
The lot requirements for more than one row of townhouses are less than for a single row of townhouses. That does not make sense, multiple row house will even create more parking demands.	<ul style="list-style-type: none"> <li>Revise newly numbered Policy 6.12. to increase the lot size required for double row of townhouses to 30 metres (100 feet) to encourage site layouts with more green space and more sensitive transitions to surrounding development. This makes this development type more difficult.</li> </ul>
Concerns about unmaintained multi-unit houses - reference to existing fourplexes	<ul style="list-style-type: none"> <li>Outside scope of neighbourhood plan – existing complaints can be referred to Bylaw for follow up</li> </ul>
<b>Design &amp; Character</b>	
<ul style="list-style-type: none"> <li>Important that new housing (esp. townhouses) in all traditional residential areas are well designed and respect the context in which adjacent heritage homes exist.</li> <li>Concern that new development will destroy the character and feeling of established character houses.</li> </ul>	<ul style="list-style-type: none"> <li>(NEW) New policy 6.5 reduces the area where townhouses in 2 rows are supported (by excluding from the North Catherine Street area) and revised policy 6.13 (new numbering) makes it harder to build this housing type by requiring a larger lot size, for the purpose of supporting more sensitive site layout, building and open space design.</li> </ul> <p>No further change recommended.</p> <ul style="list-style-type: none"> <li>Design guidelines would apply for all new duplexes, triplexes, townhouses, houseplexes, multi-unit buildings (4+ units), commercial and industrial development. These guidelines focus on good design that fits with surroundings, and minimizing impacts on adjacent properties. Heritage or historic context of surrounding properties would be taken into account.</li> <li>New development permit guidelines are proposed for townhouse development in Vic West. These guidelines include requirements for compatibility of form and materials.</li> <li>The plan identifies an update to duplex development permit guidelines as a short-term action.</li> <li>Revised Sections 6.8, 10.3 contain enhanced policies to encourage the retention and reuse of houses of heritage merit.</li> </ul>
<ul style="list-style-type: none"> <li>Ensure open space is retained on lots, and not paved, to adequately serve functions of food growing, rain water permeation, habitat.</li> </ul>	<p>Some revisions made:</p> <ul style="list-style-type: none"> <li>For duplexes, townhouses in one row, and houseplexes, guidance for zoning has been revised to support 35% of the lot depth as a rear setback. (Renumbered policy 6.7.4.)</li> <li>Proposed Development Permit Area guidelines for Townhouses and Attached Housing include guidelines for addressing tree planting spaces on-site and in boulevards (Guidelines 5.c., d., e.), on-site landscaped areas (Sections 2.c., 5), and stormwater management on-site (Guideline 5.h.).</li> </ul> <p>Further changes not recommended. Rationale:</p> <ul style="list-style-type: none"> <li>Zoning sets minimum set-backs and open space requirements.</li> <li>Total number of units in some housing types (e.g. houseplexes) has been limited to reduce impacts of surface parking on open space.</li> <li>Homeowners are encouraged to install rainwater harvesting technologies (pervious pavement, etc.).</li> <li>Through city-wide processes, the City will be updating the tree preservation bylaw and related policies, and proposing stormwater requirements, in 2018.</li> <li>Through a City-wide process, the City will be considering further regulations for the treatment of stormwater on-site.</li> </ul>
Ensure neighbours' access to light and privacy is respected by new buildings.	<p>Some changes proposed:</p> <ul style="list-style-type: none"> <li>The proposed Revitalization Guidelines for Corridors, Villages and Town Centres contain enhanced guidance on sensitive transitions to lower density development.</li> <li>Draft townhouse guidelines for building separation have been enhanced (Guideline 2.b.)</li> </ul>

	<ul style="list-style-type: none"> <li>The building height anticipated by the plan has been reduced in some places (e.g. south of Esquimalt Road, per Map 11 and New Policy 6.6.) or additional policies for transition in scale has been added (revised policy 6.1.2). Further changes not recommended. Rationale:</li> <li>Design guidelines would apply for all new duplexes, triplexes, townhouses, houseplexes, multi-unit buildings (4+ units), commercial and industrial development. Focus is on good design that fits with surroundings, and minimizing impacts on adjacent properties.</li> <li>For single family homes, light, privacy and overlook regulated through zoning setbacks and maximum height.</li> </ul>
Preserve public views	<ul style="list-style-type: none"> <li>(NEW) New policies 6.1.5, 6.1.8., 8.5.6, and revised Maps 10, 15 indicate new development should respect the view of the Olympic Mountains from Catherine Street at Edward Street, and the view of the Sooke Hills from Catherine Street at Skinner Street. These view corridors have been included in the proposed content specific to Vic West in the Revitalization Guidelines for Corridors, Villages and Town Centres.</li> <li>The existing development permit guidelines for various areas (Songhees, Docksider, Roundhouse and Bayview lands) protect public view corridors in these areas.</li> </ul>
<ul style="list-style-type: none"> <li>Discourage flat roofs, preference for heritage style roofs</li> <li>Others interested in more progressive or modern designs which may or may not have a peaked or pitched roof.</li> </ul>	<ul style="list-style-type: none"> <li>City cannot regulate style of single detached housing outside of a Heritage Conservation Area. Public engagement did not indicate interest in additional Heritage Conservation Areas in Vic West.</li> <li>In Traditional Residential Areas, 6.6.1.c. (renumbered) already requires that a half-storey above the second storey be contained within a peaked roof in order to provide for massing sensitive to adjacent buildings.</li> <li>(NEW) Policy 6.6.1. (renumbered) has been revised to clarify the heights in storeys and metres which may be considered, to provide additional opportunity/incentive for infill housing types with a peaked roof.</li> <li>Development Permit Guidelines address compatibility of built form in buildings of 2 or more units. There are different ways of achieving compatibility within different architectural styles – style &amp; roof type is only one</li> <li>No further change recommended.</li> </ul>
<b>Parking demand &amp; traffic</b>	
<ul style="list-style-type: none"> <li>Many expressed concern about potential increases (or on-site costs) of parking demand. Some feel changes in technology and car ownership will negate or reverse parking demand in pace with intensification.</li> <li>Some feel parking requirements should be relaxed to encourage alternatives and in light of likely changes in car ownership and technological changes (e.g. car-share, self-driving vehicles).</li> </ul>	<p>No further changes recommended.</p> <ul style="list-style-type: none"> <li>In 2018, the City will be proposing an updated Zoning Bylaw Schedule C regarding on-site parking requirements, based on a study of parking occupancy at existing developments.</li> <li>Overall parking and traffic demand expected to decrease over life of plan due to decreasing car ownership, improved transit and other alternatives.</li> <li>Community's long-term desire for added housing choice (ownership + rental) was expressed as a priority over immediate concern for on-street parking. Pace of change is expected to be incremental.</li> <li>Parking requirements add significantly to cost of new commercial and multi-unit residential developments. Reduced parking requirements can help address overall housing cost.</li> <li>No further changes recommended.</li> </ul>
<b>Suites, renting, and cohousing</b>	
<p>Desire for introducing additional types of housing with rental suites beyond those proposed:</p> <ul style="list-style-type: none"> <li>Allow more than one legal suite in single family homes (not just duplexes)</li> <li>Support allowing suites even if house does not have heritage designation</li> <li>Garden suites behind duplexes should also be allowed</li> <li>More parking flexibility</li> <li>More total floor space</li> </ul>	<ul style="list-style-type: none"> <li>Revised policy 10.1. to add the option of a heritage house with two suites.</li> </ul> <p>No further changes recommended.</p> <ul style="list-style-type: none"> <li>Staff does not support a policy for garden suites behind duplexes at this time. Remove the option for garden suites behind houseplexes. (Flexibility may be considered in heritage properties)</li> <li>Garden suites are already supported for single detached homes in all Traditional Residential areas where they meet city-wide policies.</li> <li>Draft plan already supports 2 suites in single detached houses which are subject to heritage designation (Policy 10.3.1).</li> <li>Heritage policy (Policy Section 10.3) supports adaptive and creative re-use of buildings (10.4) which may support a variety of site designs.</li> </ul>



<ul style="list-style-type: none"> <li>Consider some case-by-case flexibility on site requirements for allowing additional rental units in traditional residential areas.</li> <li>Consider allowing Garden suites in Trad Res Area 1</li> </ul>	
Some suggest that suites should only be rentable in houses when owner-occupied.	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>The city-wide secondary suites policy has not included an intent to limit rental suites to owner-occupied houses.</li> <li>Would be difficult to regulate and enforce. No change recommended.</li> </ul>
Accommodate tiny houses (e.g. in driveways)	<p>No changes recommended.</p> <ul style="list-style-type: none"> <li>City-wide garden suite policy recently updated.</li> <li>Tiny houses in driveways would be regulated by City bylaws for recreational vehicles, not by zoning/land use controls.</li> </ul>
Encourage cohousing	<p>No changes recommended.</p> <ul style="list-style-type: none"> <li>Co-housing, where residents own individual homes and share amenities, could be suitable in a variety of multi-unit housing types proposed in the plan (e.g. apartment building, townhouses, rowhouses). Would be achievable under land use policies in proposed plan; no additional policies needed.</li> </ul>
Allow airbnb usage in suites	<p>No further changes recommended.</p> <ul style="list-style-type: none"> <li>Short-term vacation rentals being addressed at City-wide level.</li> </ul>
<b>Pioneer Cooperative Block</b>	
<ul style="list-style-type: none"> <li>Affordable housing role in any redevelopment should be prioritized.</li> <li>Pioneer Co-op – Replacement for retained units should be non-market (not “affordable”) and similar bedroom configuration – family friendly</li> </ul>	<ul style="list-style-type: none"> <li>Former policy 7.1.9. has been moved to become the first policy in the Pioneer Cooperative Special Planning Area sub-section (newly numbered policy 6.4.1.)</li> <li>This policy supports a similar number and floor space (rather than bedroom configuration) of affordable units. In a rezoning conferring density, the City would seek a housing agreement to secure the affordability level of units.</li> </ul>
Policy regarding community garden for the Pioneer Cooperative site is too prescriptive; the primary public benefit of this site is the provision of affordable housing.	<ul style="list-style-type: none"> <li>Newly numbered policy 7.1.6. (formerly 6.4.8.) has been softened to encourage opportunities for urban food production.</li> </ul>
Policy 7.1.5. regarding limited commercial or community-serving uses at Pioneer Housing Cooperative may be interpreted too broadly in the future. This policy is not evident on Map 9: Traditional Residential Areas.	<ul style="list-style-type: none"> <li>Newly numbered policy 6.4.6. (former policy 7.1.5.) clarified to support commercial services rather than commercial uses, specify a limited area, and that the primary use of the site be maintained for housing.</li> <li>New Policy 6.10.2. added in new sub-section 6.10: Limited Commercial Uses, limited commercial uses, to cross-reference to Pioneer Housing Cooperative. Limited commercial also included as a use in Policy 6.5 and new Figure 11: Traditional Residential Housing Types.</li> </ul>
The boundaries of this Special Planning Area include adjacent parcels which are not part of the cooperative property.	<ul style="list-style-type: none"> <li>Map 11: Traditional Residential Sub-Areas (formerly Map 9) has been revised to only include the coop property.</li> </ul>
<b>Skinner Street (Proposed apartments and 3-storey building height)</b>	
<ul style="list-style-type: none"> <li>Concerns about development of apartment buildings on Skinner (Traditional Residential Area negatively impacting character of area - particularly character and heritage houses</li> <li>Some opposed to tall buildings (i.e. 4 storeys) going up at Raynor and Skinner as you go up the hill – concern that they will overshadow duplex-zoned houses</li> </ul>	<p>Revisions to Skinner Street Traditional Residential Areas</p> <ul style="list-style-type: none"> <li>(NEW) New policy 6.5 removes 3-storey apartment buildings as an option in Traditional Residential areas of Skinner Street (formerly identified as “Sub-area 1” in the Draft Plan”).</li> <li>Revised policy 6.6.1. (renumbered) reduces the height supported along Skinner Street to 9 metres and 2.5 storeys (with or without basement) rather than the 3 storeys supported in the Draft Plan, and encourages more sensitive transitions to adjacent properties and peaked roof forms. (Newly numbered Policy 6.6.1, was 6.1.5.1)</li> <li>Revised policy 6.15.3. (renumbered) supports houseplexes with more than 4 units on Skinner Street where all design guidance can be met.</li> </ul>

<ul style="list-style-type: none"> <li>Some preference for houseplex and townhouses instead of apartments,</li> <li>Others supportive of higher buildings along Skinner Street.</li> <li>Conserving heritage is significant part of opposition to townhouses and apartments around Skinner and Catherine St.</li> <li>Consider additional policies for Skinner Street for properties adjacent to Heritage Conservation Area on Catherine Street</li> </ul>	<ul style="list-style-type: none"> <li>Proposed Revitalization Guidelines for Corridors, Villages and Town Centres for Vic West include guidelines for compatibility of scale and character with surrounding development and heritage buildings, and design which considers views of the Sooke Hills from Skinner Street and Catherine Street.</li> </ul> <p>Rationale:</p> <ul style="list-style-type: none"> <li>Skinner Street/ is frequent transit corridor; opportunity to add more housing units and diversity and promote housing that is less reliant on vehicles. Townhouses, houseplexes, triplexes and duplexes would still add new housing in a form that is more traditional residential in character.</li> </ul> <p>Revisions to Skinner Street Urban Residential Areas</p> <ul style="list-style-type: none"> <li>Revised Policy 6.1.2. (renumbered) specifies height which may be considered at Raynor</li> </ul>
<p><b>Suffolk Street (Proposed apartments and 3-storey building height)</b></p> <ul style="list-style-type: none"> <li>Concerns about development of apartment buildings on Suffolk (Traditional Residential Area 1) changing character of area</li> <li>Concern from surrounding residents about change in character. Topography and lot size are constraints on apartments.</li> <li>Some preference for houseplex and townhouses instead of apartments, some feel townhouses should not be supported in traditional residential areas either densification on Suffolk would mean more noise</li> <li>Concern about traffic impacts – some houses on Bowlsby do not have on-site parking; short-cutting is an issue</li> <li>Esquimalt/Suffolk Area – we want to preserve the little green, semi-private space that exists now between houses/houseplex</li> </ul> <p>Others support proposed changes.</p>	<ul style="list-style-type: none"> <li>(NEW) New Map 11 (formerly Map 9) and policy 6.5 removes 3-storey apartment buildings as an option in Traditional Residential area south of Suffolk Street (formerly identified as “Sub-area 1” in the Draft Plan”). This area is identified as part of the “General Area” in the proposed plan, supporting various infill housing types depending on lot size, with height of up to of 2 -2.5 storeys.</li> <li>Along Esquimalt Road, plan would support buildings up to 3 storeys (existing OCP policy for arterial roads)</li> </ul>
<p><b>Affordability</b></p>	
<ul style="list-style-type: none"> <li>Neighbourhood plan needs to help with affordable housing</li> <li>Need to better define affordability</li> <li>Interest in more affordable housing for middle income</li> <li>need family friendly affordable housing</li> </ul>	<ul style="list-style-type: none"> <li>Revise all policies referring to bonus density in Chapter 6 (Urban Residential section), Chapter 7 (Urban Villages), Chapter 8 (Employment Areas), to include reference to non-market housing, secured by a housing agreement for the life of the building, and affordable to households identified by the City’s Housing Strategy or other City-wide policy. This language replaces more general references to “Affordable Housing” in the Draft Plan.</li> <li>(NEW) Chapter 16 includes Affordable Housing as a desired Neighbourhood Improvements and Amenity.</li> <li>(NEW) Clarify glossary definitions to distinguish between affordable housing and non-market housing.</li> <li>No further changes recommended.</li> </ul> <p>Rationale:</p> <ul style="list-style-type: none"> <li>The neighbourhood plan contains policies to help support more affordable housing as well as ownership opportunities: new opportunities for mortgage-helping suites (to make home ownership more attainable and create rental housing), affordable housing through developer contributions in certain locations, policies to support revitalization of housing co-ops.</li> <li>Tools are limited to address affordable housing through neighbourhood plan; most initiatives to be achieved through City-wide projects and by other levels of government.</li> </ul>

Concern that affordability will decline as single family lots are marketed for development potential	<ul style="list-style-type: none"> <li>Land values make rapid redevelopment unlikely. Potential development on any one lot is limited by various policies on lot size, density, number of units, desired setbacks, and housing types supported in various sub-areas.</li> <li>Plan is not proposing city-initiated rezoning of traditional residential areas following adoption of this plan. Rezoning process and restrictions on lot sizes/areas would be disincentive to land speculation.</li> <li>Staff believe that increase in land value in response to greater diversity of housing types is unlikely based on recent economic analysis; however can be monitored over time.</li> </ul>
<b>Environmental Development Permit Area</b>	
Add new section to housing chapter regarding guidance for new housing along Gorge Arm (Selkirk Ave), in an environmental DPA.	<p>(NEW) Revised Map 11 and Policy 6.5 identify a Gorge Waterfront Sub-Area. This sub-area carries forward policies developed in 2003 for the Victoria Harbour Plan, which does not support rezoning for further density in this area. The plan notes that a future process would evaluate the merits of seeking waterfront easements, with consideration of rezoning, at the same time that park planning is done for Arm Street and Burleigh Parks. (Renumbered policies 4.2.2., 4.6.2.)</p> <p>The Action Plan identifies this as a long-term action (2028+).</p> <p>Existing environmental Development Permit Area 8 provides guidance for the protection of shoreline areas.</p>
<b>Urban Residential Areas</b>	
<b>Overall</b>	
Map clarity	Revise Map 10 (formerly Map 8): Urban Residential Areas to label all nearby streets, for clarity
<b>Affordability</b>	
Don't locate all affordable housing on busy streets	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>Affordable housing supported in a variety of locations in Vic West, including some blocks adjacent to (but not on) Esquimalt Road, in Westside Large Urban Village, if new housing is added/redeveloped at housing cooperatives.</li> <li>Non-market housing which is included as part of a private development generally requires Urban Residential or Large Urban Village densities, at a minimum, to be viable.</li> <li>Non-market housing may be built by affordable housing providers anywhere residential development is supported.</li> <li>The plan supports the creation of rental housing suites which could provide market rental opportunities in various location.</li> </ul>
Make affordable housing mandatory rather than bonusing for new development	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>City looking at ways to encourage/ require affordable housing in new developments through upcoming city-wide inclusionary housing study (Fall/Winter 2017-2018).</li> </ul>
Need middle income housing also	<p>No change recommended</p> <ul style="list-style-type: none"> <li>New types of housing which include rental suites, which provide for added ground-oriented housing units (e.g. townhouses), or which provide multi-unit rental or strata ownership units would provide market housing options.</li> <li>Additional support for housing types with suites may help middle-income households qualify for a mortgage.</li> <li>Housing prices largely set by the market.</li> </ul>
Can anything be done to support people to own their own units rather than development which seems to make money for developers?	<ul style="list-style-type: none"> <li>A variety of ownership housing forms are anticipated by the plan</li> <li>Homeownership programs are outside scope of neighbourhood plan. City is looking at a variety of other housing initiatives through the implementation of the Victoria Housing Strategy.</li> </ul>
On Esquimalt Rd. (south side) between Catherine St. and Mary St. – large parking lot – could be converted to low-income housing	<ul style="list-style-type: none"> <li>Choice to redevelop will be up to property owner. Portion of this property is right-of-way for E&amp;N Trail.</li> </ul>

<b>Amenities &amp; service</b>	
How can we continue development without overloading existing capacity (social, transport, services and parks)	<ul style="list-style-type: none"> <li>• (NEW) Chapter 16, Neighbourhood Improvements and Amenities, has been added to identify the priority improvements identified by the community.</li> <li>• (NEW) Policy 7.1.4. identifies community amenities (community facility or public space improvement) desired where rezoning for additional density is considered in Westside Large Urban Village.</li> <li>• (NEW) Chapter 9, Core Songhees and Master Planned Areas, describes amenities which are required as part of Master Development Agreements.</li> <li>• New developments will provide Development Cost Charges (DCCs) to offset impact to infrastructure.</li> <li>• Amenities and infrastructure improvements to be sought from individual developments to offset impact of new development in the immediate area.</li> <li>• Other City systems/ networks expected to be able to meet future capacity Vic West Transportation Study (2004) and Parks and Open Space Master Plan (2017) confirmed sufficient capacity for transportation and parks, respectively.</li> <li>• Capacity/need for community facilities and services to be assessed through upcoming city-wide recreational planning.</li> </ul>
Require accessibility (e.g. wheelchair accessibility) in portion of new units.	<p>No further action recommended.</p> <ul style="list-style-type: none"> <li>• Individual property owners/ developers encouraged to add features to developments that meet the needs of future residents.</li> <li>• Accessibility considerations now highlighted in all reports to City Council, including for new developments.</li> <li>• Accessibility requirements included in the BC Building Code.</li> </ul>
<b>Character &amp; design</b>	
Concern that design guidelines have not been effectively implemented in past, or will get negotiated away by developers	<ul style="list-style-type: none"> <li>• General comment</li> <li>• New City-wide Development Permit Guidelines adopted in 2012 as part of the Official Community Plan Update. Additional guidelines proposed through the neighbourhood plan.</li> <li>• Development Permit Guidelines are bylaws.</li> </ul>
Need deeper setbacks from all sides than what is done downtown	<ul style="list-style-type: none"> <li>• Setbacks will be determined by zoning. Existing zoning for outside of the Downtown Core Area contains different setbacks than downtown zoning.</li> <li>• City-wide Development Permit Area guidelines and proposed guidelines for Vic West are different than those for the downtown, and typical zoning also differs with respect to setbacks. These guidelines address sensitive transitions to adjacent development. The design of individual developments will be reviewed against surrounding character of the area.</li> </ul>
Important to retain open spaces for families with children, growing food, habitat.	<p>No additional changes recommended.</p> <ul style="list-style-type: none"> <li>• Existing City-wide <i>Guidelines For: Multi-Unit Residential, Commercial, Industrial</i> require open space for residents (private and/or shared, depending on type of housing). Further guidelines are included in the Revitalization Guidelines for Corridors, Villages and Town Centres proposed to be applied in Vic West.</li> <li>• Need for on-site open space balanced with proximity to waterfront areas and parks.</li> </ul>
<b>Esquimalt Road and South of Esquimalt Road</b>	
Preserve views of water from within the neighbourhood, even if approving taller buildings on Esquimalt Rd. and Lime Point	<ul style="list-style-type: none"> <li>• (NEW) New policies 6.1.5, 6.1.8., 8.5.6, and revised Maps 10, 15 indicate new development should respect the view of the Olympic Mountains from Catherine Street at Edward Street, and the view of the Sooke Hills from Catherine</li> </ul>



	<p>Street at Skinner Street. These view corridors have been included in the proposed content specific to Vic West in the Revitalization Guidelines for Corridors, Villages and Town Centres.</p> <ul style="list-style-type: none"> <li>The existing development permit guidelines for various areas (Songhees, Dockside, Roundhouse and Bayview lands, Harbour Road Industrial Area) protect public view corridors in these respective areas.</li> </ul>
<ul style="list-style-type: none"> <li>Many comments that 5 storeys is too high for Esquimalt Road that 3, maybe 4 storeys should be the max.</li> <li>Others suggested allowing taller buildings in these areas for sustainability, to support affordable housing contributions, or to relate to Roundhouse area.</li> <li>Esquimalt Road and Lime Point area: 5 storey should only be allowed where there would be no impact on the light, privacy, green space or liveability of the existing residences.</li> <li>Buildings should be higher (6 storeys or more) to provide opportunity for affordable housing</li> </ul>	<p>(NEW) Revised Map 10 indicates maximum height to be considered in metres and approximate storeys (given that storey height may vary). Heights proposed along the north side of Esquimalt Road remain at 5-6 storeys in most areas, with the corner of Esquimalt and Dalton limited to 3 storeys; 6 storeys on the south side of Esquimalt Road; and lower heights along Mary Street.</p> <p>Rationale:</p> <ul style="list-style-type: none"> <li>Policy 6.2.6. (renumbered, formerly 6.3.5.), existing City-wide Development Permit Guidelines, and the proposed Revitalization Guidelines for Corridors, Villages and Town Centres include guidance for sensitive transitions to adjacent lower-density development.</li> <li>Policies 6.1.8., 8.5.6. identify public view corridor to be respected.</li> </ul> <p>Staff have reviewed building heights and feel that higher building appropriate along Esquimalt Road due to mix of building types, wide street and frequent transit route. Recommend areas be added to Development Permit Area with new design guidelines to address potential impacts on neighbouring properties. Existing buildings on the south side exceed 6 storeys. Buildings above 4 storeys are more likely to support inclusion of non-market housing.</p>
Desire for a more welcoming/engaging streetscape than seen in Songhees	<ul style="list-style-type: none"> <li>Building of Roundhouse development will improve streetscape and vibrancy in area.</li> <li>Pedestrian and cycling improvements planned along Kimta Road.</li> <li>City-wide Development Permit Guidelines, and the proposed Revitalization Guidelines for Corridors, Villages and Town Centres include guidance for creating pedestrian-friendly environments for different contexts (residential or commercial frontages).</li> </ul>
Along Esquimalt Road - encourage shop fronts and good pedestrian access	<ul style="list-style-type: none"> <li>Policy 6.2.3. already supports commercial uses on the ground floor.</li> <li>Ancillary retail is also supported in light industrial areas.</li> <li>Development Permit guidelines related to shopfront design.</li> <li>Pedestrian improvements recently completed along Esquimalt Road. Additional improvements for sidewalks and individual frontages to be achieved through redevelopment.</li> </ul>
<b>South of Esquimalt Road Area (formerly Lime Point Area)</b>	
<ul style="list-style-type: none"> <li>Concern about changing traditional residential designation in Lime Point area (south of Esquimalt Road) to urban residential designation</li> <li>Many comments that 5 storeys is too high for Lime Point, and that 3, maybe 4 storeys should be the max.</li> <li>Others suggested allowing taller buildings in these areas, to support sustainability, affordable housing contributions or relate to Roundhouse area.</li> <li>Desire to maintain ground oriented character in the portion of these areas that have homes now.</li> <li>Concerns about impacts to privacy, scale and change in character of Lime Point Area</li> </ul>	<p>Based on community feedback and additional urban design analysis, revise policy 6.5 (newly numbered) and Maps 10, 11 (formerly Map 8, Urban Residential Areas and Map 9, Traditional Residential Sub-areas) to support development more compatible with existing character around Robert Street. Specific changes include:</p> <ul style="list-style-type: none"> <li>Retain Traditional residential designation on the east side of Robert Street and the the south end of the block between Mary Street and Russell Street, considering development up to 3 storeys and 1.2 floor space ratio. This built form is compatible with townhouses and courtyard forms of housing found in the area.</li> <li>Reduce the anticipated height on the east side of Mary Street to approx. 4 storeys (proposed as 5 storeys in draft plan)</li> <li>Policies 6.1.8., 8.5.6. identify public view corridor to be respected. City does not have the authority to protect private views.</li> </ul>

<ul style="list-style-type: none"> <li>• Preserve views of water from within the neighbourhood, even if approving taller buildings in Lime Point</li> <li>• Move lots at south end of Russell-Mary block be moved to Area 3 of Trad Res (3 storey development) for compatibility to existing development and consistency with waterfront policies/development</li> <li>• Consider different designation for Trad Res 4-storey apartment on waterfront west of Robert Street</li> </ul>	
<ul style="list-style-type: none"> <li>• Important to step down heights toward the water and reduce allowable heights for sites on higher ground to maintain views and effective height limit</li> <li>• Design should be sensitive to the West Song walkway</li> </ul>	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>• Policy 6.2.1. directs lower buildings/ stepping down on lots facing the waterfront.</li> <li>• Traditional Residential designation is retained adjacent to the waterfront.</li> <li>• Existing and proposed Development Permit Guidelines require sensitive design towards all public spaces, including streets, parks, pathways and the waterfront.</li> </ul>
<b>Commercial &amp; industrial uses</b>	
<ul style="list-style-type: none"> <li>• There are many vacant office/retail spaces available already. If you build more office, how do we know they will be used?</li> <li>• Many comments regarding maintaining employment areas, including light industrial.</li> <li>• Other comments regarding supporting office employment, transitioning some light industrial uses to office employment</li> </ul>	<p>No change recommended</p> <ul style="list-style-type: none"> <li>• Decisions about whether to build office/ retail spaces would be determined by property owner. Evaluation of existing demand and what exists in the area is typically part of the decision about whether to redevelop a property or not.</li> <li>• Analysis by CRD, recent trends and consultation show high current and expected demand for new light industrial and office space in Victoria.</li> <li>• Proposed plan policies support a mix of retaining light industrial areas with some smaller parcels transitioned to employment and residential use.</li> </ul>
Consider allowing retail (e.g. coffee shop) at foot of Mary St. rather than just residential.	<p>No change recommended</p> <ul style="list-style-type: none"> <li>• Mary Street at Esquimalt Road is identified as Industrial Employment or Employment with residential, to support light industrial uses, ancillary retail of products produced on-site, or commercial uses.</li> <li>• A retail/food services use at the waterfront would generate traffic through the residential neighbourhood.</li> </ul>
<b>Pre-zoning</b>	
Suggestion to be proactive, pre-zone to match stated land use goals (e.g. the industrial zone at Lime Bay & along Esquimalt Rd. should be changed to a new artisanal activity land use zone)	<p>No change recommended</p> <ul style="list-style-type: none"> <li>• Pre-zoning is generally not proposed at this time.</li> <li>• Pre-zoning of light industrial areas may occur to update uses. This would be determined through a subsequent rezoning process. Existing light industrial zoning already supports artisan activity and ancillary sales.</li> <li>• Pre-zoning land would remove opportunity for bonus density (e.g. for community amenities/ affordable housing) to be negotiated and for community input.</li> </ul>
<b>Special Planning and Master Planned Areas</b>	
Confusing section – add more context, introduction	<ul style="list-style-type: none"> <li>• (NEW) This section is revised as a new chapter Chapter 9: Core Songhees and Master Planned Areas), and includes descriptions of each Master Planned Area based on existing Master Development Agreements, Development Permit Guidelines, and zoning.</li> <li>• One Special Planning Area which is located in Traditional Residential (Pioneer Housing Cooperative) has been moved to the Residential Areas chapter.</li> </ul>

	<ul style="list-style-type: none"> <li>• (NEW) Create separate chapter for Master Planned Areas</li> <li>• (NEW) Move Pioneer Co-op policies to Traditional Residential Areas; move Tyee Co-op and site near Johnson Street Bridge to new Master Planned Areas chapter</li> </ul>
Encourage urban food production at Tyee Cooperative with redevelopment	<ul style="list-style-type: none"> <li>• Revised policy to encourage opportunities for urban food production at Tyee Co-op, and moved to Tyee Co-op section.</li> <li>• Original wording was too prescriptive as primary public benefit for co-op is affordable housing</li> </ul>
Require affordable housing at Pioneer Co-op	<ul style="list-style-type: none"> <li>• Policy (7.1.) is worded to allow Pioneer Co-op membership to determine housing needs in the future (eg tenure, cost and size) as these needs may change over time.</li> </ul>
Clarify that Bayview Place is being planned as integrated neighbourhood, while Bayview and Roundhouse have separate zoning and MDAs.	<ul style="list-style-type: none"> <li>• Master Planned Areas: the new introduction refers to Roundhouse, and to Bayview Place as a residential Sub-area of Roundhouse development.</li> </ul>
Reference to Bayview MDA should be changed to Roundhouse MDA – include both “Sitkum Park” and “Lime Bay Park”	<ul style="list-style-type: none"> <li>• Improvements to both parks have been identified in the description of the Roundhouse</li> </ul>
More detail as to how master planned sites contribute to overall neighbourhood: greater diversity, vibrancy and density for the city and lessen the development pressure on the lower-density, traditional neighbourhoods, while also supporting neighbourhood retail, greater transportation choice, and overall urban sustainability.	<ul style="list-style-type: none"> <li>• The introduction the Master Planned Areas section of Chapter 9 elaborates on the role medium- and higher-density development and associated commercial areas, parks and open spaces, and trail connections support broader goals for Vic West and the City’s core. Some photos have been added.</li> <li>• (NEW) Added Map 12 showing existing/approved building footprints/ block plans and snapshot images.</li> </ul>
Give incentive / compel the Roundhouse development to build the promised commercial spaces	<p>No change recommended</p> <ul style="list-style-type: none"> <li>• Master Development Agreement provides details of planned development and phasing for Roundhouse. Changes to MDA would be proposed by property owner.</li> </ul>
<b>Urban Villages</b>	
<b>All Villages</b>	
Support for focus on walking and cycling as higher priority than cars and parking	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>• Plan supports more focus on active transportation, in accordance with OCP transportation hierarchy.</li> </ul>
Encourage more variety of shops and businesses in Westside Village, Craigflower Village.	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>• Plan policies (and existing zoning) support diversity of commercial, office, service uses.</li> <li>• Plan policies support a focus on pedestrian-friendly spaces including patio dining along Wilson Street.</li> <li>• Individual tenants will be determined by property owner. No change recommended</li> </ul>
More community services (including a church) and retail needed for existing residents in Songhees/ Roundhouse Area	<ul style="list-style-type: none"> <li>• (NEW) New Policy 7.1.4 identifies community space as a desired public amenity in Westside Large Urban Village; also identified in New Chapter 16: Neighbourhood Improvements and Amenities.</li> <li>• These policies reflect existing policy 13.1.2. for future community facility in area of Westside Village</li> <li>• Retail space and community space (or cash-in-lieu) identified in Roundhouse Master Development Agreement (see 13.1.4.).</li> </ul>
Need to see more community benefit from new development	<ul style="list-style-type: none"> <li>• Revise plan to add new Chapter 16: Neighbourhood Improvements and Amenities to clearly indicate desired amenities for Vic West.</li> <li>• Additional clarity has been added regarding desired site-specific amenities indicated in urban residential, urban villages and employment sections (e.g. public realm improvements in villages, community space in Westside Village area, on-site non-market housing).</li> <li>• New development contributes to Development Cost Charges</li> </ul>

	<ul style="list-style-type: none"> <li>Financial contributions and taxes from new development go into City-wide revenues, to be redistributed according to City's Financial Plan.</li> </ul>
Concerns regarding whether additional parking and traffic demand can be managed as more people are added	<ul style="list-style-type: none"> <li>Parking and traffic demand expected to decrease over life of plan. Majority of new housing concentrated near transit, within walking distance of downtown and cycling facilities. No changes recommended.</li> </ul>
Introduction needs more recognition of adding housing along transit and heritage character of this urban village Add goal to support housing along transit route	<ul style="list-style-type: none"> <li>Revised text to add more emphasis for transit and heritage character</li> <li>Revised goal 2 to: Improve the walkability, bikeability, transit use and public realm in and around urban villages</li> </ul>
<b>Westside Village</b>	
<ul style="list-style-type: none"> <li>Strong survey support for proposed vision/ policies/amenities for Westside Village.</li> <li>Some concern about 6 storeys being too tall in Westside Village, Da Vinci site - desire for more human scale</li> <li>Others supportive of 6 storey heights.</li> <li>Others suggested that if Westside shopping centre redevelops, require community amenities as a result of any new density allowed on site.</li> <li>Concern that future development density on Westside Village site should be limited due to past agreements.</li> <li>Consider more density on Westside shopping centre site at the corner of Bay/Skinner Street.</li> <li>Desire for additional services/ businesses in area:</li> <li>New library, community space for classes, new school.</li> </ul>	<ul style="list-style-type: none"> <li>(NEW) New Policy 7.1.4 identifies community space as well as previously identified public realm improvements and non-market housing (former policies 8.5.3.e, 8.5.7.) as desired improvements in Westside Large Urban Village; also identified in New Chapter 16: Neighbourhood Improvements and Amenities.</li> </ul> <p>No change to density and height recommended:</p> <ul style="list-style-type: none"> <li>Area around Westside Village features higher buildings, wide roads, large-sized lots and distance from low-scale residential houses. Six storey buildings would be compatible with surrounding area.</li> <li>Limited taller buildings are supported in two areas because of context: the Bay Street/Skinner Street corridor to support development at an area identified as a key opportunity for an architectural statement during engagement; and in the Westside Shopping Centre site to support more flexibility in design which can include a mix of lower and taller buildings, with human scaled buildings at the edges.</li> <li>Additional density not recommended as increasing beyond the 2.75 floor space ratio would create densities identical to Old Town; and the intent of allowing taller buildings at the Westside Shopping Centre site is to allow for a more flexible mix of lower-scale and taller buildings.</li> <li>Future redevelopment of Westside Village to higher may require a rezoning; densities will be revisited at the time based on the specifics of the application.</li> </ul>
<ul style="list-style-type: none"> <li>Ensure sunlight still shines into Westside Village and other residential areas as development happens / after</li> </ul>	<ul style="list-style-type: none"> <li>Proposed application of the <i>Guidelines for: Residential, Commercial and Industrial</i>, and <i>Revitalization Guidelines for Corridors, Villages and Town Centres</i> with Vic West-specific content added, provide additional guidance for compatibility, form and massing and livability.</li> </ul>
<ul style="list-style-type: none"> <li>Clarify that large format retailers in this area should be designed for an urban environment, not a suburban one</li> </ul>	<ul style="list-style-type: none"> <li>Revise policy 8.5.2.b. to "...encourage large ground-oriented retail in an urban format"</li> <li><i>Revitalization Guidelines for Corridors, Villages and Town Centres</i> with Vic West-specific content added, provide this guidance as a bylaw</li> </ul>
<ul style="list-style-type: none"> <li>Minimize and modify surface parking design to improve pedestrian movement and safety in Westside Village</li> <li>Calm traffic in Westside Village – particularly quieting truck traffic servicing the village.</li> <li>Green the roof at Westside Village</li> </ul>	<ul style="list-style-type: none"> <li>Changes to the configuration of Westside Village would be subject to zoning and design guidelines if redevelopment occurs.</li> <li>Comments to be referred to property owner.</li> </ul>
Desire for Dockside and Roundhouse to create villages that are well linked to Westside Village	<ul style="list-style-type: none"> <li>Plan proposes better pedestrian and cycling connections between the two, improved crossings, and more continuous urban design and public realm treatments. Chapter 3: Transportation and Mobility includes an integrated system of pedestrian and bicycle links in and outside of Master Planner developments (see Map 3: Neighbourhood Active Transportation Network).</li> </ul>



	<ul style="list-style-type: none"> <li>New Chapter 9: Core Sonhgees and Master Planned Areas describe the amenities including commercial areas, park and public space improvements, and pedestrian connections to be included in these developments.</li> </ul>
<ul style="list-style-type: none"> <li>Close Wilson St. along Westside Village to cars from Bay to park edge; remove parking</li> <li>Don't remove parking on Wilson between Tyee and Bay.</li> </ul>	<ul style="list-style-type: none"> <li>Existing Urban Design Polices for Westside Urban Village (newly numbered policy section 7.1.7. and 7.2, formerly 8.5.3 and 8.6) envision Wilson Street as a pedestrian-friendly high street for the area, with streetscape improvements, trees, furnishings rather than a pedestrian-only space. Improvements would occur primarily through redevelopment. New development would include pedestrian-friendly storefront and pedestrian spaces accommodating patio dining.</li> <li>Pedestrianized streets generally do not support retail uses except in high-density or tourist areas. Closing street would worsen congestion and traffic on adjacent streets.</li> </ul>
<ul style="list-style-type: none"> <li>Need new library, school</li> </ul>	<ul style="list-style-type: none"> <li>(NEW) New Policy 7.1.4 identifies community space as desired improvements in Westside Large Urban Village; also identified in New Chapter 16: Neighbourhood Improvements and Amenities. This reflect existing policy 13.2. from public engagement.</li> <li>Need for new schools determined by School District 61; final plan will be referred for review. Population under age 18 not expected to increase dramatically due to small housing size.</li> </ul>
<b>Craigflower Village</b>	
<ul style="list-style-type: none"> <li>Strong support from survey and open houses for proposed vision/policies for area.</li> <li>Concern with increasing scale of Craigflower Village and surrounding density - particularly regarding feel, impact on neighbours, sunlight, noise, traffic.</li> <li>Suggest re-visiting this section with a more moderate, gradual approach to change (i.e 2-3 stories maximum, fewer garden suites, etc)</li> <li>Concern that higher buildings would change the quiet and small scale character of Craigflower village.</li> <li>Opposition to adding more people near village; others support for adding more people in and near the village</li> <li>Desire to emphasize heritage, historic feel of area</li> <li>Analysis on need for new commercial space considering closing businesses downtown</li> </ul>	<p>Revisions for clarity and design guidance:</p> <ul style="list-style-type: none"> <li>Add Craigflower Village to new Development Permit Area. Apply Revitalization Guidelines for Corridors, Villages and Town Centres with specific content for Craigflower Village, including small storefronts, to ensure compatibility of new development, and respect of historic character.</li> <li>In newly numbered policy 7.3.3. (revised from 8.1.2., 8.1.6.), the location of specific development has been clarified by the addition of addresses.</li> <li>Properties north of the village have been moved to Traditional Residential (see newly numbered Map 11, Policy 6.5., Policy 6.10) to replace former policy 8.1.6. Clarity is added that commercial use is only supported in converted houses, similar to existing examples on these blocks.</li> <li>Revise Craigflower concept sketch to remove brown residential frontages on side streets outside the village; this is already supported and is confusing.</li> </ul> <p>Rationale:</p> <ul style="list-style-type: none"> <li>Community feedback indicated strong support for adding more housing and attracting more customers with a few new retail spaces. New development will achieve housing and business objectives and support frequent transit route. Future development is expected to be incremental due to height limits.</li> </ul>
<ul style="list-style-type: none"> <li>Proposed 3-4 mixed use building at Raynor/ Russell Streets:</li> <li>Concern about impact of 3-4 mixed use building proposed at Skinner St and Russell Street: change in character, impacts on adjacent neighbours, traffic impacts from vehicles accessing Raynor Street, too much density and change</li> <li>Others support the idea of changes in this location</li> <li>Some feel townhouses would be more appropriate on this site than apartments</li> <li>Concern about 3-storey buildings up Raynor Street</li> </ul>	<ul style="list-style-type: none"> <li>Revised Policy 6.1.2 to specify a maximum building height as viewed from Russell Street (the lowest elevation of the property).</li> <li>(NEW) Added policy 6.1.5. to maximize views of the Sooke Hills from Skinner Street at Catherine Street when designing new development.</li> <li>(NEW): Add Skinner Street to new Development Permit Area and apply Revitalization Guidelines for Corridors, Villages and Town Centres with specific guidelines for Skinner Street Corridor, including considering the scale of development adjacent to Raynor Street and adjacent houses, and respecting the public view corridor.</li> <li>No other changes recommended.</li> </ul> <p>Rationale: Existing commercial zoning on "Chicken on the Run site" would allow 3-4 storeys (up to 12 m). Strong community support for mixed use/residential building on corner, provides gateway at entrance to village, provides affordable housing opportunity due to City-owned right of way, on frequent transit corridor, grade between Russell and Catherine lessens visual impact of 3-4 storey building.</p>

