

I. REPORTS OF COMMITTEES

I.1 Committee of the Whole

I.1.a Report from the July 18, 2019 COTW Meeting

Councillor Young withdrew from the meeting at 9:00 p.m. due to a pecuniary conflict of interest with the following item, as the proponent is a client of his firm.

I.1.a.c Rezoning Application No. 00598 and Development Permit with Variance Application No. 00506 for 953 Balmoral Road (North Park)

Moved By Councillor Alto

Seconded By Councillor Loveday

Rezoning Application No. 00598

That Council instruct staff to prepare the necessary Zoning Regulation Bylaw Amendment that would authorize the proposed development outlined in Rezoning Application No. 00598 for 953 Balmoral Road, that first and second reading of the Zoning Regulation Bylaw Amendment be considered by Council, and a Public Hearing date be set once the following conditions are met:

1. Preparation of the following documents, executed by the applicant to the satisfaction of City Staff:
 - i. Statutory Right-of-Way of 1.22m on Balmoral Road.
 - ii. The applicant provide an amenity contribution in the amount of \$76,694.69 toward the Local Amenities Reserve Fund in accordance with the City of Victoria Density Bonus Policy to the satisfaction of City Staff.

Development Permit with Variance Application No. 000506

That Council, after giving notice and allowing an opportunity for public comment at a meeting of Council, and after the Public Hearing for Rezoning Application No. 00598, if it is approved, consider the following motion:

"That Council authorize the issuance of Development Permit with Variance Application No. 000506 for 953 Balmoral Road, in accordance with:

1. Plans date stamped May 14, 2019.
2. Development meeting all Zoning Regulation Bylaw requirements, except for the following variances:
 - i. reduce the required number of parking spaces from 12 to 5
 - ii. Part 3.3(10): reduce the front yard setback from 10.50m to 2.00
 - iii. Part 3.3 (10): reduce the side (east) yard setback from 6.10m to 1.52m
 - iv. Part 3.3(10): reduce the side (west) yard setback from 6.10m to 3.64m

- v. Part 3.3(4)(1): increase the site coverage from 30% to 43%
- vi. Part 3.3(4)(6)(1): reduce the open site space from 30% to 15.30%.
- 3. Registration of legal agreements on the property's title to secure the MODO Car Share Vehicle and parking space, car share memberships, one monthly transit pass for each unit over a period of three years (396 monthly passes), and one bicycle for each unit to the satisfaction of City Staff.
- 4. The Development Permit lapsing two years from the date of this resolution."

CARRIED UNANIMOUSLY

Mayor Helps recalled the motion.

Moved By Councillor Alto

Seconded By Councillor Loveday

Rezoning Application No. 00598

That Council instruct staff to prepare the necessary Zoning Regulation Bylaw Amendment that would authorize the proposed development outlined in Rezoning Application No. 00598 for 953 Balmoral Road, that first and second reading of the Zoning Regulation Bylaw Amendment be considered by Council, and a Public Hearing date be set once the following conditions are met:

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 - 4. The Development Permit lapsing two years from the date of this resolution."

FOR (6): Mayor Helps, Councillor Alto, Councillor Collins, Councillor Loveday, Councillor Potts, and Councillor Thornton-Joe

OPPOSED (1): Councillor Isitt

CARRIED (6 to 1)

Councillor Young returned to the meeting at 9:02.

F. LAND USE MATTERS

F.1 Rezoning Application No. 00598 and Development Permit with Variance Application No. 00506 for 953 Balmoral Road (North Park)

Councillor Young left the meeting at 9:10 a.m. due to non-pecuniary conflict with this item as his former clients are involved with this project.

Council received a report from the Acting Director of Sustainable Planning and Community Development in regards to a proposal to build an 11 unit rental dwelling apartment building. Staff recommend the application be declined due to the lack of changes to massing and side yard setbacks.

Committee discussed:

- *Advantages and disadvantages of the project*

Moved By Councillor Alto

Seconded By Councillor Dubow

Rezoning Application No. 00598

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Development Permit with Variance Application No. 000506

That Council, after giving notice and allowing an opportunity for public comment at a meeting of Council, and after the Public Hearing for Rezoning Application No. 00598, if it is approved, consider the following motion:

"That Council authorize the issuance of Development Permit with Variance Application No.

000506 for 953 Balmoral Road, in accordance with:

1. Plans date stamped May 14, 2019.
2. Development meeting all *Zoning Regulation Bylaw* requirements, except for the following variances:
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 - ii. Part 3.3(10): reduce the front yard setback from 10.50m to 2.00

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3. Registration of legal agreements on the property's title to secure the MODO Car Share Vehicle and parking space, car share memberships, one monthly transit pass for each unit over a period of three years (396 monthly passes), and one bicycle for each unit to the satisfaction of City Staff.
4. The Development Permit lapsing two years from the date of this resolution."

FOR (6): Mayor Helps, Councillor Alto, Councillor Loveday, Councillor Potts, Councillor Collins, and Councillor Thornton-Joe

OPPOSED (1): Councillor Dubow

CARRIED (6 to 1)

Councillor Young returned to the meeting at 9:37 am.



Committee of the Whole Report For the Meeting of July 18, 2019

To: Committee of the Whole **Date:** July 4, 2019
From: Andrea Hudson, Acting Director, Sustainable Planning and Community Development
Subject: Rezoning Application No. 00598 and Development Permit with Variance Application No. 000506 for 953 Balmoral Road

RECOMMENDATION

That Council decline Rezoning Application No. 00598 and Development Permit with Variance Application No. 000506 for the property located at 953 Balmoral Road.

EXECUTIVE SUMMARY

The purpose of this report is to present Council with an update on the Rezoning and Development Permit with Variance Applications for the property located at 953 Balmoral Road. The proposal is to rezone the subject property from the R-2 Zone, Two Family Dwelling District, to a new zone in order to construct a four-storey, multi-unit residential building with a density of approximately 1.38:1 floor space ratio (FSR).

On September 6, 2018, Council passed the following motion (attached):

Rezoning Application No. 00598

That Council instruct staff to prepare the necessary Zoning Regulation Bylaw Amendment that would authorize the proposed development outlined in Rezoning Application No. 00598 for 953 Balmoral Road, that first and second reading of the Zoning Regulation Bylaw Amendment be considered by Council, and a Public Hearing date be set once the following conditions are met:

1. *Preparation of the following documents, executed by the applicant to the satisfaction of City Staff:*
 - a. *Statutory Right-of-Way of 1.22m on Balmoral Road.*
 - b. *The applicant provide an amenity contribution in the amount of \$76,694.69 toward the Local Amenities Reserve Fund in accordance with the City of Victoria Density Bonus Policy to the satisfaction of City Staff.*
 - c. *Following consideration of Rezoning Application No. 00487, if approved, that Council authorize staff to prepare and enter into an Encroachment Agreement for a fee of \$750 plus \$25 per m² of exposed shored face during construction, to the satisfaction of the City staff.*

Development Permit with Variance Application No. 000506

That, subject to review by the Advisory Design Panel and report back to the Committee of the Whole, that Council, after giving notice and allowing an opportunity for public comment at a meeting of Council, and after the Public Hearing for Rezoning Application No. 00598, if it is approved, consider the following motion:

"That Council authorize the issuance of Development Permit with Variance Application No. 000506 for 953 Balmoral Road, in accordance with:

- 1. Plans date stamped January 18, 2018*
- 2. Development meeting all Zoning Regulation Bylaw requirements, except for the following variances:*
 - i. reduce the required number of parking spaces from 12 to 5*
 - ii. Part 3.3(10): reduce the front yard setback from 10.50m to 2.00*
 - iii. Part 3.3 (10): reduce the side (east) yard setback from 6.10m to 1.52m*
 - iv. Part 3.3(10): reduce the side (west) yard setback from 6.10m to 3.64m*
 - v. Part 3.3(4)(1): increase the site coverage from 30% to 43%*
 - vi. Part 3.3(4)(6)(1): reduce the open site space from 30% to 15.30%*
- 3. Registration of legal agreements on the property's title to secure the MODO Car Share Vehicle and parking space, car share memberships, one monthly transit pass for each unit over a period of three years (396 monthly passes), and one bicycle for each unit to the satisfaction of City Staff.*
- 4. Revise the landscape plan to indicate floating pavement where the proposed parking spaces overlap with the tree's critical root zone in accordance with the arborist report prepared by Talbot Mackenzie & Associates.*
- 5. The Development Permit lapsing two years from the date of this resolution."*

In accordance with Council's motion above, on October 24, 2018, the Advisory Design Panel reviewed the proposal and provided a recommendation that the applicant make changes related to the massing and side yard setbacks, and provide design consideration to the landscaping, privacy of ground-oriented units, front entryways and balconies on the upper units (minutes attached). In response to the ADP's comments and recommendation, the applicant made some revisions to the landscaping by adding some landscaping in the front yard, a landscaping strip on the east side of the building and the surface parking area and a trellis system along the rear property line. The applicant also added glazed front doors to soften the front entryways. There are no changes related to the massing and side yard setbacks.