<ul style="list-style-type: none"> <li>Concern about existing businesses forced out because of future development.</li> </ul>	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>Key goal is to support existing Craigflower Village businesses with more people living nearby, and a few more businesses to attract more customers to the area. Village will face future competition for customers due to new commercial areas at Dockside and Roundhouse.</li> <li>Existing zoning would allow 12-metre tall redevelopment on some sites. The plan scales this back and provides nuance and encouragement to keep and reuse existing buildings.</li> </ul>
<p>Various comments regarding streetscape in Craigflower Village area:</p> <ul style="list-style-type: none"> <li>Concerns about losing trees by adding parking to both sides of Craigflower - suggestions to have no change or separated bike lane only</li> <li>Confusion about whether parking and/or trees would be removed along Banfield Park</li> <li>Improve pedestrian friendliness and connectivity across Craigflower Road</li> <li>Extend orchard to Craigflower Road to engage the other side of street</li> <li>Allow mobile (bike based) food entrepreneurs on the Banfield Park fringe to enhance the commercial, festive feel of the village.</li> </ul>	<p>No change recommended</p> <ul style="list-style-type: none"> <li>Newly numbered policy 7.4.2. already specifies retaining trees and shifting sidewalk to be separate from street, Sufficient width exists for this opportunity.</li> <li>Plan policies support several initiatives to improve pedestrian conditions, including crossing improvements, added housing/mixed use (more on-street activity), on-street parking (to slow traffic), and added features on park edge.</li> <li>Specific improvements may be determined as opportunity arises through development in the area.</li> <li>Action Plan identifies streetscape design to be undertaken at the same time as an improvement plan for Banfield Park (Medium-term).</li> <li>Suggestion for mobile food vendors, extending orchard to be referred to for exploration of Commercial Uses in Parks (in progress) and for future park planning</li> </ul>
Rename to Banfield Village instead of Craigflower Village	No change recommended.
Support for a renewal/refresh of the community centre. Could make better use of same building footprint and need more gym space.	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>13.1.1. supports collaboration between City and Community Association to enhance facilities and other aspects of centre.</li> </ul>
Comments regarding additional businesses/ services for Craigflower Village: breakfast place, pub, grocery store as anchor, performance space.	<ul style="list-style-type: none"> <li>Plan policies support diversity of commercial/ retail uses at this location. Individual tenants up to property owner and zoning. Grocery store would require much larger retail floor area/ parking for Craigflower Village. No changes recommended.</li> </ul>
<ul style="list-style-type: none"> <li>Consider amount of additional parking and traffic demand created by more commercial space</li> <li>Concern that policy not requiring parking for village businesses is too broad – suggest that certain businesses at grade (e.g. smaller footprint retail, food services and personal services) businesses not be required to provide on-site parking.</li> </ul>	<ul style="list-style-type: none"> <li>Newly numbered policy 7.3.8.(formerly 8.1.5.) revised to not require additional parking for a change in use in existing buildings or for ground-floor commercial use at 405 Craigflower Road (“Chicken on the Run” site)</li> <li>Overall parking and traffic demand expected to decrease over life of plan due to decreasing car ownership, improved transit and other alternatives. Craigflower Village located on frequent transit line, close to regional trail/bike network, within walking distance of neighbourhood housing.</li> <li>Parking requirements add significantly to cost of new commercial development. Relying on shared public parking (e.g. on-street) is beneficial to small urban villages by reducing the overall amount of parking needed and supporting better urban design.</li> </ul>
<b>Catherine St at Edward St</b>	
<ul style="list-style-type: none"> <li>Strong community support from early engagement, survey</li> <li>Concern about new small urban village competing with existing/future commercial areas at Craigflower, Westside Village, Roundhouse and Dockside</li> </ul>	<ul style="list-style-type: none"> <li>Boundaries of proposed small urban village revised to include only existing commercially zoned properties.</li> <li>New Traditional Residential Sub-area (Policy 6.5) proposed to allow residential buildings similar in character, height (3 storeys) and density to small urban village, to create a unified character along Catherine Street between Edward Street and Langford Street.</li> <li>(NEW) Policy 7.5.2. added to support canopy street trees with any new development.</li> </ul>
<p>Some concern with height of small apartment buildings and shadowing (esp. given elevation)</p> <p>New development should respect the heritage style in the area</p>	<p>Development Permit Area proposed, with specific guidelines added to support a unified character for this block, including street trees, pedestrian-friendly design, and sensitive transitions to lower density properties.</p>

Bella Street improvements	Renumbered policy 7.4.10., 7.5.2. (formerly 8.4.1.) revised to clarify name of street, support an alternative road design comfortable to pedestrians
<b>Employment Lands</b>	
<b>Amenities</b>	
Need housing that is attainable for those employed in the light industrial jobs. Suggest requiring affordable housing for residential above industrial.	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>• Non-market housing is included as a desired amenity in all areas which include residential uses. Individual amenities are typically negotiated on a case-by-case basis, guided by City-wide policy and neighbourhood plans.</li> <li>• Affordable housing may be difficult to require in industrial/employment-based buildings due to lower economic viability of industrial spaces.</li> <li>• While employees may need affordable housing, jobs located in light industrial areas tend to pay more than retail services jobs.</li> </ul>
Add a dramatic theatre (not movie) or a dance / art school. But those things might be developed in the pre-existing light industry areas.	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>• Depending on zoning, these types of uses may already be supported in these areas. If not, property owners can apply for a change of use/ rezoning if new business type is proposed.</li> </ul>
<b>Compatibility and feel</b>	
Many emphasized need to ensure uses are good neighbours with residential. Some feel that light industrial cannot be compatible.	<ul style="list-style-type: none"> <li>• (NEW) Proposed Revitalization Guidelines for Corridors, Villages and Town Centres contain guidelines regarding supporting compatibility of uses through design.</li> </ul> <p>No change recommended to plan:</p> <ul style="list-style-type: none"> <li>• Plan policies recognize that many light industrial uses are not compatible with residential or commercial uses, and emphasize limiting uses to those which are compatible.</li> <li>• Suitability will be evaluated when zoning is updated – whether for a proposed development or in a City-initiated rezoning to modernize light industrial uses.</li> </ul>
Some feel noise from car crushing (in Burnside) and shipyard is excessive.	Operational/enforcement issue. Referred to Bylaw Enforcement.
<b>Environmental Quality</b>	
Need more assurance of environmental stewardship by waterfront industry	<ul style="list-style-type: none"> <li>• Waterfront industry required to comply with all environmental regulations and discharge bylaws.</li> </ul>
Plan doesn't sufficiently recognize Marine Industrial use as designated in the Harbour Road Area	<ul style="list-style-type: none"> <li>• (NEW) Added policy 8.1.1. supporting Marine Industrial uses consistent with the Harbour Road Industrial Waterfront Guidelines and related zoning.</li> </ul>
<b>Height</b>	
<ul style="list-style-type: none"> <li>• Many expressed concern regarding height of proposed light industrial mixed use buildings, suggesting 2 -3 storeys as a maximum. Others felt 4 storeys was acceptable.</li> <li>• Some concern about height of buildings in Employment Residential Areas south of Esquimalt Road (5 storeys)</li> <li>• Ensure that any increases in height allowed do not impact views, light, privacy, or green space.</li> </ul>	<ul style="list-style-type: none"> <li>• (NEW) Revised Map 15 specifies anticipated height in metres, and approximate number of storeys, given that industrial and commercial floor heights may vary and are typically taller than other development types.</li> <li>• Proposed heights are generally consistent with intent of draft plan heights, while number of storeys has generally been reduced.</li> <li>• (NEW) Height supported between Tyee Road and Skinner Street has proposed at 14m (approx. 4 storeys rather than 5 storeys) due to grade of site and the way height is measured on a sloping site, to consider transition to uses across Tyee Road.</li> </ul>

<ul style="list-style-type: none"> <li>Concern that the way height is measured on a sloped site would result in excessive height facing Tyee Road.</li> </ul>	<ul style="list-style-type: none"> <li>(NEW) Proposed Development Permit Area and associated guidelines, including the Revitalization Guidelines for Corridors, Villages and Town Centres contain guidelines regarding supporting sensitive transitions to adjacent lower-scale development.</li> <li>Rationale: Proposed heights support the inclusion of residential or upper floor commercial uses.</li> </ul>
<b>Parking &amp; Traffic</b>	
Parking needed for businesses in employment areas.	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>Parking requirements are determined by Schedule C of the Zoning Bylaw, which is proposed for revision in 2018 based on study of actual parking demand in the city.</li> </ul>
Minimize impervious pavement of parking spaces.	<p>No change recommended.</p> <ul style="list-style-type: none"> <li>New development in all areas (including employment areas) encouraged to add rainwater management features, including impervious surface, through Rainwater Rewards program.</li> <li>Additional City-wide initiatives forthcoming, including exploring requirement for storwater run-off to be managed on-site.</li> </ul>
Concern about the 700 block of Tyee for General Employment with limited residential. Too many people and not enough space or road access for commercial vehicles. Albion has been a challenging for Railyards residents.	<ul style="list-style-type: none"> <li>(NEW) Revised Map 15 supports height between Tyee Road and Skinner Street has proposed at 14m (approx. 4 storeys rather than 5 storeys proposed in the draft plan) due to grade of site and the way height is measured on a sloping site, to consider transition to uses across Tyee Road.</li> <li>Minimizing impacts on residential properties is a key concern for any future housing here. 9.5.2. – 9.5.6. provide direction to minimize commercial transportation (and other) impacts on residential properties.</li> </ul>
<b>Viability</b>	
Concerns raised about economic trends, need for industrial space / sufficiency of existing stock.	<p>No change recommended</p> <ul style="list-style-type: none"> <li>Analysis by CRD, recent trends and consultation show high current and expected demand for new light industrial and office space in Victoria.</li> </ul>
<b>Heritage</b>	
Heritage map shows the previously unconsolidated lot lines for the Roundhouse site – update needed. Also – the Heritage Property colour should only cover DA-1 lands north of the rail corridor.	Revised Map 16 to show correct lot lines.
Some properties missing from Heritage map	<p>Revise Map 16</p> <p>Show existing Heritage Conservation Area</p>
<p>Heritage section could include more description of Vic West's unique history, the history of the place, and the important heritage buildings/sites.</p> <p>Include more First Nations history, recognition of past occupation in Heritage section</p> <p>Include reference to heritage interpretation and celebration</p>	Revise introduction to plan, add preamble and text to Heritage Chapter 8 to incorporate comments/ address feedback.
<b>Infrastructure and Green Building</b>	



Eliminate heating oil in Vic West	Reworded 11.4.5 to emphasize aim to move away from heating oil in Vic West, as well as explore other heating options. (NEW) 11.4.6. to work with utility companies and province to explore solar and other energy sources (Utility companies, rather than the City, would provide leadership on incentive programs) City of Victoria to prepare education and outreach materials to encourage and support shift to solar power in residential properties.
Why include sea level rise map? Neighbourhood plan does not have policies that address sea level.	Removed. Sea level rise policies to be addressed through future city-wide level initiatives.
Infrastructure part of the plan has no new ideas. We should be thinking beyond natural gas and thinking about incentives to include solar energy production. Natural gas is not green energy.	Added various (NEW) policies 11.4.1. – 11.4.7 to acknowledge other infrastructure/ green energy initiatives underway. Green energy initiatives will be tackled through city-wide Climate Leadership Plan, outside neighbourhood planning. No further changes recommended.
<b>Neighbourhood Food Systems</b>	
Suggestions that the City should step in to provide more leadership, resources and support for community gardens Opinions on City role in allotment gardens mixed; some feel the City should create more allotment gardens (e.g. in Vic West Park), others feel this is not appropriate on public land. Consider planting fruit trees along some city streets mixed with native trees Concerned about fruit trees in the "public gardens". unless maintained for things like cherry moth and other pests these can affect trees in the entire neighbourhood	Food production on City land currently guided by city-wide policies on Community Gardens, Community Orchards and Boulevards (updated in 2016). Neighbourhood plan identified potential locations, but the establishment of food features would rely on community interest, leadership and support No change recommended.
Concern that gardens need to rely on density bonuses Some opposition to the transaction of density in return for amenity that could be better directly funded by the city from taxes on public land.	Density bonus is just one tool to achieve community gardens. Community gardens supported through other means under City's community gardens policy. Many different community amenities are desired through new development. Developers encouraged to add features that will meet needs/ interests of residents, as these vary widely based on occupants, maintenance needs, etc. City has limited tax base and many competing demands. Taxes not collected from public lands. No changes recommended.
More farmer market-type initiatives would add to the character of the area of the newer area of Vic West	Farmers markets supported in plan (12.1.1). Operator/ community group can approach City with proposal. No change recommended.
Food systems section needs to be stronger	Many food systems initiatives are addressed at city-wide level through existing programs, policies and regulations. Staff have reviewed the chapter and community feedback, and propose no further changes.
<b>Neighbourhood Well-Being</b>	

Support for improvements/ rebuilding of Vic West Community Centre to improve facilities. Need more gym space. Expand for daycare for infants (1-3 years old).	Plan supports continued work with Victoria West Community Centre to sustain/enhance facilities (13.1.1.) over time. No change recommended. Feedback to be shared with Vic West Community Association.
Vic West needs more services for homeless/drug/alcohol addictions/PTSD – all prevalent here  Plan for water holding system for community emergency preparedness	Important suggestions but outside the scope of neighbourhood plan.
Allow development/renovations for more daycare spaces in the area.	13.2.2. already supports daycare and eldercare as a use in accordance with zoning. (NEW) 13.2.2. Revised policy to add that facilities should be suitable to scale of immediate surroundings (consistent policy as in other neighbourhood plans)
Recognize the importance of Esquimalt's recreational facilities for Vic West residents	Recreation facilities in Saanich and other Victoria neighbourhoods also used by Vic West residents. Added acknowledgement to introduction.

### Arts, Culture and Placemaking

Formatting incorrect for locations and themes for public art	Re-format 14.2 and 14.3.
Avoid proposing thematic frames for potential projects public art projects as this may change over time. Keep the potential sites, though.	Revise 14.1.6. to reword as “suggestions from the community include” in more narrative (rather than policy-based) format
For the goals and intent statement, is it possible to say ‘Encourage art and Placemaking initiatives’ and leave out ‘..that celebrate Vic West’s identity’?	Goal was identified through early engagement to reflect what was heard; not intended to limit placemaking and public art initiatives
Add new gateway/ welcome to Vic West signs at key entry points Improve wayfinding signage at all entry points to Vic West	(NEW) 14.1.7. Add new policy to encourage gateway signs, in partnership with community

### Action Plan

Have individual actions reference policy numbers in the plan Add cross-referencing from plan policies to action plan	Added numerical references to Action Plan
Want more clear and detailed plans of actions for each initiative and area, coordination with other levels of government and agencies, and community ownership.	Specific details worked out annually through work plans and based on annual Council budget approvals; would be difficult to provide more detail at this time.
Add introduction to Action Plan to encourage other sources of funding, including community-led efforts such as fundraising, community tax areas	(NEW) 15.2 Added to Action Plan
Under the Action Plan, I disagree strongly with the sentence beginning with: "Create Zoning to support ground-oriented..."	Comment unclear

### General / Other Comments

Methods for continued input and involvement as the plan changes over time	
Questions regarding Lime Bay and other north harbour ferry services and stops? Ferry service from the western communities into Victoria's core? Consider a water taxi service that has more hours for commuter traffic.	Water taxis are operated by private companies. Would be up to individual business to propose service changes.
Plan for water holding system for community emergency preparedness	Water supply currently provided through bottled water.
Confine lighting to site and minimize light pollution (lighting of sky).	Zoning bylaw and Development Permit Area Guidelines address lighting for some types of uses

**Attachment 4:**  
**Engagement Summary and Feedback on**  
**Proposed Vic West Neighbourhood Plan and OCP Amendments**

- Part 1: Engagement Summary
- Part 2: Survey Report
- Part 3: Open House Feedback
- Part 4: Correspondence
- Part 5: Responses to Formal Referrals



**Attachment 4:**  
**Engagement Summary and Feedback on**  
**Proposed Vic West Neighbourhood Plan and OCP Amendments**

Part 1: Engagement Summary



## Victoria West Neighbourhood Plan: Engagement Summary on Draft Plan Revisions and Official Community Plan Amendments

Community engagement on the proposed Vic West Neighbourhood Plan was held between January 10 and February 28, 2018. The community was invited to attend an open house and complete an online survey on revisions to the draft plan and amendments to the Official Community Plan to align with the new plan. 130 people attended the open house, and 68 online survey responses and 15 emails were received. There was also a community-led engagement with meetings and a survey organized by the Vic West Community Association land use committee, described on the following page.



The following support levels combine “very” and “somewhat” supportive rankings.

### FEEDBACK ON REVISIONS TO THE DRAFT PLAN

We heard from many people that they are generally supportive of the plan and the revisions to the plan. However, some community members continue to have concerns about transportation management, protecting the character of the neighbourhood, and insufficient detail in the plan on integration of the master planned areas. While some felt that building heights are too high (particularly along Esquimalt Road), others felt that the plan does not go far enough in allowing growth and expressed disappointment that apartment buildings (especially on Skinner Street) and townhouses have been scaled back too much.

From the correspondence received, we heard concerns about insufficient protection of character homes, additional density and building heights, secondary suites in small lot homes, and transportation management.

### AMENDMENTS TO THE OFFICIAL COMMUNITY PLAN

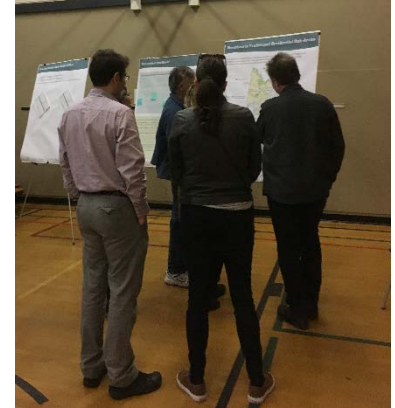
**Traditional Residential (Amendments 1 – 5).** View proposed changes [here](#).

Support for the amendments to traditional residential urban place designations ranged from 58 to 83% in the survey and from 37 to 72% at the open house. The highest level of support was for amendments adjacent to Craigflower with a combined total of 73%, closely followed by changes adjacent to Edward and Catherine Streets Village at 72%. The lowest support was for amendments for the Pioneer Co-op site at a combined 56% and revisions to building heights throughout the neighbourhood at a combined total of 57%.

Correspondence received on changing the designation for the Pioneer Co-op site and south of Esquimalt Road to consider townhouses and small apartment buildings was mixed. For Pioneer Co-op, those in support felt that the location is an ideal site to address affordable housing issues in the neighbourhood. Others were concerned about impact to privacy and incompatibility with the surrounding neighbourhood with higher buildings at the Pioneer Co-op site and in the traditional residential areas south of Esquimalt Road. Opinions were mixed about whether density was too great or not enough.

### **Urban Residential and Village Areas (Amendments 6 – 11). View changes [here](#).**

Support for the amendments to urban place designations for residential areas and villages ranged from 51 to 80% support in the survey and 43 to 74% at the open house. The highest level of support was to establish a new small urban village at Edward and Catherine Street at a combined 78%. Amendments to Craigflower Village and Westside Village had good support at 68%. The lowest levels of support were for a change from traditional residential to urban residential designation on South Mary Street (South of Esquimalt Road) at a combined 49%. Comments relating to this area were mostly that the building heights are too high. However, some felt building heights could be higher in this area. The next lowest level was for the amendments to support a mixed use 4 storey building at Russell Street at Skinner at 60%, with similar concerns about building heights.



### **Employment areas (Amendments 12 – 14). View changes [here](#).**

Support for the amendments to urban place designations for employment areas ranged from 70 to 78% in the survey and 59 to 89% at the open house. The highest level of support was for the new employment-residential designation in certain locations at a combined 81%. The lowest support was the new industrial employment designation along Esquimalt Road and near Westside Village at 67%. Concerns included building heights, impacts to parking and traffic, flexibility in employment types, and a consistent approach to similarly situated employment lands.

### **Densities (Amendment 15). View change [here](#).**

The amendments for revised densities for urban place designations for large urban villages, urban residential, employment-residential and industrial employment-residential had a combined level of support of 68%. While some were concerned that the additional density will impact parking, others felt that not enough density is included.

### **Development Permit Areas (Amendments 16 – 19). View changes [here](#).**

Amendments to expand and create development permit areas ranged in support from 65 to 74% in the survey (with high levels of neutral response) and 50 to 67% at the open house. There are concerns from some that the building heights need to be reduced before the Development Permit Areas can be supported.

### **Engagement**

63% of survey respondents felt that they were as involved in the plan as they wanted to be. Some people in the community were not aware of the process, while others had trouble participating due to busyness.

### **COMMUNITY-LED ENGAGEMENT**

The Victoria West Community Association (VWCA) consulted with their membership on the neighbourhood plan through series of community meetings and an online survey. The survey report and 26 recommendations were supplied to City staff for consideration (for the complete submission, see “Part 4: Correspondence” of the Raw Feedback Attachment to the April 26 Council Report). Based on levels of support, the VWCA strongly endorsed recommendations to add population projections and visualization of future development; additional detail for parks and transportation improvements; more reference to urban design transitions; and information regarding prior zoning and consultation on future amenities for Westside Village. The VWCA also highlighted other recommendations that received majority support, although at lower levels, which included the reconsideration of heights, densities, and land uses for various proposed OCP amendments, and increased food security content in the plan.

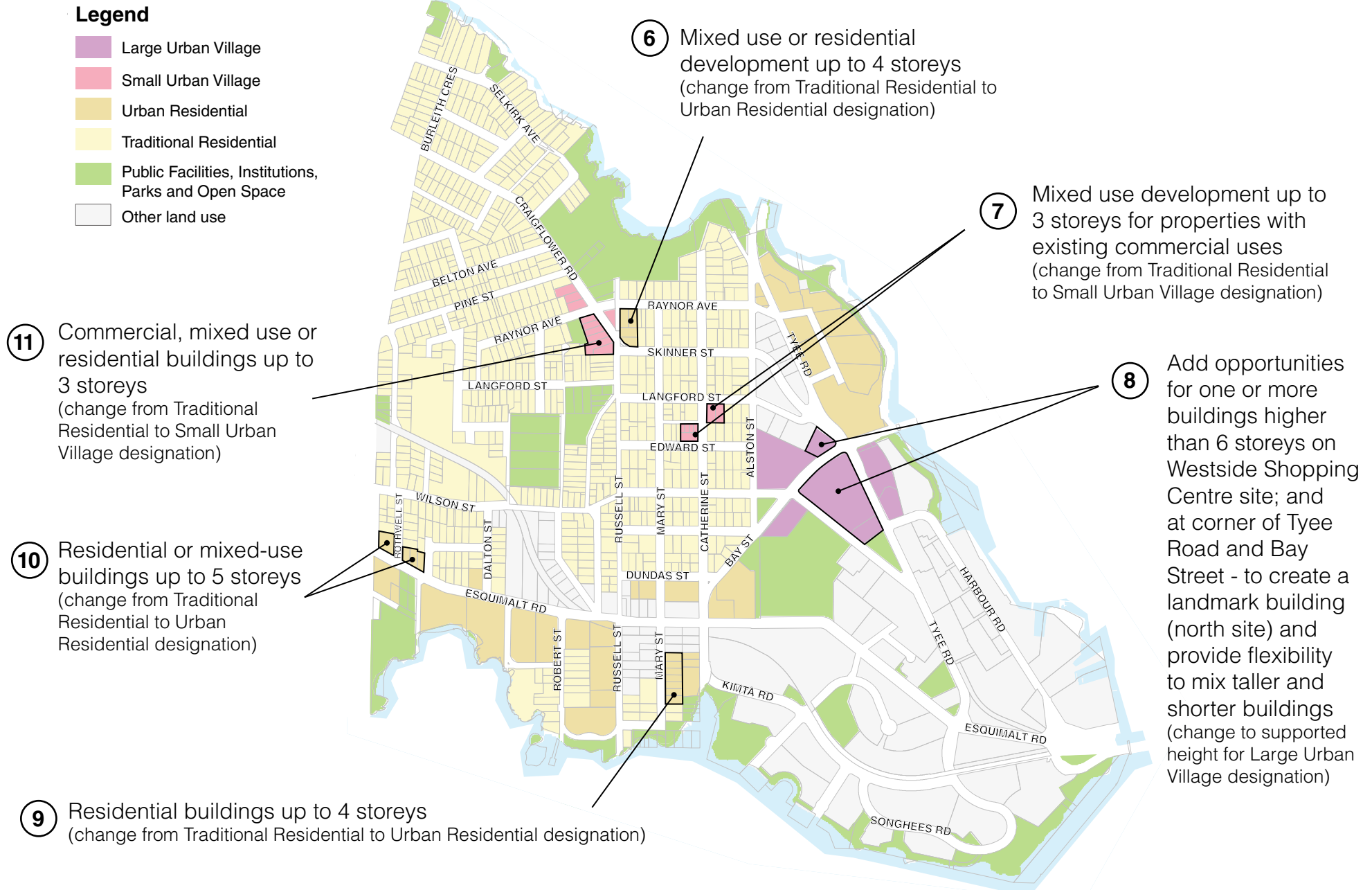
**Attachment 4:**  
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Part 2: Survey Report





# Proposed Amendments to Official Community Plan: Urban Village and Urban Residential

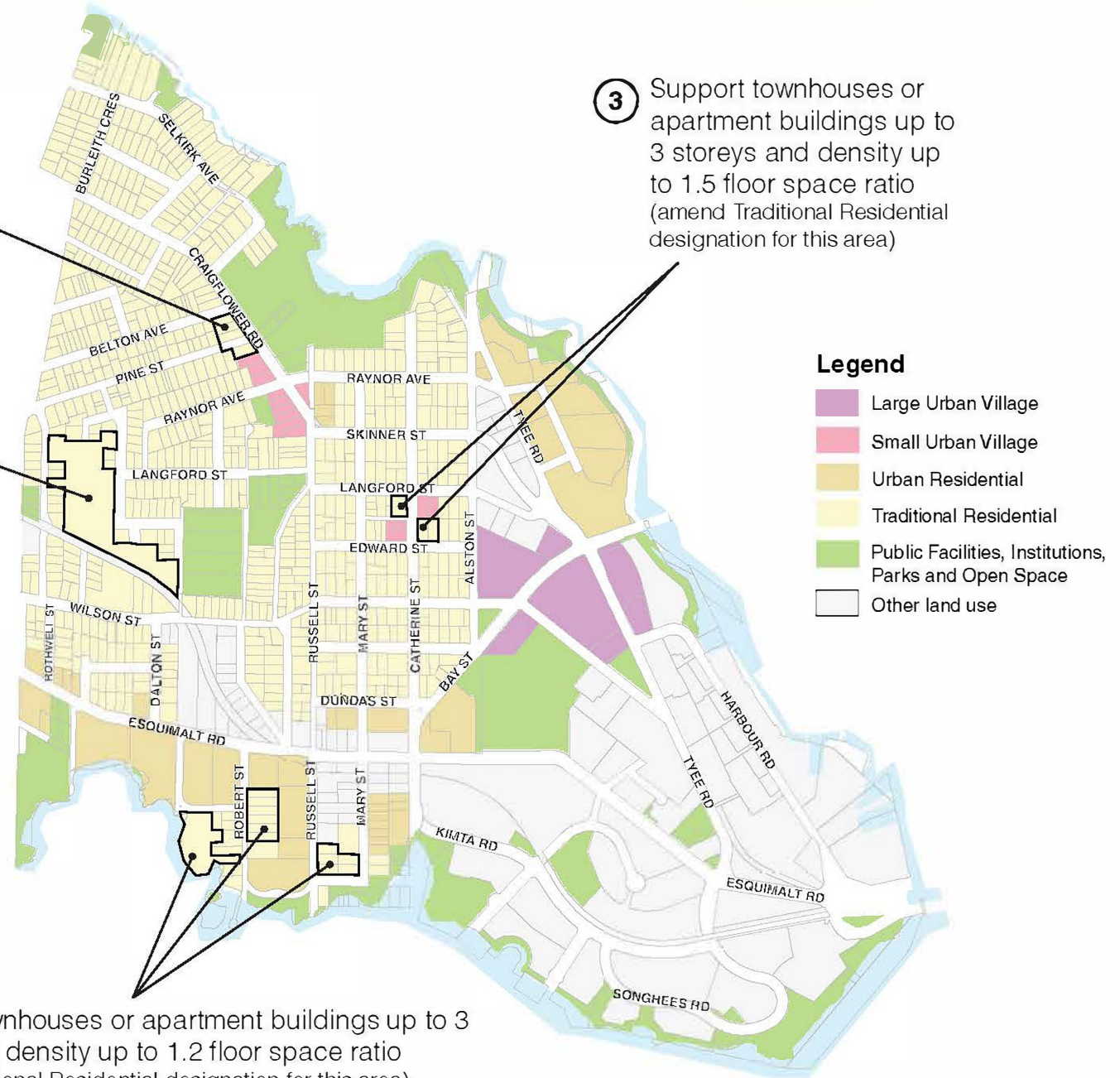


# Proposed Amendments to Urban Place Designations: Traditional Residential

- ① Support commercial uses where an existing house is retained and reused (amend Traditional Residential designation for this area)

- ② Support townhouses and small apartment buildings up to 3 storeys, and some limited commercial or community-serving uses (amend Traditional Residential designation for this area)

- ③ Support townhouses or apartment buildings up to 3 storeys and density up to 1.5 floor space ratio (amend Traditional Residential designation for this area)



## Legend

- Large Urban Village
- Small Urban Village
- Urban Residential
- Traditional Residential
- Public Facilities, Institutions, Parks and Open Space
- Other land use







- ⑤ **All other Traditional Residential areas:** Support development up to 2.5 storeys (rather than the current 2 storeys) where appropriate, to align with how building heights are calculated under zoning.

Exception: Where 3 storeys is supported.

- ④ Support townhouses or apartment buildings up to 3 storeys and density up to 1.2 floor space ratio (amend Traditional Residential designation for this area)

# Proposed Amendments to OCP Urban Place Designations to Employment Designations

## Legend

-  General Employment
-  General Employment with limited residential
-  Industrial Employment
-  Industrial Employment with limited residential
-  Public Facilities, Institutions, Parks and Open Space
-  Other land use

- 13** Retain light industrial uses and add opportunities for commercial development, (Change from General Employment to Industrial Employment, proposed neighbourhood plan supports up to 14 m [approx. 3 storeys])

- 14** Retain light industrial uses while supporting residential or commercial uses in upper floors (Change from General Employment to Industrial Employment-Residential, proposed neighbourhood plan supports up to 15-20 m [approx. 3-5 storeys] in this area)

- 12** Change from Urban Residential to Employment-Residential to retain employment uses and support additional residential uses (Change from Urban Residential to Employment-Residential, proposed neighbourhood plan supports up to 14 m [approx. 3-4 storeys] in this area)

- 13** Retain light industrial uses and add opportunities for commercial development, on eastern portion of block (Change from General Employment to Industrial Employment, proposed neighbourhood plan supports up to 18 m [4 storeys] on eastern portion of this block)

- 14** Change from General Employment to Industrial Employment-Residential to retain light industrial uses while supporting residential or commercial uses in upper floors on frontage along Alston Street (Change from General Employment to Industrial Employment-Residential, proposed neighbourhood plan supports up to 11-20 m [3-5 storeys] in this area)

- 12** Retain employment uses while supporting residential or commercial uses in upper floors (Change from General Employment to Employment-Residential, OCP continues to support up to 4 storeys)

- 12** Retain employment uses while supporting residential or commercial uses in upper floors (Change from General Employment to Employment-Residential, proposed neighbourhood plan supports up to 5 storeys in this area)

**\*Note:** The difference between "General Employment with limited residential" and "Industrial Employment with limited residential" is what uses may be considered on the ground floor (all uses must be compatible with residential neighbourhood). For more information see the info sheets regarding proposed amendments 12 and 14.



# Proposed Amendments to Development Permit Areas in the OCP

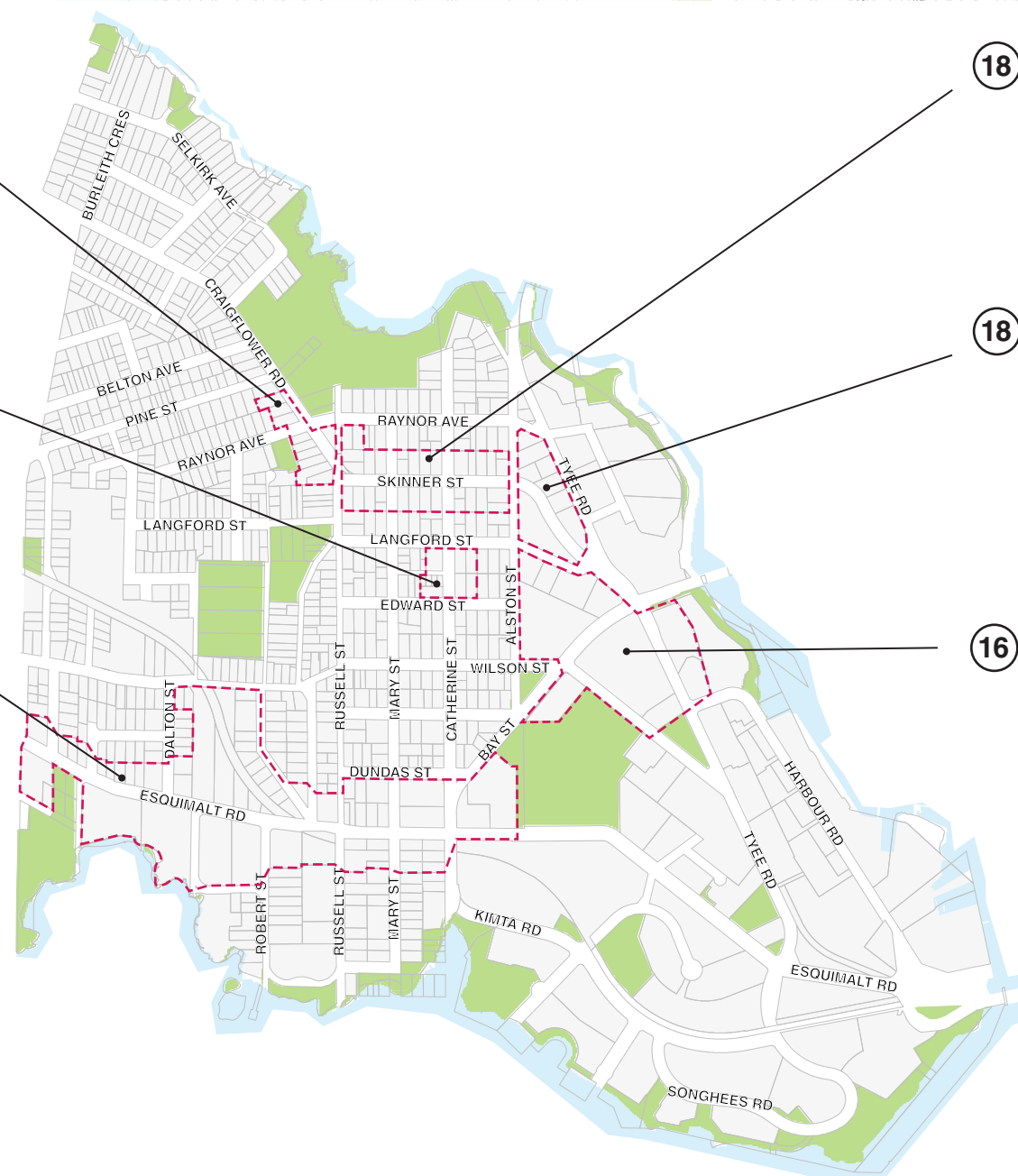
- 17** Include Craigflower Village in DPA 6A: Small Urban Villages. Apply the *Revitalization Guidelines for Corridors, Villages and Town Centres* with content specific to this area\*

- 17** Include the Catherine Street at Edward Street Village and adjacent lots in DPA 6A: Small Urban Villages, and apply the *Revitalization Guidelines for Corridors, Villages and Town Centres* with content specific to this area\*

- 18** Include the Esquimalt Road corridor in DPA 7A: Corridors, and apply *Revitalization Guidelines for Corridors, Villages and Town Centres*, and content specific to this corridor\*

\* For area-specific content, see "Area-Specific Guidelines" within *Revitalization Guidelines for Corridors, Villages and Town Centres* document.

- 19** **All Traditional Residential Areas in Vic West:** Apply Development Permit Area 15F: Intensive Residential - Townhouse and Rowhouse to any new townhouse development in Traditional Residential Areas of Vic West.



- 18** Include these blocks of Skinner Street in DPA 7A: Corridors, and apply *Revitalization Guidelines for Corridors, Villages and Town Centres* with content specific to this area\*

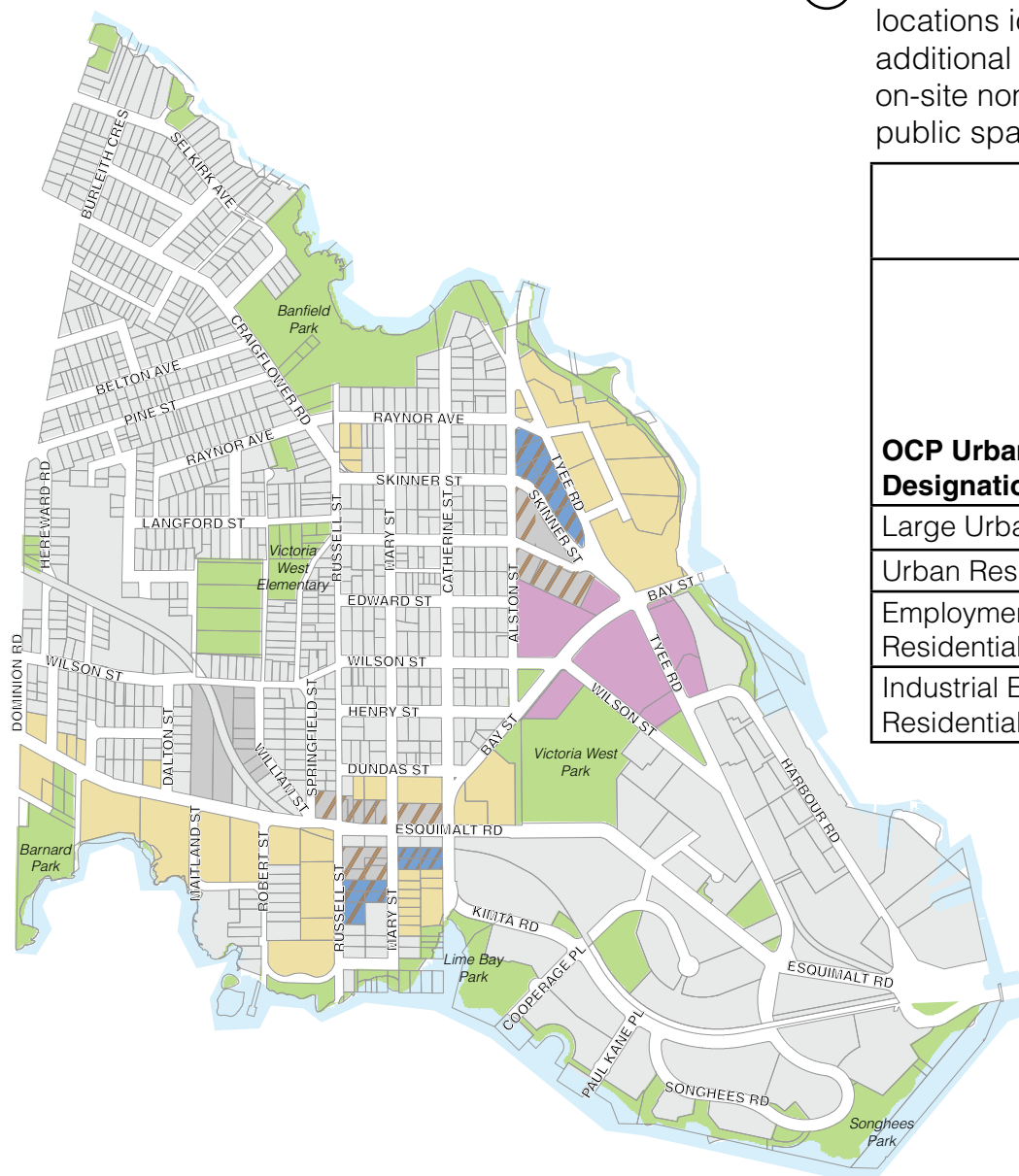
- 18** Include these two blocks fronting Skinner Street in DPA 7A: Corridors, and apply *Revitalization Guidelines for Corridors, Villages and Town Centres* with content specific to this area\*

- 16** Include properties south of Langford Street, and property at Wilson and Bay Streets, adjacent to Victoria West Park in DPA 5: Large Urban Villages – Victoria West Village. Apply *Revitalization Guidelines for Corridors, Villages and Town Centres*, and content specific to this area\*

# Densities In Vic West by OCP Urban Place Designation

- 15 Revise densities for selected Urban Place Designations to align with locations identified in the neighbourhood plan that would support additional density with the provision of community amenities (i.e. on-site non-market housing secured through a housing agreement, public space improvements)

OCP Urban Place Designation	Densities currently identified in the OCP		Proposed OCP Amendment
	Base Density (Floor Space Ratio)	Maximum Density (Floor Space Ratio) considered where community amenities are contributed	Maximum Density (Floor Space Ratio) extra density may be considered where on-site non-market housing is included (up to 10% extra)
Large Urban Village	1.5	2.5	2.75
Urban Residential	1.2	2.0	2.2
Employment-Residential	*	2.5	2.75
Industrial Employment-Residential	*	2.0	2.2



## Legend

- Large Urban Village
- Urban Residential
- General Employment with limited residential
- Industrial Employment with limited residential

## Q1 Do you have any comments regarding the revisions to the draft plan?

Answered: 47 Skipped: 21

#	RESPONSES	DATE
1	Thank you for listening to our input and responding	2/18/2018 9:30 PM
2	I am afraid that I have become jaded during the course of this process. City staff have held open houses during which they do their utmost to sell their plan to the public. Question and answers are not allowed during the presentation portions of the open houses in order to ensure any negative feedback or thoughtful questioning is heard only by the handful of people present at any one of the posters at one time. Developers bring their minions to stack the feedback tables with "green dots" that are used by staff to confirm as evidence that their plans are supported by the neighbourhood. It was an extremely discouraging experience and it's really changed my attitude towards so-called city engagement towards the negative.	2/18/2018 5:44 PM
3	I like most of the changes. Nice to see you listening to the community.	2/18/2018 2:11 PM
4	no, would it matter if I do? Developers rule!	2/18/2018 12:03 PM
5	Not in favour of taking away the possibility of apartment buildings on Skinner. Skinner is a major transport route & affordable housing for people (especially the elderly and young who cannot afford to live in single family dwellings) would be appropriate here.	2/18/2018 8:40 AM
6	I am very sceptical about the plans for Vic West and the revisions to the initial plan. Some of the plan is still very vague like taming major roads and managing traffic. What do these ideas entail and what do they mean? Despite the revisions, I still feel that the overall density of Vic West is going to change this neighbourhood dramatically - and not for the better.	2/16/2018 11:54 PM
7	Lots of great work has gone into the plan. It would be great to see some specific language regarding signage and other measures to assist with traffic calming, especially along Skinner Street. While it is a busy road used for commuting traffic between Victoria and other municipalities, people who live in the residences along the route should be able to pull in and out of their driveways or cross bicycle lanes in order to parallel park in front of their homes without being terrorized by speeding, tailgating drivers who do not want to yield. Perhaps a plan at this level is not intended to go into that detail, but it is important to note that aggressive driving is a real problem in the area.	2/16/2018 10:46 PM
8	In regard to Residential Parking and the Barricade on Belton Avenue, I just want to make note that there are ONLY 4 properties that DO NOT HAVE DRIVEWAYS and are required to Park on the street. I believe that those places NEED at least one spot of On Street Parking to be reserved (723,725,727, and partial 729). So when you think about the permanent barricade between Craigflower & Dominion near Reno think about a way to increase street parking on the Dominion side of Belton Avenue.	2/16/2018 11:27 AM
9	I like the proposals around Craigflower Village. I like the idea of permitting businesses in houses, parking along the park side and strengthening the crossing from the business side to the park side. I think the crossing at Pine st. needs to reassessed, as cars sometimes don't stop at the lights and it can be confusing for drivers coming out of Pine St whether they have right of way or not (they can't see the pedestrian lights very clearly). Perhaps an all-way pedestrian crossing schedule might be considered	2/16/2018 10:15 AM
10	I think that the original plan was really good and I wonder if a few louder people drove the change, not necessarily the majority. The complaints I heard about the original plan sounded like those people did not understand the plan nor the timeline. Density is going to increase whether we like it or not (or we will all get priced out!) and I loved the idea about increasing density so mindfully in the proposed way around the villages. I wonder if a better explanation of way this would calm traffic not increase it and also encourage less driving for future residents would have been more beneficial? I thought the way you reached out to our community to hear what we said was so great and reflected so well in the original plan.	2/16/2018 10:15 AM

## Proposed Vic West Neighbourhood Plan Update

11	It is not sustainable to limit Building height when units are required to meet demand . Preserve ground level space for pedestrian linkage and view corridors with height. height south of Esquimalt is good - sensible - economic and sustainable - townhouses are expensive to the end user as are short squat buildings and are in fact urban sprawl and NOT economic in any way for the user or the neighbourhood. Parks surround the Esquimalt region - what's missing is a place to put people in a multiplicity of high rise environments ( smaller units = affordable and age appropriate) - think about the math - get smart today for tomorrow and STOP asking families with yards " what they think " - the world is changing, the urban foot print is changing and cutting off your ability to have access to an urban street scape with smaller footprints ( sustainable ) and use height on the harbor is a perfect solution. Out with old thinking and in with modern urban planning.	2/16/2018 9:18 AM
12	Please do not allow buildings more than 3 stories to be built on the south side of Esquimalt Rd regardless of their use for affordable housing.	2/15/2018 3:01 PM
13	I think that the area around Alston/Tyee/Skinner is not uniformly designated. Seems there should be a more cohesive plan there, rather than several pieces of land designated differently.	2/15/2018 2:33 PM
14	Make using Craigflower as morning commute undesirable.	2/14/2018 10:21 PM
15	Lack low income zoning or whatever	2/14/2018 10:11 PM
16	I don't see anything specifically addressing garden suites. Still have concern about parking on Pine Street 600 block. It is currently residential parking only but residents often have to fight for parking due to people parkinf to visit Craigflower Village or the rec centre. The more serious concern is the number of cars that use upper Pine Street as a u-turn. Children are at risk when playing in their driveway, damage to fences and garages have been sustained due to people trying to turn around and we've experienced not being able to access driveways because they are blocked by non-residents. Calls to the City Parking department have led to comments that this area is not as important as other residential parking only areas and enforcement is almost nil. These are current concerns so not sure how increased density or commercial and recreational use will take this into consideration	2/14/2018 10:28 AM
17	There seems to be some improvements over the original plan.	2/13/2018 9:38 PM
18	I think 3 storey apartments should be allowed on Skinner and Suffolk . You cannot provide affordable housing at the current land price without increasing the density .	2/13/2018 7:33 PM
19	I did not see any mention of encouraging medical professionals (GPs, dentists, chiropractors, etc) to locate their offices in Vic West.	2/13/2018 5:44 PM
20	It looks good! The only thing I'm sad to see going is the plans to increase density. I think we need to build more apartments and townhouses	2/13/2018 2:10 PM
21	I see climate change addressed piecemeal in this new document, and largely in the section on buildings. Mitigating climate change won't be achieved solely through greening buildings; it will be achieved by changing how we move through our cities, or don't. Climate mitigation should be discussed in the zoning and transportation sections. And it means that decisions to remove zoning to permit 3 storeys from Skinner St., from Suffolk St., and for zones east of Russell St, bring us further from our climate mitigation goals. There is almost no other place where density makes more sense than along major thoroughfares (esp. Skinner St), where transit options are easier to access. Vic West is an urban neighbourhood, and I expect it to become more so as the Johnson St Bridge replacement makes active transportation to downtown more feasible. Enabling densification in the neighbourhood enables people to move into our neighbourhood, helps our local businesses and reduces potential greenhouse gas emissions. Either people move into Victoria, or they move further out into communities where the commute is longer. While I understand the concerns of those who don't want towers in residential neighbourhoods, I would welcome 4-6 storeys in the eastern half of the neighbourhood, and 3-4 stories west of Alston to Russell. I say this from the perspective of someone who lives a block from Skinner St and works on green buildings.	2/10/2018 5:51 PM
22	Love to see that more history of the neighbourhood will be included. I'm sorry to see building heights being reduced - we can more easily build up than out.	2/9/2018 2:17 PM
23	VWCA trying to make last minute changes :( the Association only represents small portion of community. Closed group. Doesn't care about the rest of us want.	2/7/2018 10:51 PM
24	What about tech infastructure? VW wide WiFi?	2/7/2018 10:44 PM



## Proposed Vic West Neighbourhood Plan Update

25	Do not agree with additional building heights along Esquimalt Road. It has already turned into a high rise corridor of the bridge and has impacted visibility and openness of the neighborhood. Have concerns that changes were not made to reducing proposed density overall (eg small lot zoning, adding suites to existing small lots) A housing shortage in victoria is a region wide issue and I moved to Vic West because of its size and small scale amenities and density.	2/6/2018 10:41 AM
26	Where are the roof top gardens.	2/3/2018 7:12 AM
27	I should be denser. People need places to live. The future is coming.	2/3/2018 7:04 AM
28	I don't understand all of it, but I like it.	2/3/2018 6:59 AM
29	Very good. Thank you for listening. Trying preserve the great qualities, while making room for the future. Making clear houses of historic values matters, over innovation.	2/3/2018 6:50 AM
30	Yes, I am concerned about what appears to be a reduction in density and family friendly affordable housing possibilities such as 2 or 3 bedroom apartments and townhouses.	2/2/2018 2:11 PM
31	Agree that Triangle park should remain as is. Not more trees or a garden	1/31/2018 5:25 PM
32	I am disappointed in the reduction in density in some areas (e.g. Skinner, Catherine). Feedback at community association meetings from residents who expressed a desire to maintain a "traditional neighbourhood" allowed excessive weight to be given to the specific concerns of a few over broader concerns like affordable housing.	1/30/2018 8:20 PM
33	Thanks for listening to us!	1/28/2018 8:40 AM
34	Well thought out revisions, enough detail allowing me to integrate my existing building on skinner st into new houseplex plans	1/23/2018 7:02 PM
35	I am happy to see more awareness of the value of houses that are not designated heritage, but contribute to the 'traditional' neighbourhood feel. I am happy to see the removal of plans for three story bldgs in several areas. I am happy to see more attention paid to concerns about pedestrians at various crossings. My biggest concern remains the underlying assumption that Vic West ought to have increased density. Why? Songhees, Dockside Green and the Railyards have added SO much density recent decades. I believe other part of Victoria have not taken an equal share of the load.	1/21/2018 8:21 PM
36	Figure 33 on page 78 references 3 story buildings on Raynor. Section 6.1.1 references 3 story buildings on Skinner and Raynor. Section 6.1.3 references stepping back to give the appearance of 3 stories which implies more than 3 stories. Apartments versus townhouses is just semantics. Much more detail about the master planned areas would be useful. The heights of the buildings in these areas should be included on the appropriate maps. Vic West villages should include the villages that these master planned areas will have. Buildings are still too high in many areas such as Esquimalt Street and West Side Village. Pioneer Coop having a 3 story building is totally out of context for the neighbourhood it is in. Taming Traffic section has very few actions to slow down traffic and make Vic West roads safer.	1/21/2018 6:35 PM
37	Yes, I think it's unfortunate that the plan is scaling back the initially envisioned density. Vic West is a perfect place to increase density in the city; it's a shame residents in the neighbourhood don't feel this way.	1/21/2018 5:14 PM
38	Can areas marked in the yellow section, residential be rezoned to include mixed use after the plan is submitted?	1/21/2018 4:19 PM
39	not at this time	1/21/2018 8:41 AM
40	No	1/20/2018 10:30 PM
41	Westside Village is NOT Heart of Vic West	1/20/2018 5:14 PM
42	Looks like you are listening to folks.	1/20/2018 2:23 PM
43	The team is to be commended on listening to the community and making changes as appropriate	1/20/2018 1:21 PM
44	Thank you. Looks like you put in lots of hardwork into it.	1/19/2018 5:06 PM
45	Beteeen Skinner Str., Tyee Str., Bay Str., Galloping Goose Regional Trail, Central Spur Road and Landgst Regatte area, is oversaturated of housing. Sould you stop more constructiobn and create a green area with large trees in the little space they are leaving. Living in these conditions creates many social problems.	1/18/2018 7:11 PM
46	Great	1/18/2018 6:48 PM

## Proposed Vic West Neighbourhood Plan Update

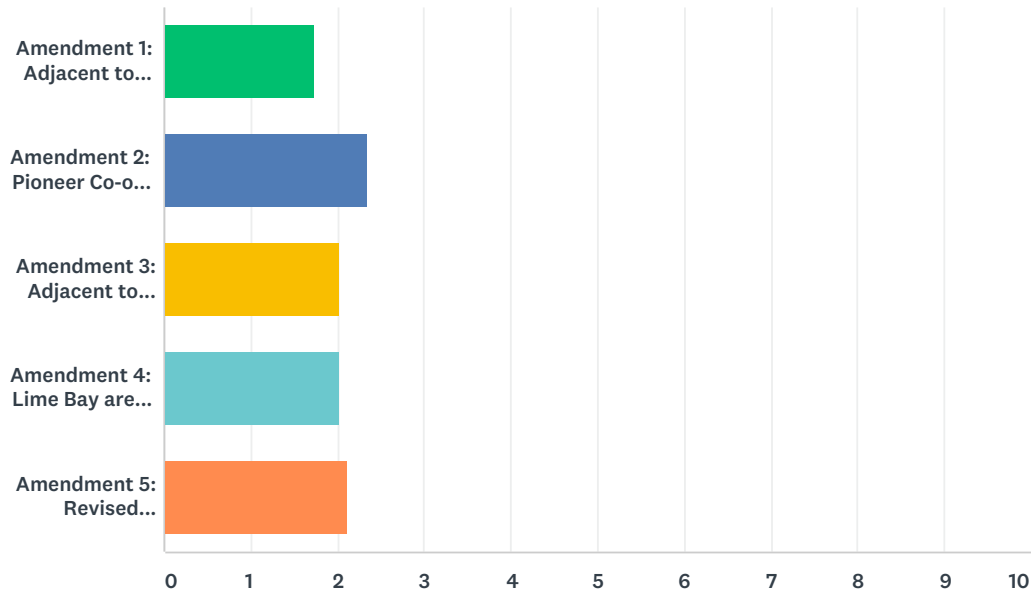
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47	Generally good. Glad you scaled back height proposal for Craigflower and Catherine villages. Sorry to see mixed-use zoning removed from Catherine village proposal -- only residential does not exactly encourage formation of a 'village'.	1/11/2018 6:15 PM
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## Q2 How supportive are you of the Official Community Plan amendments?

Answered: 60 Skipped: 8



	VERY SUPPORTIVE	SOMEWHAT SUPPORTIVE	NEUTRAL	SOMEWHAT OPPOSED	VERY OPPOSED	TOTAL	WEIGHTED AVERAGE
Amendment 1: Adjacent to Craigflower Village	55.93% 33	27.12% 16	8.47% 5	5.08% 3	3.39% 2	59	1.73
Amendment 2: Pioneer Co-op site	31.58% 18	26.32% 15	24.56% 14	10.53% 6	7.02% 4	57	2.35
Amendment 3: Adjacent to Edward at Catherine Streets Village	39.66% 23	32.76% 19	13.79% 8	12.07% 7	1.72% 1	58	2.03
Amendment 4: Lime Bay area (south of Esquimalt Road)	39.66% 23	34.48% 20	15.52% 9	3.45% 2	6.90% 4	58	2.03
Amendment 5: Revised building heights, neighbourhood-wide	41.38% 24	29.31% 17	12.07% 7	10.34% 6	6.90% 4	58	2.12

#	COMMENTS FOR "AMENDMENT 1: ADJACENT TO CRAIGFLOWER VILLAGE"	DATE
1	All well thought through residential requires supportive commercial to alleviate the need to use vehicles to get living supplies - therefore, any residential which claims to be sustainable particularly with an aging or older " stay at home" population requires forms of retain and forms of medical as well as social interaction opportunities to support community unity.	2/16/2018 9:46 AM
2	I wish the question was "Are you supportive of the revisions?," to which I would answer to all five revisions that I am "very opposed". I think the original plan's proposals were great. I this is a good location for mixed use and reasonable height (up to four storeys), along with more FSR (1.5-2.0).	2/10/2018 6:02 PM
3	Local shops	2/7/2018 10:54 PM
4	Perfect	2/7/2018 10:44 PM
5	As long as there are design standards, set backs and parking space and regulation. The danger is that increased traffic will start to impact residential parking on adjacent streets.	2/6/2018 10:48 AM
6	We need more local shop.	2/3/2018 7:19 AM
7	Use for housing. Expand stores to south along Craigflower, from Raynor to Skinner.	1/28/2018 9:07 AM

## Proposed Vic West Neighbourhood Plan Update

8	always good to have new service choices	1/23/2018 7:06 PM
9	I would support a new bldg on the lot immediately north of Sailor Jack.	1/21/2018 8:27 PM
10	Great compromise!!!	1/18/2018 6:57 PM
11	Yes, building height needs to be carefully cocontrolled here,	1/11/2018 6:17 PM
#	COMMENTS FOR "AMENDMENT 2: PIONEER CO-OP SITE"	DATE
1	Use should be focused on childcare and the like	2/18/2018 2:15 PM
2	What have the folks at the Co-op said about this?Would the buildings be part of the Co-op?	2/17/2018 12:10 AM
3	I'm not sure how maintaining the same maximum FSR enables more affordable housing. I would encourage permitting a maximum FSR of 1.5 if the co-op agrees.	2/10/2018 6:02 PM
4	Unfamiliar with area	2/7/2018 10:54 PM
5	Height changes typically negatively impact surrounding neighbors.	2/6/2018 10:48 AM
6	It will ruin my view.	2/3/2018 7:19 AM
7	Yes!!!!	2/3/2018 7:05 AM
8	3 stories are out of context for this part of Vic West. Commercial operations would not positively impact neighbours. Coop is fine as is.	1/21/2018 6:50 PM
#	COMMENTS FOR "AMENDMENT 3: ADJACENT TO EDWARD AT CATHERINE STREETS VILLAGE"	DATE
1	2.5 should be the limit	2/18/2018 2:15 PM
2	the term "small urban village" sounds quaint, but does it really reflect the kind of changes that are proposed? How does the idea affect traffic? Does the idea mean tearing down single family homes?	2/17/2018 12:10 AM
3	Roundhouse is NOT part of the proposed area - it is separate and should be omitted from this discussion ( Heritage )	2/16/2018 9:46 AM
4	I'm not sure if 3 story is appropriate but would love to see more commercial in Catherine Village	2/13/2018 8:51 PM
5	too little . we need alot more density	2/13/2018 7:59 PM
6	So far, I liked the original plan and dislike the revisions. I would love to see a small urban village here. The east side of Catherine is basically a parking lot; this is suboptimal use of frontage.	2/10/2018 6:02 PM
7	Good. It's sad looking in present state	2/7/2018 10:54 PM
8	I think the concerns of neighbors closest to those areas should be considered.	2/6/2018 10:48 AM
9	Very supportive if extra stories are greenhouses.	2/3/2018 7:19 AM
10	good to have more selection	1/23/2018 7:06 PM
11	I think a little more low-scale retail here would be lovely.	1/21/2018 8:27 PM
12	I would welcome grond levels businesses here with residents above.	1/21/2018 6:50 PM
13	More mixed use better	1/19/2018 5:08 PM
14	Same compromise or similar to Craigflower village better for supporting current business & possible expansion of market	1/18/2018 6:57 PM
15	Height restriction is fine; adjacent lots should allow mixed-use development if this is to be a 'village'.	1/11/2018 6:17 PM
#	COMMENTS FOR "AMENDMENT 4: LIME BAY AREA (SOUTH OF ESQUIMALT ROAD)"	DATE
1	What selected properties do you mean? This is an example of the plan being vague.	2/17/2018 12:10 AM
2	No sqwat building - tie the east Bayview Place site to the west with high density for more people - better amenties for all	2/16/2018 9:46 AM
3	In support for Lime Bay area but NOT all areas south of Esquimalt Rd	2/15/2018 3:07 PM
4	Keeping building hieight lower towards the water, and encouraging sympathetic design to existing older structures.	2/13/2018 9:46 PM



## Proposed Vic West Neighbourhood Plan Update

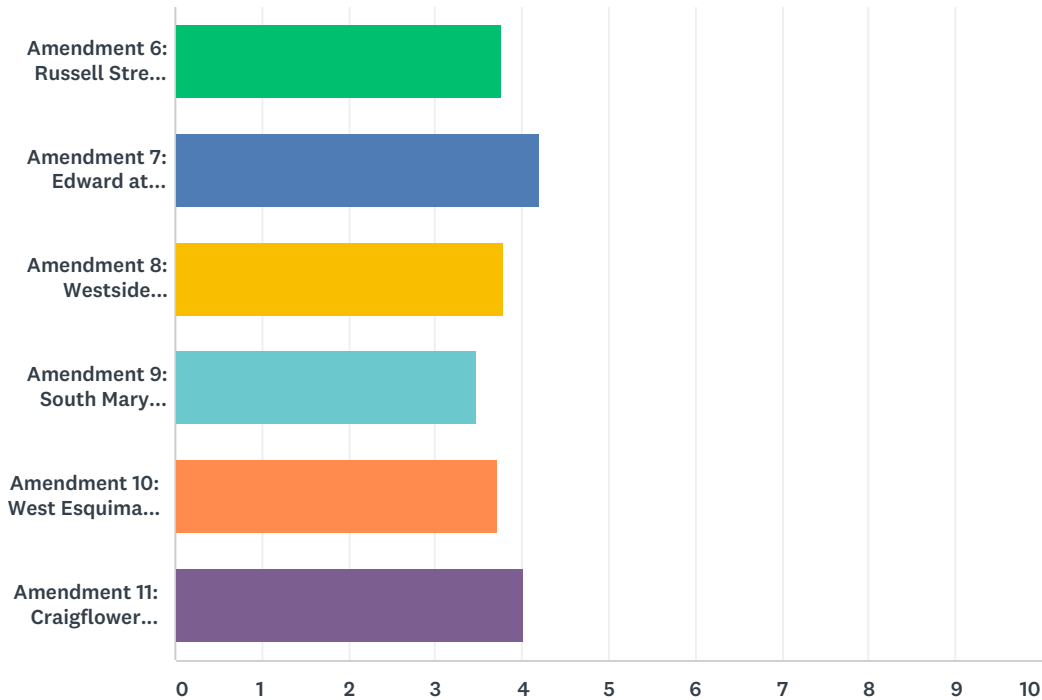
5	Yet again, the original plan's ambition was great and the update waters it down. This area makes sense for 3-5 storeys.	2/10/2018 6:02 PM
6	Again my concern is increased density with height.	2/6/2018 10:48 AM
7	Building heights of 3 stories should be the maximum for people on the third level to be connected with people on the ground. People love old town Victoria for this reason.	1/21/2018 6:50 PM
#	COMMENTS FOR "AMENDMENT 5: REVISED BUILDING HEIGHTS, NEIGHBOURHOOD-WIDE"	DATE
1	2.5 should be the limit	2/18/2018 2:15 PM
2	I am concerned that densifying Vic West will lead to more traffic congestion. During my 14 years in the neighbourhood, I have seen an enormous increase in traffic along Esquimalt Rd. Tyee Rd., Bay St. and Wilson St. - to name a few! With the increase in traffic, I see more drivers taking more chances i.e. running red lights and I am very concerned that the plans for Vic West are not taking into account how further densification of this neighbourhood will affect the flow of traffic in and through this area.	2/17/2018 12:10 AM
3	Parking is always an issue, I believe that we need to consider a way to provide for that without leaning too far the other way (ie: bicycles). There are mobility issues that the City does not seem to consider when planning residential housing,	2/16/2018 11:30 AM
4	Future housing demand requires ( tall building containers ) located on smaller sites for more open urban spaces to socialize, circulate and receive benefits associated with the new nature of the area. It is un thinkable to assemble the over sized foot prints of multi residential buildings based on archaic thinking by non informed older planners or City officials who have no idea what modern urbanism looks like. Victoria requires smaller - affordable - units for more people - Townhouses are expensive strata fee hogs - these should be eliminated and instead be absorbed into the height of a tall slim building form - Townhouses are the " hogs " of both "land " on the ground plane and are indulgent since only one or two families get a benefit of what could be many. Town houses should instead be replaced with small supporting commercial to support the multi residential population which takes the need for more vehiclular trips to get food, medicine, post or other living chores completed. As tall and as thin as you can possibly go with enough supporting functions as noted is optimum. Crime loves townhouses ( I speak from numerous tales of break ins reported by current townhouse owners) - instead being in secure buildings is preferred. More people vertically is the international preference benefitting many not a select few in smaller buildings which creates un necessary sprawl.	2/16/2018 9:46 AM
5	I would like to see 2.5 storey plus basements allowed in any existing house. I have an existing 1908 character house with a small footprint but 2.5 storey height with a 6 foot basement and I would like to raise the house to give it a full basement, but apparently this is not permissible. There should be more allowances for retention of character houses even if they aren't heritage designated.	2/15/2018 9:52 PM
6	you are crazy not to increase the building heights . Also 2 1/2 storey structures should be allowed to have basements .The future of affordable housing can't be wished into existence . It has to be built . I redeveloped my own property and know full well the costs involved . If older homes cannot be redeveloped into 4 or 5 plex units professionally then it will not not be affordable to take on these challenges. Go higher in density and go vertical . Your first proposal for skinner and craigflower was not high enough in my opinion .	2/13/2018 7:59 PM
7	I think they should be 3 or 4 stories. Build up the air space is free!	2/13/2018 2:13 PM
8	I don't agree with most of the changes made; they keep people out of the neighbourhood. I think that making it harder for row houses and for increased height (e.g. three stories west of Russell) reduces the likelihood that the services that are needed and desired to be provided, and drives growth outwards. This is not beneficial to the region, for our already stressed transportation networks, or for mitigating climate change.	2/10/2018 6:02 PM
9	Caution with too much change in traditional neighbourhood .	2/7/2018 10:54 PM
10	The designation should remain the same, it is a key feature of this neighborhood and will contribute to the density which I believe is already sufficient.	2/6/2018 10:48 AM
11	Additional story should be for food production	2/3/2018 7:19 AM
12	Even taller	2/3/2018 7:05 AM
13	Your diagram shows basement 12 metres above ground, not 1.2 metres.	1/28/2018 9:07 AM

## Proposed Vic West Neighbourhood Plan Update

14	allows me to stay within part 9 of bcbs1212 but save greenspace	1/23/2018 7:06 PM
15	Additional height will help with subtly increasing density. I don't think there is a need to mandate peaked roofs	1/21/2018 6:50 PM
16	The lime bay area would benefit from additional park for walking and crossing the property to get to the Vic West park via Esquimalt road.	1/21/2018 4:23 PM

## Q3 How supportive are you of the above Official Community Plan amendments?

Answered: 59 Skipped: 9



	VERY SUPPORTIVE	SOMEWHAT SUPPORTIVE	NEUTRAL	SOMEWHAT OPPOSED	VERY OPPOSED	TOTAL	WEIGHTED AVERAGE
Amendment 6: Russell Street at Skinner Street	47.27% 26	20.00% 11	12.73% 7	1.82% 1	18.18% 10	55	3.76
Amendment 7: Edward at Catherine Street Village	55.36% 31	25.00% 14	8.93% 5	7.14% 4	3.57% 2	56	4.21
Amendment 8: Westside Village and Bay Street/Tyee Road	43.86% 25	24.56% 14	10.53% 6	8.77% 5	12.28% 7	57	3.79
Amendment 9: South Mary Street	29.09% 16	21.82% 12	29.09% 16	7.27% 4	12.73% 7	55	3.47
Amendment 10: West Esquimalt Road	32.14% 18	33.93% 19	16.07% 9	10.71% 6	7.14% 4	56	3.73
Amendment 11: Craigflower small urban village	53.57% 30	21.43% 12	7.14% 4	8.93% 5	8.93% 5	56	4.02

#	COMMENTS FOR "AMENDMENT 6: RUSSELL STREET AT SKINNER STREET"	DATE
1	The limit should be 2 stories	2/18/2018 2:19 PM
2	What selected properties are you talking about? The idea is vague! How would any changes affect traffic?	2/17/2018 12:26 AM
3	Would prefer that it be mixed use rather than pure residential - commercial on ground floor, residential above.	2/15/2018 9:53 PM
4	Too high	2/14/2018 7:33 PM

## Proposed Vic West Neighbourhood Plan Update

5	This would be great, and the corner provides an excellent opportunity for densification. It would also be excellent that, if this corner were developed, active transportation networks here were enhanced.	2/10/2018 6:20 PM
6	Supporting local shops.	2/7/2018 10:57 PM
7	Greenhouse roof	2/3/2018 7:23 AM
8	This amendment will damage the village and adjacent residential area.	1/28/2018 9:39 AM
9	More retail here, yes, but no to 4 story buildings.	1/21/2018 8:32 PM
10	4 story building here is too high and out of context with the adjacent houses. Old town Victoria is delightful because it is not 4 stories.	1/21/2018 6:58 PM
11	attention to placement of traffic visibility obstructions at intersection including fences. Pedestrian and cyclist safety of concern there	1/21/2018 4:28 PM
<b>#</b>	<b>COMMENTS FOR "AMENDMENT 7: EDWARD AT CATHERINE STREET VILLAGE"</b>	<b>DATE</b>
1	The limit should be 2 stories	2/18/2018 2:19 PM
2	Don't let existing commercial get replaced with just residential. New development should be required to be mixed use.	2/15/2018 9:53 PM
3	Ideally, this would be for the whole block and would be street-oriented, so that the parking is moved behind the buildings on the east side of Catherine.	2/10/2018 6:20 PM
4	Area needs improvement	2/7/2018 10:57 PM
5	Nice!	2/7/2018 10:46 PM
6	1st level local shop, 2nd places to live, 3rd food to eat. All local	2/3/2018 7:23 AM
<b>#</b>	<b>COMMENTS FOR "AMENDMENT 8: WESTSIDE VILLAGE AND BAY STREET/TYEE ROAD"</b>	<b>DATE</b>
1	The limit should be 4 stories	2/18/2018 2:19 PM
2	As proposed NO! This area is already high in traffic volume. Without any ideas on how to ensure that vehicles can move in and out of the "Large Urban Villages" (I really dislike these terms) without causing more of a traffic snarl - NO! And creating an architectural statement???? It seems to me that City Hall is trying to sell residents of Vic West a massive densification plan that is dressed up with terms like this and the "urban village" concept. Frankly, I find these terms insulting!	2/17/2018 12:26 AM
3	While I support density, I'm not sure sure this is the right place for more 6 story buildings	2/16/2018 10:51 PM
4	I feel that the Large Urban Village should extend to the north and include the current industrial lands. That will allow for growth of amenities that can service the Railyards and the Dockside Green development, not to mention the traditional neighbourhoods. The current size for the Large Urban Village is not large enough.	2/15/2018 2:37 PM
5	Too high	2/14/2018 7:33 PM
6	While most changes will take time, this would be great if it could happen as quickly as possible. The parking lot on the NW corner of Bay and Tyee is a blight and creates a hole in our neighbourhood.	2/10/2018 6:20 PM
7	Make it same as adjoining properties	2/7/2018 10:57 PM
8	Yes. Currently wasted space	2/7/2018 10:46 PM
9	This area with Dockside and other current developments is dense enough!	2/6/2018 10:51 AM
10	Large greenhouses on top like Wholefoods in NYC.	2/3/2018 7:23 AM
11	Limit height to 12 storeys. Retain parking for retail users.	1/28/2018 9:39 AM
12	No to taller buildings.	1/21/2018 8:32 PM
13	6 stories is too much. More than 6 stories is definitely not how I would like Vic West to evolve.	1/21/2018 6:58 PM
<b>#</b>	<b>COMMENTS FOR "AMENDMENT 9: SOUTH MARY STREET"</b>	<b>DATE</b>
1	The limit should be 2 stories	2/18/2018 2:19 PM
2	Too high.	2/14/2018 7:33 PM

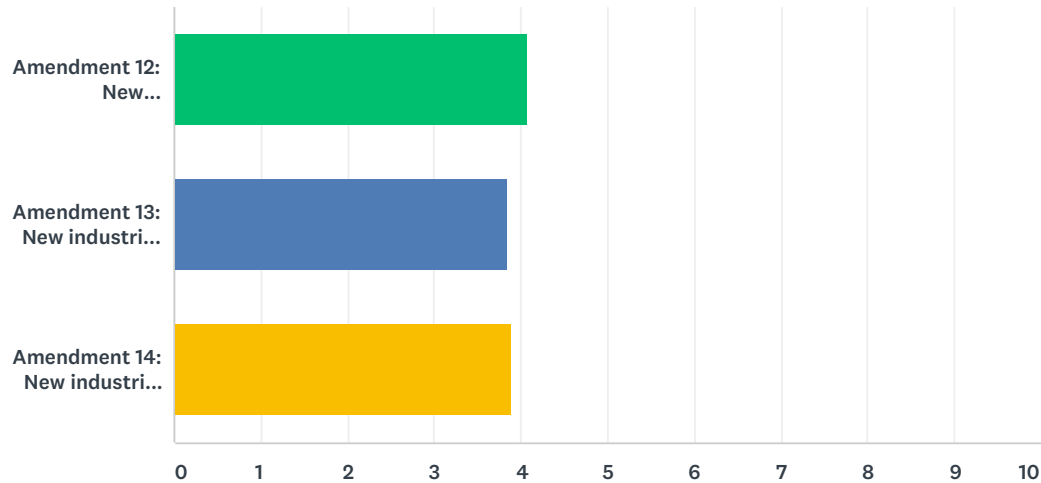


## Proposed Vic West Neighbourhood Plan Update

3	Why remove properties from the urban residential zoning? It appears that these lots are already fairly dense with townhouses, and even different zoning wouldn't affect the lots.	2/10/2018 6:20 PM
4	What about the two or three traditional residential houses?	2/7/2018 10:57 PM
5	What's there now?	2/3/2018 7:06 AM
6	Limit height to 3-storeys.	1/28/2018 9:39 AM
7	too high! Leave this as traditional residential. There is enough height in the master planned developments.	1/21/2018 6:58 PM
8	rail area and adjacent parking and industrial ideal for walking paths	1/21/2018 4:28 PM
9	Skeptical	1/18/2018 7:17 PM
#	<b>COMMENTS FOR "AMENDMENT 10: WEST ESQUIMALT ROAD"</b>	<b>DATE</b>
1	The limit should be 2 stories	2/18/2018 2:19 PM
2	Too much density. Esquimalt road is becoming more and more choked with vehicles.	2/17/2018 12:26 AM
3	Too high.	2/14/2018 7:33 PM
4	I don't see why you need 5 story buildings for mixed commercial/residential. Surely living on a busy street is less than optimum. Walking a few blocks to public transit would seem preferable.	2/13/2018 5:52 PM
5	Why not all the way to Dalton?	2/10/2018 6:20 PM
6	Limit to 4-storeys, not 5.	1/28/2018 9:39 AM
7	Too high! Leave as 3 story buildings. We already have enough tall buildings in Vic West	1/21/2018 6:58 PM
8	REmaining garry oak meadow and natural areas preserved for wildlife	1/21/2018 4:28 PM
#	<b>COMMENTS FOR "AMENDMENT 11: CRAIGFLOWER SMALL URBAN VILLAGE"</b>	<b>DATE</b>
1	The limit should be 2 stories	2/18/2018 2:19 PM
2	Again, be specific. What selected properties are you talking about? Would trees have to be cut down? Currently, the large trees in the area add to its character.	2/17/2018 12:26 AM
3	There are some interesting character homes in this area that are probably not heritage designated and would therefor be likely torn down to make way for higher density residential, rather than adapting and reusing the existing. I would rather see those interesting houses retained and adapted.	2/15/2018 9:53 PM
4	Supportive despite the effects it will have on residents adjacent due to increased traffic	2/14/2018 10:32 AM
5	Would be good if the FSR could be increased to 2.0.	2/10/2018 6:20 PM
6	Support local. I want to open small business in Vic West. Currently no places to open my shop	2/7/2018 10:57 PM
7	Love it	2/7/2018 10:46 PM
8	Yes, yes, yes. Shop local.	2/3/2018 7:23 AM
9	Additional retail should be extended southward to Skinner/ Russel St. across from existing fast-food joint.	1/28/2018 9:39 AM
10	wonderful to have more amenities	1/23/2018 7:07 PM
11	These are delightful old houses. Don't encourage them to be torn down.	1/21/2018 6:58 PM
12	proper lighting and improved sidewalk with bustop access essential for development in this area	1/21/2018 4:28 PM

## Q4 How supportive are you of the above Official Community Plan amendments?

Answered: 54 Skipped: 14



	VERY SUPPORTIVE	SOMEWHAT SUPPORTIVE	NEUTRAL	SOMEWHAT OPPOSED	VERY OPPOSED	TOTAL	WEIGHTED AVERAGE
Amendment 12: New employment-residential designation (various properties)	51.85% 28	25.93% 14	7.41% 4	7.41% 4	7.41% 4	54	4.07
Amendment 13: New industrial employment designation (various properties)	41.51% 22	28.30% 15	13.21% 7	7.55% 4	9.43% 5	53	3.85
Amendment 14: New industrial-residential designation (various properties)	43.40% 23	28.30% 15	11.32% 6	7.55% 4	9.43% 5	53	3.89

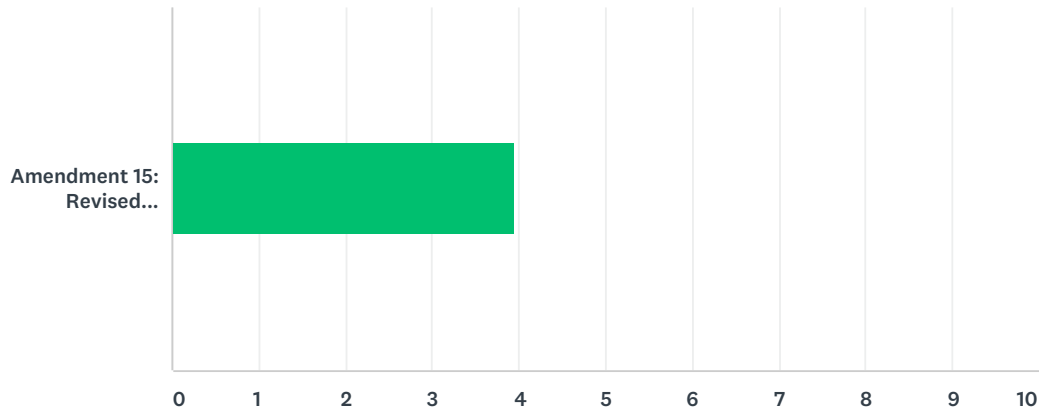
#	COMMENTS FOR "AMENDMENT 12: NEW EMPLOYMENT-RESIDENTIAL DESIGNATION (VARIOUS PROPERTIES)"	DATE
1	I don't mind the designation, I mind the allowable building heights.	2/18/2018 5:45 PM
2	The limit should be 3 stories	2/18/2018 2:20 PM
3	I am very supportive of this property designation. The only reason I have ticked the somewhat supportive box is that I would like to see this designation apply to all three properties that border the Railyards development. Consistency of use across this geographical area would provide a good transition from Railyards to Single Family Residential. Furthermore all these properties are on a major transit route between VicWest and the downtown core. It would be very appropriate for all three properties to have residential. The middle property only has residential on a portion of the property. Seems odd to me.	2/18/2018 8:43 AM
4	I want to see how the City plans to deal with the increase in traffic in this neighbourhood should this plan go through. Also, I am concerned that some of Vic West's character is going to be changed by having buildings that are nothingmore than everyday boxes and rectangles.	2/17/2018 12:31 AM
5	As long as the designation doesn't frustrate the possible use of the land for residential above the industrial designations.	2/15/2018 2:42 PM
6	Too high.	2/14/2018 7:35 PM
7	With food guaranteed in plan	2/3/2018 7:24 AM

## Proposed Vic West Neighbourhood Plan Update

8	4&5 story buildings are too high.	1/21/2018 7:05 PM
9	vehicle access, parking and roadway improvements needed	1/21/2018 4:30 PM
#	COMMENTS FOR "AMENDMENT 13: NEW INDUSTRIAL EMPLOYMENT DESIGNATION (VARIOUS PROPERTIES)"	DATE
1	I don't mind the designation, I mind the allowable building heights.	2/18/2018 5:45 PM
2	The limit should be 3 stories	2/18/2018 2:20 PM
3	I am not in favour "downgrading" properties from General Employment to Light Industrial. I want to keep employment opportunities in our community. If under the current designation properties are being used for light industrial then why is a more limiting designation necessary to "retain" them? As the economy of Greater Victoria changes, commercial and office uses might be more needed. I am in favour of our employment properties having as much flexibility in use as possible.	2/18/2018 8:43 AM
4	Some of those properties may be more suited for residential or mixed use.	2/15/2018 9:56 PM
5	As long as there is not a mix of designations that frustrates the possible residential opportunities above the first floor.	2/15/2018 2:42 PM
6	Too high.	2/14/2018 7:35 PM
7	I'm assuming there is current zoning and regulations in place to support the current industrial footprint.	2/6/2018 10:54 AM
8	Work local	2/3/2018 7:24 AM
9	4 story buildings are too high.	1/21/2018 7:05 PM
10	vehicle access, parking and roadway improvements needed	1/21/2018 4:30 PM
#	COMMENTS FOR "AMENDMENT 14: NEW INDUSTRIAL- RESIDENTIAL DESIGNATION (VARIOUS PROPERTIES)"	DATE
1	I don't mind the designation, I mind the allowable building heights.	2/18/2018 5:45 PM
2	The limit should be 3 stories	2/18/2018 2:20 PM
3	See comments in Amendment 12 and 13. I am not really sure what this designation accomplishes. I think all the properties should have the designation Employment-Residential.	2/18/2018 8:43 AM
4	Some areas would not be appropriate for the designation	2/16/2018 10:56 PM
5	This seems like the most flexible designation and mirrors the General Employment designation that currently exists on these properties.	2/15/2018 2:42 PM
6	Too high.	2/14/2018 7:35 PM
7	I hope this can capture and lead to changes in the eyesore beer distribution building on Esquimalt between Mary and Catherine. While the distribution facility is important (I like beer), the blank faces of the building are a detriment to the community in their current state.	2/10/2018 6:41 PM
8	I've not seen the city put forward any issues with current zoning and regulations that already support industrial use.	2/6/2018 10:54 AM
9	Good hybrid idea	2/3/2018 7:24 AM
10	Limit to four storeys.	1/28/2018 9:49 AM
11	4 story buildings are too high.	1/21/2018 7:05 PM
12	vehicle access, parking and roadway improvements needed	1/21/2018 4:30 PM

## Q5 How supportive are you of this amendment?

Answered: 56 Skipped: 12



	VERY SUPPORTIVE	SOMEWHAT SUPPORTIVE	NEUTRAL	SOMEWHAT OPPOSED	VERY OPPOSED	TOTAL	WEIGHTED AVERAGE
Amendment 15: Revised densities	50.00% 28	19.64% 11	16.07% 9	5.36% 3	8.93% 5	56	3.96

#	COMMENTS FOR "AMENDMENT 15: REVISED DENSITIES"	DATE
1	All of this density throughout neighbourhoods, PLUS huge densities in enormous towers, plus full build-outs of already planned areas, yet NO PLANS for more or bigger schools or schoolyards, and extremely vague plans for managing transportation.	2/18/2018 5:47 PM
2	I am not opposed to changes in general, but the kinds of changes being proposed to Vic West will, I fear, fundamentally change this neighbourhood. I am not convinced that the plan for Vic West has really looked at how issues such as traffic will be affected, how the character of this neighbourhood will be retained.	2/17/2018 12:35 AM
3	Parking & Mobility issues need to be considered!	2/16/2018 11:33 AM
4	Taking is nice but the City needs to give also and could benefit by managing the parks and the dog parks better - increasing the landscape maintenance crews in Victoria West instead of grabbing everything and everyone for down town - the parks are all in disrepair thanks to a labour shortage and bad management by the City of Victoria Parks - Density is owed to many of the current developers by the City and the City has benefitted from certain developers whom the City has treated shabbily -	2/16/2018 9:57 AM
5	Lots with character buildings on them should be identified and excluded from the increased density as a way to retain the character. Lots with unremarkable buildings can benefit from increased density to redevelop.	2/15/2018 10:01 PM
6	So long as the community amenities are actually delivered by this increased density, I am in favour.	2/13/2018 9:16 PM
7	I'd support even more than a 10% boost for non-market housing.	2/10/2018 6:44 PM
8	City lacks housing.	2/7/2018 10:58 PM
9	With food on top.	2/3/2018 7:24 AM
10	We need density. Stop wasting valuable land. We have so little. They are destroying our wilderness in the West Shores. Sprawl is bad.	2/3/2018 7:08 AM
11	it will be great to finally have more housing and less car traffic	1/23/2018 7:09 PM
12	We've had a huge increase in density in Vic West since I moved here in 1990. Songees, Railyards, Dockside Green. Has James Bay or Fairfiled had equiva	1/21/2018 8:37 PM

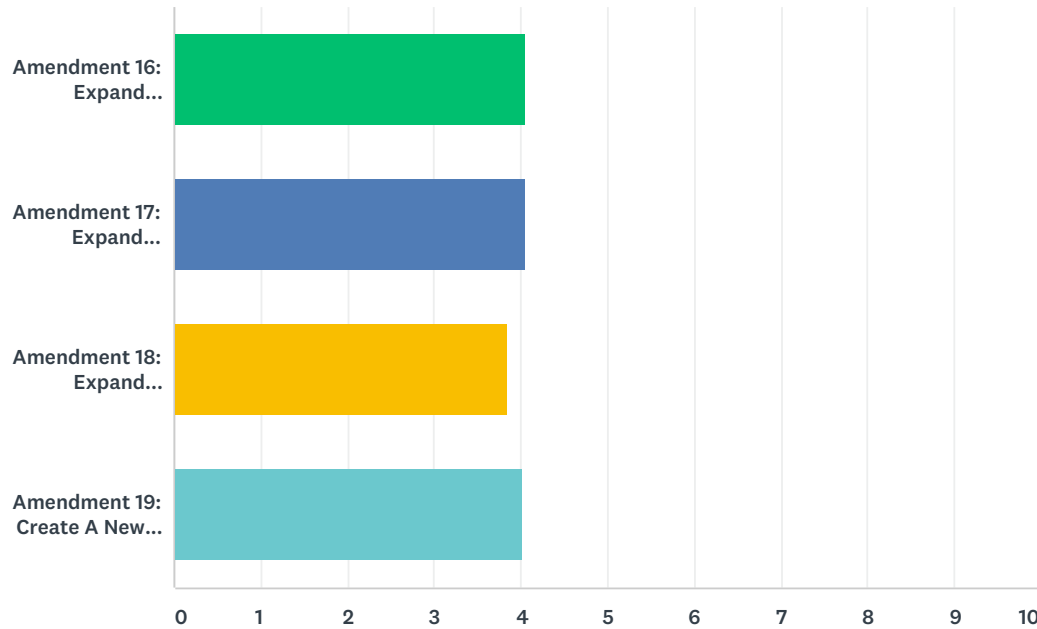


## Proposed Vic West Neighbourhood Plan Update

13	Bonus density for amenities is too large!	1/21/2018 7:07 PM
14	Our neighbourhood is growing. This is good. Keep housing affordable for families, singles, those on fixed and low incomes, seniors, assisted living for seniors and people with mental health and addiction issues.	1/21/2018 4:32 PM

## Q6 How supportive are you of the above Official Community Plan amendments?

Answered: 50 Skipped: 18



	VERY SUPPORTIVE	SOMEWHAT SUPPORTIVE	NEUTRAL	SOMEWHAT OPPOSED	VERY OPPOSED	TOTAL	WEIGHTED AVERAGE
Amendment 16: Expand Development Permit Area 5	50.00% 25	24.00% 12	16.00% 8	2.00% 1	8.00% 4	50	4.06
Amendment 17: Expand Development Permit Area 6A	52.00% 26	20.00% 10	16.00% 8	6.00% 3	6.00% 3	50	4.06
Amendment 18: Expand Development Permit Area 7A	40.82% 20	24.49% 12	20.41% 10	6.12% 3	8.16% 4	49	3.84
Amendment 19: Create A New Development Permit Area 15F	48.94% 23	23.40% 11	14.89% 7	6.38% 3	6.38% 3	47	4.02