COMMENTS

On October 24, 2018, the Advisory Design Panel passed the following recommendation for Council's consideration:

That Development Permit with Variances Application No. 000506 for 953 Balmoral Road be approved subject to the following changes:

- increase the side yard setbacks and redistribute the massing to reduce negative impacts on neighbours' properties, allow for enhanced soft landscaping and improve liveability*
- provide design consideration to enhanced landscaping, attention to street frontage, main entrance sequence, private walk-up entrance and rear parking lot area*
- reconsider the privacy of ground-oriented suites*
- update drawings to include a more consistent depiction of the proposal, including how the windows are detailed, the depth of the façade, shadow studies and 3D renderings*

- *consider the entrances in context of the public realm and streetscape*
- *consider the addition of balconies for the upper units.*

In response to the ADP's comments and recommendation, the applicant made some revisions to the landscaping by adding some landscaping in the front yard, a landscaping strip on the east side of the building and surface parking area and a trellis system along the rear property line. The applicant also added glazed front doors to soften the front entryways. There are no changes related to the massing and side yard setbacks. Staff still have concerns with the overall size, scale and massing, building setbacks, window placement, limited soft landscaping and outdoor open space and the transition between the public and private realm.

From a policy perspective, staff continue to have concerns with the impact that this proposal would have on the future redevelopment of Balmoral Road. The OCP encourages the logical assembly of development sites to enable the best realization of development potential for the area. Given the existing neighbourhood context and development potential, land assembly with the adjacent properties is strongly encouraged. If the subject property were consolidated with one or both of the adjoining properties on Balmoral Road, then this scenario would achieve a higher-density residential development with a greater number of dwelling units consistent with the policies and objectives in the OCP, avoid mid-block, piecemeal development and realize a better site plan with fewer impacts to the adjoining properties. Staff's recommendation is to decline the Rezoning and Development Permit with Variance Applications; however, an alternate motion is provided should Council wish to advance these applications to a Public Hearing.

ALTERNATE MOTION

Rezoning Application No. 00598

That Council instruct staff to prepare the necessary Zoning Regulation Bylaw Amendment that would authorize the proposed development outlined in Rezoning Application No. 00598 for 953 Balmoral Road, that first and second reading of the Zoning Regulation Bylaw Amendment be considered by Council, and a Public Hearing date be set once the following conditions are met:

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Development Permit with Variance Application No. 000506

That Council, after giving notice and allowing an opportunity for public comment at a meeting of Council, and after the Public Hearing for Rezoning Application No. 00598, if it is approved, consider the following motion:


"That Council authorize the issuance of Development Permit with Variance Application No. 000506 for 953 Balmoral Road, in accordance with:

1. Plans date stamped May 14, 2019.
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4. The Development Permit lapsing two years from the date of this resolution."

Respectfully submitted,


 Leanne Taylor
 Senior Planner
 Development Services Division

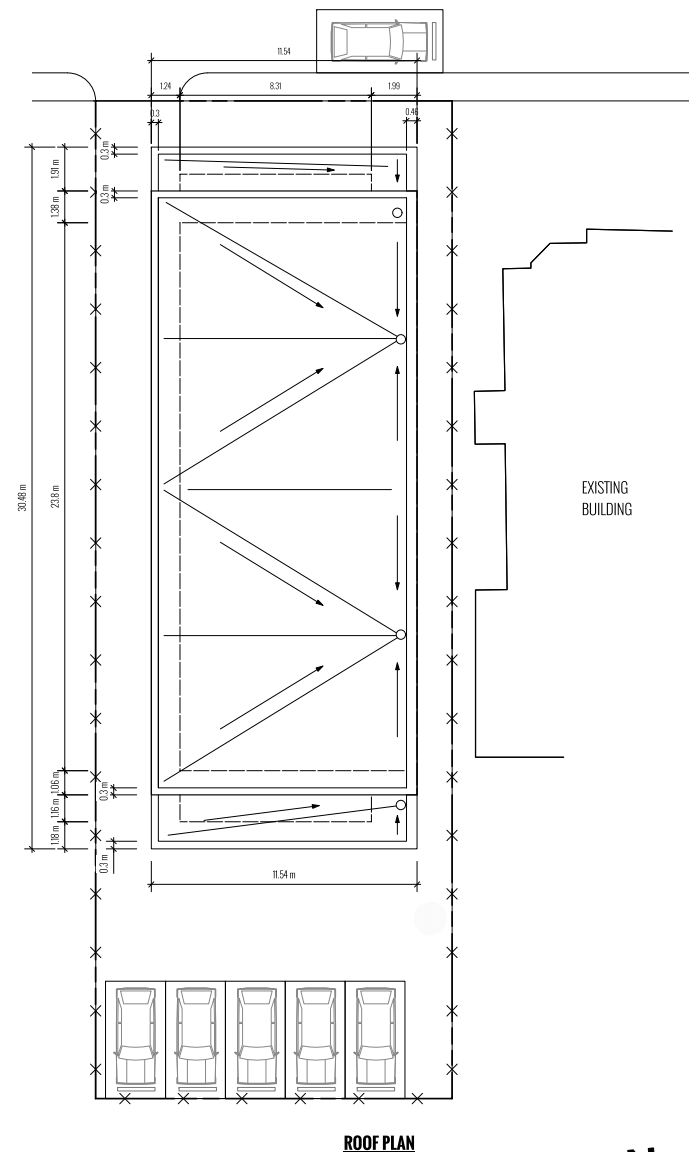
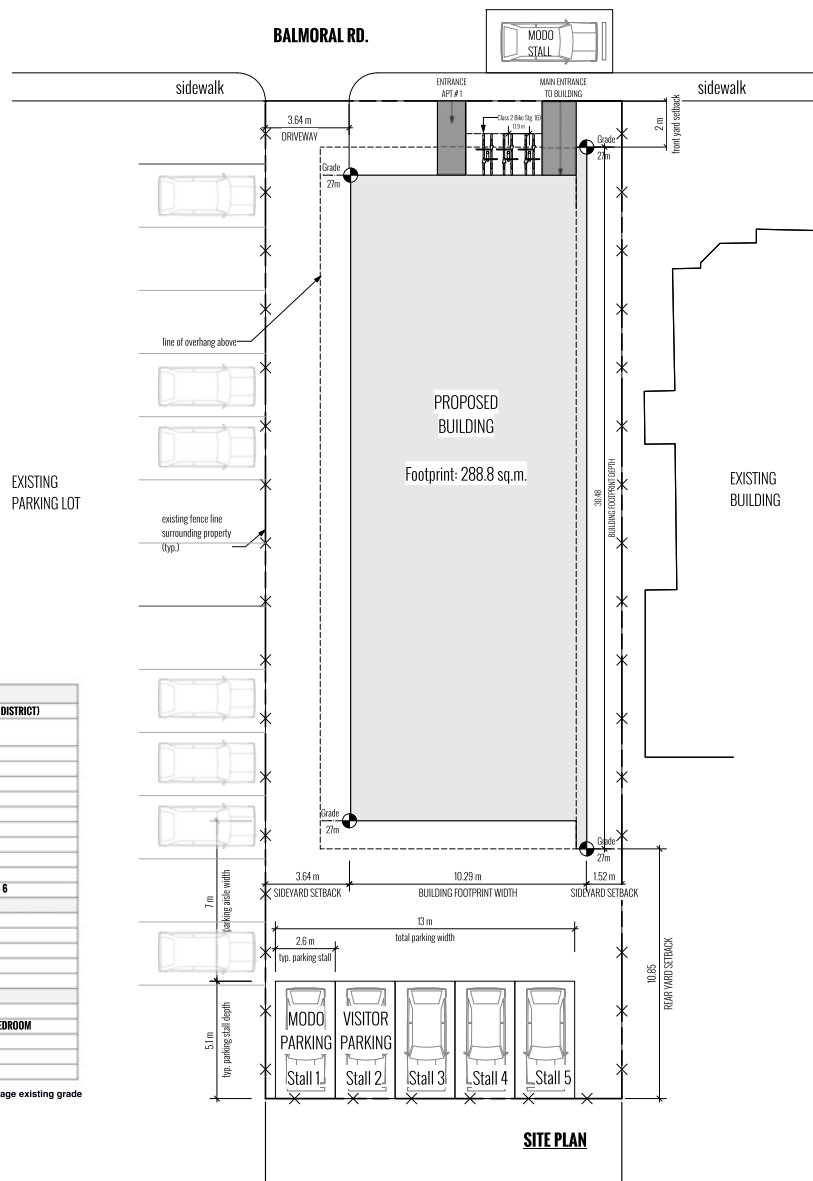