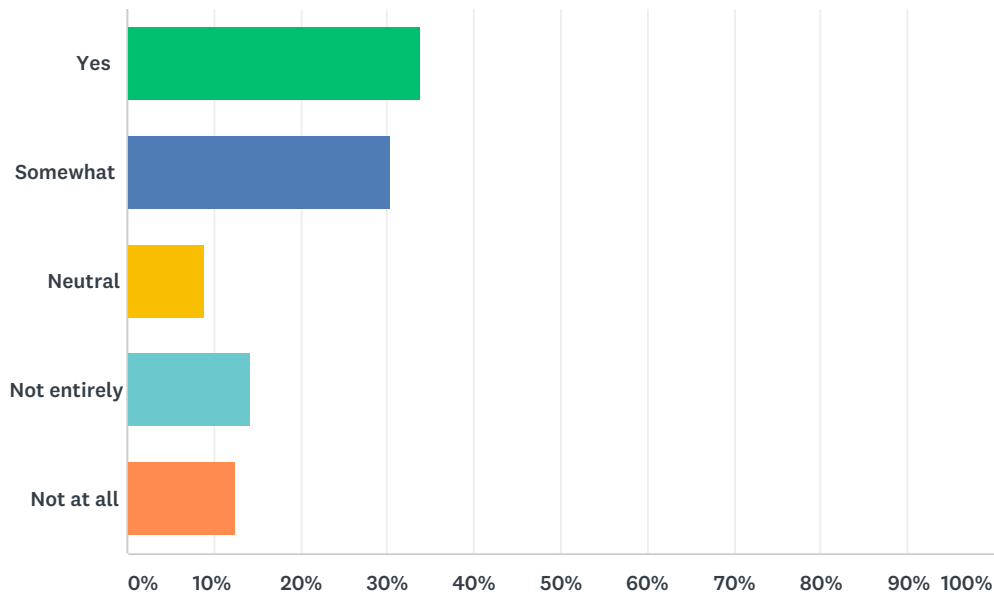
#	COMMENTS FOR "AMENDMENT 16: EXPAND DEVELOPMENT PERMIT AREA 5"	DATE
1	Important security for neighbourhood	2/14/2018 10:14 PM
2	Make current property owners provide a safe walk way for pedestrians on Langford and Alston Streets.	2/14/2018 7:38 PM
3	I like the expansion. Can this be used to enhance the park on the northwest corner of Bay and Wilson?	2/10/2018 7:49 PM
4	DPA's are important to maintain communities needs	2/7/2018 10:48 PM
5	Agreeing to this assumes agreement to other revisions that relate to adding density and changing the designations of certain areas.	2/6/2018 10:57 AM
6	Supportive assuming heights are reduced from proposed levels.	1/21/2018 9:34 PM
#	COMMENTS FOR "AMENDMENT 17: EXPAND DEVELOPMENT PERMIT AREA 6A"	DATE
1	Ditto	2/14/2018 10:14 PM
2	I would particularly approve of buildings being constructed to the sidewalk on Catherine St.	2/10/2018 7:49 PM

## Proposed Vic West Neighbourhood Plan Update

3	Same	2/7/2018 10:48 PM
4	Agreeing to this assumes agreement to other revisions that relate to adding density and changing the designations of certain areas.	2/6/2018 10:57 AM
5	Supportive assuming heights are reduced from proposed levels.	1/21/2018 9:34 PM
#	<b>COMMENTS FOR "AMENDMENT 18: EXPAND DEVELOPMENT PERMIT AREA 7A"</b>	<b>DATE</b>
1	Ditto	2/14/2018 10:14 PM
2	I generally disagree with the low number of storeys permitted along this corridor, as I believe this corridor should be densified, and I would approve of development to the sidewalk on this street.	2/10/2018 7:49 PM
3	DPA's good	2/7/2018 10:48 PM
4	Agreeing to this assumes agreement to other revisions that relate to adding density and changing the designations of certain areas.	2/6/2018 10:57 AM
5	I am opposed to allowing this area to be redeveloped as a "corridor".	1/28/2018 10:01 AM
6	Supportive assuming heights are reduced from proposed levels.	1/21/2018 9:34 PM
#	<b>COMMENTS FOR "AMENDMENT 19: CREATE A NEW DEVELOPMENT PERMIT AREA 15F"</b>	<b>DATE</b>
1	Great idea	2/15/2018 10:03 PM
2	Ditto	2/14/2018 10:14 PM
3	Need more areas with the same protection	2/7/2018 10:48 PM
4	Agreeing to this assumes agreement to other revisions that relate to adding density and changing the designations of certain areas.	2/6/2018 10:57 AM
5	restricted design can be very subjective for these smaller buildings	1/23/2018 7:14 PM
6	Supportive assuming heights are reduced from proposed levels.	1/21/2018 9:34 PM

## Q7 Were you as involved in the development of this plan as you wanted to be?

Answered: 56 Skipped: 12



ANSWER CHOICES	RESPONSES	
Yes	33.93%	19
Somewhat	30.36%	17
Neutral	8.93%	5
Not entirely	14.29%	8
Not at all	12.50%	7
TOTAL		56

#	COMMENT	DATE
1	I found it extremely frustrating that there were only 2 levels of involvement. The first being an extremely shallow understanding based on 10 minutes at an open house whereupon you are deemed "informed" enough to place a green dot on your "approval" of the city staff's plans, or effectively spending the same amount of time as that of an undergraduate degree in order to read, ask questions, and try to understand what is actually going on in the 100's of pages of content of required reading.	2/18/2018 5:48 PM
2	I am a property owner. Somehow I did not receive communication re: the process until it was well underway.	2/18/2018 8:43 AM
3	I have had several conversations with planners and feel that quite often, my concerns were not heard.	2/17/2018 12:38 AM
4	I attended some community meetings but did not have the capacity to be as involved as I would have liked.	2/16/2018 11:00 PM
5	I should have got on it earlier in the process, so its my own fault.	2/16/2018 11:34 AM
6	The community consultations were good, but I had trouble finding a time that suited my family.	2/16/2018 10:18 AM

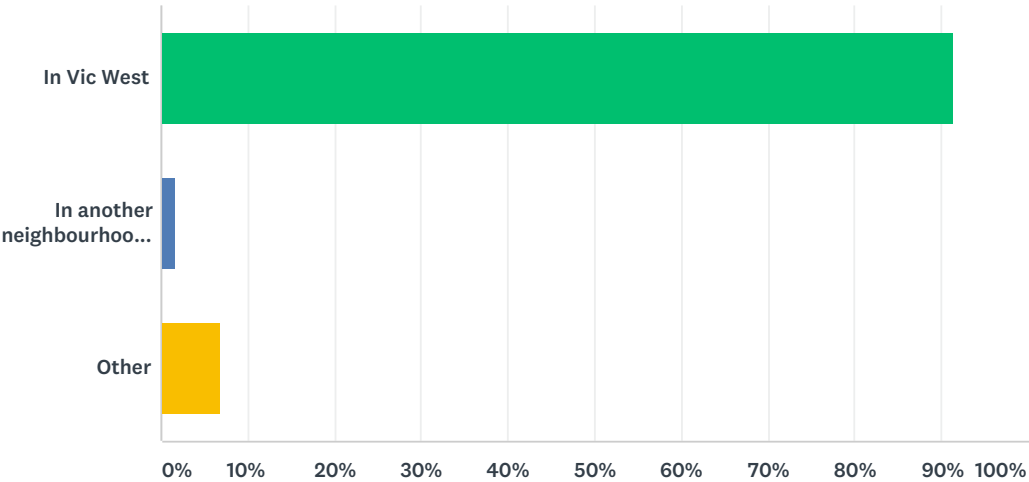
## Proposed Vic West Neighbourhood Plan Update

7	Modern principles of urban planning particularly in the wake of increased real estate pricing on all forms of housing requires " thinking " that learns from the past 10 years or more of City led mistakes based on " old " outdated modes of education. The urban sprawl and urban and suburban neighbourhoods created in the 50's and 60's no longer apply. Using up precious land with large footprints of structures whether a single family home, duplex, fourplex, apartment building or townhouse configuration is " old" thinking. Functionally the damage created by NOT understanding traffic - security - drug use areas - dog park disease from non cleaning - car and vehicular problems - ease of delivery of goods and services to adjacent " neighbourhoods" gets omitted from proper evaluation in lieu of a fight over creating height that makes sense, parking that makes sense, security that makes sense. Indulgent neighbors ask for townhouse or low cost housing but forget that in order to live well, live safe, have social contact and receive goods and services requires more than a demand but more importantly an overall - long term vision. We know townhouse DO NOT WORK because they are traditionally like single family residences in terms of square footage and they are expensive for the owner to maintain with strata fees and a mortgage. We know that frontages ( front doors ) that are not secured with proper monitored entrances result in break ins and violence by drug crazy street people but we build them without proper surrounds, fences or protection - this practice should be stopped. All multi residential units should have a central entrance system which is monitored by FOB, by a security person or communal watch - to continue to add buildings to the mix without thinking through the consequences of the building type is short sighted. This is why modern urbanism ( residential ) resembles the traditional hotel design model - ( a common entry - a monitoring system - containment of people and pets - public spaces for people/pets and socializing - a place for a secured vehicular storage - The point is being responsible means being responsible not only for the buildings being safe and secure, sustainable ground plate size ( not sprawl ) , maintained parks and walkways, accessible food-medical - post - fuel - ( all neighbourhoods and sub neighbourhoods of larger neighbourhoods) but it means not creating row houses, row apartments, row townhouses - because these are not maintained, not secured and not a SMART use of land.	2/16/2018 10:16 AM
8	Would have liked to be more involved but travel plans got in the way. My fault, not the planners - communication was very good	2/15/2018 10:04 PM
9	Was not informed of the process until very late and only through stumbling across it on the website.	2/15/2018 2:44 PM
10	More collaboration is vital.	2/14/2018 7:38 PM
11	Not as much as I would have liked but that was due to my work schedule and travel	2/14/2018 10:35 AM
12	I have lived here over 60 years...it seems to me plans are great, but they can always be changed by people with power and influence.	2/13/2018 9:55 PM
13	the community association has been very unfriendly to residents that don't share the opinions of its directors . This is something that I have experienced .	2/13/2018 8:31 PM
14	I did all the surveys and reading, but with a new baby I wasn't able to make it out to the in person meetings	2/13/2018 2:19 PM
15	I gave feedback but was limited in how involved I could be as I have a young child. I really liked the initial plan, and am disappointed that it was watered down so much. I thought that this was very forward looking in its initial design, and now we risk missing an opportunity for really improving our neighbourhood.	2/10/2018 7:51 PM
16	Don't let the loud minority be heard over the quiet majority. If they didn't write it. They will hate it. They are not OUR voices	2/3/2018 6:57 AM
17	Away for the meetings	1/31/2018 5:31 PM
18	many many chances for questions and feedback	1/23/2018 7:14 PM
19	Community input that I witnessed did not seem to influence outcomes. There were not enough interactions between community and the City to make this a collaboration. It seemed like some people's input had far more influence than others. Meetings seemed to defend positions rather than to seriously understand people's concerns.	1/21/2018 9:38 PM
20	I can see by the work completed that I have not been involved enough.	1/21/2018 4:36 PM
21	On the border of Vic West so feel aloof.	1/20/2018 2:36 PM
22	Thank you for listening	1/19/2018 5:15 PM
23	Most of the time through this medium and not directly on site.	1/18/2018 7:30 PM



# Q8 Where do you live?

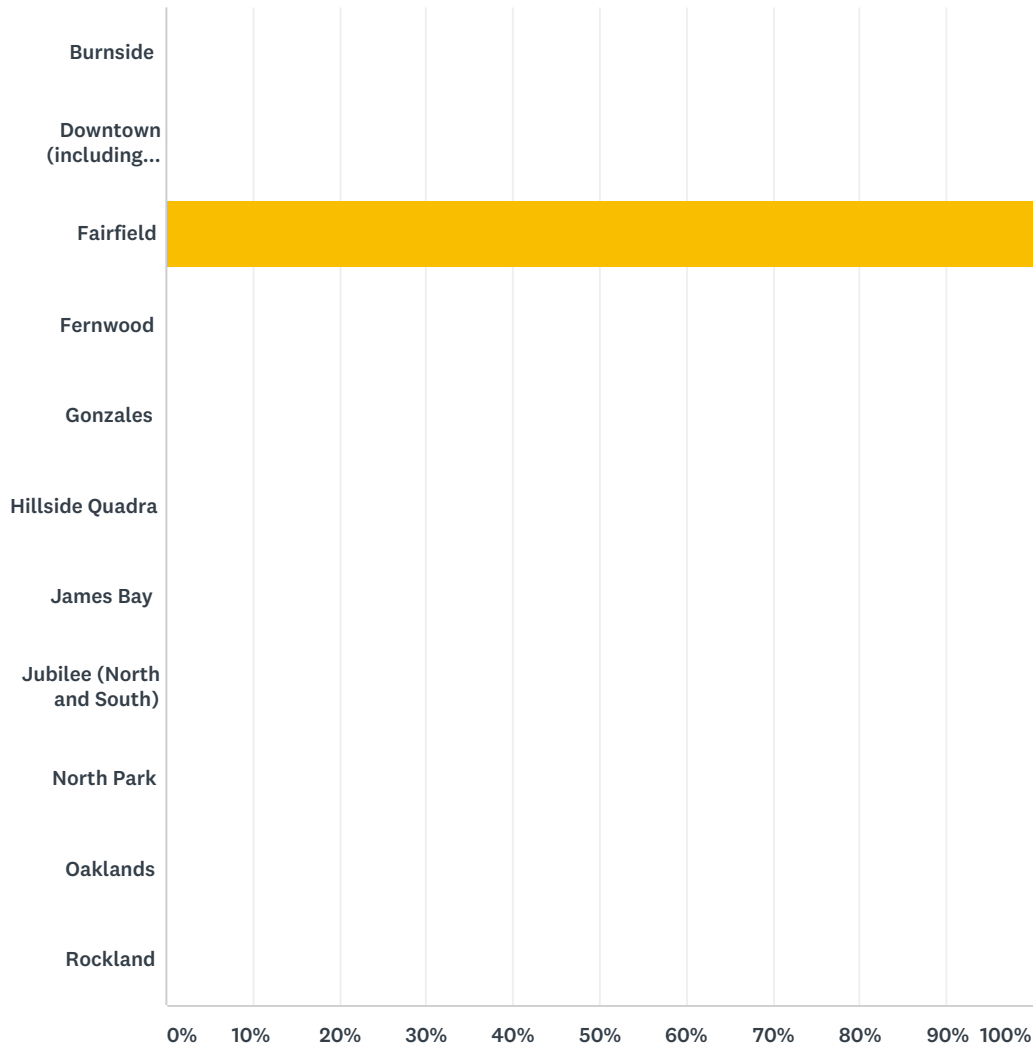
Answered: 58    Skipped: 10



ANSWER CHOICES		RESPONSES	
In Vic West		91.38%	53
In another neighbourhood in the City of Victoria		1.72%	1
Other		6.90%	4
TOTAL			58

## Q9 Where do you live?

Answered: 1   Skipped: 67



ANSWER CHOICES	RESPONSES	
Burnside	0.00%	0
Downtown (including Harris Green)	0.00%	0
Fairfield	100.00%	1
Fernwood	0.00%	0
Gonzales	0.00%	0
Hillside Quadra	0.00%	0
James Bay	0.00%	0
Jubilee (North and South)	0.00%	0
North Park	0.00%	0
Oaklands	0.00%	0

## Proposed Vic West Neighbourhood Plan Update

Rockland	0.00%	0
TOTAL		1

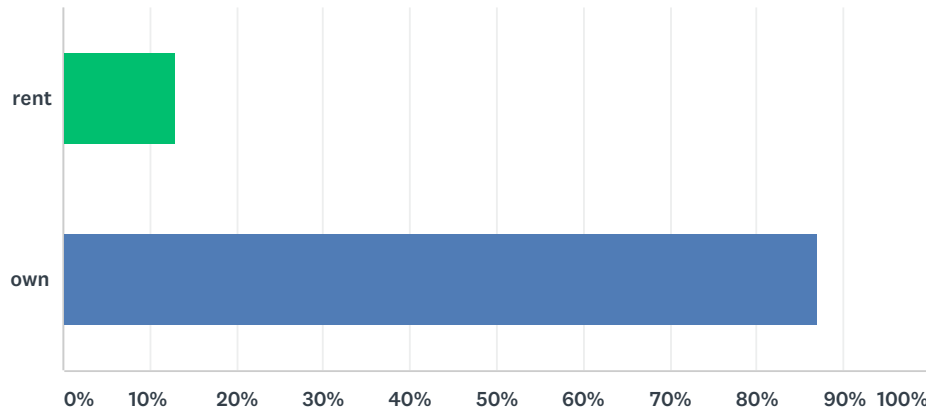
## Q10 Where do you live?

Answered: 1   Skipped: 67

#	RESPONSES	DATE
1	Mill Creek, WA	2/15/2018 4:42 PM

## Q11 Do you own or rent your home?

Answered: 54 Skipped: 14

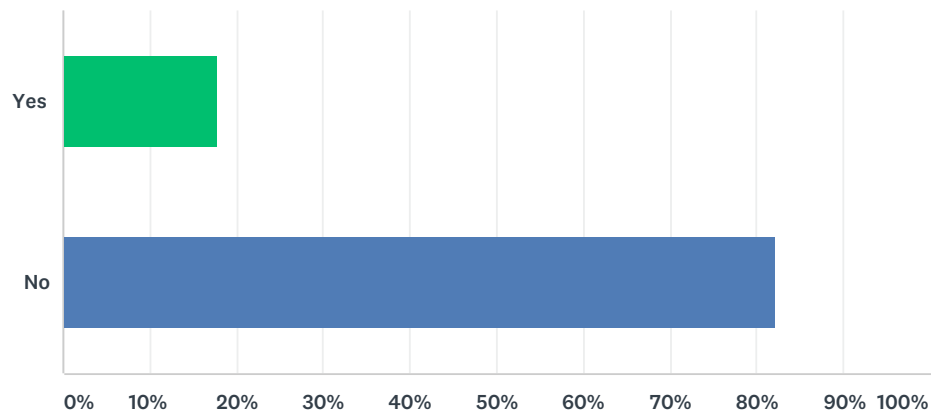


ANSWER CHOICES	RESPONSES	
rent	12.96%	7
own	87.04%	47
TOTAL		54



Q12 Do you own a business in Vic West?

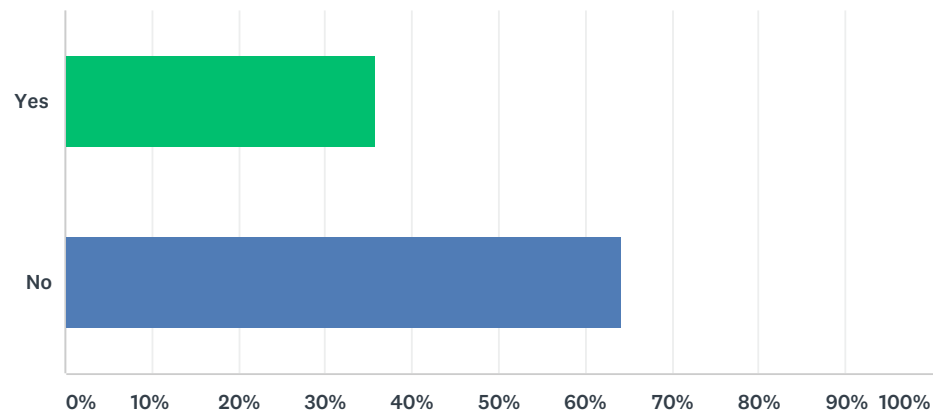
Answered: 56    Skipped: 12



ANSWER CHOICES		RESPONSES	
Yes		17.86%	10
No		82.14%	46
TOTAL			56

Q13 Do you work in Vic West?

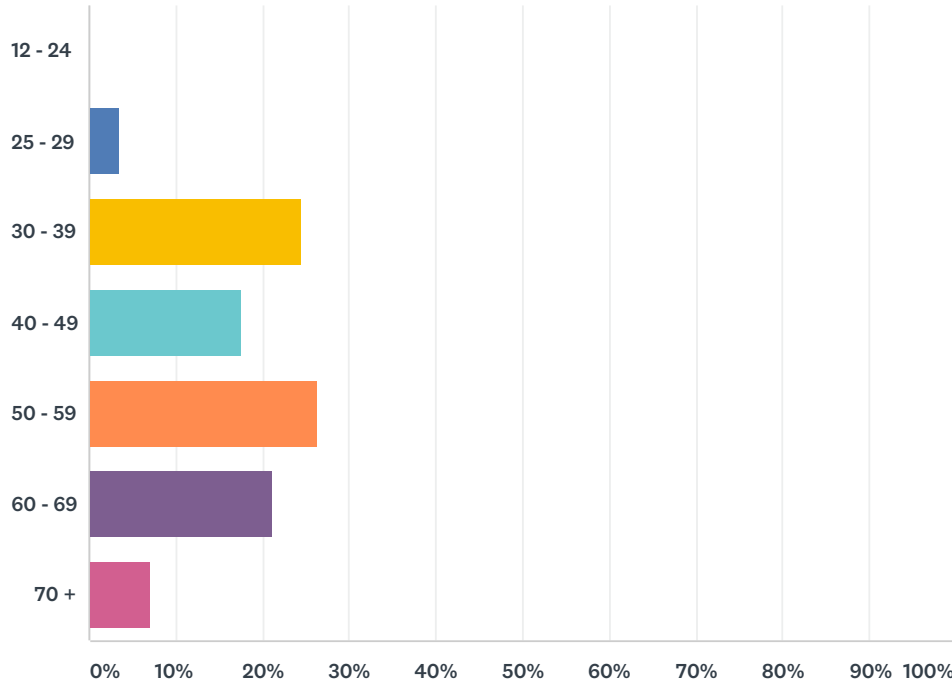
Answered: 56    Skipped: 12



ANSWER CHOICES		RESPONSES	
Yes		35.71%	20
No		64.29%	36
TOTAL			56

## Q14 How old are you?

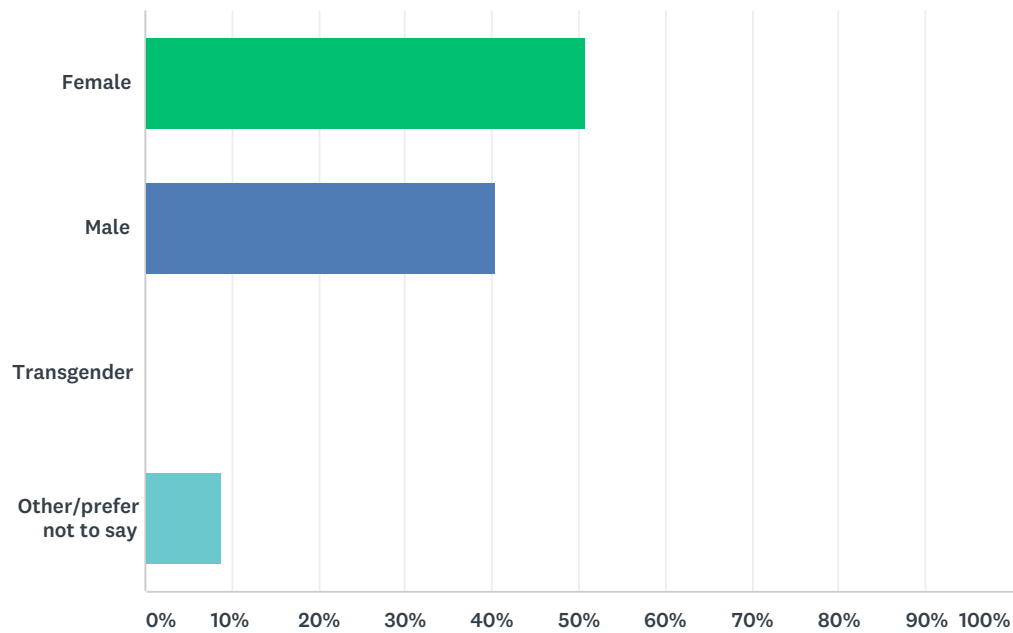
Answered: 57 Skipped: 11



ANSWER CHOICES	RESPONSES	
12 - 24	0.00%	0
25 - 29	3.51%	2
30 - 39	24.56%	14
40 - 49	17.54%	10
50 - 59	26.32%	15
60 - 69	21.05%	12
70 +	7.02%	4
TOTAL		57

Q15 Do you identify as

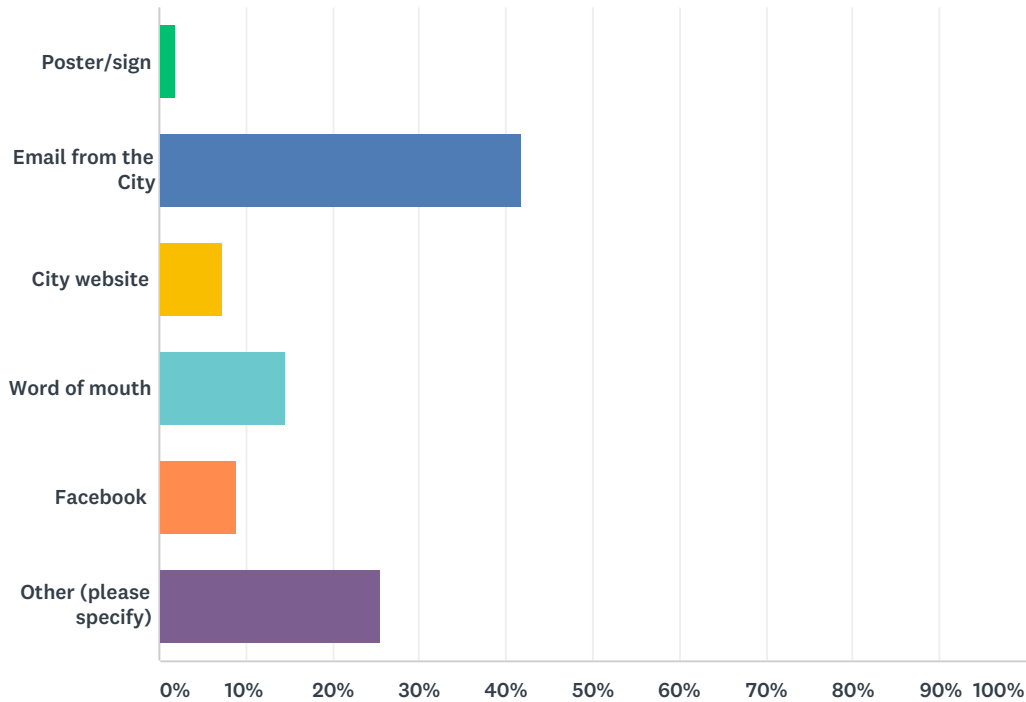
Answered: 57    Skipped: 11



ANSWER CHOICES	RESPONSES	
Female	50.88%	29
Male	40.35%	23
Transgender	0.00%	0
Other/prefer not to say	8.77%	5
TOTAL		57

## Q16 How did you find out about this survey?

Answered: 55 Skipped: 13



ANSWER CHOICES	RESPONSES	
Poster/sign	1.82%	1
Email from the City	41.82%	23
City website	7.27%	4
Word of mouth	14.55%	8
Facebook	9.09%	5
Other (please specify)	25.45%	14
<b>TOTAL</b>		<b>55</b>

#	OTHER (PLEASE SPECIFY)	DATE
1	email from community association	2/18/2018 12:14 PM
2	Community meeting	2/18/2018 8:44 AM
3	I received a pamphlet several months ago about the proposed changes to this neighbourhood.	2/17/2018 12:40 AM
4	Vic West Community Association	2/16/2018 11:01 PM
5	Several of the above	2/16/2018 10:25 AM
6	Attending community meeting	2/15/2018 2:45 PM
7	Other residents forwarded to us as our property is identified for possible use ammendment	2/14/2018 10:39 AM
8	Session at Vic West Elementary school	2/6/2018 10:58 AM
9	Don't remember	2/3/2018 7:03 AM



## Proposed Vic West Neighbourhood Plan Update

10	Vic West Community Association	1/30/2018 8:29 PM
11	I attended the open house	1/21/2018 4:38 PM
12	Association emails	1/20/2018 5:20 PM
13	Email VWCA	1/19/2018 5:16 PM
14	VicWest Community Association e-mail	1/18/2018 7:33 PM

**Attachment 4:**  
**Engagement Summary and Feedback on**  
**Proposed Vic West Neighbourhood Plan and OCP Amendments**

Part 3: Open House Feedback

## **Vic West Proposed Neighbourhood Plan Feedback**

**Date:** January 20, 2018

**Event Type:** Open House

**Location:** Vic West Elementary School gym

**# of attendees:** 130

Transcribed comments are in italics.

Welcome!

No feedback on this board.

## Big Moves in the Draft Vic West Neighbourhood Plan

Comments written directly on board:

### Create strong village hearts

- Establish Westside Village area as the heart of the neighbourhood

~~Heart~~— Not heart

Center (with one check mark beside)

### Knit old and new Vic West together

~~old and new~~ – North or south east + west

### Strengthen park and food systems

Nothing in plan that supports this big move – plan states current opportunities.

## Changes made to the Draft Plan

Comments directly on board under text points:

- **Better integration of master planned areas (Dockside, Railyards and Roundhouse Properties)**  
(Not enough info - detail)  
Agree not enough detail re. development?  
How will this be better integrated?
- **Apartment buildings no longer proposed on Skinner Street and Suffolk Street**  
Map 10 shows apartment building  
But still allow 3 storey double row townhouses!
- **Reduced area where double rows of townhouses would be supported, to exclude most lots within and east of the Catherine Street Heritage Conservation Area**  
But this west area has character homes as well, we want to protect character! (pointing to Catherine Street)
- **Reduced footprint of the mixed-use area at Craigflower Village. Encourage commercial uses on adjacent properties if houses are retained.**  
But still increasing density (pointing to Craigflower Village)

- **In the area south of Esquimalt Road, reduced building heights and several properties changed back to the “traditional residential” designation with opportunity for 3-storey apartments or townhouses.**  
*Still too high (max 3 stories)* (pointing to “reduced building heights”)  
*Agreed!* (pointing to comment above)
- **Additional building height proposed for some properties along Esquimalt Road, where on-site affordable housing is provided.**  
*6 stories too high* (pointing to “additional building height”)  
*Too high!* (pointing to comment above)  
*Disagree* (next to comments above)
- **Revised boundaries of the Pioneer Co-op Special Planning Area to remove non co-op properties**  
*?* (pointing to “Pioneer Co-op Special Planning Area”)
- **Identified new locations for potential transportation improvements**  
*There’s a requirement now so will definitely need improvements with all the development!*

General comments on board:

*Did not change re-development everywhere in traditional neighbourhoods!*



## Revisions to Traditional Residential Sub-Areas

Comments directly on board:

### **4. Pioneer Housing Co-op (Special Planning Area)**

*Don't expand!* (1 sticky dot beside comment)

### **3. Esquimalt Road Sub-Area**

*Too high* (1 sticky dot beside comment)

### **5. Skinner Street Sub-Area**

*Too high* (10 sticky dots beside comment)

### **Traditional Residential Areas – Envisioned housing types, height and density**

*Too much!* (Circle around "density", 5 sticky dots beside comment)

General comments directly on board or on sticky notes:

*What sub-area?* (Pointing to grey area between Sub-Areas 8, 7 and 5)

*Flashing light at x-walk by Barber Shop intersection Raynor/Craigflower*

*Unsafe/awkward intersection Williams-Springfield-Esquimalt Rd*

*Traffic lights on Esquimalt are not visible from Williams St. Doesn't seem to have a pattern? (Hinges on crosswalk @ Robert?)*

*Crossing Wilson very unsafe. Blind spot on right corner. ☹️ (Note is placed south of Esquimalt Rd, west of Maitland St. Hand drawn map on not shows crossing of Wilson and Springfield with an x marking blind spot in north-west corner)*

*People speed thru Springfield st over 60 kmph, often. Make it a dead end at the corner of Springfield William & Esquimalt as this is a bad intersection.*

*Please finish the "road" or thru-way to make Russel St. a viable path. Currently, there is no access at all thru to Wilson. K. Vopnfjord*

*Skinner is a sensible street to add density. Walking traffic will slow vehicle traffic.*

## Detailed Revisions to the Draft Plan

No comments on this board.

Tell us what you think!

### Do you have any comments about the revisions?

Directly on board:

*Revisions states no apartment building on Skinner but map 10 shows different*

*Design excellence is key.*

*Streetscaping / public art / signage control / control on folk art*

*Still too much height*

*Site of new school development for all the new families?*

*Pg. 58 – what is outlined is not possible. Cannot provide infill housing in that area and retain front yards as it says.*

*Non-market housing should be built as market housing. Why is developer gifted extra density for housing type that is city priority?*

*?? Not agree (pointing to “built as market housing” in comment above)*

On sticky notes:

*Information on fowl and plants cards/or on rails public art on walkway marina? controls*

*Ensure parking management issues are considered when secondary suites are approved.*

*Some small lot houses may be too small for suites. Need to look at minimum size for secondary suites*

*Some streets where there are existing small lots may be already adequate density – no need to increase. Eg. Reno St.*

*There is already enough density & population increase in Vic West. Just because there is a need – Vic West doesn't have to keep absorbing more – ↑ traffic & it will change the neighbourhood*

*Skinner is a sensible place for height/density & would slow traffic. Please bring back. (1 sticky dot on note)*

*Feedback: Topic: Transportation & Mobility page 108. Please revise the bullet on Hereward Road to also explicitly include text on ‘pedestrian and cyclist safety’. This is important for local users. Thanks.*

*Need to anticipate the “investments” & “levers” that will support sensible growth*

- Trans strategy*
- Housing strategy*
- Economic development strategy*

*More details on how density will be supported – what linkages to*

*1) Transportation*

*2) Parks & Greenspace*

*3) Social amenities*

*- Daycare*

*- Eldercare*

*- Services*

*More height on skinner -> 3 storeys*

*Good job city staff*

*Need to take climate targets, mitigation into consideration – more density could help reduce demand for carbon fuel-based transportation*

*Keep in mind this is a 20 year plan. We are a growing city, if we don't densify, it will encourage close in sprawl and congestion will increase. A weak plan will be better for Langford than Victoria.*

### **Any other comments on the plan?**

Comments directly on board:

*Remove apartments on Rynor in Craigflower Village – we asked, but hey are still there. (4 sticky dots)*

*The main thrust of this plan is still re-develop the traditional neighbourhoods! Wrong! (Unclear if the “wrong” is part of or in reaction to the comment) It won't be affordable! (3 sticky dots placed beside the word “wrong”)*

*Westside Village is not the “heart” of the community – we asked for that to be removed but that reference is still there. (5 sticky dots)*

*Does not equal commercial shopping centre (pointing to “heart” in comment above)*

*The commercial centre. We have multiple “hearts” (pointing to “heart” in comment above)*

*We think the Oceanic Market area is more the heart (5 stick dots)*

*Allowing 3 storeys in Pioneer Co-op – do not agree for that building height in that local of surrounding low height residential houses (1 sticky dot)*

*There is no mention in the “changes made to the draft plan” to lessen the heavy traffic on Mary Street & the big truck traffic on Henry St. Residents need something to slow traffic & stop the big beer trucks*

*Nothing in the plan that support Big Move #8, only states status quo.*

*More townhouses please! 3 storey apartments are also great – want all sorts of neighbours, not just rich ones.*

*Have you checked the prices of townhouses in Victoria? This is not affordable housing. (Next to comment above)*

*Westside Village is not the “heart” – maybe center. (3 sticky dots)*

*Community (added to “center” in comment above)*

*Pioneer Co-op – no more density*

*Overarching assumption of this plan is that after build-outs of current developments (eg. Dockside & Bayview) that resulting density is insufficient and Vic West must absorb even more residents. Why? This assumption skews the whole plan to the detriment of current residents. (5 sticky dots)*

*Too fast. Our population has skyrocketed, along with traffic. More infrastructure needed, please, before okaying more developers making \$ on housing.*

*Movement/action must take place to designated character homes otherwise they will be lost to development – we value the character so preserve it! (3 sticky dots)*

*Agree! (Next to comment above)*

*Big Move #8 – include private land development must include food production/growing space – time for developers to contribute to growing or food system*

*Retain view corridor on Skinner to east/towards downtown so property south of Railyards should have low height.*

## **Proposed amendments to the Official Community Plan**

### **What happens to my feedback?**

Directly on board:

*Needs to be given to the community other than just on the City website 1 week before Council.*

*Agree! (next to comment above, 3 sticky dots beside)*

On sticky note:

*Need opportunity to hear from those most impacted – that said, I like the revisions – good job!!*

## Proposed Amendments 1-5: Revisions to Traditional Residential Urban Place Designations

Comments directly on board with map:

*Is not a village!* (Pointing at “Large Urban Village” in legend of map, 1 sticky dot beside comment)

### **How supportive are you of the Official Community Plan amendments?**

#### **Amendment 1: Adjacent to Craigflower Village**

Very supportive	Somewhat supportive	Neutral	Somewhat opposed	Very opposed
13	4	-	1	14

#### **Comments:**

*Was supportive of height (3) and type (apartment) on Skinner*

*Greater heights please*

*Would like to see 2 storey retail/residential along Craigflower Russel -> Belton (1 sticky dot on note)*

*What opportunity for more commercial & apartment north of park on Craigflower Rd. (Align with Esquimalt buildings) (1 sticky dot on note)*

#### **Amendment 2: Pioneer Co-op site**

Very supportive	Somewhat supportive	Neutral	Somewhat opposed	Very opposed
12	8	1	-	16

#### **Comments:**

*You will be increasing traffic along some narrow and dead-end streets*

*The opportunity to provide affordable housing for seniors + people w special needs*

*No 3 storey building (1 sticky dot beside)*

*Agree (pointing to comment above)*

*Assists (?) more aff. / social housing*

*No commercial use! (2 sticky dots beside)*

*3 storeys is too high for this residential area*

*Pioneer Co-op site should be only residential use do include a commercial use here as it splits with Westside or Craigflower Village areas*



**Amendment 3: Adjacent to Edward at Catherine Streets Village**

Very supportive	Somewhat supportive	Neutral	Somewhat opposed	Very opposed
12	6	3	1	3

**Comments:**

*Would support mixed-use there as commercial on ground level and/or live/work would enhance that area + increase street vitality + safety (which really feels like the “heart” of Vic West)*

*Not ambitious enough anymore – is this enough to support e.g. Market Garden?*

**Amendment 4: Lime Bay area (south of Esquimalt Road)**

Very supportive	Somewhat supportive	Neutral	Somewhat opposed	Very opposed
14	4	-	1	19

**Comments:**

*This makes more sense as 4-6 storeys*

*No apartments*

*- not amenable with current neighbourhood*

*-2 ½ storey townhouses are fine*

*Area 4 – support 2.5 storey, not 3, especially on east side of Robert St. While lots are deep, street is narrow & increased density is a concern for parking & traffic!*

*Apartments can = air b&b, Not amenable with neighbourhood*

*Robert St south of Esq. Rd has 12 single family homes. The revised proposal would remove 6 of these & destroy streetscape. There is already 4 multi residential complexes (3 large) on Robert*

*3 storeys for townhouse or apartments still too high!*

**Amendment 5: Revised building heights, neighbourhood-wide**

Very supportive	Somewhat supportive	Neutral	Somewhat opposed	Very opposed
11	3	-	1	23

**Comments:**

*I wish double row townhouses were allowed everywhere. It would be great if Vic West could still support families 15 years from now.*

*Heights should be increased*

*3 to 6 storeys would allow more opportunity for everyone. Business, residential, people, employment*

## Proposed amendments 6-11: Revisions to Urban Place Designations for residential areas and villages

### Comments on board with map:

#### **6. Mixed use or residential development up to 4 storeys (change from Traditional Residential to Urban Residential designation)**

*Too high!*

*Agree!* (Beside comment above)

*4 storeys is excessive!*

#### **8. Add opportunities for one or more buildings higher than 6 storeys on Westside Shopping Centre site; and at corner of Tyee Road and Bay Street - to create a landmark building (north site) and provide flexibility to mix taller and shorter buildings (change to supported height for Large Urban Village designation)**

*Too high* (2 sticky dots beside)

*I agree* (beside comment above)

#### **9. Residential buildings up to 4 storeys (change from Traditional Residential to Urban Residential designation)**

*Too high* (2 sticky dots beside)

#### **10. Residential or mixed-use buildings up to 5 storeys (change from Traditional Residential to Urban Residential designation)**

*Too high* (1 sticky dot beside)

#### **11. Commercial, mixed use or residential buildings up to 3 storeys (change from Traditional Residential to Small Urban Village designation)**

*Higher 5!*

### General comments on board (on sticky notes or written on board):

*This is concerning!! There needs to be a buffer into the traditional community 3 storeys max!! 5 storeys presents dichotomy – streetscape is not maintained & sitelines eroded!!* (Pointing to three Urban Residential blocks north of Esquimalt Road south of Dundas St and Bay St)

*No! No! No!* (Written on each of the blocks referenced in comment above)

*Too high – people love old town heights* (pointing to lots on south side of Esquimalt Road between Rothwell St and Russel St where each lot has a figure 6 scribbled on them as part of the comment, and to blocks on north side of Esquimalt Rd between Russel and Vic West Park where figure 5, 5 and 6 are scribbled on each block from west going east)

5 (pointing to parcels on Catherine St directly east of amendment 9)

*Need pedestrian controlled crossing light – Raynor t Craigflower – Must – School children cross Craigflower to get to community center for before and after school care (pointing to Craigflower/Raynor intersection, 1 sticky dot on note)*

*Skinner should be 3 storeys*

*Reflective cones to differentiate bike lanes to help slow drivers all on Skinner/Craigflower*

*No access on Alston St*

*Bay + Wilson is crazy – turning into westside (left) for example*

*After the road improvements on Skinner St speeds increased substantially:*

- The speed limit signs of 30 are poorly placed and not seen by drivers*
- The corner “suggestion” sign of 30 tends to encourage drivers to think the speed limit is higher*
- The wide open street without parking now increases speed drivers see the light at top and “green means go”*
- Alston and Skinner corner is dangerous. Need pedestrian access  
mark@stopover.ca*

### **How supportive are you of these Official Community Plan amendments?**

#### **Amendment 6: Russell Street at Skinner Street**

Very supportive	Somewhat supportive	Neutral	Somewhat opposed	Very opposed
12	3	1	-	15

#### **Comments:**

*4-storey apartment buildings next to heritage homes doesn't work.*

*Skinner heights should be 3 storey old homes on Skinner need to be updated but won't attract development as single family residence*

*#6 should be Traditional Residential not Urban Residential. Too high as urban class*

*Not tall and dense enough on major streets ie Skinner*

*Too high!*

*Too high! Too dense!*

*Disagree (Beside comment above)*

*Too tall*

**Amendment 7: Edward at Catherine Street Village**

Very supportive	Somewhat supportive	Neutral	Somewhat opposed	Very opposed
17	-	-	1	5

**Comments:**

*With care, two storey? Possible one three storey with mixed use.*

**Amendment 8: Westside Village and Bay Street/Tyee Road**

Very supportive	Somewhat supportive	Neutral	Somewhat opposed	Very opposed
16	3	-	-	9

**Comments:**

*Align Leigh with other buildings on Wilson*

*Point 8 one or more... What is planned for Alston + Da Vinci Centre?*

*Love the increased height*

*Too tall*

*Disagree (Beside comment above)*

*Density was already transferred to the Parc site so why has it returned? There is no density to reallocate for Save On mall*

*Agreed! (Beside comment above, 1 sticky dot beside)*

**Amendment 9: South Mary Street**

Very supportive	Somewhat supportive	Neutral	Somewhat opposed	Very opposed
8	2	-	1	12

**Comments:**

*Too tall*

*Disagree (beside comment above)*

**Amendment 10: West Esquimalt Road**

Very supportive	Somewhat supportive	Neutral	Somewhat opposed	Very opposed
13	2	-	-	6

**Comments:**

*Way to tall*

*Disagree (beside comment above)*

*6 storeys is too high!*

**Amendment 11: Craigflower small urban village**

Very supportive	Somewhat supportive	Neutral	Somewhat opposed	Very opposed
16	-	-	-	9

**Comments:**

*Still too high and dense!*

*Would also support 4 storeys here*



## Proposed Amendments 12 – 14: Revisions to Urban Place Designations for employment areas

### **How supportive are you of these Official Community Plan amendments?**

#### **Amendment 12: New employment-residential designation (various properties)**

Very supportive	Somewhat supportive	Neutral	Somewhat opposed	Very opposed
15	1	-	1	1

#### **Comments:**

*I live nearby – having a fish place w/o residential mixed isn't compatible so this is great*

*Ensure residential + industrial buildings are dvpd properly – e.g. noise mitigation (it can be done! I've seen bldgs. w autobody shops on the ground floor + residential above in Sweden ☺)*

*Concern about impact/transition to traditional residential*

#### **Amendment 13: New industrial employment designation (various properties)**

Very supportive	Somewhat supportive	Neutral	Somewhat opposed	Very opposed
10	3	1	1	7

#### **Comments:**

*Support residential along transportation route – Skinner. Add limited residential across all property here*

*Feel that the limited residential should be for the entire lot, not just Alston St.*

*Would like to see retail opportunities here*

*Look what happened to Pt. Ellice House it was ruined by industrial development, need a buffer*

#### **Amendment 14: New industrial-residential designation (various properties)**

Very supportive	Somewhat supportive	Neutral	Somewhat opposed	Very opposed
16	-	1	-	4

#### **Comments:**

*Like the live/work option so why not all properties here?*

*Provide as much flexibility for employment options as possible. Don't limit options to only industrial*

*Mixed use is good, optimizes space for housing, work (one sticky dot on note)*

*Want to maintain General Employment to have job flexibility – light industrial too limiting*

*The General Employment designation should be allowed for flexibility of tenants.*

## Proposed Amendment 15: Revisions to densities for some Urban Place Designations

### **How supportive are you of these Official Community Plan amendments?**

#### **Amendment 15: Revised densities**

Very supportive	Somewhat supportive	Neutral	Somewhat opposed	Very opposed
6	2	-	-	5

#### **Comments:**

*More density enables us to have more nice things!*

*Less ambitious than previous plan; would prefer it to remain as previous version*

*Increase maximum density more than 10%*

## Proposed Amendments 16 – 19: Expanding and creating new Development Permit Areas

**17. Include Craigflower Village in DPA 6A: Small Urban Villages. Apply the *Revitalization Guidelines for Corridors, Villages and Town Centres* with content specific to this area\***

? (“DPA 6A” underlined)

**17. Include the Catherine Street at Edward Street Village and adjacent lots in DPA 6A: Small Urban Villages, and apply the *Revitalization Guidelines for Corridors, Villages and Town Centres* with content specific to this area\***

? (“DPA 6A” underlined)

?

**18. Include the Esquimalt Road corridor in DPA 7A: Corridors, and apply *Revitalization Guidelines for Corridors, Villages and Town Centres*, and content specific to this corridor\***

? (“DPA 7A” underlined)

**18. Include these blocks of Skinner Street in DPA 7A: Corridors, and apply *Revitalization Guidelines for Corridors, Villages and Town Centres* with content specific to this area\***

? (“DPA 7A” underlined)

**18. Include these two blocks fronting Skinner Street in DPA 7A: Corridors, and apply *Revitalization Guidelines for Corridors, Villages and Town Centres* with content specific to this area\***

? (“DPA 7A” underlined)

**16. Include properties south of Langford Street, and property at Wilson and Bay Streets, adjacent to Victoria West Park in DPA 5: Large Urban Villages – Victoria West Village. Apply *Revitalization Guidelines for Corridors, Villages and Town Centres*, and content specific to this area\***

? (“DPA 5” underlined)

*Not in document* (pointing to “DPA 5”)

**19. All Traditional Residential Areas in Vic West: Apply Development Permit Area 15F: Intensive Residential - Townhouse and Rowhouse to any new townhouse development in Traditional Residential Areas of Vic West.**

? (“Development Permit Area 15F” underlined)

*To keep large franchise’s from moving into these commercial apace*

*Keep local businesses giving them the opportunity to thrive*

General comments on board:

*Walking + cycling is a value many vic westers have and we to protect walkers/cyclers.*

*Crosswalk to get to playground* (Pointing to northwest corner of Vic West park, 2 sticky dots beside comment)

*Crosswalk needs flashing lights* (pointing to Tyee Rd at south end of Triangle Park, 1 sticky dot)

*After road development speeding went way up!* (Pointing to Skinner Street)

*Would strongly prefer non-chain businesses. (local) le local > Tim Horton’s*

*Flashing lights needed* (Pointing to northeast corner of Vic West Park at Wilson Street)

**How supportive are you of these Official Community Plan amendments?**

**Amendment 16: Expand Development Permit Area 5**

Very supportive	Somewhat supportive	Neutral	Somewhat opposed	Very opposed
8	-	-	-	8

**Comments: -**

**Amendment 17: Expand Development Permit Area 6A**

Very supportive	Somewhat supportive	Neutral	Somewhat opposed	Very opposed
8	2	-	-	5

**Comments: -**

**Amendment 18: Expand Development Permit Area 7A**

Very supportive	Somewhat supportive	Neutral	Somewhat opposed	Very opposed
8	2	-	-	7

**Comments:** -

**Amendment 19: Create A New Development Permit Area 15F**

Very supportive	Somewhat supportive	Neutral	Somewhat opposed	Very opposed
7	1	-	-	4

**Comments:**

*Yes, encourage pedestrian traffic.*



**Attachment 4:**  
**Engagement Summary and Feedback on**  
**Proposed Vic West Neighbourhood Plan and OCP Amendments**

Part 4: Public Correspondence

## Rob Gordon

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**Subject:** FW: V.W.C.A. - L.U.C. Vic-W.est Neighborhood Plan Letter

**From:** Sean Dance [[mailto:](#)**Personal info**]

**Sent:** Wednesday, February 28, 2018 1:07 PM

**To:** Kristina Bouris <[KBouris@victoria.ca](mailto:KBouris@victoria.ca)>; Marc Cittone <[mcittone@victoria.ca](mailto:mcittone@victoria.ca)>; Michael Hill <[mhill@victoria.ca](mailto:mhill@victoria.ca)>

**Cc:** Sean Dance <[landuse@victoriawest.ca](mailto:landuse@victoriawest.ca)>; Dar Purewell <[secretary@victoriawest.ca](mailto:secretary@victoriawest.ca)>; Johanne Thompson

<[coordinator@victoriawest.ca](mailto:coordinator@victoriawest.ca)>; Mike Medland <[treasurer@victoriawest.ca](mailto:treasurer@victoriawest.ca)>; Supriya Crocker

<[vicepresident@victoriawest.ca](mailto:vicepresident@victoriawest.ca)>; Nan Judd <[governance@victoriawest.ca](mailto:governance@victoriawest.ca)>; Laura Floyd <[hr@victoriawest.ca](mailto:hr@victoriawest.ca)>; Julie

Bull <[volunteer@victoriawest.ca](mailto:volunteer@victoriawest.ca)>; Kate Thring <[littlesteps@victoriawest.ca](mailto:littlesteps@victoriawest.ca)>; Amanda Allan

<[transportation@victoriawest.ca](mailto:transportation@victoriawest.ca)>; **Personal info**

Justine Semmens <[president@victoriawest.ca](mailto:president@victoriawest.ca)>; **Personal info**  
[skowalko@bayviewplace.com](mailto:skowalko@bayviewplace.com);

**Subject:** V.W.C.A. - L.U.C. Vic-W.est Neighborhood Plan Letter

Hello Kristina,

Thank you for your patience and the deadline extensions regarding the Neighborhood Plan. Please find attached a letter, and the LUC Complete Survey, of the VWNP Recommendations. We greatly appreciate your patience, cooperation and understanding throughout the process. If you have any questions, and if I may be any further help or clarification, please do not hesitate to ask.

I hope you have a wonderful day,

Sean

February 27, 2018

Ms. Krista Bouris  
Senior Planner  
Sustainable Planning and Community Development  
City of Victoria  
1 Centennial Square,  
Victoria BC  
V8W 1P6

Dear Ms. Bouris:

Thank you for your flexibility in extending the deadline to the end of February, for the VWCA Land Use Committee (VWCA LUC) to provide this feedback to the City on the Vic West Neighbourhood Plan. Thank you also for the time that you and your City colleague Marc Cittone spent over January and February with the VWCA LUC in addressing questions about the latest draft Vic West Neighbourhood Plan.

Before we provide our feedback, we would like to acknowledge the extraordinary amount of time and energy that has gone into the development of the proposed Vic West Neighbourhood Plan. You and your staff along with VWCA and other community members have invested an immense amount of effort to date in coming up with the proposed plan. Our feedback is not intended in anyway to minimize those efforts. On the contrary, our intention has always been to support the Vic West Neighbourhood planning process, through encouraging and facilitating community members to understand both the process and the outcomes of the plan and to provide feedback to the City, either directly or as in this case, through the VWCA LUC.

In that regard, we think it may be useful to include in this letter a brief explanation of the process that was used to develop the following VWCA LUC feedback:

- After the first draft review process was complete in September of 2017, members of the VWCA suggested the VWCA LUC provide formal feedback to the City on the second draft.
- Once the second draft was released in January of 2018, the VWCA LUC invited its membership to participate in a review of the second draft
- Three meetings were held following the City open house on January 20<sup>th</sup> (January 23<sup>rd</sup> & 29<sup>th</sup> and February 5<sup>th</sup>) at which 20-25 people participated in each meeting.
- City staff effectively answered questions of clarity at two of these meetings.
- Proposed recommendations to improve the VWNP were drafted at the third meeting.
- Other non land use recommendations were suggested outside of the LUC meetings
- A survey of these recommendations was sent out to the VWCA membership on Feb. 09.
- The survey closed on Feb. 20 and the results are the basis for the following feedback.

The VWCA LUC facilitation process resulted in 26 recommendations for improvement to the Vic West Neighbourhood Plan. The VWCA survey of these 26 recommendations resulted in 150 surveys responses from the 495 VWCA members the survey was sent to.

A document is attached to this letter in which the survey results are categorized, ordered (from highest to lowest endorsement) and summarized (see “**SUMMARY OF RESULTS**”). The ordering is from highest percentage to lowest percentage endorsement. The “**COMPLETE SURVEY RESULTS**” are also included (in a separate document) for detailed reference.

The VWCA proposes that the City take the following actions for each category:

- **CATEGORY 1 - 80% PLUS ENDORSEMENT (12 RECOMMENDATIONS):**  
These recommendations received endorsement from a high majority of participants, therefore, the VWCA **STRONGLY URGES THE CITY TO INCLUDE THESE RECOMMENDATIONS** into the Vic West Neighbourhood Plan:
- **CATEGORY 2 - 65% to 79% ENDORSEMENT (7 RECOMMENDATIONS):**  
These recommendations received endorsement from a significant majority of participants, therefore, the VWCA **PROPOSES THAT THE CITY INCLUDE THESE RECOMMENDATIONS** into the Vic West Neighbourhood Plan:
- **CATEGORY 3- 48% to 64% ENDORSEMENT (7 RECOMMENDATIONS):**  
These recommendations received endorsement from a slim majority\* of participants, therefore, the VWCA **ENCOURAGES THE CITY TO CONSIDER THESE RECOMMENDATIONS** with other feedback they have received, for possible inclusion in the Vic West Neighbourhood Plan. (\* Note 1 recommendation was 48%).

If you have any questions or concerns about this, please do not hesitate to contact me.



Sean Dance  
VWCA, Land Use Committee Chair

ATTACHMENT 1: SUMMARY OF RESULTS - VWCA LAND USE COMMITTEE SURVEY:

ATTACHMENT 2: COMPLETE SURVEY RESULTS - VWCA LAND USE COMMITTEE SURVEY:

**ATTACHMENT 1: SUMMARY OF RESULTS:  
VWCA LAND USE COMMITTEE SURVEY:**

The following are the high level results from the VWCA LUC Survey that has been sorted according to level of endorsement from the survey (i.e. from highest to lowest endorsement). The complete survey results can be found in a separate document included with this letter.

The VWCA proposes that the City take the following actions for each category:

- **CATEGORY 1 - 80% PLUS ENDORSEMENT (12 RECOMMENDATIONS):**  
These recommendations received endorsement from a high majority of participants, therefore, the VWCA **STRONGLY URGES THE CITY TO INCLUDE THESE RECOMMENDATIONS** into the proposed Vic West Neighbourhood Plan:
- **CATEGORY 2 - 65% to 79% ENDORSEMENT (7 RECOMMENDATIONS):**  
These recommendations received endorsement from a significant majority of participants, therefore, the VWCA **PROPOSES THAT THE CITY INCLUDE THESE RECOMMENDATIONS** into the proposed Vic West Neighbourhood Plan:
- **CATEGORY 3- 48% to 64% ENDORSEMENT (6 RECOMMENDATIONS):**  
These recommendations received endorsement from a slim majority of participants, therefore, the VWCA **ENCOURAGES THE CITY TO CONSIDER THESE RECOMMENDATIONS** with other feedback they have received.

<b>CATEGORY 1 - 80% PLUS ENDORSEMENT:</b>	
<b>The following recommendations received endorsement from a high majority of participants, therefore, the VWCA STRONGLY URGES THE CITY TO INCLUDE THESE RECOMMENDATIONS into the proposed Vic West Neighbourhood Plan:</b>	
<b>QUESTION 1. RESULTS:</b>	<b>%</b>
VWNP AS IS: Without future population estimates.	<b>5</b>
VWCALUC RECOMMENDATION: To include population estimates in VWNP.	<b>95</b>
<b>QUESTION 21. RESULTS:</b>	<b>%</b>
VWNP AS IS: Water access points NOT INCLUDED in VWNP.	<b>5</b>
VWCALUC RECOMMENDATION: INCLUDE water access points in the VWNP.	<b>95</b>
<b>QUESTION 23. RESULTS:</b>	<b>%</b>
VWNP AS IS: Some parks have little or no mention.	<b>5</b>
VWCALUC RECOMMENDATION: That ALL PARKS are included in the VWNP including a vision and plans for each park.	<b>95</b>
<b>QUESTION 22. RESULTS:</b>	<b>%</b>
VWNP AS IS: The new Gorge Waterway Marine Park is NOT SHOWN on maps in VWNP.	<b>6</b>
VWCALUC RECOMMENDATION: SHOW the new Marine Park on maps in the VWNP plus the park's intent, vision and policies.	<b>94</b>

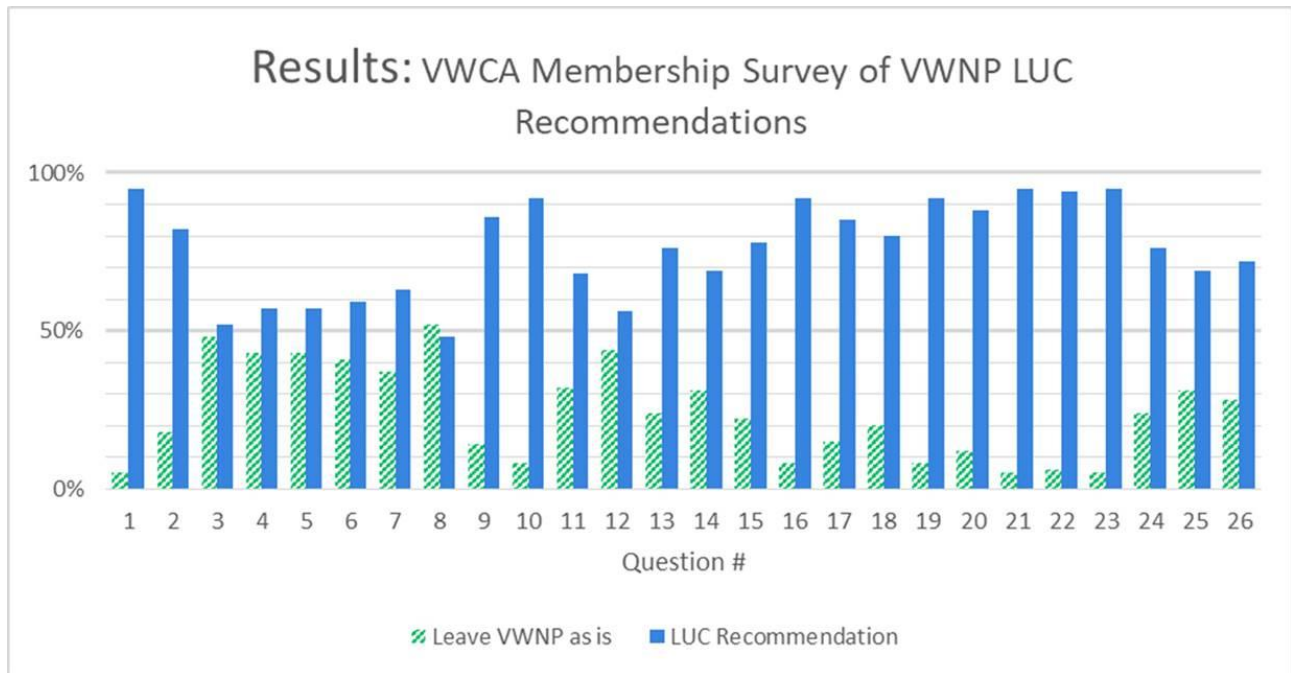


<b>QUESTION 10. RESULTS:</b>	<b>%</b>
VWNP AS IS: City makes NO COMMITMENT to consulting community/VWCA re: amenities, in the areas referenced in OCP Amendment 8 &15.	<b>8</b>
VWCALUC RECOMMENDATION: That the City makes A COMMITMENT to consulting community/ VWCA re: amenities, in the areas referenced in OCP Amendment 8 & 15.	<b>92</b>
<b>QUESTION 16. RESULTS:</b>	<b>%</b>
VWNP AS IS: NO comprehensive visualization of what Vic West will look like.	<b>8</b>
VWCALUC RECOMMENDATION: Include in the VWNP a comprehensive visualization of what Vic West will look like (e.g. massing model, 3-D mapping, etc.).	<b>92</b>
<b>QUESTION 19. RESULTS:</b>	<b>%</b>
VWNP AS IS: Little mention of the Bay Street Bridge issues and resolution.	<b>8</b>
VWCALUC RECOMMENDATION: That the issues of plans for the Bay Street bridge be included in the VWNP.	<b>92</b>
<b>QUESTION 20. RESULTS:</b>	<b>%</b>
VWNP AS IS: No reference to improving safety on Alston and Langford Streets.	<b>12</b>
VWCALUC RECOMMENDATION: That the landowners with property being held for future development be expected to maintain the borders of their properties to a safe standard. (at a minimum install an asphalt curb to delineate a walking path.)	<b>88</b>
<b>QUESTION 9. RESULTS:</b>	<b>%</b>
VWNP AS IS: WITHOUT reference to prior bonus density transfers (Westside Village/Parc).	<b>14</b>
VWCALUC RECOMMENDATION: WITH reference to prior bonus density transfers.	<b>86</b>
<b>QUESTION 17. RESULTS:</b>	<b>%</b>
VWNP AS IS: no reference to speed limit changes on major roads, at Vic West borders	<b>15</b>
VWCALUC RECOMMENDATION: That speed limits on major roads should not change at the Vic West border (i.e. it should be 40 kph on Craigflower, Tyee, Bay, Wilson & Esquimalt while retaining existing 30 kph zones).	<b>85</b>
<b>QUESTION 2. RESULTS:</b>	<b>%</b>
VWNP AS IS: With limited specific reference to sympathetic design.	<b>18</b>
VWCALUC RECOMMENDATION: More specific reference to sympathetic design noting transition zones of particular concern.	<b>82</b>
<b>QUESTION 18. RESULTS:</b>	<b>%</b>
VWNP AS IS: No plans for traffic calming	<b>20</b>
VWCALUC RECOMMENDATION: Include plans for traffic calming, especially on Craigflower, Hereward and Catherine.	<b>80</b>

<b>CATEGORY 2 - 65% to 79% ENDORSEMENT:</b> <b>The following recommendations received endorsement from a significant majority of participants, therefore, the VWCA PROPOSES THAT THE CITY INCLUDE THESE RECOMMENDATIONS into the proposed Vic West Neighbourhood Plan:</b>	
<b>QUESTION 15. RESULTS:</b>	<b>%</b>
VWNP AS IS: ALLOW <b>5 storey buildings</b> in various “Industrial Employment-Residential” areas as noted in OCP Amendment 14.	<b>22</b>
VWCALUC RECOMMENDATION: ALLOW <b>4 storey buildings</b> in various “Employment Residential” areas as noted in OCP Amendment 14.	<b>78</b>
<b>QUESTION 13. RESULTS:</b>	<b>%</b>
VWNP AS IS: ALLOW <b>4-5 storey buildings</b> in various “Employment Residential” areas as noted in OCP Amendment 12.	<b>24</b>
VWCALUC RECOMMENDATION: ALLOW <b>4 storey buildings</b> in various “Employment Residential” areas as noted in OCP Amendment 12.	<b>76</b>
<b>QUESTION 24. RESULTS:</b>	<b>%</b>
VWNP AS IS: Less than two pages in the VWNP related to Neighbourhood Food System.	<b>24</b>
VWCALUC RECOMMENDATION: Engage the Vic West Food Security Collective in strengthening Section 12 of the VWNP (Neighbourhood Food System).	<b>76</b>
<b>QUESTION 26. RESULTS:</b>	<b>%</b>
VWNP AS IS: <b>No mention</b> of requiring developers to include food growing amenities.	<b>28</b>
VWCALUC RECOMMENDATION: Require all developments to <b>include</b> food growing amenities in their developments.	<b>72</b>
<b>QUESTION 14. RESULTS:</b>	<b>%</b>
VWNP AS IS: ALLOW <b>4 storey buildings</b> in various “Industrial Employment” areas as noted in OCP Amendment 13 (see above).	<b>31</b>
VWCALUC RECOMMENDATION: ALLOW <b>3 storey buildings</b> in various “Industrial Employment” areas as noted in OCP Amendment 13 (see above).	<b>69</b>
<b>QUESTION 25. RESULTS:</b>	<b>%</b>
VWNP AS IS: Threatening food growing potential through development Traditional Residential areas.	<b>31</b>
VWCALUC RECOMMENDATION: Limit high density development to areas outside of Traditional Residential Areas.	<b>69</b>
<b>QUESTION 11. RESULTS:</b>	<b>%</b>
VWNP AS IS: Allow <b>4 storey buildings</b> and change to a “Urban Residential” designation for Mary Street (south of Esquimalt Road)	<b>32</b>
VWCALUC RECOMMENDATION: Allow <b>2.5 storey buildings</b> and keep “Traditional	<b>68</b>

Residential” designation.	
<b>CATEGORY 3- 48% to 64% ENDORSEMENT:</b> <b>The following recommendations received endorsement from a slim majority of participants, therefore, the VWCA ENCOURAGES THE CITY TO CONSIDER THESE RECOMMENDATIONS with other feedback they have received.</b>	
<b>QUESTION 7. RESULTS:</b>	<b>%</b>
VWNP, AS IS: ALLOW <b>4 storey buildings</b> at the corner of Russell and Skinner Streets with commercial uses at ground floor.	<b>37</b>
VWCALUC RECOMMENDATION: ALLOW <b>2.5 storey buildings</b> at the corner of Russell and Skinner Streets allowing commercial uses at ground floor as above.	<b>63</b>
<b>QUESTION 6. RESULTS:</b>	<b>%</b>
VWNP AS IS: Allowing <b>3 storey</b> , multi-unit residential buildings on Robert, Mary, Russell Streets, as noted above.	<b>41</b>
VWCALUC RECOMMENDATION: Allowing <b>2.5 storey</b> , multi-unit residential buildings on Robert, Mary, Russell Streets as noted above.	<b>59</b>
<b>QUESTION 4. RESULTS:</b>	<b>%</b>
VWNP AS IS: <b>ALLOWING</b> commercial or service uses in the Pioneer Coop	<b>43</b>
VWCALUC RECOMMENDATION: <b>NOT ALLOWING</b> commercial or service uses in the Pioneer Coop.	<b>57</b>
<b>QUESTION 5. RESULTS:</b>	<b>%</b>
VWNP AS IS: Allowing <b>3 storey</b> multi-unit residential buildings, as noted above.	<b>43</b>
VWCALUC RECOMMENDATION: Allowing <b>2.5 storey</b> multi-unit residential buildings, as noted - above.	<b>57</b>
<b>QUESTION 12. RESULTS:</b>	<b>%</b>
VWNP AS IS: ALLOW <b>3 storey buildings</b> on Craigflower (south of the existing commercial centre).	<b>44</b>
VWCALUC RECOMMENDATION: ALLOW <b>2.5 storey buildings</b> on Craigflower (south of the existing commercial centre).	<b>56</b>
<b>QUESTION 3. RESULTS:</b>	<b>%</b>
VWNP AS IS: Allow 3 storey buildings in Pioneer Coop	<b>48</b>
VWCALUC RECOMMENDATION: Allow 2.5 storey buildings in Pioneer Coop - 54%	<b>52</b>
<b>QUESTION 8. RESULTS:</b>	<b>%</b>
VWNP AS IS: Allowing <b>3 storey</b> mixed use, residential or live work buildings.	<b>52</b>
VWCALUC RECOMMENDATION: Allowing <b>2.5 storey</b> mixed use, residential or live work bldg.	<b>48</b>

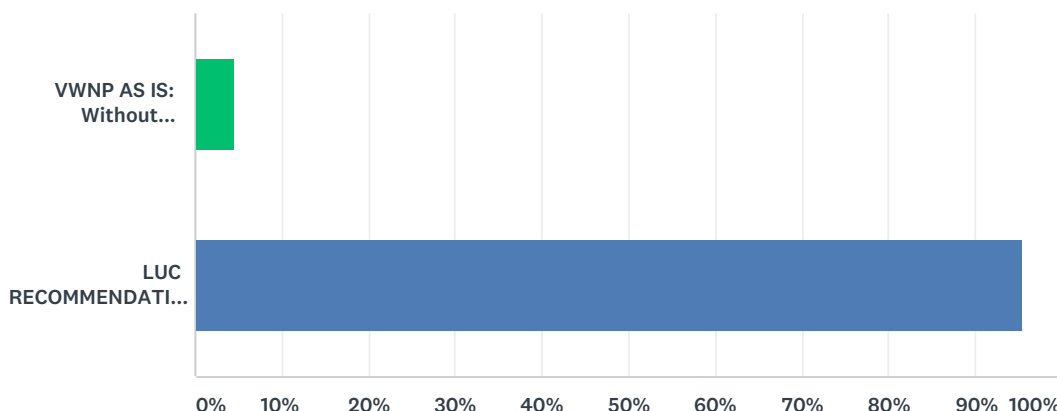
**ATTACHMENT 2: COMPLETE SURVEY RESULTS - VWCA LAND USE COMMITTEE SURVEY:**



**NOTE: COMPLETE SURVEY RESULTS ARE INCLUDED AS A SEPARATE DOCUMENT!**

**Q1 ISSUE: The VWNP does NOT include current & future population estimates for Vic West. LUC RECOMMENDATION: That the VWNP include current & future population estimates for Vic West (including Master Planned areas). PLEASE CHOOSE:**

Answered: 149 Skipped: 1

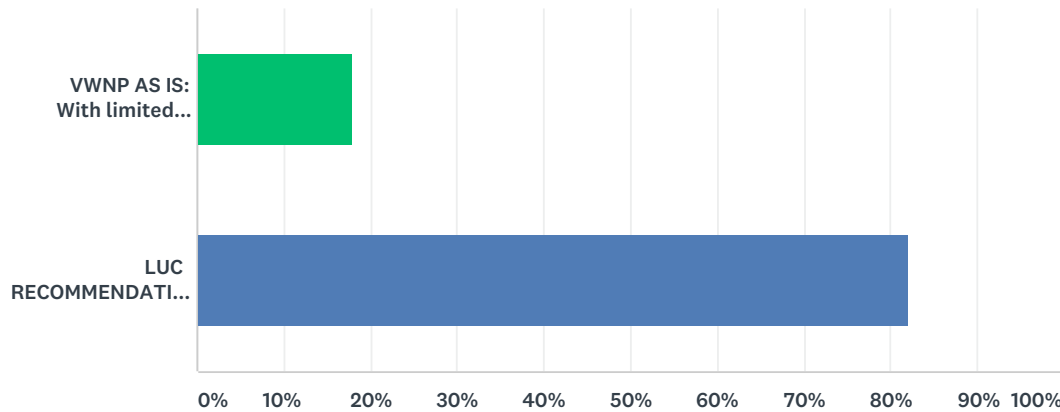


ANSWER CHOICES	RESPONSES	
VWNP AS IS: W thout popu at on est mates.	4.70%	7
LUC RECOMMENDATION: To nc ude popu at on est mates.	95.30%	142
TOTAL		149

**Q2 ISSUE: The VWNP does not provide strong enough emphasis on the importance of ensuring a sensitive transition from Traditional Residential areas to commercial and industrial areas: LUC RECOMMENDATION: That the VWNP stress how important it is to Vic West residents to respect the low rise esthetic of Traditional Residential areas and ensure there is sympathetic design of commercial and industrial buildings in proximity to Traditional Residential areas. To make this effective it is recommended that transition zones be noted in the VWNP in these sensitive areas. This would support the “Big Move” to “Knit old and new Vic West Together”. PLEASE CHOOSE:**

Answered: 133 Skipped: 17

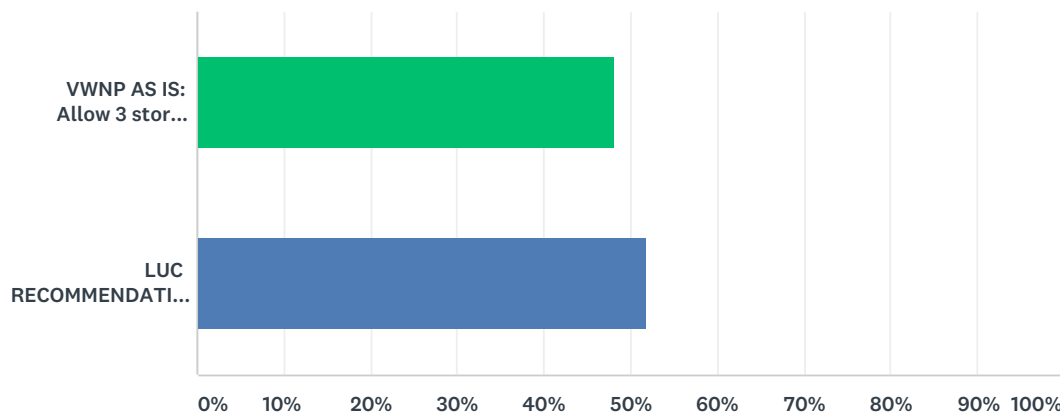




ANSWER CHOICES	RESPONSES	
VWNP AS IS: W th m ted spec f c reference to sympathet c des gn.	18.05%	24
LUC RECOMMENDATION: More spec f c reference to sympathet c des gn not ng trans t on zones of part cu ar concern.	81.95%	109
TOTAL		133

**Q3 ISSUE: The VWNP proposes, through OCP amendment 2, to allow 3 storey buildings in the Pioneer Coop property.**  
**LUC RECOMMENDATION:**  
 Due to a strong desire to maintain a low rise esthetic in Traditional Residential areas, it is recommended to limit heights in the Pioneer Coop to 2.5 storey buildings.  
**PLEASE CHOOSE:**

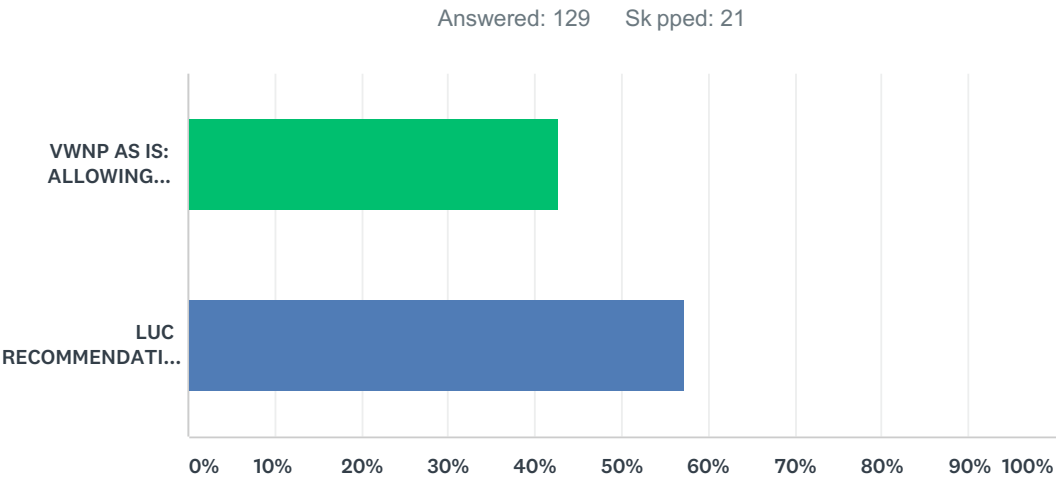
Answered: 133 Skipped: 17



ANSWER CHOICES	RESPONSES	
VWNP AS IS: A ow 3 storey bu d ngs n P oneer Coop	48.12%	64
LUC RECOMMENDATION: A ow 2.5 storey bu d ngs n P oneer Coop	51.88%	69
TOTAL		133

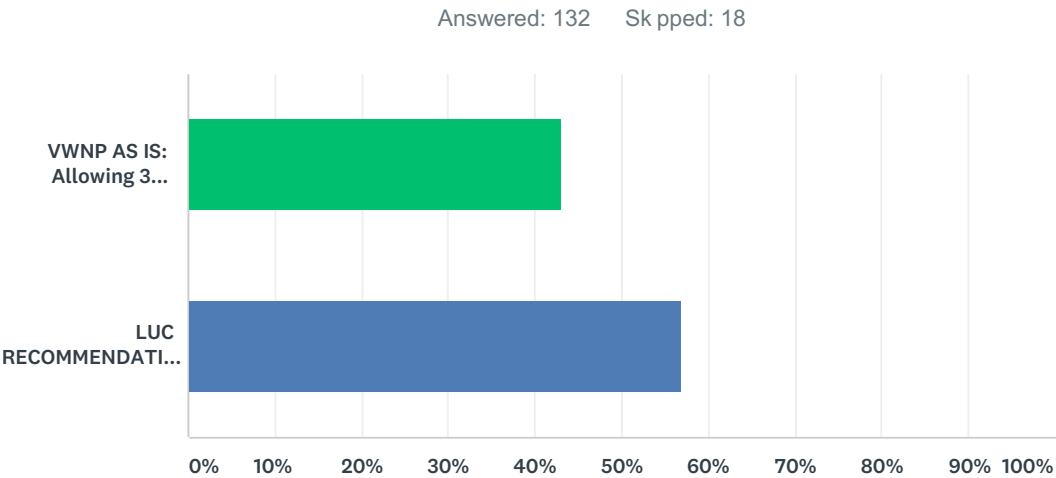
**Q4 ISSUE: The VWNP proposes through OCP amendment 2, to ALLOW commercial or service uses in the Pioneer Coop property.**  
**LUC**

RECOMMENDATION: Due to the location in a Traditional Residential area, it is recommended to NOT ALLOW commercial or service uses in the Pioneer Coop.PLEASE CHOOSE:



ANSWER CHOICES	RESPONSES	
VWNP AS IS: ALLOWING commerc a or serv ce uses n the P oneer Coop	42.64%	55
LUC RECOMMENDATION: NOT ALLOWING commerc a or serv ce uses n the P oneer Coop	57.36%	74
TOTAL		129

Q5 ISSUE: VWNP, through Amendment 3, allows 3 storey multi-unit residential buildings adjacent to the small urban village proposed at Edward & Catherine streets.LUC RECOMMENDATION: Due to this being a Traditional Residential area, it is recommended to limit heights to 2.5 storey multi-unit residential buildings.PLEASE CHOOSE:



ANSWER CHOICES	RESPONSES	
VWNP AS IS: A ow ng 3 storey mu t-un t res dent a bu d ngs, as noted above.	43.18%	57

LUC RECOMMENDATION: A low ng 2.5 storey mu t-un t res dent a bu d ngs, as noted above.

56.82%

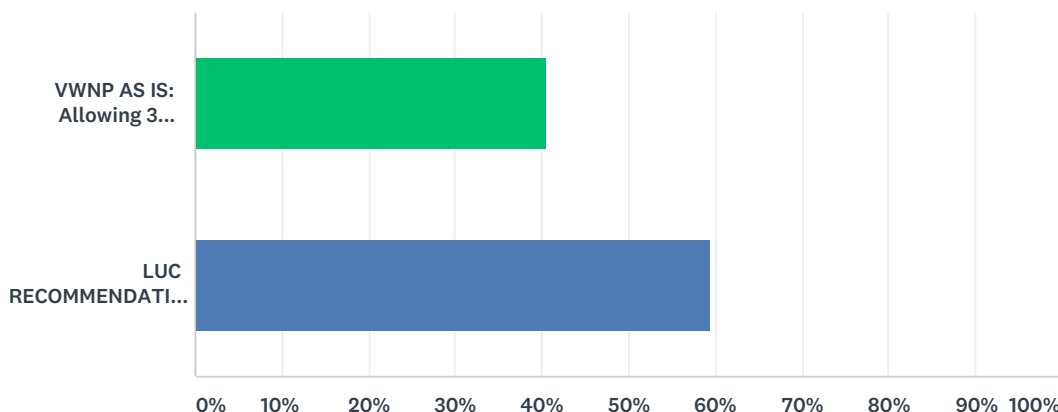
75

TOTAL

132

**Q6 ISSUE:** VWNP, through Amendment 4, allows 3 storey multi-unit residential buildings on properties south of Esquimalt Road close to waterfront (Robert, Mary, Russell Streets). **LUC RECOMMENDATION:** Due to this being a Traditional Residential area, it is recommended to limit heights to 2.5 storey multi-unit residential buildings. **PLEASE CHOOSE:**

Answered: 133 Sk pped: 17



#### ANSWER CHOICES

#### RESPONSES

VWNP AS IS: A low ng 3 storey, mu t-un t res dent a bu d ngs on Robert, Mary, Russe Streets, as noted above.

40.60% 54

LUC RECOMMENDATION: A low ng 2.5 storey, mu t-un t res dent a bu d ngs on Robert, Mary, Russe Streets as noted above.

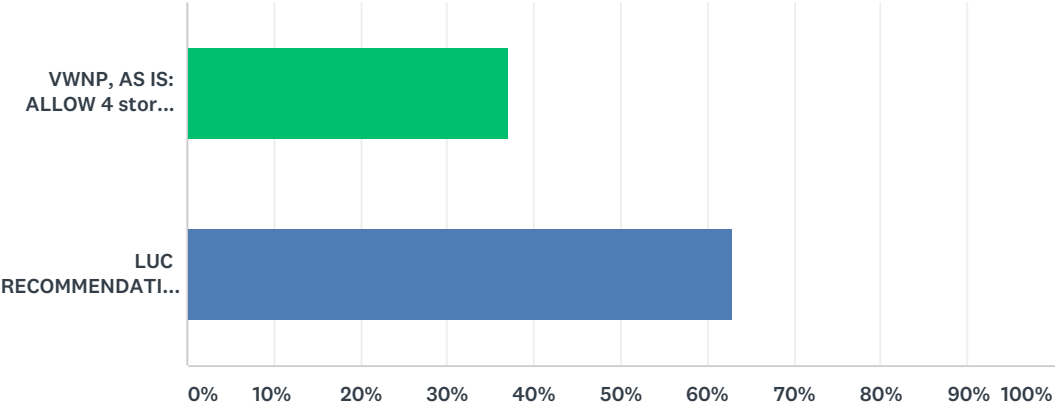
59.40% 79

TOTAL

133

**Q7 ISSUE:** VWNP, through OCP Amendment 6, allows up to 4 storey buildings at the corner of Skinner and Russell (and re-designating it from “Traditional Residential” to “Urban Residential”) with commercial uses at ground floor. **LUC RECOMMENDATION:** Due to a strong desire to not increase heights in this Traditional Residential area, it is recommended to only allow 2.5 storey buildings at this corner AND retain the “Traditional Residential” designation AND allow commercial uses at ground floor (i.e. similar to what is proposed in OCP Amendment 1 for existing residential buildings on Craigflower). **PLEASE CHOOSE:**

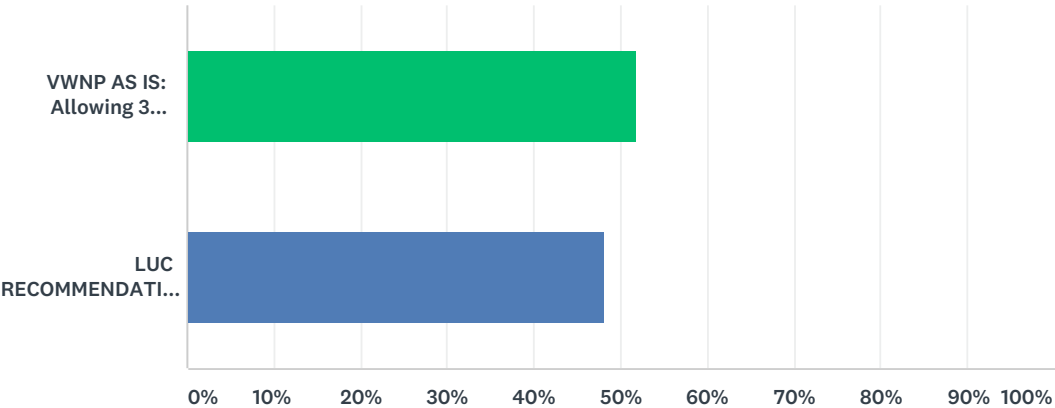
Answered: 135 Sk pped: 15



ANSWER CHOICES	RESPONSES	
VWNP, AS IS: ALLOW 4 storey bu d ngs at the corner of Russe and Sk nner Streets w th commerc a uses at ground f oor.	37.04%	50
LUC RECOMMENDATION: ALLOW 2.5 storey bu d ngs at the corner of Russe and Sk nner Streets a ow ng commerc a uses at ground f oor as above.	62.96%	85
TOTAL	135	

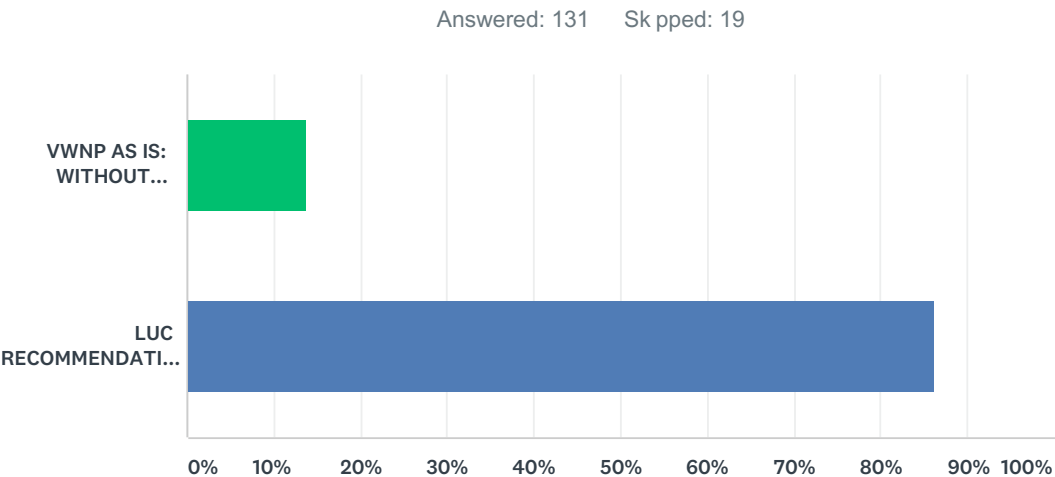
Q8 ISSUE: VWNP, through Amendment 7, proposes a designation of “Small Urban Village” AND allows 3 storey buildings for mixed use, residential or live work on Catherine Street between Edward & Langford streets (i.e. Oceanic Market and Tai Chi Centre).LUC RECOMMENDATION: VWCA supports this becoming a “Small Urban Village” BUT due to this being close Traditional Residential areas, it is recommended to limit heights to 2.5 storey multi-unit residential buildings.PLEASE CHOOSE:

Answered: 135 Sk pped: 15



ANSWER CHOICES	RESPONSES	
VWNP AS IS: A ow ng 3 storey m xed use, res dent a or ve work bu d ngs.	51.85%	70
LUC RECOMMENDATION: A ow ng 2.5 storey m xed use, res dent a or ve work bu d ngs.	48.15%	65
TOTAL	135	

Q9 ISSUE: The VWNP, through OCP Amendment 8, proposes significant increases in density at the Westside Shopping Centre but does not reference prior density transfers.LUC RECOMMENDATION: When the Westside Centre Shopping Centre was originally developed, density was transferred from it to the Parc Residences. The VWCA strongly recommends this prior bonus density transfer be noted formally to ensure it is included in future bonus density negotiations.PLEASE CHOOSE:

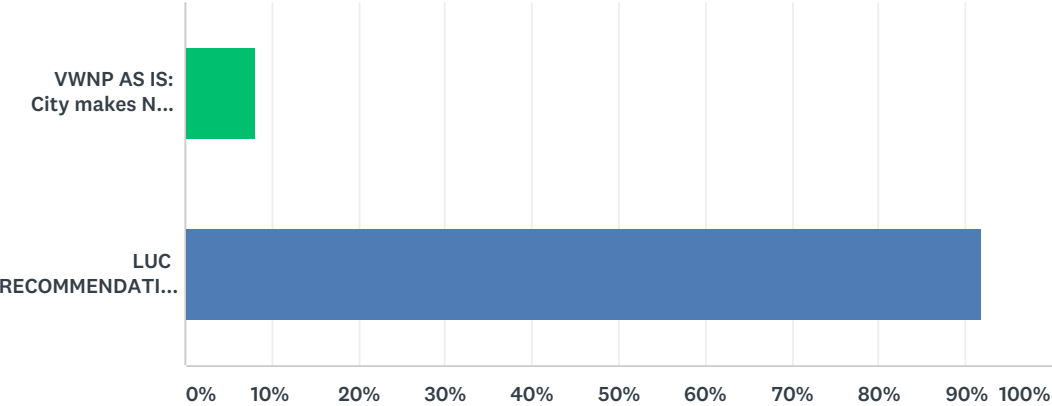


ANSWER CHOICES	RESPONSES	
VWNP AS IS: WITHOUT reference to pr or bonus dens ty transfers.	13.74%	18
LUC RECOMMENDATION: WITH reference to pr or bonus dens ty transfers.	86.26%	113
TOTAL		131

Q10 ISSUE: The VWNP, in OCP Amendment 8, proposes (for the Westside Shopping Centre and areas north of it) significant density bonus increases for “amenities” but does NOT mention consulting the community or VWCA regarding whether the amenities being proposed are desired by the community or VWCA. [NOTE: OCP amendment 15 also proposes significant density bonus increases.]LUC RECOMMENDATION: That a commitment be included in the VWNP that the community and VWCA will be consulted, by the City, on any new developments where density increases are being negotiated to determine whether the amenities being proposed are desired (see page 105 of VWNP re: amenities examples).PLEASE CHOOSE:

Answered: 134 Skpped: 16

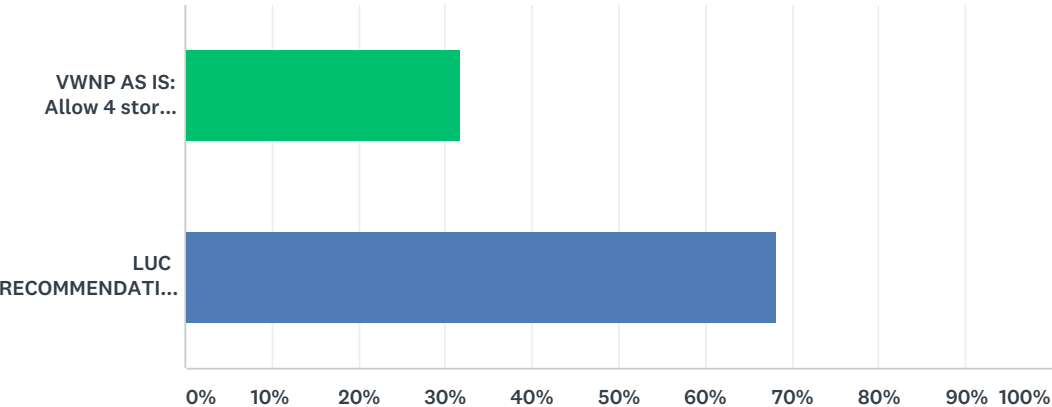




ANSWER CHOICES	RESPONSES	
VWNP AS IS: C ty makes NO COMMITMENT to consu t ng commun ty/VWCA re: amen t es, n the areas referenced n OCP Amendment 8 &15.	8.21%	11
LUC RECOMMENDATION: That the C ty makes A COMMITMENT to consu t ng commun ty/ VWCA re: amen t es, n the areas referenced n OCP Amendment 8 & 15.	91.79%	123
TOTAL		134

Q11 ISSUE: The VWNP, through Amendment 9, proposes 4 storey buildings on Mary Street south of Esquimalt Road and changing the designation of this area from “Traditional Residential” to “Urban Residential”.LUC RECOMMENDATION: Due to the high regard that Vic West residents have for existing Traditional Residential areas, it is recommended that this area retain the designation "Traditional Residential" with maximum building heights of 2.5 storeys.PLEASE CHOOSE:

Answered: 132    Skpped: 18



ANSWER CHOICES	RESPONSES	
VWNP AS IS: A ow 4 storey bu d ngs and change to a “Urban Res dent a ” des gnat on for Mary Street (south of Esqu ma t Road)	31.82%	42

LUC RECOMMENDATION: Allow 2.5 storey buildings and keep "Traditional Residential" designation.