 Andrea Hudson, Acting Director
 Sustainable Planning and Community
 Development Department

Report accepted and recommended by the City Manager:


 Date: July 11, 2019

List of Attachments

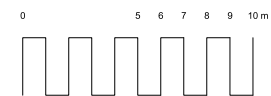
- Attachment A: Revised plans dated May 14, 2019
- Attachment B: Advisory Design Panel report, October 24, 2018
- Attachment C: Minutes from the Advisory Design Panel meeting on October 24, 2018
- Attachment D: Letter to Mayor and Council dated March 6, 2019
- Attachment E: Letter to Mayor and Council dated May 7, 2018
- Attachment F: Letter to Mayor and Council dated August 17, 2018
- Attachment G: Package from the applicant date stamped November 22, 2017, including Letter to Mayor and Council, correspondence and Parking Study dated October 27, 2017, prepared by Watt Consulting Group
- Attachment H: Council minutes, September 6, 2018
- Attachment I: Committee of the Whole report, September 6, 2018
- Attachment J: Committee of the Whole report, June 7, 2018
- Attachment K: Committee of the Whole report, March 15, 2018.

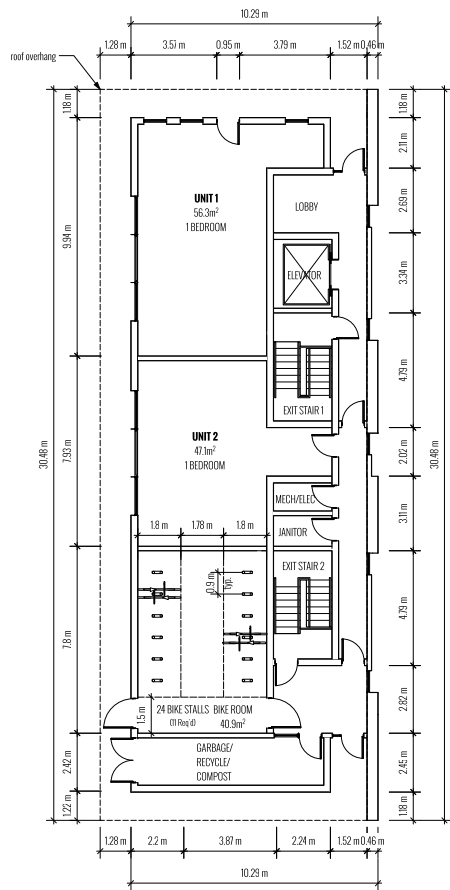


PROJECT INFORMATION TABLE	
Zone (existing)	R-2 (TWO FAMILY DWELLING DISTRICT)
Proposed zone or site specific zone If unsure, state "new zone"	NEW ZONE
Site area (m ²)	671.5 m ²
Total floor area (m ²)	929.5 m ²
Commercial floor area (m ²)	ZERO
Floor space ratio	929.5 / 671.5 = 1.38:1
Site coverage (%)	288.8 / 671.5 = 43%
Open site space (%)	103 / 671.5 = 15.3%
Height of building (m)	12.5m
Number of storeys	4
Parking stalls (number) on site	5
Bicycle parking number (Class 1 and Class 2)	Class 1 - 24 Class 2 - 6
Building Setbacks (m)	
Front yard	2.0m
Rear yard	10.85m
Side yard (indicate which side)	EAST 1.52m
Side yard (indicate which side)	WEST 3.64m
Combined side yards	5.16m
Residential Use Details	
Total number of units	11
Unit type, e.g., 1 bedroom	9 - ONE BEDROOM, 2 - TWO BEDROOM
Ground-orientated units	2
Minimum unit floor area (m ²)	47.1 m ²
Total residential floor area (m ²)	559.3m ²

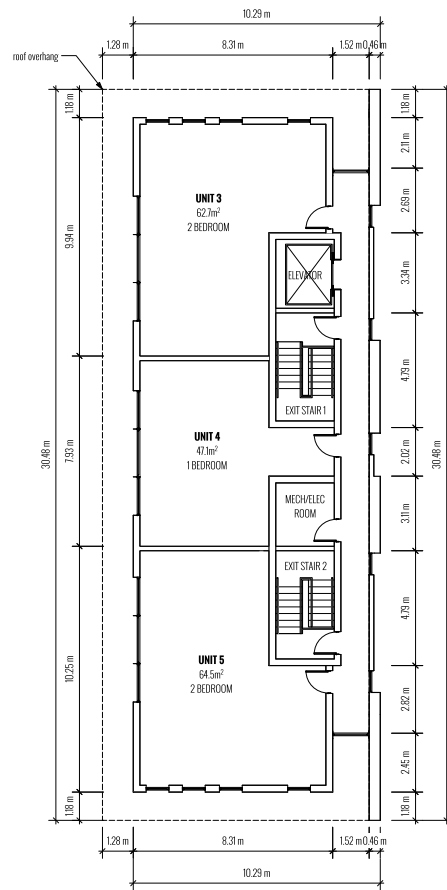
Average Existing Grade Calculation: $27.5 + 27.6 + 26.5 + 26.7 = 108.3 / 4 = 27.0$ average existing grade
Proposed Average Grade: $27 + 27 + 27 + 27 = 108 / 4 = 27$ average proposed grade

SITE PLAN & ROOF PLAN | APARTMENTS @ 953 BALMORAL | SCHEMATIC DESIGN FOR DEVELOPMENT PERMIT & REZONING APPLICATION | APRIL 2019 | SCALE 1:100