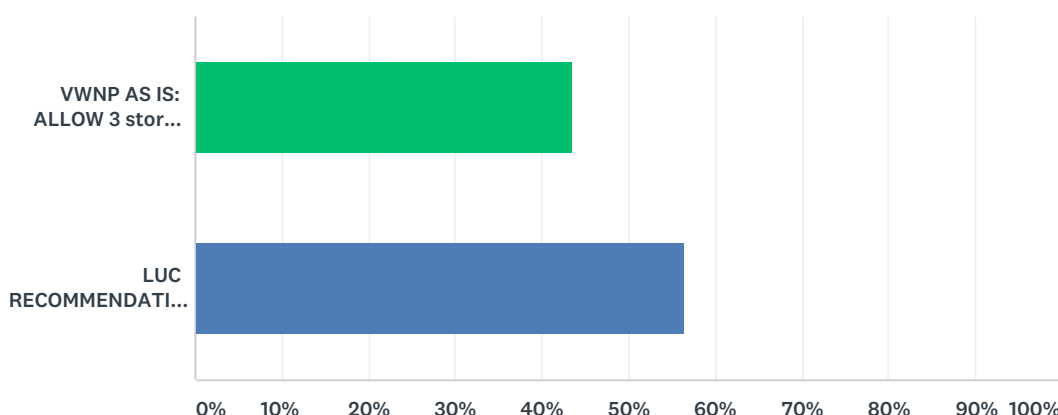
68.18% 90

TOTAL

132

**Q12 ISSUE:** The VWNP, through OCP Amendment 11, proposes 3 storey buildings for the properties south of the existing commercial centre on Craigflower Road (i.e. Spiral Café building) and designates this area a "Small Urban Village". **LUC RECOMMENDATION:** Allow 2.5 storey buildings in this area and retain the "Traditional Residential" designation AND allow commercial uses similar to OCP Amendment 1 (i.e. the properties north of the existing commercial centre). **PLEASE CHOOSE:**

Answered: 131 Skipped: 19



#### ANSWER CHOICES

#### RESPONSES

VWNP AS IS: ALLOW 3 storey buildings on Craigflower (south of the existing commercial centre).

43.51% 57

LUC RECOMMENDATION: ALLOW 2.5 storey buildings on Craigflower (south of the existing commercial centre).

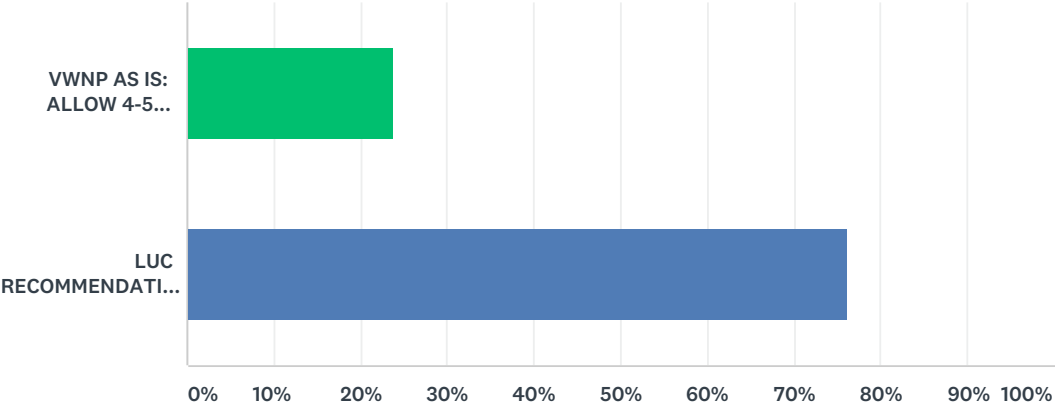
56.49% 74

TOTAL

131

**Q13 ISSUE:** The VWNP, through OCP Amendment 12, proposes to change to "Employment Residential" and ALLOW 4-5 storey buildings (14-17 m), in various areas including Mary & Catherine (south of Esquimalt Road); Russell & Mary (north & south of Esquimalt Road); Alston/Skinner/Tyee). **LUC RECOMMENDATION:** Due to the proximity to existing Traditional Residential areas, it is recommended to ALLOW 4 storey buildings (14m) in the areas referenced in OCP Amendment 12. **PLEASE CHOOSE:**

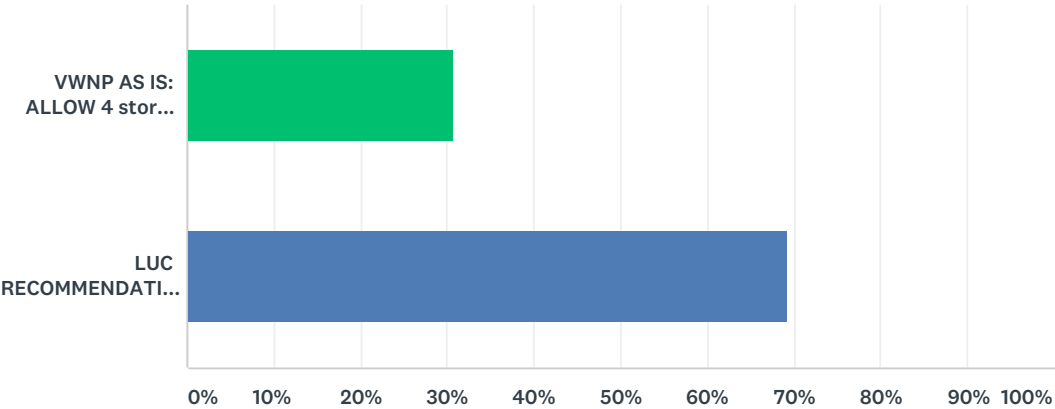
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ANSWER CHOICES	RESPONSES	
VWNP AS IS: ALLOW 4-5 storey bu d ngs n var ous “Emp oyment Res dent a ” areas as noted n OCP Amendment 12.	23.85%	31
LUC RECOMMENDATION: ALLOW 4 storey bu d ngs n var ous “Emp oyment Res dent a ” areas as noted n OCP Amendment 12.	76.15%	99
TOTAL		130

Q14 ISSUE: The VWNP, through OCP Amendment 13, proposes to change to “Industrial Employment” and ALLOW 4 storey buildings , in various areas including Skinner/Langford/Alston block and Esqimalt/William/Wilson block.LUC RECOMMENDATION: Due to the proximity to existing Traditional Residential areas, it is recommended to ALLOW 3 storey buildings in the areas referenced in OCP Amendment 13.PLEASE CHOOSE:

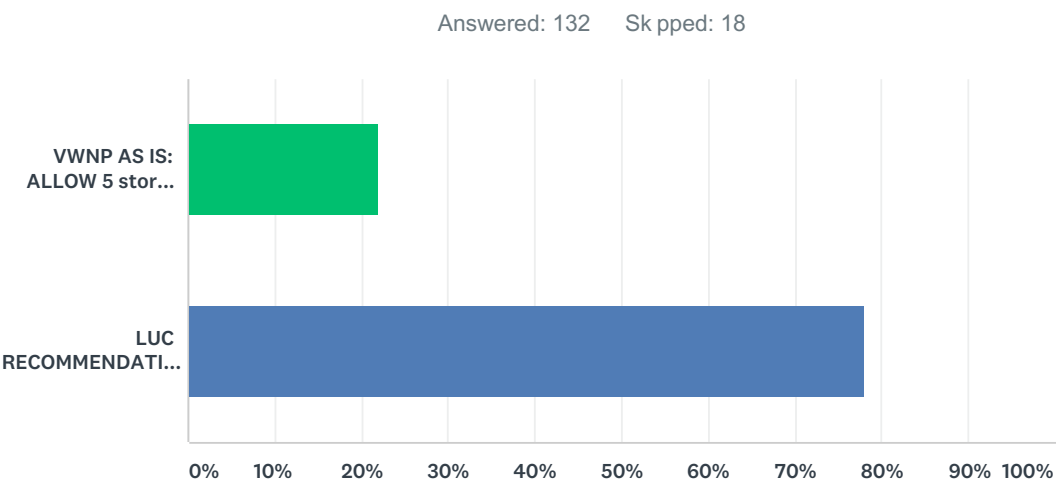
Answered: 130    Sk pped: 20



ANSWER CHOICES	RESPONSES	
VWNP AS IS: ALLOW 4 storey bu d ngs n var ous “Industr a Emp oyment” areas as noted n OCP Amendment 13 (see above).	30.77%	40
LUC RECOMMENDATION: ALLOW 3 storey bu d ngs n var ous “Industr a Emp oyment” areas as noted n OCP Amendment 13 (see above).	69.23%	90

TOTAL	130
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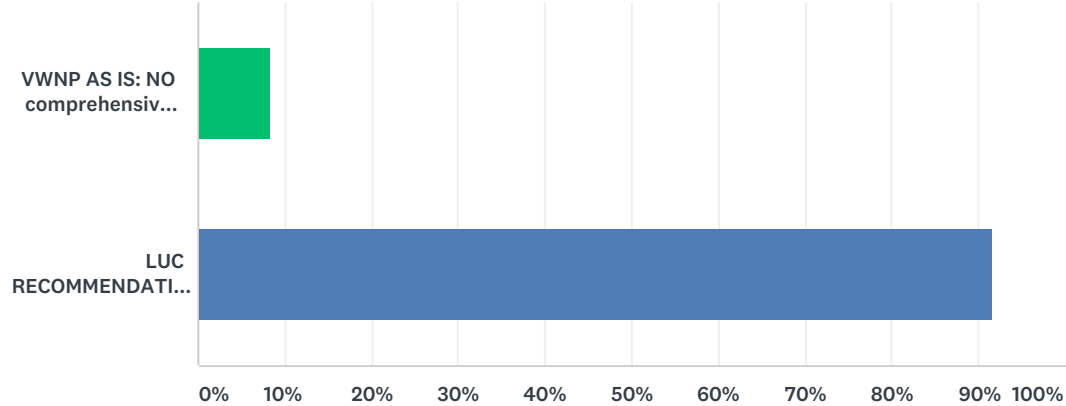
Q15 ISSUE: The VWNP, through OCP Amendment 14, proposes to change to “Industrial Employment-Residential” and ALLOW 5 storey buildings, in various areas including Esquimalt Road from Springfield to Catherine; Skinner/Langford/Alston block. LUC RECOMMENDATION: Due to the proximity to existing Traditional Residential areas, it is recommended to ALLOW 4 storey buildings in the areas referenced in OCP Amendment 12. PLEASE CHOOSE:



ANSWER CHOICES	RESPONSES	
VWNP AS IS: ALLOW 5 storey buildings in various "Industrial Employment-Residential" areas as noted in OCP Amendment 14.	21.97%	29
LUC RECOMMENDATION: ALLOW 4 storey buildings in various "Employment Residential" areas as noted in OCP Amendment 14.	78.03%	103
TOTAL		132

Q16 ISSUE: The VWNP, in OCP Amendment 15 and throughout the VWNP references heights and densities but there is NO comprehensive map or massing model that clearly visualizes the outcomes from the VWNP.LUC RECOMMENDATION: In order to help people in Vic West clearly understand the possible outcomes of implementing the VWNP, it is recommended that some means of visualization of what Vic West will look like as a result of the VWNP. This visualization should include both Master Planned Areas (such as Dockside Green, Bayview, Railyards, etc.) and non-master planned areas. This visualization should also help City staff and Council to see the “big picture”. PLEASE CHOOSE:

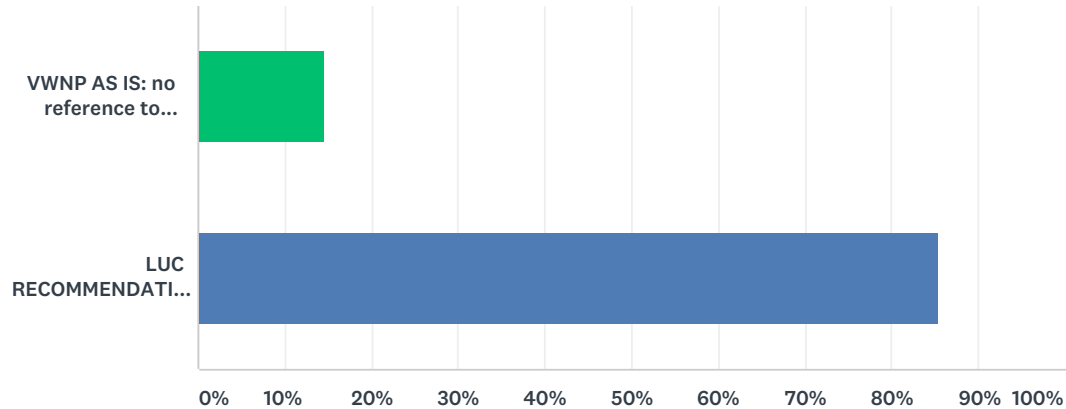
Answered: 133 Skipped: 17



ANSWER CHOICES	RESPONSES	
VWNP AS IS: NO comprehens ve v sua zat on of what V c West w ook ke.	8.27%	11
LUC RECOMMENDATION: Inc ude n the VWNP a comprehens ve v sua zat on of what V c West w ook ke (e.g. mass ng mode , 3-D mapp ng, etc.).	91.73%	122
TOTAL		133

Q17 ISSUE: VWNP does not reference the need for speed limits to NOT change when entering Vic West, particularly from Esquimalt. LUC RECOMMENDATION: That speed limits on major roads in Vic West should be consistent with neighbouring roads and NOT exceed 40 kph (e.g. Craigflower, Tyee, Bay, Wilson & Esquimalt). Existing 30 kph speed limits would remain as is. PLEASE CHOOSE:

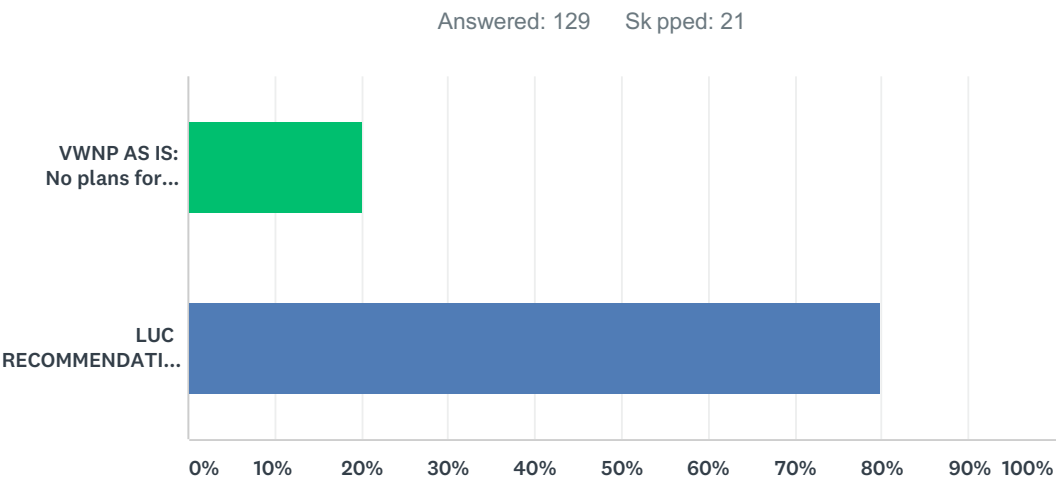
Answered: 130    Sk pped: 20



ANSWER CHOICES	RESPONSES	
VWNP AS IS: no reference to speed m t changes on major roads, at V c West borders	14.62%	19
LUC RECOMMENDATION: That speed m ts on major roads shou d not change at the V c West border ( .e. t shou d be 40 kph on Cra gfower, Tyee, Bay, W son & Esqu ma t wh e reta n ng ex st ng 30 kph zones).	85.38%	111
TOTAL		130



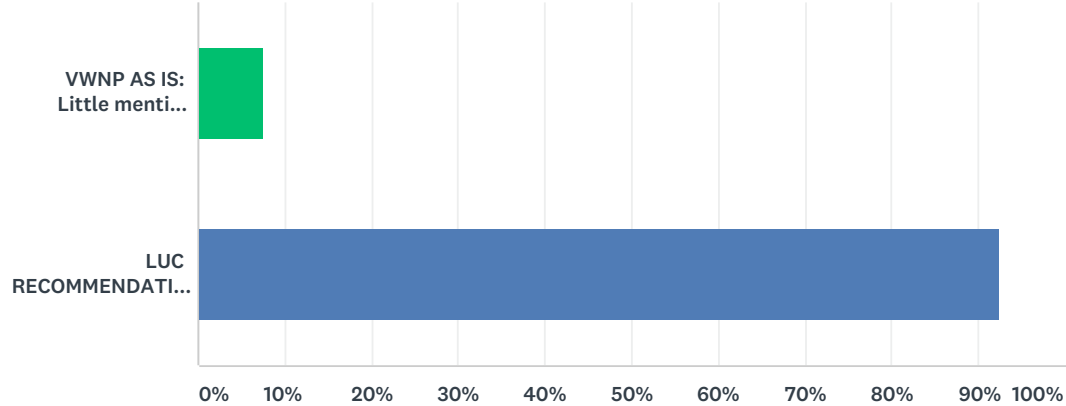
Q18 ISSUE: Speeding remains a serious issue on major roads but is not addressed as aggressively as needed.LUC RECOMMENDATION: Traffic calming is needed, especially on Craigflower, Wilson, Hereward and Catherine.PLEASE CHOOSE:



ANSWER CHOICES	RESPONSES	
VWNP AS IS: No plans for traffic calming	20.16%	26
LUC RECOMMENDATION: Include plans for traffic calming, especially on Craigflower, Wilson, Hereward and Catherine.	79.84%	103
TOTAL		129

Q19 ISSUE: Vehicle traffic congestion and cycling safety on the Bay Street bridge is a serious concern and not addressed in the VWNP.LUC RECOMMENDATION: The future of the bridge should have a more prominent vision and plan in the Vic West Neighbourhood Plan. A timeline of actions (e.g. concept development; design; construction; etc.) would be appreciated by community members. PLEASE CHOOSE:

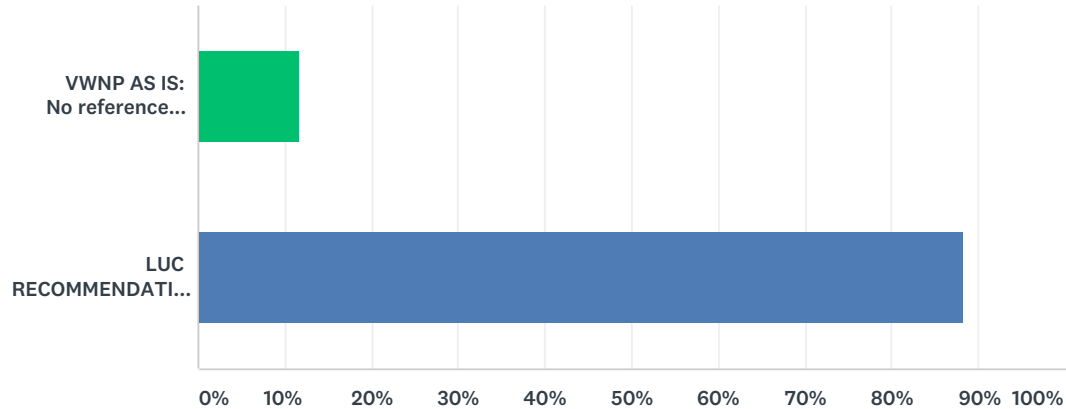
Answered: 132 Skipped: 18



ANSWER CHOICES	RESPONSES	
VWNP AS IS: L t t e ment on of the Bay Street Br dge.	7.58%	10
LUC RECOMMENDATION: That the ssues of p ans for the Bay Street br dge be nc uded n the VWNP.	92.42%	122
TOTAL		132

Q20 ISSUE: Pedestrian safety on Langford Street (from Skinner to Alston) and Alston Street (from Langford to Wilson) remains a serious issue. These streets have no sidewalks or curbs and cars park where sidewalks would normally be, which causes pedestrians to walk on the road between the parked cars and traffic. LUC RECOMMENDATION: Modify the current strategy, which ties sidewalk installation to redevelopment, to a “good neighbour” strategy whereby people holding land for future development are expected to be “good neighbours” and keep the borders of their properties safe and maintained. At a minimum, temporary curbs, should be installed to limit parking on the boulevard areas. PLEASE CHOOSE:

Answered: 127    Sk pped: 23

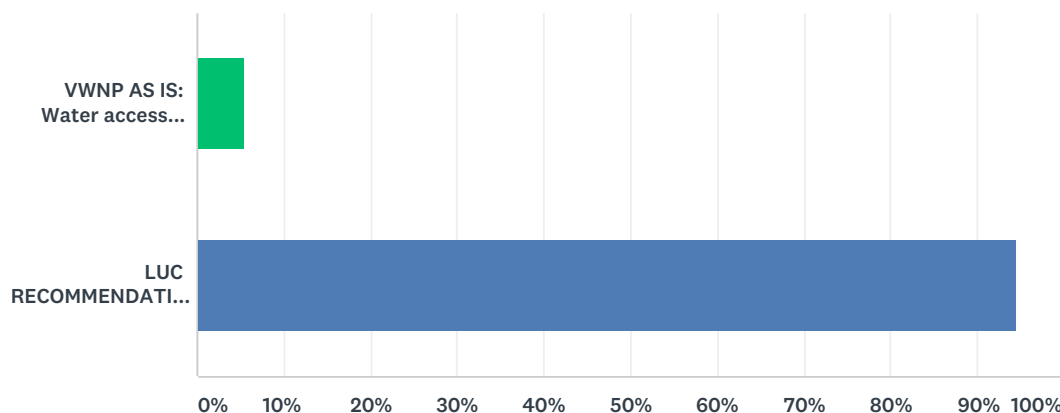


ANSWER CHOICES	RESPONSES	
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VWNP AS IS: No reference to improving safety on Aston and Langford Streets.	11.81%	15
LUC RECOMMENDATION: That the landowners with property being held for future development be expected to maintain the borders of the properties to a safe standard.	88.19%	112
TOTAL		127

**Q21 ISSUE:** The VWNP maps do not show water access points (current and future) nor the new marina.**LUC RECOMMENDATION:** Show all docks and water access points plus the new marina on Maps 6, 7, 8 & 9 (e.g. potential and future water access points Arm Street, Burleith Street [future], Banfield Park, Railyards, Dockside Green, Songhees, etc.).  
**PLEASE CHOOSE:**

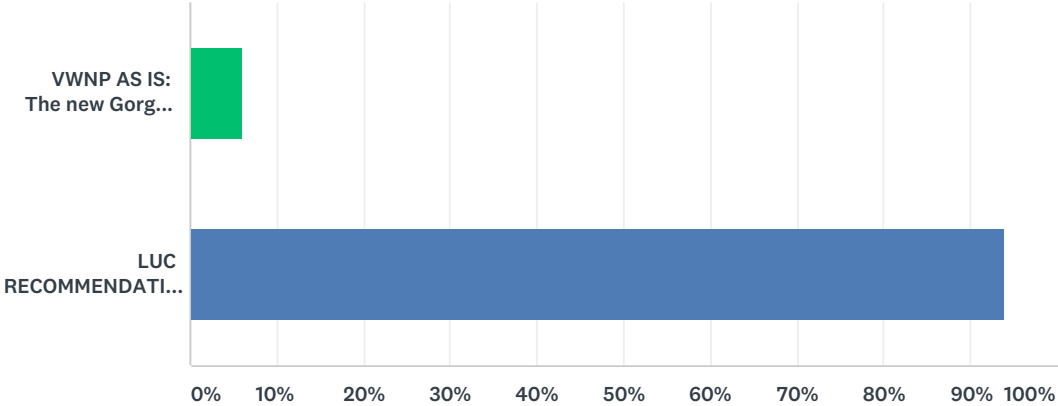
Answered: 131 Skipped: 19



ANSWER CHOICES	RESPONSES	
VWNP AS IS: Water access points NOT INCLUDED in VWNP maps.	5.34%	7
LUC RECOMMENDATION: INCLUDE water access points in the VWNP maps.	94.66%	124
TOTAL		131

**Q22 ISSUE:** New Gorge Waterway Marine Park is not shown on the maps nor is there any discussion about the intent, vision and policies of the marine park.**LUC RECOMMENDATION:** Include the Gorge Waterway Marine Park in the maps in the VWNP and include the intent, vision and policies of the marine park.**PLEASE CHOOSE:**

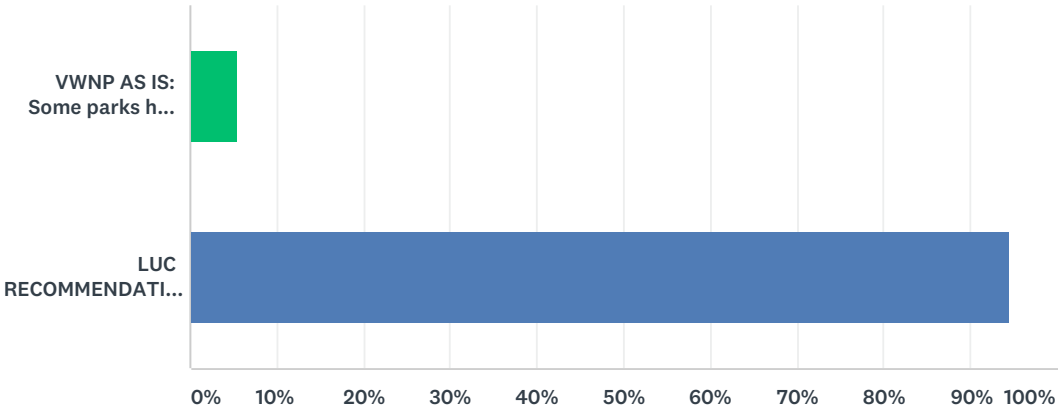
Answered: 132 Skipped: 18



ANSWER CHOICES	RESPONSES	
VWNP AS IS: The new Gorge Waterway Marine Park is NOT SHOWN on maps in VWNP.	6.06%	8
LUC RECOMMENDATION: SHOW the new Marine Park on maps in the VWNP plus the Marine Park's intent, vision and policies.	93.94%	124
TOTAL		132

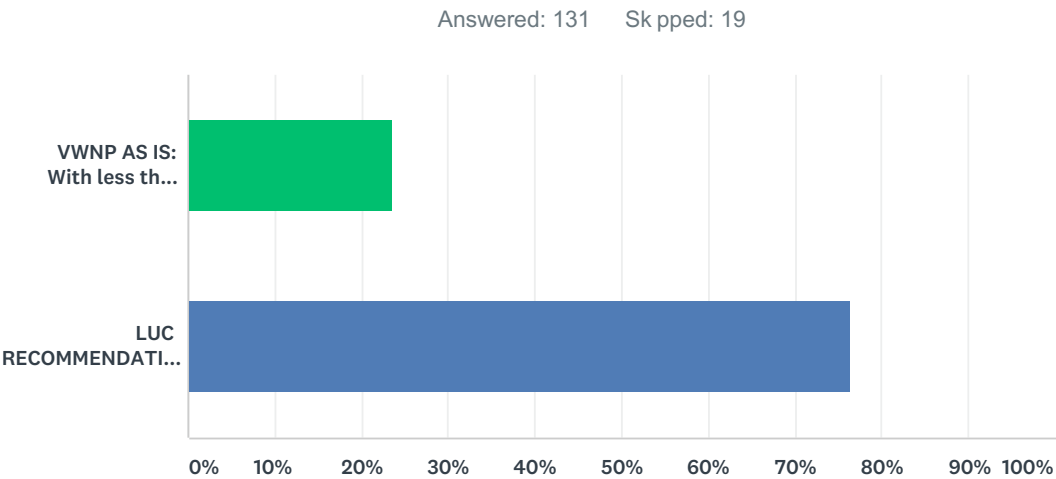
Q23 ISSUE: Vision and plans are noted for some parks but NOT ALL PARKS in Vic West.LUC RECOMMENDATION: Acknowledge the existence and importance of ALL PARKS by developing the possible future uses and vision and plans for each park in Vic West (including Regatta Park and dock; Point Ellice Park; Songhees Dock; Westsong Walkway; Lime Bay Park; Mary Street Park; Rainbow Park; Colville Island; Coffin Island; Barnard Park; Songhees Hillside Park; Vista Park: Tenas Park).PLEASE CHOOSE:

Answered: 131    Skipped: 19



ANSWER CHOICES	RESPONSES	
VWNP AS IS: Some parks have little or no mention.	5.34%	7
LUC RECOMMENDATION: That ALL PARKS are included in the VWNP including a vision and plans.	94.66%	124
TOTAL		131

Q24 ISSUE: Big Move "Strengthen Parks and Food Systems" is NOT a "Big Move" as very few additional or new ideas are in the Vic West Neighbourhood Plan (Section 12 is not even two full pages).LUC RECOMMENDATION: Engage the Vic West Food Security Collective in strengthening Section 12 of the Vic West Neighbourhood Plan.PLEASE CHOOSE:

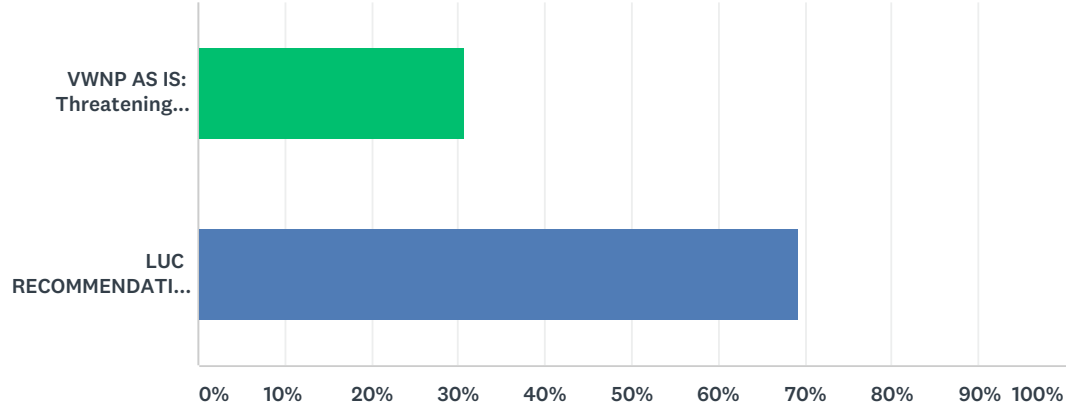


ANSWER CHOICES	RESPONSES	
VWNP AS IS: W th  ess than two pages  n the VWNP.	23.66%	31
LUC RECOMMENDATION: Engage the V c West Food Secur ty Co ect ve n strengthen ng Sect on 12 of the VWNP.	76.34%	100
TOTAL		131

Q25 ISSUE: The VWNP threatens existing food growing in Vic West by encouraging development in Traditional Residential areas which, by their nature, have food growing potential.LUC RECOMMENDATION: That the VWNP limit high density development to areas outside of Traditional Residential Areas. PLEASE CHOOSE:

Answered: 127 Skipped: 23

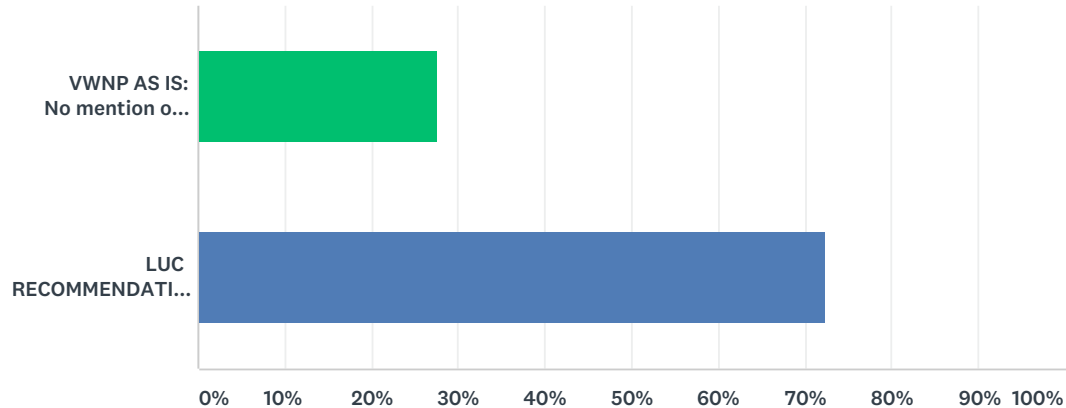




ANSWER CHOICES	RESPONSES	
VWNP AS IS: Threaten ng food grow ng potent a through ncreased deve opment n Trad t ona Res dent a areas.	30.71%	39
LUC RECOMMENDATION: L m t h gh dens ty deve opment to areas outs de of Trad t ona Res dent a Areas.	69.29%	88
TOTAL		127

Q26 ISSUE: The VWNP threatens existing food growing in Vic West by encouraging development where gardens currently exist without requiring food-growing amenities in the developments. LUC RECOMMENDATION: That the VWNP require developers to include food growing amenities in their developments. PLEASE CHOOSE:

Answered: 130 Sk pped: 20



ANSWER CHOICES	RESPONSES	
VWNP AS IS: No ment on of requ r ng deve opers to nc ude food grow ng amen t es.	27.69%	36
LUC RECOMMENDATION: Requ re a deve opments to nc ude food grow ng amen t es n the r deve opments.	72.31%	94
TOTAL		130

**Rob Gordon**

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**Subject:** FW: Pioneer Co-op - Vic West Neighborhood Plan

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**From:** Avis [[mailto:Personal info](#)]

**Sent:** Sunday, January 28, 2018 9:08 AM

**To:** Kristina Bouris <[KBouris@victoria.ca](mailto:KBouris@victoria.ca)>; Jeremy Loveday (Councillor) <[jloveday@victoria.ca](mailto:jloveday@victoria.ca)>

**Subject:** Pioneer Co-op - Vic West Neighborhood Plan

Last week we met at the presentation at the Vic West School Kristina. As you suggested, Peter and I composed a letter outlining our hopes and dreams for the future of Pioneer Co-op.

If you require anything further, please let us know.

And Jeremy, I know you will support this, right?

Cheers

Avis

Chair of the Board  
Pioneer Housing Co-op  
[Personal info](#)

Personal info

Avis & Michalovsky  
922 McCaskill Street  
Victoria, BC V9A 4B9  
Personal info

January 27, 2018

Attention: Kristina Bouris  
Cc: Jeremy Loveday

Re: Victoria West Neighborhood Plan  
Pioneer Co-op Site

We have lived at Pioneer Co-op for **Personal info** Pioneer  
Co-op sits on 7.1 acres of land that houses 74 member households. Our unit sizes include:

- 4 one bedroom townhouse units
- 34 two bedroom townhouse units
- 33 three bedroom townhouse units
- 3 four bedroom townhouse units

None of our units are accessible. It is our hope that we can look at changing that.

In 2017, Pioneer Co-op completed a 3 million dollar upgrade to the outside of our buildings – including insulation, new siding, new windows, etc. Our co-op is vibrant and looking to the future as we continue to be a part of the Vic West Community.

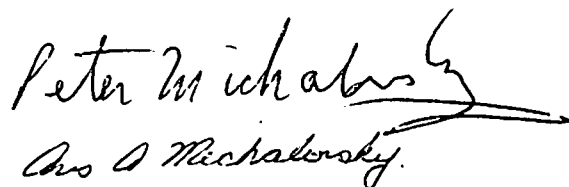
There is a core group of our co-op members who are keen to have the community plan approved with the amendments proposed. With affordable housing in crisis in Victoria, we would like to explore developing one bedroom and adapted units to meet the needs of an aging population. By doing this, a few of our existing co-op members could downsize to the smaller units, which would allow new families access to suitable housing that would be affordable housing. The remainder of the units could be filled by people seeking a community environment and affordable housing. There is a shortage of affordable units for singles, seniors and those requiring adapted units.

In order to make something like this a reality, we would need to be able to build three stories high in order to make it financially feasible. It would be our hope that we would be able to include a community room and office space in the planned building. Interestingly enough, when Pioneer was first built, there was no community room – since 1974, it has been necessary to rent space for committee and general meetings.

We support the plan for Pioneer Co-op and hope that the City of Victoria will see our vision for future.

Regards **Personal info**

Avis and Michalovsky  
Note: Avis is the Chair of the Pioneer  
Housing Co-op



Peter Michalovsky  
Avis & Michalovsky

**Rob Gordon**

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**Subject:** FW: comments re draft Vic West OCP

**From:** Personal info

16 PM

**To:** Marc Cittone <[mcittone@victoria.ca](mailto:mcittone@victoria.ca)>; Jeremy Loveday (Councillor) <[jloveday@victoria.ca](mailto:jloveday@victoria.ca)>; Engagement <[engage@victoria.ca](mailto:engage@victoria.ca)>

**Subject:** comments re draft Vic West OCP

Comments & feedback on the latest draft version of the Vic West OCP

Please note that there were references to Hereward Street...that should be ROAD

Thanks - Personal info

## Proposed Revisions to Traditional Residential Designations:

Amendment 2 re small apartment buildings up to 3 storeys on Pioneer Housing Co-op site plus option for limited community-oriented commercial or service use (Special planning area #4)

From 901 to 923 Hereward Road, there are 5 single family or duplex homes fronting on Hereward with the Pioneer Co-op on 1000 McCaskill Street directly behind. With the proposed change to allow townhouses, stacked townhouses or small apartment buildings up to 3 storeys (change from the current 2 storeys) there would be a loss of privacy for the rear yards for all these homes as well as sunlight from the East. Perhaps for this site, new structures closest to any single family home that back onto this site be limited to 2 storeys. Along McCaskill or elsewhere on this site, if no homes adjoining this site, then 3 storeys could be considered.

The proposal to allow for limited community commercial or service use would take away from the other commercial areas (Westside, Craigflower Road Small Urban Village, Catherine Street Small Urban Village). This site should remain as residential only. If this is to cater to E&N trail users (eg convenience store?) hours of operation & litter could be an issue for residents. I do not know what the co-op housing mission is but something like a daycare would be better than a cafe, bike repair business or medical facility.

Amendment 4 re townhouses & small apartment buildings up to 3 storeys on selected properties south of Esquimalt Road

This proposal for 3 storeys should be only along Esquimalt Road & not in the Traditional Residential Areas.

Under the heading “Why is this proposed?” the reasoning is that “the exceptionally deep lots on Robert Street present the opportunity for courtyard-oriented townhouse and apartments integrating open spaces, providing a desirable housing choice”. Townhouses or apartments of up to 3 storeys are not a compatible fit in this traditional residential area or the surrounding historic homes.

The following is the existing housing on Robert Street, from the waterfront north to Esquimalt Road on both the west & east sides of the street:

On west side:

206 built 1946; 216 built 1955; 224 circa 1940 with addition 1970; 236 built 1955...all 4 are single family homes

230 built 1901 single family home (Victoria designated Heritage)

242 built 1901 4 strata units (Victoria designated Heritage)

310 built 1971 multi unit rental apartment building 4 storeys

614 Seaforth built 1901 around the corner from Robert Street 4 unit rental conversion (Victoria designated Heritage)

On east side:

211 built 1982 consisting of 16 townhouses fronting on waterfront between Robert & Russell Streets

223 built 1904 single family (National Historic Site designated Federally & Victoria designated Heritage)

233, 241, 249, 255, 301 & 311 all single family homes built between 1939 & 1943



331 built 1976 consisting of 25 strata units

This is a unique small pocket of single family homes, hemmed in by strata townhouses, apartments & rental apartment building at the north end on Esquimalt Road, at the south end on the waterfront & Rainbow Park & behind houses on the west side on Seaforth. There are four City designated Heritage buildings, two of which have been converted into strata & rental units, maintaining the streetscape, scale & character of the other single family homes & in keeping visually with neighbouring homes. The two heritage buildings that have been converted to either strata or rental units were sensitive to the surrounding properties & provided additional affordable housing without imposing or destroying the overall feel of this traditional residential area. That is why I feel that townhouses or apartments, whether 2 1/2 or 3 stories, would not be appropriate for Robert Street. Other houses built pre-war in the late 1930's or early 40's have heritage merit even not registered or designated (policy 10.1.1). See 10.1 on page 93 & the two short term action plans at the top of page 110 of the proposed OCP for adoption.

What would be welcomed would be for garden suites or 2 story low-rise structures built behind the existing single family homes which would retain the visual streetscape on the front. The lots on the east side & one on the west side are very deep which could allow for new structures & provide additional housing. This has already been done on two properties at the north end of Robert Street with the frontage maintained with the original houses & little impact. This would address the wish by City to provide affordable housing choices & maintain green open spaces without the need for townhouse or apartment developments which are not sensitive infill housing in this traditional residential area. Keep the current 2 story policy in the OCP so any new construction would be compatible in scale with the built homes in the immediate surrounding area!

Perhaps this area of Robert Street could be designated as a Heritage Conservation area, similar to the northerly section of Catherine Street from Skinner to the Gorge waterfront (policy 10.2.1). Or, this could be designated as a Development Permit Area so proposals for any new construction would be considered on its own merits but taking into consideration the existing structures.