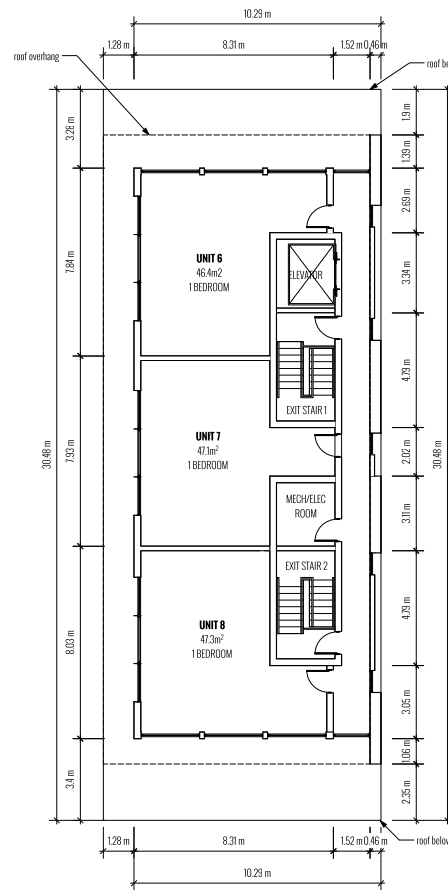




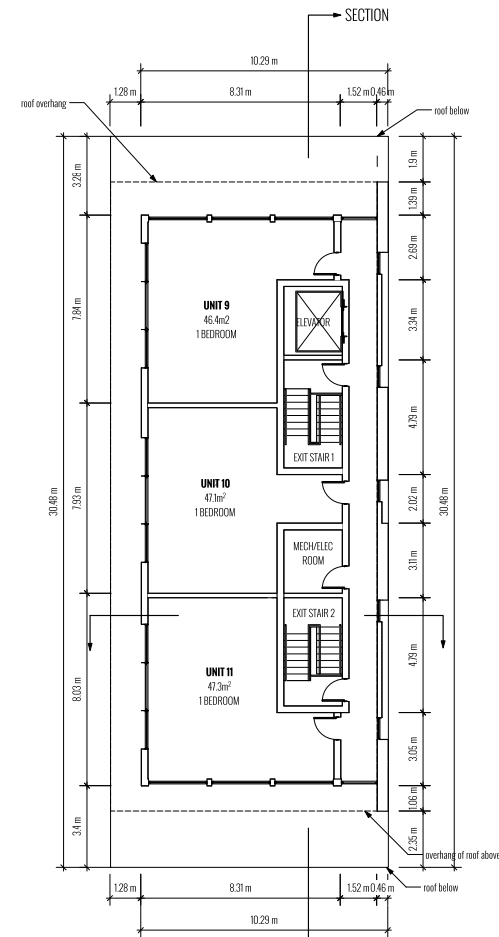
GROUND FLOOR PLAN



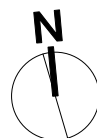
SECOND FLOOR PLAN

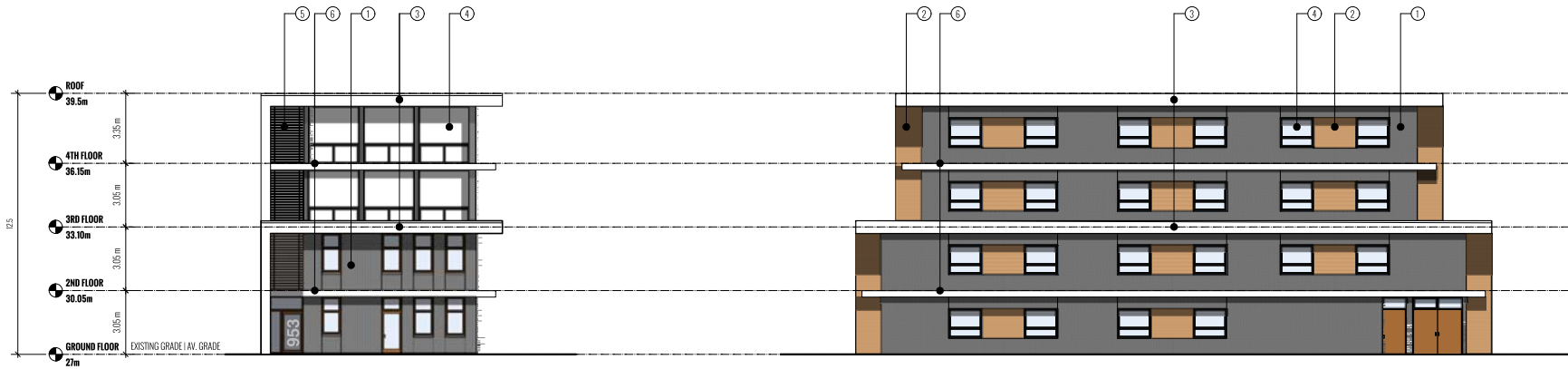


THIRD FLOOR PLAN



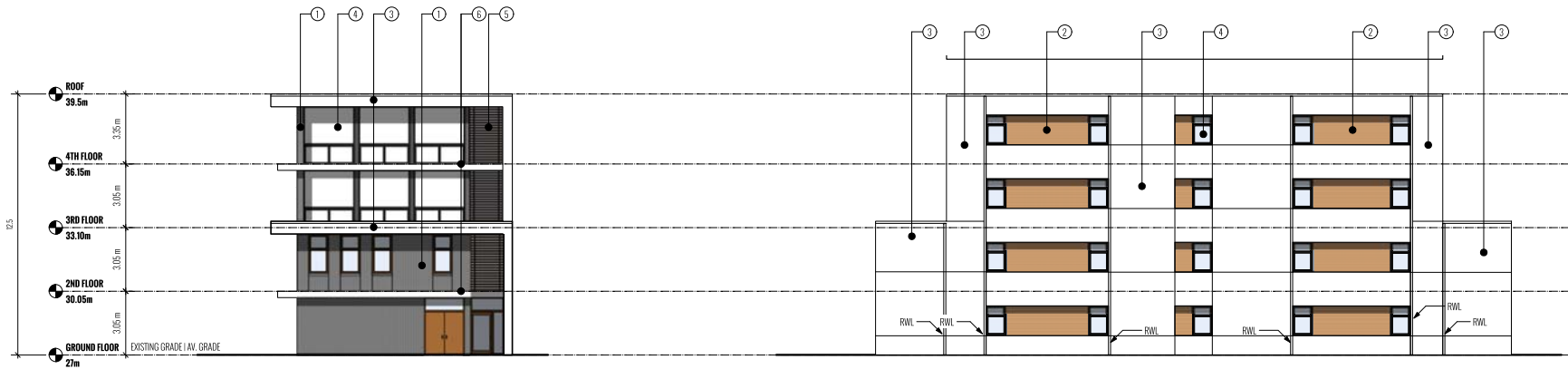
FOURTH FLOOR PLAN





NORTH ELEVATION

WEST ELEVATION

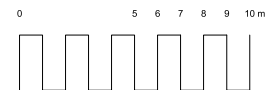


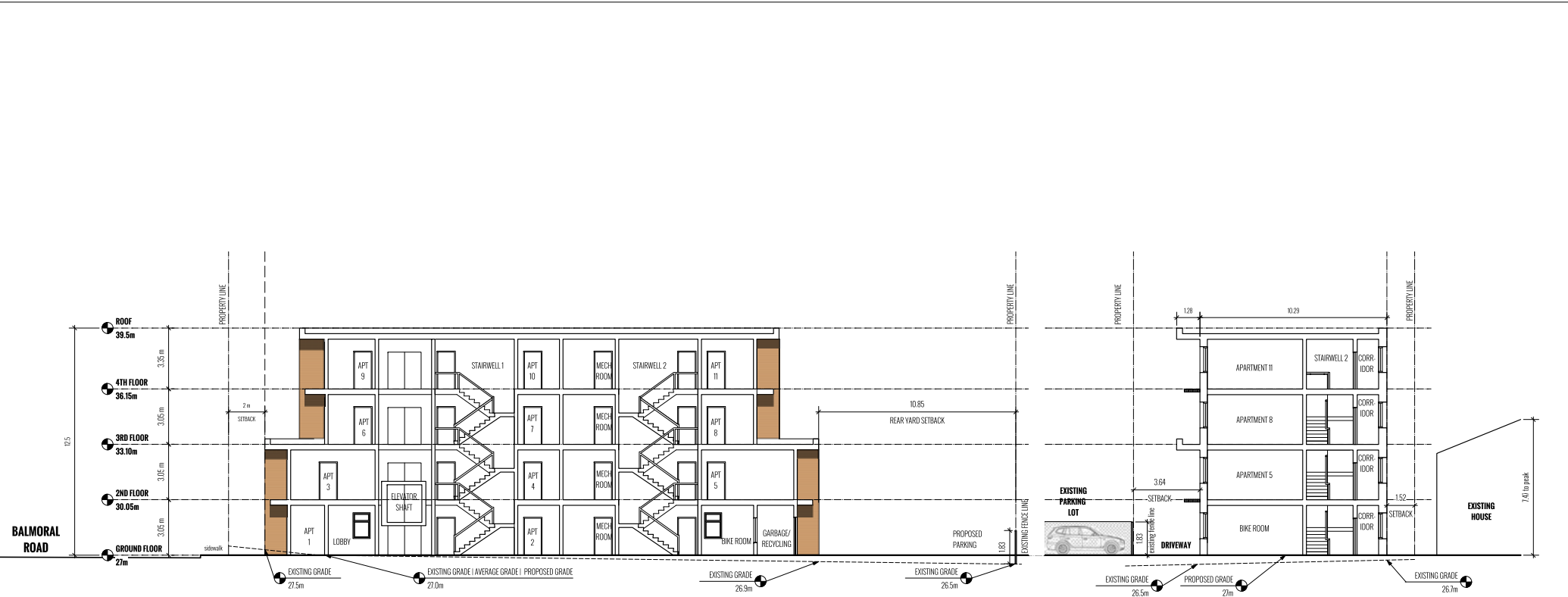
SOUTH ELEVATION

EAST ELEVATION

MATERIALS

- ① Variable Charcoal Brick
- ② Stained Wood Siding
- ③ Stucco
- ④ Glazing
- ⑤ Screen
- ⑥ Brise Soleil





LONG SECTION

SHORT SECTION

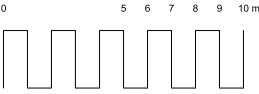


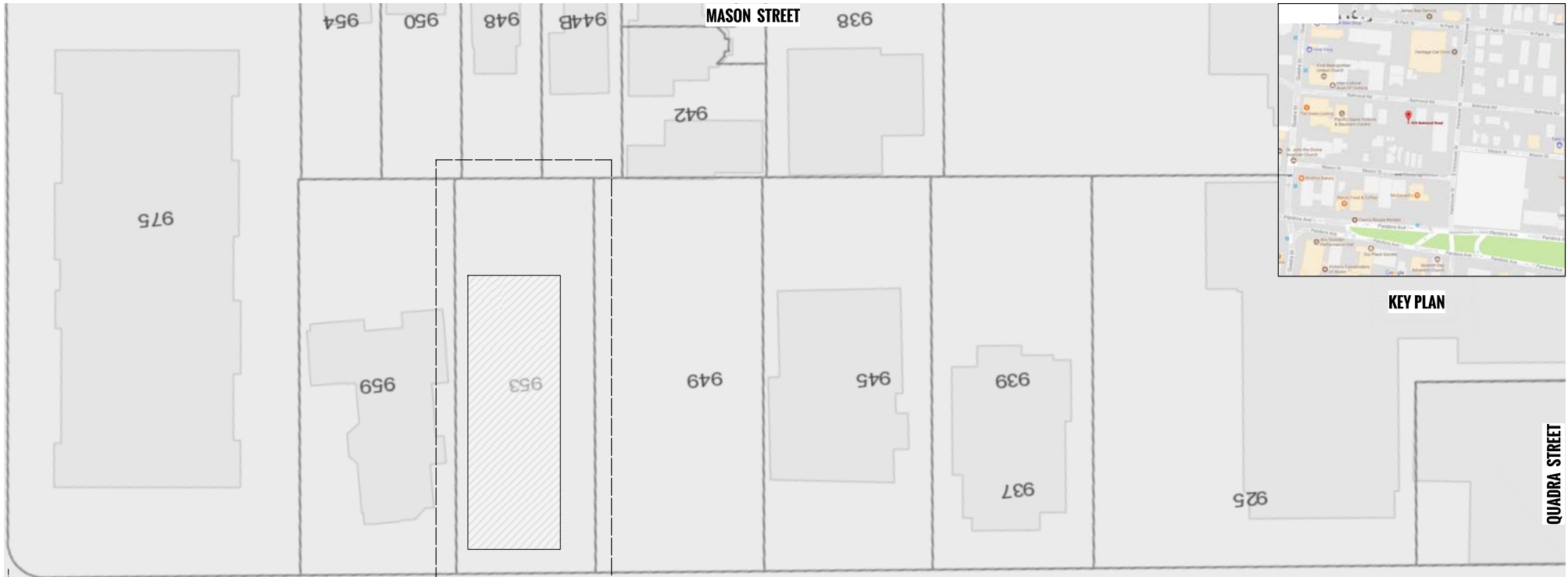


FIGURE 1: RENDERING OF NORTHWEST VIEW OF BUILDING



FIGURE 2: RENDERING OF NORTH VIEW OF BUILDING

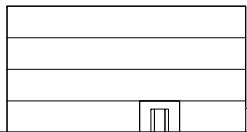
VANCOUVER STREET



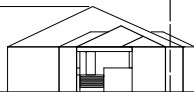
KEY PLAN

QUADRA STREET

1970's FOUR STOREY APARTMENT BUILDING



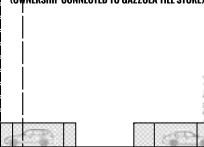
1950'S HOUSE/MEN'S SHELTER



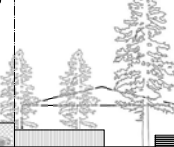
953 BALMORAL



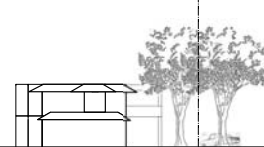
PRIVATE PARKING LOT
(OWNERSHIP CONNECTED TO GAZZOLA TILE STORE)



1950'S PRIVATE HOUSE



2000'S PRIVATE HOUSE



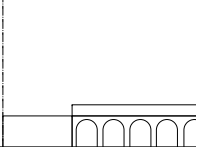
PARKING FOR OPERA CENTRE



PACIFIC OPERA CENTRE



GAZZOLA TILE



7.47

COAST
+ BEAM

CONTEXT ELEVATION | APARTMENTS @ 953 BALMORAL | SCHEMATIC DESIGN FOR DEVELOPMENT PERMIT & REZONING APPLICATION | MARCH 2019 | SCALE NTS

INTER-CULTURAL ASSOCIATION OF GREATER VICTORIA



PRIVATE APARTMENT BUILDING

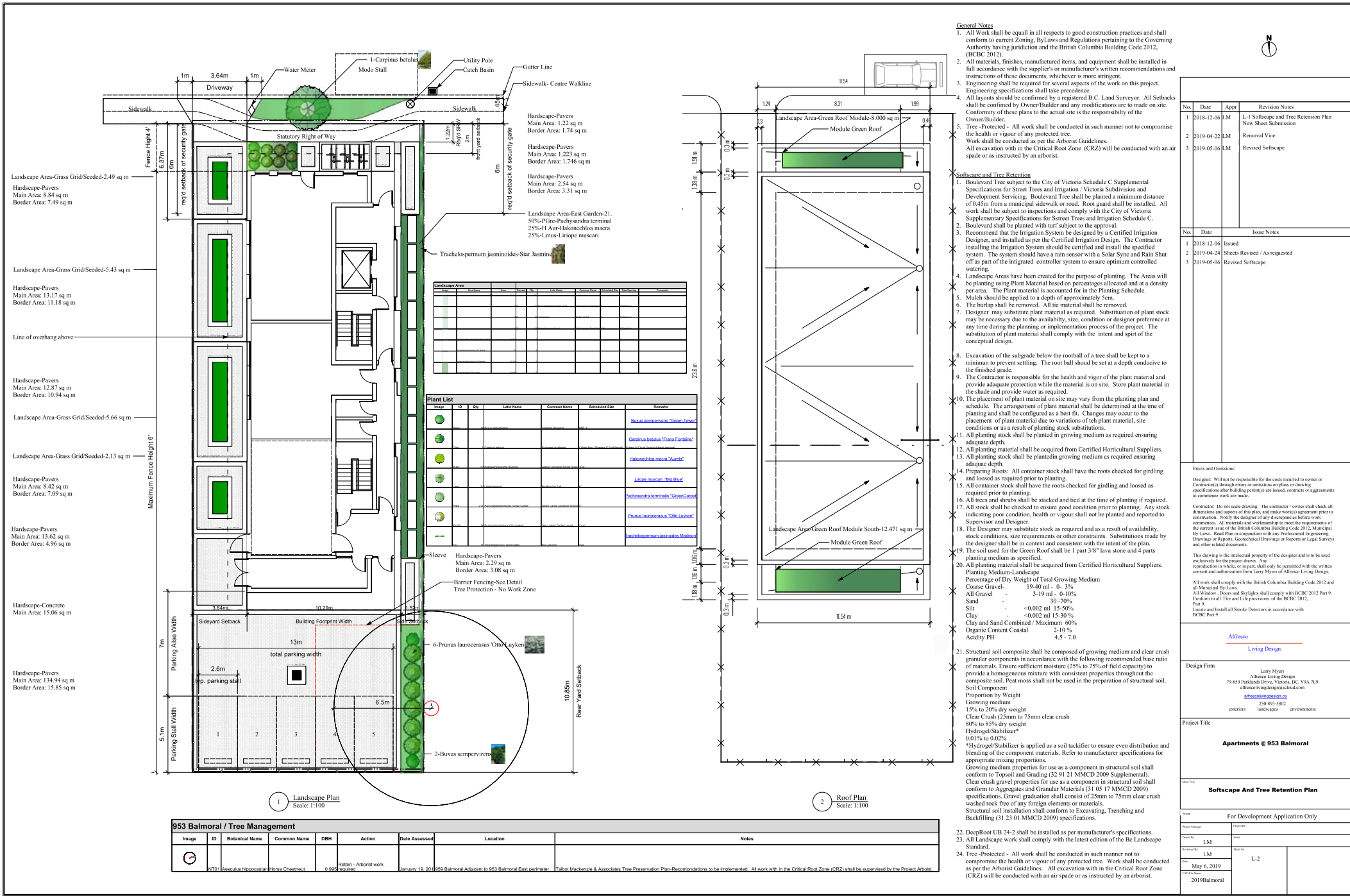


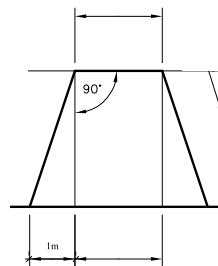
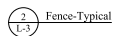
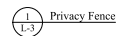
CANADIAN LINEN AND UNIFORM SERVICE