In summary, the residents & homeowners of the remaining single family dwellings on Robert Street would like to retain the feel of the traditional residential designation & not have 2 1/2 or 3 story houses, townhouses or apartments built whether facing the street or at the rear of the deep lots. This desire was expressed last fall with a neighbourhood meeting on Robert Street hosted by City Planning official Marc Cittone with over 30 residents, also attended by Councillor Jeremy Loveday.

**Rob Gordon**

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**Subject:** FW: Comments on Vic West Draft Plan

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**From:** Personal info

**Sent:** February 26, 2018 6:49 PM

**To:** Engagement <[engage@victoria.ca](mailto:engage@victoria.ca)>

**Cc:** Victoria Mayor and Council <[mayorandcouncil@victoria.ca](mailto:mayorandcouncil@victoria.ca)>; S. 22

[coordinator@victoriawest.ca](mailto:coordinator@victoriawest.ca)

**Subject:** Comments on Vic West Draft Plan

Please find enclosed a letter which summarizes our views on the draft plan.

February 18, 2018

City of Victoria  
Planning Department

**Re: Vic West Neighbourhood Plan**

Dear Sirs/Mesdames

We are writing to you to make some suggestions to fix shortcomings in the Vic West Neighbourhood Plan which is currently nearing completion.

In general, we are very supportive of the Plan and the process that has been followed to get where we it is today. We love the Vic West neighbourhood and support compatible densification. However, there are a couple of areas where we feel that the Plan proposals could backfire.

The aspect that is not adequately addressed is the preservation and improvement of character houses which are not heritage registered. There are many of these in Vic West. There are also many unremarkable boxes of various ages. The diversity is one of the things we like about the neighbourhood, but if change must be made then doesn't it make sense to incentivise keeping the character homes and allow redevelopment of the unremarkable houses? The plan has very little to say about this. But without guidance from the Plan, zoning regulations tend to work against character houses. For example:

1. Many of these older homes have quirks, which the owners and neighbours often love but City Hall has problems with. Things like non-standard setbacks, and steeply pitched roofs with dormers which cause problems with height limits.
2. Many of these homes have low height basements with aging foundations and/or poor drainage. Zoning regulations put severe restrictions on raising the house to create a full height basement suite which would help pay the cost of the foundation replacement and support the retention of the house.
3. It is well known that difficulties with getting City approval to retain and improve a character house often swing the economics in favour of replacement. And that's where the Plan policies can backfire: by allowing greater height and higher floor space ratios, the economics get even more skewed in favour of replacement. You certainly see that in other neighbourhoods like James Bay: beautiful character homes are purchased by speculators, who rent them out and don't maintain them. After a decade or so of 'demolition by neglect', the owners claim that repairs are too expensive and the only option is to tear it down and put up a modern building.

You may have guessed that our own house is one of those non-registered character houses which could go either way – upgraded to last another 100 years, or fall into disrepair and be torn down. So we have a vested interest in this issue.

However, the Plan can help improve the odds of keeping these character houses. Here are our suggestions:

- The Plan has many sections which pertain only to heritage designated properties. Why not have them pertain to all houses over say 100 years old?
- If a character house is situated on a lot which would support higher density, and so is another non-character house nearby, allow a density transfer from the character house to the non-character lot in exchange for retaining or improving the character house.
- Have an explicit policy of leniency when it comes to variance applications for character houses under renovation. This would apply to such things as height variances, setback variances, etc.
- When designating certain properties in the Plan for higher densities in certain sub areas, look at the quality of the existing house when deciding whether or not it should be included. Obviously, the overall objectives of the Plan (more density, diverse housing types, and retaining neighbourhood character) would be better achieved by keeping certain buildings and replacing others. And those differing types and objectives may be on the same street. But that doesn't mean that the plan can't treat them differently, in a manner more in tune with meeting the objectives. Don't assume that the more interesting ones will naturally be preserved and the less interesting ones won't – economics will usually rule the day and the Plan needs to actively help level the playing field, or even tilt it in favour of the neighbourhood character.

Lest you presume that your current programme for adding suites to character homes is enough to remedy these challenges, in our experience, these programmes remain lacking. While they purport to provide flexibility, these programmes are in fact filled with many costs for the homeowner combined with a lack of certainty or ability to get a straight answer from City staff about requirements without having to first spend several thousands of dollars in surveys and drawings. This makes these processes risky and costly for average, non-developer homeowners, compounding the likelihood that such individuals will have to give up and sell to someone likely to demolish the home.

We love our Victoria home and neighbourhood and want to see a plan which enables us to remain there, while aging in place and also providing for one or more additional units which could also house others –particularly in the younger generation that struggles with affordability. We urge you to adjust the plan to make this easier—not just for us, but for

others in the neighbourhood and the city at large.

Sincerely,

Personal info

Cc: Mayor and Council



## Rob Gordon

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**Subject:** FW: Draft Vic West Plan - input on 945 Alston Street - Block bounded by Skinner/Langfrod/Alston

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**From:** Deane Strongitharm [<mailto:dstrongitharm@cityspaces.ca>]

**Sent:** Friday, February 16, 2018 4:33 PM

**To:** Kristina Bouris <[KBouris@victoria.ca](mailto:KBouris@victoria.ca)>; Marc Cittone <[mcittone@victoria.ca](mailto:mcittone@victoria.ca)>

**Subject:** Draft Vic West Plan - input on 945 Alston Street - Block bounded by Skinner/Langfrod/Alston

Thanks again for your availability and willingness to listen to and respond to questions and concerns.

I have attached a longish memo regarding the site, making specific requests for amendments, and hopefully providing rationale for the reasons why.

There are also a couple of concerns, identified in the memo, that the family (owner) has expressed to me, and I wanted to make sure that I captured that in my notes to you.

We can also follow-up once you have had a chance to digest this, and would be most happy to do so.

Again, thank you for the opportunity to provide this feedback and request for amendments.

Deane

---

Deane Strongitharm, MCIP  
CitySpaces Consulting Ltd. | Strongitharm Consulting Ltd.

Tel/Cel 250.889.1862  
Fax 250.383.7273  
Web [www.cityspaces.ca](http://www.cityspaces.ca)

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STRONGITHARM CONSULTING LTD.

February 16, 2018

To: Kristina Bouris & Marc Cittone, City of Victoria Planning (via email)

RE: VIC WEST PLAN & RECOMMENDATIONS FOR 945 ALSTON STREET

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## REQUEST

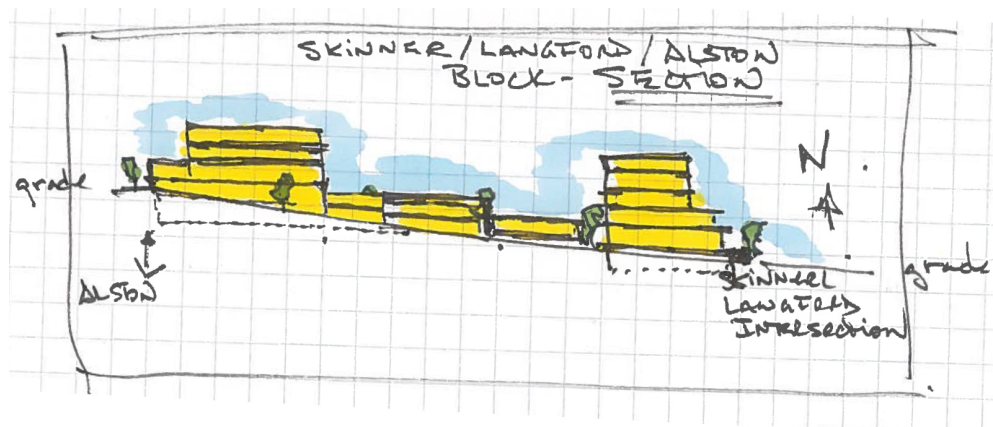
Following our earlier site visit, and conversations about the status of the lands at 945 Alston Street (bounded by Skinner, Langford and Alston Streets), on behalf of the owner, we respectfully request that the subject lands, in their entirety, be designated ***"General Employment with Limited Residential"*** on the Draft Land Use Plan.



## BACKGROUND/CONTEXT: ABOUT THE PROPERTY

1. The consolidated parcel consists of approximately 4,800 m<sup>2</sup> (50,000 sf). It is a prominent site that provides opportunities for a future comprehensive development.
2. The property faces onto a main collector road (Skinner Street), and has a significant 160 m road frontage along Skinner. The site mirrors the parcel(s) on the other (north) side of Skinner Street.
3. The property has an approximately seven (7) metre grade change, from the lowest end fronting toward Bay Street to the highest point near the Langford/Alston Streets intersection.
4. The configuration of the property, and the grade changes do not favour industrial uses, which typically require flat sites and gentle grades for truck ingress and egress.
5. The existing buildings are principally of one and two-storey concrete block construction, with wood frame interior walls. While well maintained, they have a limited future.
6. The curved triangular configuration of the property lends itself to an innovative design approach for property redevelopment. The site is conducive to higher densities given its context, and its proximity to the designated "Large Urban Village", Downtown, and transportation networks.

*Schematic Section of a Possible Option*



## CURRENT DRAFT PLAN VIC WEST LAND USE DESIGNATION

1. The Draft Vic West Plan currently designates the subject property as *"Industrial"*, and as *"Industrial Employment with Limited Residential"* assigned to the western part of the property. Originally it was designated as *"General Employment"*. We understand that the current designation was assigned because of 1) its historic use, and 2) general comments from Vic West community members' regarding the desire to retain industrial lands. This was considered without communication with the land owners, and likely limited consideration of actual site conditions.
2. The current Draft Vic West Plan designates the block on the north side of Skinner Street as entirely *"General Employment with Limited Residential"*... the land with the most similar characteristics to 945 Alston Street.
3. Amendment 18 of the Draft Vic West Plan designates both sides of Skinner Street the same DPA (7A), underscoring the relationship between the two blocks. The land south of Langford Street is designated DPA 5: "Large Urban Village".
4. The subject site is sandwiched between lands designated as "Large Urban Village" and "General Employment/Residential".
5. Under the *"Big Moves"* statement (figure 1) of the Draft Vic West Plan, Skinner Street is shown as a route for *"[creating] more places to live near transit and amenities"*, implying the opportunity to have residential throughout the block.

## A VISION FOR THE SITE

The following represents broad objectives related to the site:

1. The site should be considered for redevelopment in a comprehensive way.
2. The site should relate to the properties to its north and south.
3. The site can command a mixed-use development combining, commercial, technology, service commercial, live-work, artisan, and residential uses.
4. Flexibility in design and use should be encouraged for the site.

5. Given the prominence of the property, its size, and grade changes, there are multiple design solutions, including either one integrated building, or more than one building.
6. A design vision could consider the southernmost point at the east end of the property as a location for a prominent structure that contains mixed uses, with residential on the upper floors, to take advantage of the grades and viewpoints.
7. Along Skinner Street, a building or buildings could include retail on the lower floor(s), and commercial/service commercial, live-work, or residential above.
8. The design of the northernmost corner at the intersection of Skinner and Alston Streets should take advantage of the prominent bend in Skinner Street approaching from the east.
9. Residential and live-work along Alston Street could also incorporate commercial/service commercial on the interior side of the block as the site drops away from the grade of Alston Street.

## LAND USE DESIGNATION OPTIONS

Section 5 of the draft plan ("Future Land Use") defines land classifications and uses. There are four potential options within these designations:

1. *General Employment*: Commercial uses and/or light industrial uses allowing for commercial, including retail and light industrial uses, on the ground level with retail generally at ground level.
2. *General Employment with Limited Residential*: Supports General Employment with limited residential, including retail or commercial on the ground level, and commercial or residential above the first floor.
3. *Industrial Employment*: Permits light industrial and ancillary retail on the ground floor, with commercial limited above the first floor.
4. *Industrial Employment with Limited Residential*: Permits industrial and ancillary retail on the ground floor, with commercial or residential on upper floors.



## DRAFT POLICIES 8.2.2 & 8.2.3 & ACCOMPANYING FIGURE 36

The draft plan provides specific statements and a schematic drawing for the site in the absence of consultation with the owners.

The schematic design shown (Figure 36) does not fit on the site, given the site's configuration. The drawing is also not to scale. As the final design solution for redevelopment will require detailed site analysis, the drawing shown may misrepresent what the best solution could be.

Wording in Sections 8.2.2 and 8.2.3 does not reflect the scope of potential opportunities for the site.

We would request that:

- a) Figure 36 be removed from the plan; and
- b) The wording in Sections 8.2.2 and 8.2.3 be amended to reflect a broader, and more flexible range of uses and design, allowing for residential and general employment uses throughout.

## CONCLUSION

Based on all factors identified above, the most appropriate land use designation for 945 Alston Street, in its entirety, is "***General Employment with Limited Residential***".

cc: Blackburn Rentals

## Rob Gordon

---

**Subject:** FW: Families First

---

**From:** Marc Cittone

**Sent:** Monday, January 22, 2018 8:33 AM

**To:** **Personal info** Kristina Bouris <[KBouris@victoria.ca](mailto:KBouris@victoria.ca)>; Rebecca Penz <[rpenz@victoria.ca](mailto:rpenz@victoria.ca)>

**Cc:** Justine Semmens <[president@victoriawest.ca](mailto:president@victoriawest.ca)>; [landuse@victoriawest.ca](mailto:landuse@victoriawest.ca); Malcolm Maclean <[mmaclean@victoria.ca](mailto:mmaclean@victoria.ca)>

**Subject:** RE: Families First

Hi **Personal info**

Thanks for your comments and some of the background. We certainly looked at these three houses in researching lot sizes, since there was a lot of interest in smaller homes/small lot homes. We'll take your comments into account as we review all the input on this policy move. From the lot size and expected design criteria, we can consider the expected size of houses we'd see although I expect a number of factors (including economics) will impact who may be attracted to these houses in the future.

**Personal info**

**From:** **Personal info**

**Sent:** Sunday, January 21, 2018 2:33 PM

**To:** Kristina Bouris <[KBouris@victoria.ca](mailto:KBouris@victoria.ca)>; Marc Cittone <[mcittone@victoria.ca](mailto:mcittone@victoria.ca)>; Rebecca Penz <[rpenz@victoria.ca](mailto:rpenz@victoria.ca)>

**Cc:** Justine Semmens <[president@victoriawest.ca](mailto:president@victoriawest.ca)>; [landuse@victoriawest.ca](mailto:landuse@victoriawest.ca)

**Subject:** Families First

Hello All,

(Kristina & Rebecca, Sorry about earlier email. My thoughts are clearer in this one).

Currently the three small infill lots on Reno are the same sizes of new proposed small lot 220m2. Each has couples w/out children living in them. A few families have tried **Personal info**, the largest of the three. According to neighbours, the previous owners with children all moved out, because they believed the house too small to raise children. In other words these houses are conducive for couples w/out children.

After the city built these three homes, they conducted 8 weeks of Open House with survey. Even the survey respondents thought these would be good for childless families.

If the Vic West plan allows new builds on small lots of 220m2 with single detached home and with Secondary Suites. I believe the builder will not be thinking family. Instead about creating two, one bedroom apartments. This does not help with density if both are rented by single people.

What it does do is push families further outside the city. I think this has a negative affect on what the plan calls Traditional Residential Areas.

I have included Justine & Land Use Chair. Maybe I'm way off base.

I know people asked for smaller lots with suites, but I do not think they truly understand the sizes or the consequences for families.

I also have nothing to gain or lose from this. In other words NOT NIMDYism.

Thank you,

Personal info

## Rob Gordon

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**Subject:** FW: Reminder: Vic West Neighbourhood Plan Open House: Jan 20

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**From:** Personal info

**Sent:** January 18, 2018 5:28 PM

**To:** Engagement <[engage@victoria.ca](mailto:engage@victoria.ca)>

**Subject:** Re: Reminder: Vic West Neighbourhood Plan Open House: Jan 20

Thanks for letting me know but I'm VERY out of town at the moment-- I'm actually out of the northern hemisphere altogether-- so can't make the meeting. I tried slogging my way thru the revised plan doc but found it overwhelming tedious. Perhaps you've gotten the following comment already and it's reflected in the doc; the one thing I thought of since my last interactions with this project is that it would be nice to have professional like doctors and dentists within walking distance as well.

Thanks,

Personal info

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**From:** Engagement <[engage@victoria.ca](mailto:engage@victoria.ca)>

**Sent:** January 18, 2018 6:51:43 PM

**To:** Engagement

**Subject:** Reminder: Vic West Neighbourhood Plan Open House: Jan 20



We look forward to seeing you at the open house about the Vic West Neighbourhood Plan and amendments to the Official Community Plan.

Saturday, January 20, 2018

1 p.m. – 3:30 p.m.

Vic West Elementary School gymnasium, 750 Front Street

➤ 15 minute presentations at 1:15 and 2:30

We have made more than 100 changes to the draft neighbourhood plan based on your feedback. The revisions are highlighted in red text in the proposed neighbourhood plan. We are also proposing a number of amendments to the Official Community Plan bylaw to reflect the directions in the proposed plan.

Read the plan and fill out the survey at:

[victoria.ca/vicwest](http://victoria.ca/vicwest)

*Unsubscribe: Reply to this email if you would like to unsubscribe from email updates for the Vic West Neighbourhood plan.*

**Rob Gordon**

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**Subject:** FW: Revised Vic West Plan and Survey now live

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**From:** Personal info  
**Sent:** January 12, 2018 6:43 AM  
**To:** Engagement <[engage@victoria.ca](mailto:engage@victoria.ca)>  
**Cc:** Personal info  
**Subject:** Re: Revised Vic West Plan and Survey now live

Dear Vic West Community Association,

Thanks for this e-mail. Even though I haven't been involved in the process I am very excited about the developments and the progress of the plan, thus far. I am grateful to my neighbours for the work they have put into it. It seems we 'fit' in this neighbourhood nicely as we seem to share many values with our neighbours.

Please add to your e-mail distribution mine and combined e-mail account for mailings like this as we are both interested: Personal info

Thank-you,  
Personal info

On Thursday, January 11, 2018, 4:43:14 PM PST, Engagement <[engage@victoria.ca](mailto:engage@victoria.ca)> wrote:



Dear Vic West Neighbours,

The revised neighbourhood plan, proposed amendments to the Official Community Plan, and survey are now live [here](#)!

We have made more than 100 changes to the draft neighbourhood plan based on your feedback. The revisions are highlighted in red text in the proposed neighbourhood plan. We are also proposing a number of amendments to the Official Community Plan bylaw to reflect the directions in the proposed plan.



We encourage you to:

1. read the revisions to the plan
2. attend the open house to talk to staff about the revisions and proposed amendments to the Official Community Plan to align with the neighbourhood plan
3. fill out the online survey (closes Feb 4)

### **Open House**

Saturday, January 20, 2018

1 p.m. – 3:30 p.m.

Vic West Elementary School gymnasium

➤ 15 minute presentations at 1:15 and 2:30

**Please let your neighbours know!**

[victoria.ca/vicwest](http://victoria.ca/vicwest)

*Unsubscribe: Reply to this email if you would like to unsubscribe from email updates for the Vic West Neighbourhood plan.*

## Rob Gordon

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**Subject:** FW: Revised Vic West Plan and Survey now live

---

**From:** Personal info

**Sent:** January 15, 2018 11:00 AM

**To:** Engagement <[engage@victoria.ca](mailto:engage@victoria.ca)>

**Subject:** Re: Revised Vic West Plan and Survey now live

Hello - is there a red-line copy of the new draft Plan with deletions as strike-through text? To do a true comparison of the changes I would like to see what was removed, not just what was added (red text). Or is the first draft available?

thank you,  
Personal info

---

**From:** "Engagement" <[engage@victoria.ca](mailto:engage@victoria.ca)>

**To:** "Engagement" <[engage@victoria.ca](mailto:engage@victoria.ca)>

**Sent:** Thursday, January 11, 2018 4:43:06 PM

**Subject:** Revised Vic West Plan and Survey now live



Dear Vic West Neighbours,

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Vic West Elementary School gymnasium

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**Please let your neighbours know!**

[victoria.ca/vicwest](http://victoria.ca/vicwest)

*Unsubscribe: Reply to this email if you would like to unsubscribe from email updates for the Vic West Neighbourhood plan.*

**Rob Gordon**

---

**Subject:** FW: Revised Vic West Plan and Survey now live

**From:** Personal info

**Sent:** January 23, 2018 11:48 PM

**To:** Engagement <[engage@victoria.ca](mailto:engage@victoria.ca)>

**Subject:** Re: Revised Vic West Plan and Survey now live

Hi there,

Small detail, but in "important moments in Vic-West history" the original Johnson St. bridge isn't mentioned. There seems to be a gap between 1920-1930...

Arguably, that was probably one of the most important dates in Vic-West history wouldn't you say?

Cheers,

Personal info

On Thu, Jan 11, 2018 at 4:43 PM, Engagement <[engage@victoria.ca](mailto:engage@victoria.ca)> wrote:



Dear Vic West Neighbours,

The revised neighbourhood plan, proposed amendments to the Official Community Plan, and survey are now live [here](#)!

We have made more than 100 changes to the draft neighbourhood plan based on your feedback. The revisions are highlighted in red text in the proposed neighbourhood plan. We are also proposing a number of amendments to the Official Community Plan bylaw to reflect the directions in the proposed plan.

We encourage you to:

1. read the revisions to the plan
2. attend the open house to talk to staff about the revisions and proposed amendments to the Official Community Plan to align with the neighbourhood plan
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## **Open House**

Saturday, January 20, 2018

1 p.m. – 3:30 p.m.

Vic West Elementary School gymnasium

➤ 15 minute presentations at 1:15 and 2:30

**Please let your neighbours know!**

[victoria.ca/vicwest](http://victoria.ca/vicwest)

*Unsubscribe: Reply to this email if you would like to unsubscribe from email updates for the Vic West Neighbourhood plan.*



## Rob Gordon

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**Subject:** FW: Revising the Vic West Plan - Da Vinci Centre Site

**From:** Keith Barbon [<mailto:kubdevelopments@gmail.com>]

**Sent:** Sunday, January 21, 2018 8:03 AM

**To:** Kristina Bouris <[KBouris@victoria.ca](mailto:KBouris@victoria.ca)>

**Subject:** Revising the Vic West Plan - Da Vinci Centre Site

Hi Kristina,

Thanks for your time yesterday. No one can say that City staff did not provide significant opportunities for feedback and comments on the Vic West plan.

With respect to the Da Vinci property I would like to again suggest the possibility for flexibility on heights over 6 stories. The plan recognizes these opportunities at the Westside Village which is right across the street.

The plan should allow for policy and required amenity contributions for a taller building to prevent massing along Bay Street and create a more attractive interface with the park.

The plan is meant to pave the way for the future. Density and creative uses of land and amenities contributions are always required and that will especially be the case in the future.

Any development proposal submitted on this property will require public consultation and the City's approval.

Please consider leaving the door open to some creative ideas that could create a win win situation and result in amenities and/or non-market housing initiatives on this site.

Thank you again,

Keith,

Da Vinci Centre  
195 Bay Street

**Keith Barbon, CPA, C.A.**

KUB Developments Ltd.

Mobile: 250.588.9425

----- Forwarded message -----

From: **Engagement** <[engage@victoria.ca](mailto:engage@victoria.ca)>

Date: Mon, Dec 4, 2017 at 10:02 AM

Subject: Revising the Vic West Plan

To: Engagement <[engage@victoria.ca](mailto:engage@victoria.ca)>



Dear Vic West neighbours,

We are currently revising the Victoria West Neighbourhood Plan based on your feedback and preparing proposed amendments to the Official Community Plan to align with the neighbourhood plan. You will be invited to provide feedback on the revisions to the plan and proposed Official Community Plan amendments from January 10 – 30 at an open house (January 20) and through an online survey.

On December 7, we will be presenting the proposed amendments to the Official Community Plan to Council and asking for their direction to consult with First Nations and other agencies and municipalities who might be affected. This is a formal step required under the Local Government Act.

The report to Council and attachments are Item 13 on the December 7 Committee of the Whole agenda, which can be viewed [here](#).

We will be back in touch when the plan is ready for public review.

*Unsubscribe: Please reply to this email with "unsubscribe" if you no longer wish to receive updates on the Vic West Neighbourhood Plan*

[victoria.ca/vicwest](http://victoria.ca/vicwest)

**Rob Gordon**

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**Subject:** FW: Rowhousing

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**From:** K RC [[Personal info](#)]  
**Sent:** January 27, 2018 10:53 AM  
**To:** Marc Cittone  
**Subject:** Rowhousing

Hi Marc,

Nice to meet you at the Vic West Plan event recently. I'm just following up on your interest to read my MA thesis on Sustainable Housing (see screen shot of the catalogue item below). It is housed at SFU as a I did a joint masters via Western Deans Agreement via SFU and UBC's SCARP.

I reviewed an existing national dataset (13,000 records) from the Canada Household Survey reviewing 3 housing types (apartments, rowhousing and SF) to test a set of sustainable indicators that I'd developed. Rowhousing scored highest fyi.

I am still looking for a .pdf version to send to you so may take another week or two. It's amazing how time passes and things go missing LOL.

Karen Ramsay Cline, MA RPP

sustain

## Details

**Title** [Sustainable housing : indicat](#)

**Author** [Karen R Ramsay >](#)

**Subjects** [Housing -- Canada >](#)  
[Public housing -- Canada >](#)  
[Sustainable development -- C](#)  
[City planning -- Canada >](#)  
[Urban policy -- Environmental](#)  
[Row houses -- Canada >](#)  
[Sociology, Urban >](#)  
[Population density >](#)

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**Creation Date** 2002

**Format** ix, 267 leaves.

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## Rob Gordon

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**Subject:** FW: vic west community plan - cycling route

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**From:** Personal info & Ray [[mailto:Personal info](#)]  
**Sent:** Wednesday, Jan 24, 2018 4:19 PM  
**To:** Steve Hutchison  
**Subject:** vic west community plan - cycling route

Mr. Hutchison, I was given your email at the engagement meeting last Saturday at Vic West school to discuss the new Vic West Community plan.

My concern is around the designation of William Street as a pedestrian and cycling route.

I raised a concern at the earlier meetings about the width of William Street and the fact that one side is mainly Industrial zoning which results in a lot of truck traffic during business hours making this a dangerous route for bike traffic.

Bike traffic outbound on the E & N trail has to cross Esquimalt Road at the intersection of Springfield & William. Cyclists at that point have the option of continuing on the E & N trail, taking Springfield or taking William St in order to connect with trail near the end of William street that runs into Vic West School.

The person at the meeting indicated that although no bike lanes are contemplated for William Street that it would be signed as a bike route. Given Springfield is at least 3 meters wider and is mainly residential it would be a much safer route to sign as a bike route for those who choose not to stay on the E & N trail and plan to connect with the trail from the end of William street that connects through to Hudson street.

I did see some arrows on one map that indicated a couple of possible connector trails from the E & N trail to the school/trail. If those are to be built I would suggest all cyclists would be safer traveling on the E & N to connect at that point with the school and trails/routes that lead to Bamfield Park. I would say this option is the best and safest for all cyclists. Pedestrian traffic is not a concern as there are sidewalks on both sides of both Springfield and William.

I raise these concerns on behalf of the business owners of Strata Corp 110 on William St.

Ray Fischer – Secretary for Strata Corp 110



## Rob Gordon

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**Subject:** FW: vic west survey

-----Original Message-----

From: **Personal info**

Sent: January 21, 2018 8:57 PM

To: Engagement <engage@victoria.ca>

Subject: vic west survey

Please pass along my additional comments to the tabulators of the Vic West planning survey, as part way thru filling it out, my ipad would not let me write comments.

I feel strongly that provision for swimming access on th Gorge is very important to many of us who live here. It gets mentioned in the plan only in a general sense. To me it is a priority. We swimmers currently share the small dock at the foot of Styles Street with a growing number of dragon boaters and it doesn't feel safe. I envision the large bay in Banfield park rehabilitated as a sandy beach. What a great amenity this would be!

I was stunned to see that less than 10% of households in Vic West are single family dwellings, yet the plan is to continue to increase density, encourage more small lots, creat more duplexes and townhouses. Really? Hasn't Vic West borne more than its share of Victoria's increased density in the past 25 years? More than any community except downtown, I suspect.

I am concerned that proposed increases in density need to be accompanied by much more thorough planning for how we are going to accomodate the increase in the number of parked cars on our streets, or, alternatively how we are going to improve transit for all these extra people.

Thank you for the opportunity to contribute.

**Personal info**

## Rob Gordon

---

**Subject:** FW: Victoria West Neighbourhood Plan - need for improved crosswalks

**From:** Personal info

**Sent:** January 21, 2018 5:54 PM

**To:** Engagement <[engage@victoria.ca](mailto:engage@victoria.ca)>

**Subject:** Victoria West Neighbourhood Plan - need for improved crosswalks

Hi there,

I just skimmed the Vic West Neighbourhood Plan. I am generally very happy with what I'm seeing in the plan.

However, one glaring omission from the plan, in terms of transportation and mobility, is the need for an improvement to the existing pedestrian crossing at **Craigflower and Raynor Street**. While I don't have any empirical data, I can confidently say through experience and observation that the crossing is very dangerous especially in the evenings. I've seen several near-misses, which are largely a function of poor lighting, speeding vehicles, and inadequate visibility. I'm always in a fearful state when crossing (and seeing others cross) as I can't help but worry about being struck.

I did see that one of the public realm policies under "Urban Villages" is to "Consider improvements to the crossing at Raynor Avenue and Pine Street, including distinctive and textured pavement, and a potential signalized crossing at Raynor if warranted."

I do not believe "consider" is adequate enough. I believe the City *must* improve this crossing as a short-term action by adding flashers and re-painting the crosswalk stripping, which is continuing to fade. I imagine the City's Engineering and Public Works Department can determine which signal is most optimal for this crossing, but based on the high number of existing pedestrian volumes (and the nature of this secondary arterial street), I would recommend something as effective as a rectangular rapid flash beacon, which is being implemented across North American cities. They have been shown to be highly effective at improving pedestrian safety.

Please consider adding this as a short-term or medium term action to the transportation and mobility section. The need for an improvement to this crossing will be even more critical as Craigflower village continues to absorb commercial and residential density and be an even more integral gathering space in Vic West.

Thank you for your attention to this matter.

Personal info

**Attachment 4:**  
**Engagement Summary and Feedback on**  
**Proposed Vic West Neighbourhood Plan and OCP Amendments**

Part 5: Responses to Formal Referrals

## Malcolm Maclean

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**Subject:** FW: Meeting regarding Vic West Plan

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**From:** Kristina Bouris  
**Sent:** February 6, 2018 2:11 PM  
**To:** Margetts, Marla <[mmargetts@sd61.bc.ca](mailto:mmargetts@sd61.bc.ca)>  
**Cc:** Leigh Campbell <[LCampbell@victoria.ca](mailto:LCampbell@victoria.ca)>  
**Subject:** RE: Meeting regarding Vic West Plan

Dear Marla,

Thank you very much for the opportunity to meet with you on January 24 regarding the Vic West neighbourhood plan. It was good to meet you. I'm glad that Mark Welsh was able to join us by phone as well.

As discussed, there were a few follow up items to come out of our meeting:

- Suggestion to revise the east-west path alignment across the Vic West field, to show the route more conceptually
- Suggestion to revise childcare policies to include the opportunity for more childcare (out of school care) at Vic West School
- We also discussed the upcoming renovation to the lower field and whether this may provide an opportunity to consider an east-west pathway in the future. Although there are no plans in place for funding or timing, it would be good for us all to meet and be aware of possible options down the road. Leigh Campbell, Manager of Parks Planning and Design, and I would be happy to meet with you to find out more about your project and have a look at the field. It might make sense to invite us once you have an almost final design for the playground. Please let me know if this sounds like a good approach. If so, I'll wait to hear from you as you get closer to finalizing the design.

I'll include your feedback on the plan (top two bullets above) in a summary for City Council. A final version of the plan should be presented to Council later this spring.

Thanks very much for your support of this project.  
Kristina

-----Original Message-----

From: Margetts, Marla [<mailto:mmargetts@sd61.bc.ca>]  
Sent: Thursday, January 18, 2018 1:31 PM  
To: Kristina Bouris <[KBouris@victoria.ca](mailto:KBouris@victoria.ca)>  
Subject: RE: Meeting regarding Vic West Plan

Thank you Kristina. See you at 3pm on Wednesday January 24th. I too look forward to meeting with you in my office.  
Regards,  
Marla

Marla Margetts  
Principal  
Victoria West Elementary  
750 Front Street  
Victoria BC V9A 3Y4  
250-382-9131

250-382-3741 fax

-----Original Message-----

From: Kristina Bouris [mailto:KBouris@victoria.ca]

Sent: January-18-18 1:26 PM

To: Margetts, Marla

Subject: Meeting regarding Vic West Plan

Hello Marla,

Thanks for reaching out regarding the Vic West Neighbourhood Plan. I wanted to confirm that I'll meet with you next Wed, January 24 in your office. Thanks, and looking forward to it.

Kristina

Kristina Bouris MCIP RPP

Senior Planner

Sustainable Planning and Community Development City of Victoria

1 Centennial Square, Victoria BC V8W 1P6

T 250.361.0532 F 250.361.0557 E Kbouris@victoria.ca

-----Original Message-----

From: Margetts, Marla [mailto:mmargetts@sd61.bc.ca]

Sent: January 18, 2018 12:51 PM

To: Malcolm Maclean <mmaclean@victoria.ca>

Subject: Re: General Inquiry

3pm if possible.

Marla

Sent from my iPhone

> On Jan 18, 2018, at 12:36 PM, Malcolm Maclean <mmaclean@victoria.ca> wrote:

>

> Hi Marla,

>

> Thanks for the clarification over the phone. It sounds like it will be most useful for you and Kristina to meet, and Wednesday of next week looks like the best overlapping availability for the two of you.

>

> What time of day would be best for you?

>

> Thanks,

> Malcolm

>

> -----Original Message-----

> From: Margetts, Marla [mailto:mmargetts@sd61.bc.ca]

> Sent: January 18, 2018 11:16 AM

> To: Malcolm Maclean <mmaclean@victoria.ca>



> Subject: RE: General Inquiry  
>  
> Hi  
> Thank you for connecting with me.  
> Are you available on Monday or Wednesday (January 22 or 24) at 3pm for a meeting at Vic West Elementary? We would meet in my office.  
> Thank you  
> Marla  
>  
> Marla Margetts  
> Principal  
> Victoria West Elementary  
> 750 Front Street  
> Victoria BC V9A 3Y4  
> 250-382-9131  
> 250-382-3741 fax  
>  
>  
>  
> -----Original Message-----  
> From: Malcolm Maclean [mailto:mmaclean@victoria.ca]  
> Sent: January-17-18 9:11 AM  
> To: Margetts, Marla  
> Subject: RE: General Inquiry  
>  
> Hi Marla,  
>  
> Thanks for getting in touch. I am one of the planners working on the Vic West Neighbourhood Plan. I am happy to answer any questions you have, and/or to set up a time to meet if that would be preferable.  
>  
> Looking forward to hearing,  
>  
> Malcolm MacLean  
> Community Planner  
> Sustainable Planning and Community Development City of Victoria  
> 1 Centennial Square, Victoria BC V8W 1P6  
>  
> T 250.361.0538  
>  
> -----Original Message-----  
> From: webforms@victoria.ca [mailto:webforms@victoria.ca]  
> Sent: January 12, 2018 1:18 PM  
> To: Public Service Centre - Internet email  
> Subject: General Inquiry  
>  
> From: Marla Margetts  
> Email : mmargetts@sd61.bc.ca  
> Reference :  
> <http://www.victoria.ca/EN/main/residents/neighbourhoods/vic-west/victoria-west-neighbourhood-plan.html>  
> Daytime Phone : 250-392-9131

> I am hoping that someone can connect with me regarding the Vic West Neighbourhood Plan. I am the principal of Vic West Elementary School. I look forward to connecting with a member of the Neighbourhood Plan committee.

> Thank you very much.

> Marla Margetts

>

>

>

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>

> IP Address: 64.114.223.13

>

>

## Malcolm Maclean

---

**Subject:** FW: Proposed OCP Amendments; Victoria West Neighbourhood Plan  
**Attachments:** DOC022818.pdf

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**From:** Rachel Dumas [<mailto:rachel.dumas@esquimalt.ca>]  
**Sent:** Wednesday, February 28, 2018 10:37 AM  
**To:** Community Planning email inquiries <[CommunityPlanning@victoria.ca](mailto:CommunityPlanning@victoria.ca)>  
**Subject:** Proposed OCP Amendments; Victoria West Neighbourhood Plan

Good Morning,

On behalf of the Township of Esquimalt, please find attached for convenience, correspondence pertaining to consideration of City of Victoria's proposed OCP Amendments. The original letter has been mailed. Should you require further information, do not hesitate to contact me on 250-414-7157.

Sincerely,

**Rachel Dumas**  
Administrative Assistant/Deputy Corporate Officer  
Township of Esquimalt | Corporate Services  
Tel: 1-250-414-7157 | [www.esquimalt.ca](http://www.esquimalt.ca)



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## CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1  
Website: [www.esquimalt.ca](http://www.esquimalt.ca) Email: [info@esquimalt.ca](mailto:info@esquimalt.ca)

Voice: (250) 414-7100  
Fax: (250) 414-7111

Via email to: [communityplanning@victoria.ca](mailto:communityplanning@victoria.ca)

February 28, 2018

Kristina Bouris, Senior Planner, Sustainable  
Planning and Community Development  
1 Centennial Square  
Victoria, British Columbia  
V8W 1P6

Dear Ms. Bouris:

**Re: City of Victoria proposed OCP Amendments and Victoria West  
Neighbourhood Plan**

Please be advised that at its Regular Meeting of Council held February 5, 2018, the Corporation of the Township of Esquimalt Council approved the following resolution:

*"That Council instruct staff to notify the City of Victoria that the Township of Esquimalt supports the proposed amendments to the City's Official Community Plan and, subject to a few minor comments as set out in Staff Report DEV-18-008, the proposed Vic West Neighbourhood Plan."*

The Township supports the proposed amendments to the City of Victoria's Official Community Plan, subject to consideration of the following:

- The Township of Esquimalt Council thanks the City of Victoria for referring both the proposed Official Community Plan amendments and the proposed Vic West Neighbourhood Plan to the Township for review;
- The Township supports increased density along Esquimalt Road as proposed in the Vic West Neighbourhood Plan. This is consistent with Esquimalt's Official Community Plan.
- The Township supports in principle, subject to its economic feasibility, the return of rail service to the E & N rail corridor as per the Vic West Neighbourhood Plan.
- The Township strongly supports the maintenance of all employment lands, particularly those of an industrial nature. In particular, the Township is promoting ship repair and fitting - an activity that is enhanced by similar activities in Vic West.
- The Township is concerned about the vacant site at 760 Esquimalt Road (former gas station). This site is located at the east entrance to Esquimalt along Esquimalt

Road. The Township aspires to eventually to have this entrance featured as a gateway to the community. It also serves as a gateway to Vic West from the west. Council requests that the City of Victoria consider identifying this location as a gateway in the proposed Vic West Neighbourhood Plan and work with the Township of Esquimalt to coordinate urban design guidelines that celebrate the reciprocal gateway nature of this area.

- Council notes that while the Vic West Neighbourhood Plan considers potential impacts of sea level rise, it does not seem to consider the potential impacts of Tsunamis. The Township is increasingly considering the potential impact of Tsunamis in its planning and development decision making processes.
- The Township hopes that the current bonds between it and Vic West will be strengthened as a result of the various initiatives outlined in the proposed Vic West Neighbourhood Plan.

Should you require further assistance, please contact me at 250-414-7157 or [rachel.dumas@esquimalt.ca](mailto:rachel.dumas@esquimalt.ca).

Yours truly,

A handwritten signature in black ink, appearing to read 'R Dumas', with a stylized flourish at the end.

Rachel Dumas  
Deputy Corporate Officer

## Matrix of Revisions to Proposed Victoria West Neighbourhood Plan and Official Community Plan Amendments

The following provide a summary of public input received, proposed changes to the proposed Victoria West Neighbourhood Plan and proposed OCP Amendments, and considerations and rationale, organized by topical area. Note Policy numbers refer to policies in the plan proposed for adoption dated April 2018.