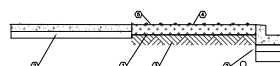
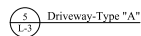
HOLLIS WEALTH SERVICES





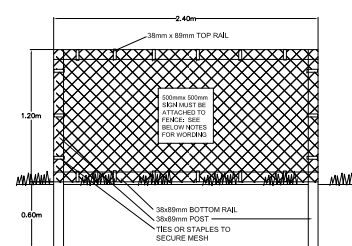


TYPE 'A'



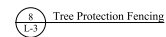
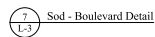
NOTES:

1. APPROVED, COMPACTED GROWING MEDIUM PLACED AS PER MMCD 32 91 21 TABLE 3.
2. FOR CURB, SIDEWALK, ASPHALT, DUCT AND ROAD BASE, REFER TO MMCD AND CITY OF VICTORIA SUPPLEMENTARY STANDARDS.
3. APPROVED SUBGRADE TO MMCD AND CITY OF VICTORIA SUPPLEMENTARY STANDARDS.
4. SOD OR SEED, AS SPECIFIED TO MMCD AND CITY OF VICTORIA SUPPLEMENTARY STANDARDS.
5. SOD NOT TO BE REINFORCED WITH MESH.



TREE PROTECTION FENCING

1. FENCE WILL BE CONSTRUCTED USING 38 mm x 89mm WOOD FRAME; TOP, BOTTOM AND POSTS * USE RANGE SNOW-FENCING MESH AND SECURE THE WOOD FRAME WITH "ZIP" TIES OR GALVANIZED STAPLES.
2. ATTACH A 500mm X 500mm SIGN WITH THE FOLLOWING WORDING: WARDEN-TREE PROTECTION AREA, THIS SIGN MUST BE AFFIXED ON EVERY FENCE OR AT LEAST EVERY 10 LINEAR METERS.
- * IN ROCKY AREAS, METAL POSTS (T-BAR OR REBAR) DRILLED INTO ROCK WILL BE ACCEPTED.



No.	Date	Issue Notes
1	2018-12-06	Issued
2	2019-04-24	Detail Sheet Updated
3	2019-05-06	Sheet Revised

Errors and Omissions:

Designer: Will not be responsible for the costs incurred to owner or Contractor(s) through errors or omissions on plans or drawing specifications after building permit(s) are issued, contracts or agreements to commence work are made.

Contractor: Do not scale drawing. The contractor / owner shall check all dimensions and aspects of this plan, and make work(s) agreement prior to construction. Notify the designer of any discrepancies before work commences. All materials and workmanship to meet the requirements of the current issue of the British Columbia Building Code 2012, Municipal By-Laws. Read Plan in conjunction with any Professional Engineering Drawings or Reports, Geotechnical Drawings or Reports or Legal Surveys and other related documents.

This drawing is the intellectual property of the designer and is to be used exclusively for the project drawn. Any reproduction in whole, or in part, shall only be permitted with the written consent and authorization from Larry Myers of Alfresco Living Design.

All work shall comply with the British Columbia Building Code 2012 and all Municipal By-Laws.
All Window , Doors and Skylights shall comply with BCBC 2012 Part 9.
Conform to all Fire and Life provisions of the BCBC 2012, Part 9.
Locate and install all Smoke Detectors in accordance with BCBC Part 9.

Alfresco

Living Design

Design Firm

Larry Myers
Alfreds Living Design
79-850 Parklands Drive, Victoria, BC, V9A 7L9
alfredslivingdesign@icloud.com
alfredslivingdesign.ca
250-893-5802
exteriors landscapes environments

Apartments @ 953 Balmoral

Sheet Title

Detail

Stamp For Development Application Only

Project Manager	Project ID	
Drawn By LM	Scale	
Reviewed By LM	L-3	
Date May 6, 2019		
CAD File Name 2019Balmoral		



Advisory Design Panel Report

For the Meeting of October 24, 2018

To: Advisory Design Panel **Date:** October 12, 2018
From: Leanne Taylor, Senior Planner
Subject: Development Permit with Variance Application No. 000506 for 953 Balmoral Road

EXECUTIVE SUMMARY

The Advisory Design Panel (ADP) is requested to review a Development Permit Application for 953 Balmoral Road and provide advice to Council.

The proposal is to construct a four-storey multi-unit residential building consisting of approximately 11 rental units. The following policy documents were considered in assessing this Application:

- *The Official Community Plan (OCP, 2012)*
- *Downtown Core Area Plan (2011)*
- *Guidelines for Fences, Gates and Shutters (2010)*
- *Advisory Design Guidelines for Buildings, Signs and Awnings (1981).*

Staff are looking for commentary from the Advisory Design Panel on the following items:

- overall size, scale and massing of the building
- window size, shape and placement of the building
- landscaping and outdoor open space
- residential entryways and articulation along the building base
- the transition between the public and private realm.

The Options section of this report provides guidance on possible recommendations the Panel may make, or use as a basis to modify, in providing advice on this Application.

BACKGROUND

Applicant: Mr. Rajinder Sahota
Method Built Homes

Architect: Ms. Pamela Úbeda, MAIBC
Coast and Beam

Development Permit Area: Development Permit Area 3 (HC): Core Mixed Use Residential

Heritage Status: N/A

Description of Proposal

The proposal is to construct a four-storey multi-unit residential building consisting of approximately 11 rental units. The development has a proposed floor space ratio (FSR) of 1.38:1. Concurrent with this Application is Rezoning Application No. 00598. The proposal includes the following major design components:

- low-rise building form containing contemporary-style design features, including a flat roofline, larger windows on the third and fourth storeys, and modern finishes
- exterior materials include brick, wood siding, stucco and aluminium privacy screen
- third and fourth storeys stepped back 2m
- one ground floor unit with a front entrance facing the road
- recessed main entrance into the building
- gated entryway into the site and to access the parking in the rear yard
- permeable pavers for driveway and surface parking lot
- no soft landscaping
- a bike room for 16 Class 1 bicycle parking spaces, and a bicycle rack for six bikes near the front entrance.

The following data table compares the proposal with the existing R-2 Zone, Two Family Dwelling District, as well as the R3-1 Zone, Multiple Dwelling District, which is seen as a comparable zone as it anticipates similar uses at a similar density. However, there are still numerous aspects of the proposal that would still not meet the requirements of the R3-1 Zone. An asterisk is used to identify where the proposal is less stringent than the R3-1 Zone.

Zoning Criteria	Proposal	Existing R-2 Zone	Zone Standard R3-1 Zone	OCP Policy	DCAP
Site area (m ²) – minimum	671.50 *	555.00	920.00		
Density (Floor Space Ratio) – maximum	1.38:1 *	0.50:1	1.20:1	2:1	2:1
Total floor area (m ²) – maximum	929.50 *	280.00	805.80		
Lot width (m) – minimum	15.48	15.00	n/a		
Height (m) – maximum	12.19	7.60	18.50		
Storeys – maximum	4.00	2	6	6	6
Site coverage (%) – maximum	43.00 *	40.00	30.00		
Open site space (%) – minimum	15.30 *	30.00	30.00		
Setbacks (m) – minimum					
Front	2.00 *	7.50	10.50		

Zoning Criteria	Proposal	Existing R-2 Zone	Zone Standard R3-1 Zone	OCP Policy	DCAP
Setbacks (m) – minimum					
Rear	10.85	15.20	6.10		
Side (east)	1.52 *	1.55	6.10		
Side (west)	3.64 *	3.00	6.10		
Parking – minimum					
Residential	4 *	9	9		
Visitor	1	1	1		
Bicycle parking stalls – minimum					
Class 1	16	14	14		
Class 2	6	6	6		

Sustainability Features

The applicant has not identified any sustainability features associated with this proposal.

Consistency with Policies and Design Guidelines

Official Community Plan

The subject property is designated Core Residential in the *Official Community Plan* (OCP, 2012), which supports a diverse range of housing types including low and mid-rise multi-unit residential buildings. The OCP also identifies this property within Development Permit Area 3 (HC): Core Mixed Use Residential, which supports a “high-quality of architecture, landscape and urban design that reflects the function of a major residential centre on the edge of a central business district in scale, massing and character.” The design guidelines contained in the *Downtown Core Area Plan* (DCAP), *Advisory Design Guidelines for Buildings, Signs and Awnings* (1981), and *Guidelines for Fences, Gates and Shutters* (2010) apply to the proposed building.

Neighbourhood Plan

The subject property is within the Residential Mixed-Use District in the *Downtown Core Area Plan* (DCAP), which supports multi-residential development up to six-storeys and a floor space ratio up to 2:1. The proposal for a four-storey multi-unit residential building with a FSR of 1.38:1 complies with the height and density policies outlined in DCAP.

Regulatory Considerations

Proposing a four-storey building on a lot with a site area of 671.5m² is tight and compromises the site planning with respect to providing sufficient landscaping and open site space, as well as impacting the relationship with adjacent properties and influencing the redevelopment of those lots in the future. By comparison, the standard R3-1 Zone requires a minimum lot area of 920.00m² and allows a maximum FSR of 1.2:1 for a four-storey building. The zone also incorporates larger setbacks to allow for some breathing room between neighbouring buildings.

A new, site-specific zone would be required to facilitate this development, and variances for setbacks, site coverage and open site space would be required. The regulations in the new zone would be similar to the R3-1 Zone, Multiple Dwelling District, except for the density provisions. The following variances would be required:

- reduce the front yard setback from 10.50m to 2m
- reduce the side (east) yard setback from 6.10m to 1.52m
- reduce the side (west) yard setback from 6.10m to 3.64m
- increase site coverage from 30% to 43%
- reduce open site space from 30% to 15.30%.

ISSUES AND ANALYSIS

The following sections identify and provide a brief analysis of the areas where the Panel is requested to provide commentary.

Overall Size, Scale and Massing

The OCP contains design guidelines that speak to the overall massing of a building and its visual impact on the site and adjacent properties. The building is long and presents a large volume on a 672m² lot. The side yard setback along the east side is only 1.5m from the property line, which would impact future development, window placement and access to sunlight on the adjacent property. The Panel's input on the overall size, scale and massing of the proposed building would be welcomed.

Window Size, Shape and Placement

The *Advisory Design Guidelines for Buildings, Signs and Awnings* contain a design guideline pertaining to building fenestration and the importance of arrangement, proportion and pattern of windows, as well as the relationship between solids and voids. The applicant is proposing substantial glazing on the third and fourth storeys along the north and south elevations; however, the windows at the ground level and second storey are smaller and present a different pattern compared to the upper storeys. A repetitive window placement along the west and east elevations is being proposed. The ADP's input on window size, shape and placement would be welcomed.

Landscaping and Outdoor Open Space

Appendix 7: Building Design Guidelines in the Downtown Core Area Plan (DCAP) encourages on-site open space such as courtyards, forecourts, plazas, patios, gardens, roof top patios/gardens for high density residential buildings that is well-designed, safe, active, visible and illuminated to encourage their use. The DCAP also encourages residential dwelling units to have direct access or views of the onsite open space. The applicant is proposing no on-site

open space or soft landscaping (plants or trees). Hardscape pavers would be introduced throughout the site and the rear yard would be a surface parking lot. The Panel's input on landscaping and outdoor open space would be welcomed.

Entryways and Articulation along the Building Base

The design guidelines outlined in *Appendix 7: Building Design Guidelines* of the DCAP encourage building entrances that are clearly identifiable from the street, and ground floor residential dwellings located adjacent to a street that provide at-grade individual entrances with direct connections to the public sidewalk, in addition to visually articulated designs and quality architectural materials and detailing in building bases, to enhance visual interest for pedestrians. One of the ground floor dwelling units and the main residential entryway would be facing the street. The ADP's input on the entryways and articulation along the building base would be welcomed.

Transition between the Public and Private Realm

Appendix 7: Building Design Guidelines in the DCAP contains a design guideline that encourages raised terraces, forecourts, landscaping, screening fences and gates to enhance residential entrances and to assist with distinguishing between the public and private realm. The Panel's input on the transition between the public and private realm and the influence this may have on the proposed entryways would be welcomed.

OPTIONS

The following are three potential options that the Panel may consider using or modifying in formulating a recommendation to Council:

Option One

That the Advisory Design Panel recommend to Council that Development Permit Application No. 000506 for 953 Balmoral Road be approved as presented.

Option Two

That the Advisory Design Panel recommend to Council that Development Permit Application No. 000506 for 953 Balmoral Road be approved with the following changes:

- as listed by the ADP.

Option Three

That the Advisory Design Panel recommend to Council that Development Permit Application No. 000506 for 953 Balmoral Road does not sufficiently meet the applicable design guidelines and policies and should be declined (and that the key areas that should be revised include):

- as listed by the ADP, if there is further advice they would like to provide on how the Application could be improved.

ATTACHMENTS

- Subject Map
- Aerial Map

- Plans date stamped January 18, 2018
- Letter to Mayor and Council dated November 10, 2017
- Letter to Mayor and Council dated May 7, 2018
- Letter to Mayor and Council dated August 17, 2018.

cc: Mr. Rajinder Sahota of Method Built Homes; Ms. Pamela Úbeda of Coast and Beam.

**MINUTES OF THE
ADVISORY DESIGN PANEL MEETING
HELD WEDNESDAY OCTOBER 24, 2018**

1. THE CHAIR CALLED THE MEETING TO ORDER AT 12:05 PM

Present: Jesse Garlick (Chair); Elizabeth Balderston; Paul Hammond; Carl-Jan Rupp; Stefan Schulson

Absent: Sorin Birliga; Justin Gammon; Deborah LeFrank
Jason Niles

Staff Present: Miko Betanzo – Senior Planner, Urban Design
Leanne Taylor – Senior Planner
Katie Lauriston – Secretary

2. MINUTES

Minutes from the Meeting held September 26, 2018

Motion:

It was moved by Elizabeth Balderston, seconded by Stefan Schulson, that the Minutes of the Meeting of Advisory Design Panel held September 26, 2018 be adopted as presented.

Carried Unanimously

3. APPLICATIONS

3.1 Development Permit with Variance Application No. 000506 for 953 Balmoral Road

The City is considering a Rezoning and Development Permit with Variance Application to construct a four-storey multi-unit residential building consisting of approximately 11 rental units.

Applicant meeting attendees:

PAMELA ÚBEDA
RAJINDER SAHOTA

COAST + BEAM
METHOD BUILT HOMES

Ms. Taylor provided the Panel with a brief introduction of the Application and the areas that staff is seeking advice on, including the following:

- the overall building size, scale and massing
- window size, shape and placement
- landscaping and outdoor open space
- entryways and articulation along the building base
- the transition between the public and private realm.

Mr. Sahota and Ms. Úbeda provided the Panel with a detailed presentation of the site and context of the proposal.

Questions of clarification were asked by the Panel on the following:

- what is the rationale behind the small second storey windows?
 - these windows were initially larger like those on the third and fourth storeys, but their size was reduced by request of the Planning department
- are the windows on the east and west façades intended to be recessed or punched?
 - the intent is for the windows to be set back within the thick exterior stucco wall, to create interest
 - there has been less and less time spent on the renderings, because this is the fourth design iteration
- are there balconies proposed?
 - no, these have been removed
- why were the previously proposed balconies removed?
 - the balconies were removed at the City's request to mitigate overlook concerns
- has the building footprint changed from the previous iteration where six storeys and a landscape buffer were proposed?
 - hard landscaping is proposed due to feedback from the property management company for similar rental buildings; hardscaping will be more easily maintained and better looking
 - the removal of the carshare parking stall in the front yard, the required Statutory Right of Way and bicycle storage also reduce the site's potential for greenspace
 - the entrance from the driveway was removed as requested by the City, and the opportunity for vegetation was lost because greenery cannot be added on the exit
 - boulevard trees could be provided through the street remediation
- is there opportunity for more of a transition from public to private space at the front of the building?
 - it is not really feasible, as it would be very difficult to plant trees at the front
 - the front setback cannot be increased to accommodate trees, as the suites are already as small as is feasible
- when will the Statutory Right of Way be utilized by the City?
 - Ms. Taylor clarified that Statutory Right of Ways can be requested as a part of any rezoning application, and that the frontage would have to be constructed to future plans for Balmoral Road
 - Ms. Taylor also noted that the applicable design guidelines do not encourage front yard parking stalls, and that the Transportation division would likely support the carshare vehicle being located on the street; however, the parking space on-site has to be secured by covenant so that it cannot be removed
- would the carshare vehicle be available to all members of the carshare program?
 - yes
- would public access be required to access the carshare vehicle?
 - yes
- have shadow studies been conducted?
 - yes; the proposal will shadow the neighbouring Cool-Aid housing in the evenings

- none of the other neighbours will be affected
- is bicycle parking provided?
 - 16 secure and six visitor stalls are proposed
- is the hardscaped walkway at the east required as a walkway, or can more soft landscaping be put in its place?
 - although it is not required to be hardscaped, the goal with the landscaping was to ensure that it was as attractive as possible over the long term
- so a walkway at the east is not required?
 - correct; however, the applicants are concerned about the amount of camping in the area and seek to address this concern through design
- why does the current proposal have a larger footprint with less soft landscaping, compared to when the proposal was six storeys tall?
 - if the Panel's recommendation is to include soft landscaping at grade, this can be changed and would be less costly
 - a previous design iteration had expensive green elements such as a green wall, but any soft landscaping will become run-down due to the type of long-term usage in the neighbourhood
 - hardscaping will be more easily maintained and better looking
- do fire safety standards allow the proposed wood siding at the east?
 - the applicants are not certain, but the material could be replaced with a less combustible material
- can the building be moved over to allow more room for a walkway at the east side?
 - the area at the east is not meant as walkway, it is just a setback.