What we heard (numbered items correspond to proposed OCP amendments as presented)	Proposed Change to Plan Policy	Rationale	Impact on Proposed OCP Amendments
<b>Traditional Residential Housing Areas</b>			
<b>OCP Amendment #1: Commercial use where an existing house is retained, north of Craigflower Village:</b> High level of support for proposal (combined survey and open house support of 73%). Specific concerns regarding: clarity if this applies only to existing houses; if there are design guidelines.	<b>No change to plan recommended</b>	<ul style="list-style-type: none"> <li>Policy 6.10 specifies that this applies to the reuse of existing houses</li> <li>City-wide Development Permit Area 16 design guidelines would apply to buildings with a commercial use</li> </ul>	<b>No change to proposal</b>
<b>OCP Amendment #2: Consider buildings up to 3 storeys, and limited commercial services or community-serving uses, at Pioneer Cooperative:</b> Moderate support for this change (combined survey and open house support of 56%). Specific concerns regarding: extent and impact (traffic) of community-serving commercial uses, desire for 3 storey development to add affordable housing options; impacts to directly adjacent properties and yards, overall compatibility; suggestion that 2 ½ storeys is more appropriate.	<b>Minor changes recommended:</b> <ul style="list-style-type: none"> <li>Revise Policy 6.4.6., 6.10.2. to indicate a limited area of floor spaces for community-serving uses, which do not result in excessive traffic impacts on local streets.</li> <li>Add to Policy 6.4.5.: "New development should be designed, considering siting and massing, to minimize impacts on access to sunlight and privacy for adjacent properties."</li> </ul>	<ul style="list-style-type: none"> <li>Proposal for modest additional height is consistent with OCP 13.24: Support the regeneration or redevelopment of older ground-oriented rental and cooperative housing developments by considering higher density redevelopment proposals on these sites if the same number, size, and tenure of units is maintained on-site, and the general rent level identified.</li> <li>Clarifies original intent for community-serving uses, concern for traffic impacts</li> <li>Design guidelines are more appropriate tool to address transitions (than limiting overall height of a large site). <i>Applicable Guidelines for Multi-Unit Residential, Commercial and Industrial</i> address transitions to lower-density development.</li> <li>Proposed density is consistent with OCP and relatively low, likely to result in mixed building heights with open space</li> <li>Coop is well located near E&amp;N Trail, services and amenities</li> </ul>	<b>No change to proposal</b>



What we heard (numbered items correspond to proposed OCP amendments as presented)	Proposed Change to Plan Policy	Rationale	Impact on Proposed OCP Amendments
<p><b>OCP Amendment #3: Consider 3 storey development up to 1.5 fsr adjacent to Catherine Street at Edward Street Village:</b>            High support (combined survey and open house support of 72%) for this proposal. Specific concerns include transitions to and compatibility with adjacent development; desire for more commercial space; desire to support urban food production; suggestion that development be limited to 2 ½ storeys.</p>	<p><b>No change to plan recommended</b></p>	<ul style="list-style-type: none"> <li>Proposed <i>Guidelines for Corridors, Villages and Town Centres</i> address specific concerns such as setbacks, landscape, privacy, massing, and compatibility of design.</li> <li>Establishes consistent form and character with proposed village properties for a unified feel to the block.</li> </ul>	<p><b>Change to proposed DPA Guidelines:</b>            Add an additional guideline regarding compatibility with adjacent lower-scale development to the Proposed <i>Guidelines for Corridors, Villages and Town Centres</i></p>
<p><b>OCP Amendment #4. Consider 3 Storey development, up to 1.2 fsr, south of Esquimalt Road:</b>            Support (combined survey and open house support of 64%) for this concept. Specific concerns include: height (too much/appropriate/too little), compatibility with existing streetscape and single-detached or house conversions; impacts on adjacent development; inability to support public benefit contributions at the proposed height and density; desire for more housing located near transit, the downtown, services and amenities; if proposed densities are not viable, will lead to retaining industrial uses at the foot of Mary Street; suggestion to support 2 ½ storeys here.</p>	<p><b>Minor change recommended:</b></p> <ul style="list-style-type: none"> <li>Clarify design guidance by the addition of Policy 6.9.12.:  <u>“To encourage building design which responds to existing context, with attention to streets or areas with a strong pre-existing character.”</u></li> </ul>	<ul style="list-style-type: none"> <li>Proposed height and density support housing diversity by supporting townhouse and courtyard housing forms with underground parking.</li> <li>Proposal responds to immediate existing context which ranges from single-detached and character conversions to 3-storey townhouses and multi-unit development.</li> <li>Appropriate location for housing near Frequent Transit, future shops and services, and within walking distance of downtown.</li> <li>Existing Development Permit Guidelines (DPA 16) address compatibility and contextual design.</li> </ul>	<p><b>No change to proposal</b></p>

What we heard (numbered items correspond to proposed OCP amendments as presented)	Proposed Change to Plan Policy	Rationale	Impact on Proposed OCP Amendments
<p><b>OCP Amendment #5. Consider 2 ½ storey development in Traditional Residential areas to support infill housing types:</b></p> <p>Moderate support for this proposal (combined open house and survey support of 57%). Concerns expressed regarding added traffic, population, while others cited the ability to meet green building goals.</p>	<p><b>Minor change recommended:</b></p> <p>Clarify in Policy 6.6.1. that 2 ½ storeys is intended to support certain infill housing types.</p> <p>Clarify in Policy 6.6.1. that design guidance addresses sensitivity to adjacent development.</p>	<ul style="list-style-type: none"> <li>• The proposal does not change total density supported.</li> <li>• Supporting an additional half-storey in infill housing types, with height limited by zoning, is meant to provide an equivalent option to building two storeys above a projecting (above-ground) basement, which is already supported.</li> <li>• Meant to enable a range of infill types and preserve green spaces.</li> <li>• All infill housing must still meet Development Permit guidelines regarding massing, transitions, privacy and shading.</li> <li>• Will not result in changes to existing zoning or single-detached properties.</li> </ul>	<p><b>No change to proposal</b></p>
<p><b>Urban Agriculture</b></p> <p>Concerns regarding space for urban agriculture (concerning high-density development in Traditional Residential areas).</p>	<p><b>Revisions recommended:</b></p> <p>Two proposed changes from Traditional Residential to Urban Residential have been removed (south Mary St., Russell at Skinner Streets). See Urban Residential section of this matrix.</p>	<ul style="list-style-type: none"> <li>• Existing infill zoning (e.g. existing duplex zoning) contains an open site space and rear setback requirement, and the City would build on this practice in future zoning.</li> <li>• Proposed <i>Design Guidelines for Intensive Residential - Townhouses and Attached Dwellings</i>, and existing DPA 16 (which would apply to other development types with 3 or more units) require open site space</li> <li>• Testing of infill proposals (e.g. density, setbacks) show these proposals support open site space and tree planting spaces.</li> </ul>	<p><b>Changes recommended:</b></p> <ul style="list-style-type: none"> <li>• Remove proposed urban residential areas on south Mary Street, Russell at Skinner Streets.</li> <li>• Add more specific <i>Design Guidelines for Intensive Residential - Townhouses and Attached Dwellings</i></li> </ul>

What we heard (numbered items correspond to proposed OCP amendments as presented)	Proposed Change to Plan Policy	Rationale	Impact on Proposed OCP Amendments
<p><b>Skinner Street:</b></p> <ul style="list-style-type: none"> <li>• Different opinions about proposed scale (townhouses and houseplexes up to 2.5 storeys + basement, and up to 1 fsr, with specific design guidelines for compatibility) of Traditional Residential development on Skinner Street:</li> <li>• Some feel draft plan proposal for 3 storey apartment buildings, or even more height, was more appropriate to provide housing near transit, downtown, and large urban village.</li> <li>• Others feel the proposal still provides too much potential to develop that would be out of character with the surrounding neighbourhood.</li> </ul>	<p><b>No change to plan recommended</b></p>	<ul style="list-style-type: none"> <li>• Proposal represents a compromise which addresses concerns for height, rhythm of street frontage, massing (through half-storey) and character</li> <li>• Houseplexes are limited to 4 units unless the proposal can demonstrate that the site achieves both site landscape and parking objectives</li> <li>• </li> </ul>	<p>N/A</p>
<p><b>Skinner Street Residential Corridor – proposed Development Permit Area</b></p> <p>Specific Development Permit Guidelines were proposed for Skinner Street, in addition to the <i>Revitalization Guidelines for Corridors, Villages and Town Centres</i>. However, with the removal of potential mixed-use development at Skinner Street and Russell Street, this area is no longer eligible for designation as DPA for purposes of revitalization.</p>	<p><b>No change to plan recommended</b></p>	<p>The following guidelines would apply to the area which had been identified as the Skinner Street Residential Corridor:</p> <ul style="list-style-type: none"> <li>• For development of 3 or more units which meet the definition of Attached Residential in the proposed DPA 15F, the proposed <i>Design Guidelines for Attached Residential Development</i> would apply.</li> <li>• For other development of 3 or more residential units, the existing Development Permit Area 16 would apply, and development would be reviewed against the existing Design Guidelines for: Multi-Units Residential, Commercial, Industrial</li> <li>• Staff finds these two guidelines to sufficiently address issues of compatibility and contextual design.</li> </ul>	<p>Changes Recommended:</p> <ul style="list-style-type: none"> <li>• Include Skinner Street Corridor in proposed DPA 15F: Intensive Residential – Attached Residential Development, rather than the previously identified DPA 7A: Corridors – Skinner Street Residential Corridor.</li> </ul>

What we heard (numbered items correspond to proposed OCP amendments as presented)	Proposed Change to Plan Policy	Rationale	Impact on Proposed OCP Amendments
<p><b>Small lot houses</b></p> <ul style="list-style-type: none"> <li>Concern that small lot houses on 220m<sup>2</sup> lots, if they include suites, would function more like 2 smaller (1 BR) rental apartments rather than a house attractive to a family with children.</li> <li>Concern that few sites would benefit from a reduction in the small lot size, unless demolition (which is not supported by existing small lot policy) is allowed.</li> </ul>	<p><b>Revision to plan recommended:</b></p> <p>Revise policy 6.15.2.a) to remove specific size minimum: Small lot houses may be considered on lots <u>smaller than 260m<sup>2</sup> (3,000 sq. ft.) where siting, location and design can meet the intent of the Small Lot Rezoning policy and associated Development Permit guidelines.</u></p>	<p>Analysis shows that:</p> <ul style="list-style-type: none"> <li>A small lot house with a suite can achieve sufficient size to contain a larger (1,000 sq. ft. +) main unit in a 1.5 storey house with a basement</li> <li>However, there are few lots which could subdivide a 220m<sup>2</sup> lot without demolition or relocation of the main house.</li> <li>Thus, while there are examples of 220m<sup>2</sup> small lots built under the RS-1 zone, staff recommend removing specificity and allow for consideration of smaller lots where all goals of the small lot policy can be met.</li> </ul>	N/A
<p><b>Townhouses – omitted policy</b></p> <ul style="list-style-type: none"> <li>Plan shows a scenario for townhouses in one row on a corner lot; however, however, the policy regarding desired site characteristics (lot size) was accidentally deleted in revision between the Draft (Aug. 2017) and the Proposed Plan</li> </ul>	<p><b>Minor change recommended:</b></p> <p>Add policy 6.12.2.a) as included in the Draft Plan (Aug. 2017): <u>On corner lots, or lots with two frontages, on lots of at least 18m (60 ft) in width.</u></p>	The added policy is consistent with Fig. 18, and consultation on the plan.	N/A
<p><b>Heritage properties</b></p> <p>Concern regarding impacts of infill housing on heritage properties</p>	<p><b>Minor revisions to Section 6.8:</b></p> <ul style="list-style-type: none"> <li>Minor corrections for clarification that retention and designation of properties of heritage merit (whether on the Register or eligible) is the preferred approach for redevelopment, and should be incentivized.</li> </ul>	<ul style="list-style-type: none"> <li>Action plan indicates updating House Conversion rules as a priority, to strengthen incentives.</li> <li>Individual applications for redevelopment (other than house conversion) to be considered through rezoning</li> </ul>	N/A

What we heard (numbered items correspond to proposed OCP amendments as presented)	Proposed Change to Plan Policy	Rationale	Impact on Proposed OCP Amendments
<b>Houseplexes - Parking</b> <ul style="list-style-type: none"> <li>Concern that parking variances should not be mentioned in neighbourhood as it implies that a variance would be favoured for this typology; each variance must be considered on its merits.</li> <li>Community has expressed concern for on-street parking, but also a desire for green space and added housing, and to avoid excessive pavement in back yards.</li> <li>Some community members wish to see reduced on-site parking and reliance on the automobile; others are concerned about impacts to on-street parking.</li> </ul>	<b>Revisions to plan recommended:</b> <ul style="list-style-type: none"> <li>Revise policy 6.15.3.c) regarding parking variance, replacing reference to considering a “justified” parking variance with the following language: <u>“A parking reduction may be considered where the location and transportation demand management approaches are expected to result in lower rates of parking use.”</u></li> </ul>	<ul style="list-style-type: none"> <li>An applicant must meet zoning requirements or may request a parking variance through the standard process for requesting such a variance, and must provide justification. No need to mention in plan.</li> <li>Urban design testing shows that a site of the dimensions suggested in the plan, with 4 parking spaces, can provide rear yard landscape and tree planting space; and that possibilities exist to provide more units in specific locations.</li> </ul>	N/A
<b>Small Urban Villages</b>			
<b>OCP Amendment #7. Designate Catherine at Edward Small Urban Village</b> General support for proposal (combined survey and open houses support of 78%). Concerns include desire to see commercial/mixed use on the whole block (not just two sites); that this is “heart” of the community; sensitive transitions to surrounding development; suggestion for a 2 ½ storey limit; concern about traffic; and suggestion for food production on rooftops.	<b>No change to plan recommended</b>	<ul style="list-style-type: none"> <li>Proposal is consistent with existing uses and provides for a 3-storey walkable streetfront with transitions to surrounding neighbourhood.</li> <li>Transitions best addressed through design guidelines, including the existing <i>Guidelines for Multi-Unit Residential, Commercial and Industrial</i>, and the proposed addition of <i>Revitalization Guidelines for Corridors, Villages and Town Centres</i> with content specific to this site.</li> </ul>	<b>Change recommended:</b> Add geographically-specific guideline content regarding sensitive transition to <i>Revitalization Guidelines for Corridors, Villages and Town Centres</i>
<b>OCP Amendment #11. Expansion of Craigflower Village:</b> General support (combined survey and open house support of 72%) for this proposal, consistent with earlier engagement. Concerns include suggestion to maintain Traditional Residential designation, with the opportunity for shops in converted houses, and concern to maintain any heritage merit houses in this area.	<b>No change to plan recommended</b>	<ul style="list-style-type: none"> <li>Proposed change received community support throughout engagement, and supports goals of adding some housing and possible opportunities for a few more shops, compatible with surrounding scale.</li> <li>During earlier engagement, 3 storeys was preferred to either 4 storeys or maintain the status quo.</li> <li>The proposed expansion of Craigflower Village to the south balances the proposal to retain most Traditional Residential to north, and results from earlier consultation.</li> </ul>	<b>No change to proposal</b>

What we heard (numbered items correspond to proposed OCP amendments as presented)	Proposed Change to Plan Policy	Rationale	Impact on Proposed OCP Amendments
<b>Urban Residential Areas</b>			
<p><b>OCP Amendment #6. Proposed change from Traditional to Urban Residential to support 4 storey development, half-block at Russell and Skinner, east of Craigflower Village.</b></p> <p>While the City's survey showed support for this amendment (combined survey and open house support of 67%), input on this proposal was mixed: some felt this was an appropriate site for housing, given the location near transit, villages and park amenities, and being at the foot of a sloping site would transition more easily to adjacent development. Others felt the proposed height was out of place with the character and form of the small urban village and surrounding low-density neighbourhood.</p>	<p><b>Revisions to plan recommended:</b></p> <ol style="list-style-type: none"> <li>1. Remove proposed Urban Residential Place Designation.</li> <li>2. Show this area as Traditional Residential Sub-Area 1, supporting townhouses or apartments up to 10.7m (approx. 3 storeys) and 1.5 fsr, consistent with adjacent urban village densities and height; and rename sub-area 1 accordingly.</li> <li>3. Remove provision for higher density where affordable housing is included.</li> <li>4. Remove policy and map reference (Map 12) to view corridor from Skinner Street at Catherine Street. The view corridor remains in the new view corridors Map 11, for information should taller development be proposed in the future.</li> </ol>	<ul style="list-style-type: none"> <li>• Community expressed goals of accommodating housing diversity, affordable housing near village, transit and amenities. At the same time, a 3-storey height received more acceptance for this area.</li> <li>• The proposed height and density is consistent with the envisioned Craigflower Village. The location relates to the village, creating a sense of enclosure and to the one-sided village.</li> <li>• The location near transit, amenities, services and the downtown is appropriate for housing.</li> <li>• The existing grade (3m change across the site) supports sensitive transitions to adjacent lower-scale development and respects an identified view corridor from Skinner at Catherine Street.</li> <li>• The proposed density is unlikely to support affordable housing contributions.</li> <li>• Development Permit Area 16 would apply and contains guidelines for compatibility with context. At 3 storeys, development would not impact relevant view corridors nor have potential impacts meriting site-specific guidelines.</li> </ul>	<p><b>Changes Recommended:</b></p> <ul style="list-style-type: none"> <li>• Maintain Traditional Residential Designation</li> <li>• Support development up to 3 storeys, and 1.5 fsr, consistent with the anticipated form and density for Craigflower Village, through OCP Amendment #3 (reference form and density in Victoria West Neighbourhood Plan)</li> </ul>



What we heard (numbered items correspond to proposed OCP amendments as presented)	Proposed Change to Plan Policy	Rationale	Impact on Proposed OCP Amendments
<p><b>OCP Amendment #9. South of Esquimalt Road Area: Proposed change from Traditional to Urban Residential on Mary Street</b></p> <p>Change received 49% support (combined survey and open house support), with mixed input from the community.</p> <p>A range of concerns were expressed, including height (4 storeys is too high/appropriate; suggestion for 2 ½ storeys); that the proposal was out of scale with the surrounding neighbourhood; alternatively, that this is an appropriate location for housing near transportation, Roundhouse, amenities and downtown; that the mix of housing already existing in the area would accommodate multi-unit development; or that more height would better support housing and public amenities. Comments also reflected a difference of opinion of priorities, with some prioritizing concerns of current residents of the area, and others prioritizing a desire for added housing in this location.</p>	<p><b>Revisions to plan recommended:</b></p> <ul style="list-style-type: none"> <li>Remove proposal for urban residential designation on south Mary Street, and policy regarding added density where on-site affordable housing is included.</li> <li>Maintain Traditional Residential designation and include in Sub-Area 2, consistent with other areas south of Esquimalt Road, up to 3 storeys and 1.2fsr.</li> </ul>	<ul style="list-style-type: none"> <li>The revised height and density are meant to support housing diversity by supporting townhouse and courtyard housing forms with underground parking, consistent in scale with surrounding 2-3 storey development.</li> <li>Location near Frequent Transit, future shops and services, and within walking distance of downtown supports housing.</li> <li>Development Permit Guidelines (DPA 16) address specific concerns such as setbacks, privacy, massing, and compatibility of design; City standards address boulevards and street trees.</li> <li>Core Songhees, Westside Large Urban Village, and Esquimalt Road Corridor already provide opportunities to add multi-unit housing higher than 3 storeys.</li> </ul>	<p><b>Change Recommended:</b></p> <ul style="list-style-type: none"> <li>Remove proposed Urban Residential designation</li> <li>Include in Traditional Residential areas considered for 3-storey development at 1.2 fsr, through OCP Amendment #4 (by reference to Victoria West Neighbourhood Plan)</li> </ul>

What we heard (numbered items correspond to proposed OCP amendments as presented)	Proposed Change to Plan Policy	Rationale	Impact on Proposed OCP Amendments
<p><b>OCP Amendment #10. Change from Traditional Residential to Urban Residential along part of Esquimalt Road, to support 5 storey development; and related plan policies.</b></p> <p>General support for this proposal (combined survey and open house support of 68%). Concerns expressed about transitions to Traditional Residential areas generally and across Dundas Street; maintaining view corridors on sidestreets other than Catherine Street. Submission from Township of Esquimalt requests policy and guidelines to create a “gateway” at Esquimalt and Dominion Road.</p>	<p><b>Maintain proposed policies with minor revisions:</b></p> <ul style="list-style-type: none"> <li>• Add New Policy 6.27. <u>The height of street walls (that portion of a building closest to the street) should relate to the width of the adjacent street and support sensitive transitions to residential areas located across the street, through setbacks, building massing, front yard landscaping, and design features.</u></li> <li>• Add Policy 6.2.11. <u>Development adjacent to the intersection of Esquimalt Road and Dominion Road should help create a gateway recognizing the transition between Esquimalt and Victoria (see 14.1.5.).</u></li> <li>• Revise Policy 6.1.2, 8.5.8 and Maps 12 and 15 to recognize view corridor from Mary Street at Henry St,</li> <li>• Maintain (renumbered) Policy 6.2.6. addressing transitions to adjacent lower-scale development.</li> </ul>	<ul style="list-style-type: none"> <li>• Existing DPA 16 and proposed <i>Revitalization Guidelines for Corridors, Villages and Town Centres</i> development permit guidelines address sensitive transitions and state the height of the streetwall should be proportional to the width of adjacent streets.</li> <li>• Design guidelines are most appropriate tool to address massing, streetwall height and transition to adjacent development (rather than overall height restrictions). Current design guidelines would result in an approx. 4 storey streetwall along Dundas Street.</li> <li>• A given height may not be achieved on small sites if it cannot meet guidelines, as clarified in the addition of Map 10 (Anticipated maximum building heights at a glance).</li> <li>• Large sites on Dundas Street are already built to 3 – 4 storeys and while unlikely to redevelop, the 5 storey height allows for the addition of housing with the potential to achieve OCP densities (and possibly 10% added density for affordable housing) with rental retention or replacement. This still represents a reduced height from the OCP maximum (6 storeys).</li> <li>• A view corridors to the south from Mary Street also exists, with opportunities for future development to frame views of the Olympic Mountains.</li> </ul>	<p><b>Change recommended:</b></p> <p>Revise proposed design guidelines to add:</p> <ul style="list-style-type: none"> <li>• geographically specific transition guideline, including for landscape and sensitive treatment of Dundas Street frontage, and guidelines regarding gateway feature.</li> <li>• reference to view corridor from Mary Street at Henry Street.</li> </ul>

Large Urban Village			
<p><b>OCP Amendment #8. Support heights above 6 storeys Vic West Village:</b></p> <p>Strong support for the proposal (combined survey and open house support of 68%). Concerns include that the height be limited to 4 storeys, that heights above 6 storeys not be supported, that there be no further development permitted here in the future, that redevelopment happen sooner to help establish a village/replace parking lots; that density has already been transferred from Westside Shopping Centre site in the past and this be considered in amenity contributions; that there is enough density in the Songhees area; that this area not be called the “heart” of the community; that there be more density supported on those sites that support more height in the plan; that the DaVinci centre site support more height where community amenities are considered. General concern expressed during process to conserve opportunities for office employment. Advisory Design Panel suggested that areas where buildings higher than 6 storeys are desired, be aligned with densities higher than 2.5 fsr.</p>	<p><b>Revisions to plan recommended:</b></p> <ul style="list-style-type: none"> <li>• Revise (renumbered) Map 9 (Neighbourhood Land Use Map), policies 7.1.2., 7.1.5, and 7.1.6. to remove consideration of heights taller than 6 storeys.</li> <li>• Revise policy 7.1.1.: “Encourage a mix of uses such as residential, local commercial, retail, restaurant, <u>employment</u>, and community services, with residential uses <u>located</u> above commercial uses.”</li> </ul>	<ul style="list-style-type: none"> <li>• General community support for change in this area during the plan horizon; desire for it to feel more like a village than a shopping centre</li> <li>• Location is buffered from lower density areas spatially and/or due to elevation change to Alston Street</li> <li>• Heights taller than 6 storeys are not necessary to achieve density of 2.5 fsr and design objectives of plan. If a future proposal comes forward which includes taller buildings, it may be considered on its merits, with an OCP amendment.</li> <li>• Address earlier concerns regarding employment potential and reflect OCP vision for Large Urban Villages</li> <li>• There was no density transfer, but rather a rezoning of the shopping centre property and the Park site which distributed commercial and residential density at an overall density lower total density than previously zoned.</li> </ul>	<p><b>Remove OCP amendment #8</b></p>

Employment Lands			
<p><b>OCP Amendment #12: Change designation from Urban Residential to Employment-Residential (Alston-Skinner-Tyee block) and from General Employment to Employment-Residential (several parcels near Esquimalt Road)</b></p> <p><b>OCP Amendment #13: Change designation of most of the light industrial area between Esquimalt Road and Wilson Street, and between Langford Street and Skinner Street, from General Employment to Industrial-Employment to maintain light industrial uses at grade</b></p> <p><b>OCP Amendment #14: Change the designation of several areas adjacent to Esquimalt Road, and Langford Street, from General Employment to Industrial Employment-Residential.</b></p> <p>Strong support for these proposals. Concerns include: height be reduced; treat similar lands the same; provide more flexibility of employment use. Some feel more height along Esquimalt Road would better support redevelopment to mixed-use to enhance vibrancy and sense of safety of the area.</p>	<p><b>Revisions for employment areas (see Chapter 8 for details):</b> Revise sections 7.1.5.-7.1.6., (renumbered) sections 8.3 – 8.6, and future land use maps to:</p> <ul style="list-style-type: none"> <li>• Designate smaller sites adjacent to Langford Street, Skinner Street and Esquimalt Road between Springfield and Bay Street as “General Employment with Limited Residential” (revise land use maps accordingly)</li> <li>• For those areas along Esquimalt Road or adjacent to the Westside Large Urban Village, where 5-6 storey buildings are envisioned, align density with OCP density for Employment-Residential (approx. 2.5 fsr). For other areas, where lower heights are anticipated, maintain a maximum density of approximately 2.0 fsr.</li> <li>• Indicate in policy that these areas’ primary function is employment, and that residential uses are meant to be limited to no more than half the total floor space.</li> <li>• Indicate that light industrial or artisan spaces should be considered</li> <li>• Clarify that retail use is not supported except as ancillary to a primary use.</li> <li>• Indicate that residential uses would generally be on upper floors and/or facing Alston Street to transition to residential areas</li> </ul>	<ul style="list-style-type: none"> <li>• The proposed changes are meant to: <ul style="list-style-type: none"> <li>○ Emphasize employment function of these areas</li> <li>○ Provide greater flexibility in employment uses in smaller sites proposed for employment-mixed use, while maintaining consideration of artisan or light industrial production integrated into the mixed use areas</li> <li>○ Continue to maintain the larger, contiguous light industrial areas while still providing opportunity for upper-floor offices.</li> </ul> </li> <li>• The proposed changes are meant to balance concern about retaining employment lands with desire to add housing and mixed-use vibrancy to these areas along Langford Street and Esquimalt Road</li> <li>• These areas are currently envisioned in OCP, zoned and used for employment</li> <li>• Change to building height is not recommended for the following reasons: <ul style="list-style-type: none"> <li>○ The proposed neighbourhood plan already indicates 4-storey (or lower) maximum heights adjacent to lower-density areas.</li> <li>○ Development Permit Area guidelines are best tool to provide for sensitive transitions.</li> <li>○ The current OCP supports up to 4 storey commercial in these areas (which may be equivalent in height to a 5-storey mixed use building).</li> </ul> </li> </ul>	<p><b>Maintain OCP Amendment #13 for most employment areas between Esquimalt Road, Dalton Street, Wilson Street, and William Street</b></p> <p><b>Proposed Changes:</b></p> <ol style="list-style-type: none"> <li>1. Designate other areas described in the proposed amendments, as “Employment-Residential” urban place designation</li> <li>2. In the proposed <i>Revitalization Guidelines for Corridors, Villages and Town Centres</i> Add content specific to transitions across streets to proposed design guidelines. (Guideline 10.e.)</li> </ol>

Core Songhees			
<b>Bayview Lands</b> Request to make minor changes to more accurately describe Bayview hillside lands, and more accurately represent open spaces and building footprints in these lands.	<b>Minor revisions to plan:</b> Revise Chapter 9 to more accurately describe extent of Bayview lands. Map revisions to Map 1, 9, 15, 16 to more accurately reflect open space and building footprints approved. Revise Map 2 to include reference to Songhees Hillside Guidelines.		
Development Permit Guidelines			
<b>OCP Amendment #16, 17, 18. Expand Development Permit Area 5: Large Urban Villages; 6A: Small Urban Villages; and 7A: Corridors</b> <ul style="list-style-type: none"> <li>Concern that light industrial buildings will be unattractive</li> <li>Recommendation for additional policies regarding transition to residential areas</li> </ul>	<b>No change to plan recommended</b>	<ul style="list-style-type: none"> <li><i>Guidelines for Multi-Unit Residential, Commercial and Industrial</i>, and the proposed addition of <i>Revitalization Guidelines for Corridors, Villages and Town Centres</i> address building design.</li> <li>Proposed specific content in <i>Revitalization Guidelines for Corridors, Villages and Town Centres</i> addresses transitions and screening for light industrial uses.</li> </ul>	<b>Proposed Changes:</b> Minor revision to <i>Revitalization Guidelines for Corridors, Villages and Town Centres</i> adding detail for strategies to transition at edges of Esquimalt Road Corridor and Urban Villages.
<b>OCP Amendment #19: Create a new Development Permit Area 15F: Intensive Residential – Townhouse and Attached Dwelling</b> Strong support (72% combined survey and open house) Few comments received, either supporting additional design control, or not able to support due to not agreeing with proposed development anticipated	<b>No change to plan recommended</b>	<ul style="list-style-type: none"> <li>General neighbourhood input indicates support for additional ground-oriented housing diversity with design guidelines. The proposed guidelines have been revised to strengthen areas related to compatibility, contextual design, and transitions to better ensure neighbourhood fit.</li> </ul>	<b>Proposed Changes:</b> Proposed Development Permit Area 15F has been renamed as “Intensive Residential: Attached Residential Development.” Content has been revised to address contextual design, compatibility, mechanical equipment and service areas, and transitions to parks and open spaces.

<b>8., 15: Amenities</b>			
<b>OCP Amendments #8, 15: Amenities and Affordable Housing:</b> Community Association submission recommends the plan should commit to consult with the community regarding amenities to be provided per Amendment 8 (Vic West Village) and 15 (density bonus for affordable housing)	<b>Revisions to plan recommended:</b> <ul style="list-style-type: none"> <li>Clarify in Policy 7.1.4. that community amenities provided at Westside Large Urban Village would be informed by public engagement.</li> <li>Revise policies 6.1.5., 7.1.4. regarding 10% bonus density to substitute a more general policy which references consideration of additional density where additional affordable housing benefit is provided, consistent with city-wide policies.</li> </ul>	<ul style="list-style-type: none"> <li>The intention of the plan is to identify the general types of amenities which are desired, as identified by a thorough engagement process, recognizing that conditions and community needs may evolve.</li> <li>Amenities offsetting impacts of density on a specific development proposal would be offered and considered through a rezoning process, which would be a public process.</li> <li>Based on Council feedback regarding consideration of bonus density, a clear policy recommending 10% additional density is not recommended for inclusion in the neighbourhood plan. The neighbourhood plan continues to mirror the OCP in describing maximum densities “up to approximately” a certain floor space ratio, providing opportunities to consider limited additional density on a site-by-site basis.</li> </ul>	<b>Remove Amendment #8</b> <b>Remove Amendment #15</b>
<b>General Land Use and Urban Design</b>			
Include future <b>population estimates</b> in plan	A page has been added to Chapter 2 of the plan, providing future population estimates.		N/A
Provide <b>visualization</b> (3-D) of what Vic West will look like. (Note: Per communication with CALUC, could simply be a compound map of all anticipated building heights)	<b>Change recommended:</b> Add new Map 10 showing anticipated building heights at a glance, to Chapter 5.		N/A
“Special Planning Area” for Esquimalt-Tyee triangle does not provide any additional guidance beyond the Core Songhees urban place designation in the OCP, and existing Development Permit Guidelines.	<b>Change recommended:</b> Remove this section	<ul style="list-style-type: none"> <li>This section was added by staff to reflect the current OCP, for a site within Core Songhees that is currently low intensity and not within an MDA.</li> <li>The policies in the proposed plan were redundant and unnecessary. The site is not a large site with special characteristics meriting a Special Planning Area. Rather, any future rezoning would be negotiating using all existing policies.</li> </ul>	N/A



Transportation and Mobility			
What we heard	Proposed change to plan policy	Rationale	Impact on Proposed OCP Amendments
<b>Bay Street Bridge</b> Mention Bay Street Bridge Issues and Resolution (Community Assoc. urges City to include)	<b>Revise Policy 3.4.1. to add:</b> <u>Complete rehabilitation and resurfacing of Bay Street Bridge.</u> Improve cycling and pedestrian facilities for Point Ellice Bridge as part of <del>future</del> <u>long-term</u> bridge improvements.	<ul style="list-style-type: none"> <li>Plan addresses Bay Street bridge in Action Plan (short term: complete rehabilitation; long term: pedestrian and cyclists improvements to Bay Street Bridge Crossing)</li> <li>Upgrading Bay Street bridge pedestrian and cycling facilities is a priority for the community. Decisions regarding Bay Street Bridge made as part of City-wide budget allocation.</li> </ul>	N/A
<b>Alston and Langford Streets</b> Improve safety on Alston and Langford Street. Suggestion to establish expectation that owners of properties along Langford Street delineate and maintain the borders of their properties to a safe standard and at a minimum install an asphalt curb to delineate walking area separate from parking area. (Community Assoc. urges City to include)	<b>Revise policy 3.3.10. and action item in plan.</b> <ul style="list-style-type: none"> <li>Evaluate opportunities for short-term sidewalk improvements on Langford Street between Alston Street and Tyee Road, and <u>potential improvements to Alston Street</u>, through update to pavement management plan (See Policy 3.3.10.)."</li> <li>Move Action Plan item regarding Langford Street (and Alston St) and pavement management planning up to bulleted list, to make it easier to find.</li> </ul>	<ul style="list-style-type: none"> <li>Langford Street is identified for pavement management plan improvements in 2018.</li> <li>Alston Street has been added to the list to be considered for pavement management plan.</li> <li>Policy 3.3.10. states:</li> <li>Alston Street and Langford Streets: Over long-term, complete sidewalk network through redevelopment. Consider short-term improvements through pavement management planning. See also 3.4.3</li> </ul>	N/A
<b>Speed Limits</b> Reduce speed limits on major roads (Craigflower, Tyee, Bay, Wilson & Esquimalt) (Community Assoc. urges City to include)	<b>Add Policy 3.6.9.</b> <u>Assess Craigflower, Tyee, Bay, Wilson and Esquimalt for consistency of speed limits along corridor, and across municipal boundaries.</u> <ul style="list-style-type: none"> <li><u>Update Map 4 accordingly.</u></li> <li>Add to bulleted list in short-term actions (2019/ pending 2018 budget)</li> </ul>	<ul style="list-style-type: none"> <li>Review speed limits for consistency</li> <li>Maintain Policy 3.4.2 (re: enhanced pedestrian-friendliness of Bay Street and possible new or enhanced crossings); and associated short-term Action Plan item.</li> <li>Maintain Policy 3.3.20 (monitor and review speed limit along Esquimalt Road after completion of the new Johnson Street Bridge) and associated Action Plan item.</li> <li>Maintain policy 3.6.5. and associated Action Plan item to evaluate Wilson Street from Dominion Road/Viewfield Street to Rothwell Street. Add this item to Map 4.</li> </ul>	N/A
<b>Traffic Calming and Crossing Improvement</b> Include plans for traffic calming, esp. on Craigflower, Hereward and	<b>Revision to Plan:</b> <ul style="list-style-type: none"> <li>Revise policy 3.4.4. to add evaluation of the crossing at Raynor Street, and add to Action Plan for short-term evaluation.</li> </ul>	<ul style="list-style-type: none"> <li>Although the Craigflower at Raynor crossing was recently improved, evaluation recommended in response to comments.</li> </ul>	N/A

<p>Catherine (Community Assoc. urges City to include)</p> <p>Improve pedestrian crossing at Craigflower and Raynor – several comments that it feels unsafe due to traffic speed, visibility and nearby signal</p>		<ul style="list-style-type: none"> <li>Catherine Street has recently had a number of transportation improvements. Hereward Street and Skinner Street are already identified in neighbourhood plan and action plan for short-term assessment for potential improvements. Craigflower Street addressed through plan policies.</li> <li>Maintain Policy 3.3.6. (re: evaluation of Pine Street and Hereward Street for improvements to pedestrian safety) and associated Action Plan item.</li> <li>Maintain Policies 3.4.3. and 3.4.4. regarding improvements to create a slower more comfortable cycling experience along Craigflower Road; and short-term action item to evaluate Skinner Street between Front Street and Langford Street for pedestrian and cyclist comfort and safety.</li> </ul>	
<p><b>William Street Bicycle Route</b></p> <p>Recommendation that bicycles not be directed along William Street because this is a relatively narrow street with truck traffic accessing businesses on the west side, and that other options exist including the E&amp;N Rail Trail, Springfield Street (which is 3 m wider than William Street), and proposed connections adjacent to the Vic West Elementary field.</p>	<p><b>No change recommended.</b></p>	<ul style="list-style-type: none"> <li>William Street carries primarily local traffic at low volumes and speeds, and is the most direct route from the south to access the people-only William Street Greenway and Vic West School.</li> <li>William Street has been identified as a greenway since 2003, and is a key pedestrian connection to Vic West School.</li> <li>No protected lanes are envisioned on William Street, rather bicycles would continue to share the road with other traffic. The plan also proposes a connection from the William Street Greenway to the E&amp;N Trail as proposed by the public comments, for those choosing an off-street route.</li> </ul>	<p>N/A</p>
<ul style="list-style-type: none"> <li>Suggestion to revise the east-west path alignment across the Vic West field, to show the route more conceptually</li> <li>Suggestion to revise childcare policies to include the opportunity for more childcare (out of school care) at Vic West School</li> </ul>	<p><b>Minor Change recommended:</b></p> <ul style="list-style-type: none"> <li>Map 3 revised to show more conceptual path alignment</li> <li>Policy 13.2.1. revised to include Victoria West Elementary School</li> </ul>	<ul style="list-style-type: none"> <li>Both policies could be supported through partnerships involving the Greater Victoria School District</li> </ul>	<p>N/A</p>
<b>Urban Agriculture</b>			

What we heard	Proposed change to plan policy	Rationale	Impact on Proposed OCP Amendments
Engage the Vic West Food Security Collective in strengthening Chapter 12. (Community Assoc. proposes City include)	<b>Minor change recommended</b> <ul style="list-style-type: none"> <li>The introduction to Chapter 12, Neighbourhood Food Systems, has been reworded to provide a greater level of context regarding the importance of urban agriculture to the neighbourhood.</li> <li>Policy 12.2.2. has been reworded to indicate support for applications for community gardens in parks identified in the neighbourhood plan, or in other locations of interest to the community.</li> </ul>	<ul style="list-style-type: none"> <li>The community has a strong interest in enhancing elements of the neighbourhood food system.</li> <li>Further changes to urban food policies are best considered and adopted on a city-wide basis.</li> </ul>	N/A
Community Association submission recommends requiring all developments to include food growing amenities.	<b>Minor change recommended</b> Add Policy 6.2.10 and 7.1.10.: Consider features which support urban food production for residents, including edible landscaping, allotment or rooftop garden areas.  Add the following to the Action Plan as a short-term item: Create voluntary guidelines for urban food production in mixed-use and multi-unit developments to short-term action plan (2018)	<ul style="list-style-type: none"> <li>The recent Growing in the City process considered but did not require food growing amenities in new development.</li> <li>City is developing voluntary guidelines for urban food production in mixed-use and multi-unit developments, per OCP 17.12</li> <li>Proposed <i>Revitalization Guidelines for Corridors, Villages and Town Centres</i> encourage open space amenities which encourage sociability, including food growing amenities.</li> <li>Plan policies for redevelopment on larger cooperative housing sites encourages food growing amenities.</li> <li>Further requirements for urban food production space in new development should be considered city-wide and not in one neighbourhood.</li> </ul>	N/A
Question as to why policy 12.3.3. mentions allotment gardens specifically on Bayview lands, whose MDA does not provide for allotment gardens?	<b>Change recommended:</b> Remove policy relating to Bayview Lands.	<ul style="list-style-type: none"> <li>Origin of this policy appears related to suggestion that if MDAs are renegotiated, urban agriculture amenities were desired by community members. Staff recommends that any specific renegotiations of MDAs, if they should occur, should include public consultation at that time.</li> </ul>	N/A

Parks Open Spaces and Waterways			
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What we heard	Proposed change to plan policy	Rationale	Impact on Proposed OCP Amendments
<p>Include policies for all parks, including a vision and plans for each park. (Community Assoc. urges City to include)</p>	<p><b>Change recommended:</b></p> <ul style="list-style-type: none"> <li>• Policies have been provided for most parks, reflecting:               <ul style="list-style-type: none"> <li>○ Input during the Vic West Neighbourhood Plan process</li> <li>○ Directions from existing Park Improvement Plans or Master Development Agreements</li> <li>○ Consideration of further planning needed for Arm and Burleith parks and the adjacent shoreline</li> </ul> </li> <li>• Add Park Condition Assessment to short-term actions in Action Plan</li> </ul>	<ul style="list-style-type: none"> <li>• Input through the neighbourhood plan process will be considered as future park improvement plans are undertaken.</li> <li>• The City will be developing a Parks Condition Assessment to help prioritize areas for investment in the coming years.</li> <li>• Some parks have not been included because there was no specific input through the plan process, nor is there a specific improvement plan for these parks. These include Songhees Hillside, Vista, Tenas, and Saghalie Parks.</li> <li>• A vision statement for each park is not included, as this should be developed through consultation during planning for each park. The Parks and Open Spaces Master Plan is a higher level document that does not individually address each of Victoria's 137 parks. Neighbourhood plan engagement sought ideas for improvements, but not a vision for each park.</li> </ul>	N/A
<p>Request for park improvements in Rainbow Park/Mary Street Park. Suggestions include picnic area, enhanced landscape, improved trail connections, and a community garden.</p>	<p><b>Change recommended:</b></p> <ul style="list-style-type: none"> <li>• Include as suggestions from the community, in parks section (above).</li> </ul>	<ul style="list-style-type: none"> <li>• See above</li> </ul>	N/A
<ul style="list-style-type: none"> <li>• Include water access points in the plan. (Community Assoc. urges City to include)</li> <li>• Concern that water and beach access do not have greater attention in plan, as they are very important to community.</li> </ul>	<p><b>Change recommended:</b></p> <ul style="list-style-type: none"> <li>• Show existing and potential public water access points and private marinas on Map 6.</li> </ul>	<ul style="list-style-type: none"> <li>• Potential water access points have been identified through the neighbourhood plan process (dependent on ecological assessment.)</li> <li>• An ecological assessment of potential for access points in Arm, Burleith and Banfield Parks is included as a short-term item in the Action Plan.</li> </ul>	N/A
<ul style="list-style-type: none"> <li>• Show the new Gorge Marine Park in the plan, with park's intent, vision and policies. (Community Assoc. urges City to include)</li> </ul>	<p><b>No change recommended</b></p>	<ul style="list-style-type: none"> <li>• "Gorge Marine Park" is a zoning designation and not a park dedication.</li> <li>• Staff will be bringing forward a proposal to create a management plan. Creation of a management plan and its scope have not yet been approved by Council.</li> </ul>	N/A

**MINUTES OF THE  
ADVISORY DESIGN PANEL MEETING  
HELD WEDNESDAY NOVEMBER 29, 2017 AT 12:00 PM**

**1. THE CHAIR CALLED THE MEETING TO ORDER AT 12:10 PM**

**Present:** Jesse Garlick, Chair; Sorin Birliga; Jason Niles;  
Carl-Jan Rupp; Justin Gammon; Paul Hammond;  
Elizabeth Balderston; Deborah LeFrank

**Staff Present:** Miko Betanzo – Senior Planner, Urban Design  
Marc Cittone – Senior Planner, Community Planning  
Katie Lauriston – Secretary, Advisory Design Panel

**2. MINUTES**

**Minutes from the Meeting held October 25, 2017**

**Action:**

It was moved by Justin Gammon, seconded by Paul Hammond, that the Minutes of the Meeting of Advisory Design Panel held October 25, 2017 be adopted as presented.

**Carried**

*Jason Niles joined the meeting at 12:14 pm.*

**3. APPLICATIONS**

**3.1 Draft Vic West Neighbourhood Plan and Draft Development Permit Guidelines**

Staff are seeking input regarding the draft *Vic West Neighbourhood Plan*, the proposed amendments to the *Design Guidelines for Corridors, Villages and Town Centres* and the revised draft *Design Guidelines for Intensive Residential Development – Townhouse and Rowhouse*.

Mr. Cittone provided the Panel with a brief introduction of the draft neighbourhood plan, amendments to the design guidelines and the areas that Council is seeking advice on, including the following:

- The draft *Design Guidelines for Corridors, Villages and Town Centres*, especially sections applicable to Vic West
- Land use and urban design guidelines
- How townhouse and rowhouse typologies fit within Vic West
- How industrial and mixed use design guidelines work with residential uses
- Any other feedback on the design guidelines.

Questions of clarification were asked by the Panel on the following:

- has staff looked to other cities with similar mixed light industrial and residential uses?
  - Yes; staff have looked at policies and studies for industrial sanctuaries and examples such as Vancouver's large-scale proposals and Sidney's light industrial and residential mixed uses
- what is the vision for Vic West?
  - accommodating a larger population with a range of housing types
  - sustainability, maintaining natural spaces, preserving the natural shoreline features, urban agriculture and green building in areas of higher density
- the rationale for not including a map illustrating density?
  - pages 47-49 show future land use and at-a-glance density building types
- was there coordination with the township of Esquimalt in forming the draft Plan?
  - staff looked at Esquimalt's emerging Official Community Plan
- how will the neighbourhood plan be used?
  - planners and applicants consult neighbourhood plans in conjunction with the OCP
  - neighbourhood plans are also a communicative piece for the public for changes in the community
- how do these plans accommodate transit?
  - the plan respects frequent transit routes, focusing on the comfort and safety of getting to key transit routes for pedestrians & cyclists
  - staff have looked at the location and distribution of additional housing in relation to existing transit routes
- with a possible train station west of the Johnson street bridge, is there any intent to identify another stop more centrally in Vic West?
  - policies have left open the possibility of another stop in Vic West, although this is not currently a priority; current policies support a stop close to the downtown core
- how to entice developers to assist with public infrastructure and public amenities to ensure vision 2050?
  - adopting the step code is a separate, city-wide process; there is an implementation schedule at the end of the draft Plan
- How will the *Design Guidelines for Corridors, Villages and Town Centres* and the revised draft *Design Guidelines for Intensive Residential Development – Townhouse and Rowhouse* be used?
  - these function as a bylaw, adding an additional layer of regulation
  - like neighbourhood plans, design guidelines are used by planners, applicants and designers and are also a communicative piece for the public
- is there anything in the design guidelines to address the desire for iconic buildings, increased density or height?
  - not in the design guidelines

The Panel proposed the following recommendations:

- increase content specific to Victoria in guidelines
- increase focus on innovation and diversity
- clearly define the character of the neighbourhood to guide the formation of a consistent density policy
- elucidate content regarding larger developments and towers
- consider recommending tower form to formalize desire for iconic buildings



- feature buildings at intersections should be a part of a consistent density policy
- support increased height for townhouses with residents' support
- consider density requirements along with the bonus density provision
- clearly articulate density objectives in the neighbourhood plan
- provide design guidelines for achieving additional density in Vic West
- support increased density along Esquimalt Road
- increase collaboration with the Township of Esquimalt
- consider increasing green space through publically accessible roof gardens
- reinforce the need to protect urban forests in Vic West
- provide illustrated examples for the types of industrial uses envisioned for integration with residential use
- encourage commercial development along regional trails and along the Westsong Walkway
- illustrate creative solutions for mixing residential and light industrial uses
- consider including guidelines for marine industrial use
- encourage underground residential parking with neighbourhood residents' support
- incorporate recommendations into maps and illustrations where possible
- ensure continual engagement to avoid stagnant policy
- clarify the long-term vision for Vic West

The Panel endorses the direction of the draft *Vic West Neighbourhood Plan*, the proposed amendments to the *Design Guidelines for Corridors, Villages and Town Centres* and the revised draft *Design Guidelines for Intensive Residential Development – Townhouse and Rowhouse* with the above-noted recommendations.

#### **4. ADJOURNMENT**

The Advisory Design Panel meeting of November 29, 2017 adjourned at 1:29 pm.

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Jesse Garlick, Chair