Panel members discussed:

- difficulty in evaluating the proposal as presented, with very little information compared to other rezoning applications
- the need to provide a shadow study and 3D renderings
- need to improve consistency in renderings, including window detailing and clarification on the depth of the façade
- need for an additional level of refinement in the plans, including resolving the design of the rain water leaders to ensure that they are not simply tacked onto the façades, providing a secondary exit and ensuring that doors do not swing into the drive aisle
- many desirable features having been lost in this design iteration as compared to previous plans
- the proposal's limited contributions to the public realm and the street's need for improvement
- the provision of reliable rental units as not being sufficient to warrant a lack of contributions to the public realm
- recognition of the need for the building to be durable while still making a long-term contribution to the public realm
- the need for greenery and trees on the site
- opportunity for trees and planters at the front of the building that will not be as easily walked on, to create a transition between the public and private realm
- the need to find a compromise in the landscaping to include easily maintained green space
- opportunity for the parking area to be developed into a mixed use area, to provide outdoor amenity space as well as vehicle storage

- appreciation for the proposed permeable paving
- desire for communal space on the ground plane
- the need for outdoor public or private space
- opportunity for soft landscaping elements between the sidewalk and the front entry to create a sense of entry and transition between public and private realms and to provide eyes on the street
- concern for the appropriateness of the front door without a transition space to separate public and private realms
- concern for the liveability of the ground floor units, given the proximity of the front door to the sidewalk as well as the proximity to the drive aisle, the limited light, and the lack of outdoor space for the middle ground floor unit
- appreciation for the general north-south orientation of most units
- no issue with the proposed height or number of storeys; however, the site is small and is not being developed in context with other sites
- desire for the lot to be developed in consolidation with adjacent sites, but understanding that this has not been achievable
- the building's mass being too great for the site, eliminating any ground-level amenity space
- the proposal's many variances to the zone contributing to the site's lack of a green buffer
- concern for the eastern wall's length and shadowing impacts on adjacent properties
- opportunity to shift the building's massing, possibly by adding one storey, to reduce and adjust the building footprint, mitigate the impact of a long eastern wall to neighbours, and maintain the proposed density
- desire for a deliberate relationship between the window size of the lower storeys and the upper storeys
- desire to see the window recesses implemented as rendered.

Motion:

It was moved by Jesse Garlick, seconded by Elizabeth Balderston, that Development Permit with Variances Application No. 000506 for 953 Balmoral Road be approved subject to the following changes:

- increase the side yard setbacks and redistribute the massing to reduce negative impacts on neighbours' properties, allow for enhanced soft landscaping and improve liveability
- provide design consideration to enhanced landscaping, attention to street frontage, main entrance sequence, private walk-up entrance and rear parking lot area
- reconsider the privacy of ground-oriented suites
- update drawings to include a more consistent depiction of the proposal, including how the windows are detailed, the depth of the façade, shadow studies and 3D renderings
- consider the entrances in context of the public realm and streetscape
- consider the addition of balconies for the upper units.

Carried Unanimously

06 March 2019

Method Built Homes Ltd.
The Garage
4566 Cordova Bay Road
Victoria, British Columbia
V8X 3V5

Attn: Mayor and Council
City of Victoria
1 Centennial Square
Victoria, British Columbia
V8W 1P6

Dear Ms. Mayor and Members of Council,

Re: 953 Balmoral Avenue: 11-unit purpose-built workforce apartment building – Rezoning and DP

Further to your 06 September 2018 motion (appended below at **Appendix A**) that this Rezoning/DP Application proceed to public hearing, please find enclosed an amended set of drawings addressing the comments of the ADP.

1. The side yard setbacks have incorporated additional soft landscaping to improve liveability. The ADP also suggested redistributing existing massing upwards to a 5th and/or 6th floor. However, though Staff initially supported the OCP maximum of 6 floors, Staff subsequently pushed back on this concept when it was presented with an initial submission and iteration. Staff subsequently has reiterated unequivocally that anything more than 4 floors would not be supported by Staff.
2. Enhanced landscaping, attention to street frontage, main entrance sequencing, private walk-up entrance, and rear parking lot area soft scaping have all been addressed in this revised landscape plan.
3. Privacy of ground-oriented suites has been considered and addressed, to the extent possible, with soft landscaping.
4. The drawings have been updated to depict a more consistent design and 3D renderings.

5. Entrances have been enhanced in the context of public realm and streetscape.
6. An earlier submission and iteration had balconies for upper floor units, but Staff expressed significant concern with overlook over neighbouring properties, and as such, balconies were removed.

Given the ongoing increases in both hard and soft costs in the market over the six years it has taken to get this proposal to this stage, and the commitment of this proposal to be a purpose-built workforce apartment building with appropriate 25-year s.219 covenants, the undersigned requests a 10-year tax holiday for this proposal to ensure its economic viability and construction.

I trust the foregoing is of assistance as these applications proceed to public hearing.

Yours very truly,

A handwritten signature in dark ink, consisting of the letters 'RS' in a stylized, cursive font.

Rajinder S. Sahota

Appendix A

H.1.a.j

953 Balmoral Road –Rezoning Application No. 00598 and Development Permit with Variance Application No. 000506 (North Park)

Moved By Councillor Loveday

Seconded By Councillor Coleman

Rezoning Application No. 00598

That Council instruct staff to prepare the necessary Zoning Regulation Bylaw Amendment that would authorize the proposed development outlined in Rezoning Application No. 00598 for 953 Balmoral Road, that first and second reading of the Zoning Regulation Bylaw Amendment be considered by Council, and a Public Hearing date be set once the following conditions are met:

1. Preparation of the following documents, executed by the applicant to the satisfaction of City Staff:
 - a. Statutory Right-of-Way of 1.22m on Balmoral Road.
2. The applicant provide an amenity contribution in the amount of \$76,694.69 toward the Local Amenities Reserve Fund in accordance with the City of Victoria Density Bonus Policy to the satisfaction of City Staff.
3. Following consideration of Rezoning Application No. 00487, if approved, that Council authorize staff to prepare and enter into an Encroachment Agreement for a fee of \$750 plus \$25 per m2 of exposed shored face during construction, to the satisfaction of the City staff.

Development Permit with Variance Application No. 000506

That, subject to review by the Advisory Design Panel and report back to the Committee of the Whole, that Council, after giving notice and allowing an opportunity for public comment at a meeting of Council, and after the Public Hearing for Rezoning Application No. 00598, if it is approved, consider the following motion:

“That Council authorize the issuance of Development Permit with Variance Application No. 000506 for 953 Balmoral Road, in accordance with:

1. Plans date stamped January 18, 2018
2. Development meeting all Zoning Regulation Bylaw requirements, except for the following variances:
 - a. reduce the required number of parking spaces from 12 to 5
 - b. Part 3.3(10): reduce the front yard setback from 10.50m to 2.00
 - c. Part 3.3 (10): reduce the side (east) yard setback from 6.10m to 1.52m

- d. Part 3.3(10): reduce the side (west) yard setback from 6.10m to 3.64m
- e. Part 3.3(4)(1): increase the site coverage from 30% to 43%vi.Part 3,3(4)(6)(1):
reduce the open site space from 30% to 15.30%.
- 3. Registration of legal agreements on the property's title to secure the MODO Car Share Vehicle and parking space, car share memberships, one monthly transit pass for each unit over a period of three years (396 monthly passes), and one bicycle for each unit to the satisfaction of City Staff.
- 4. Revise the landscape plan to indicate floating pavement where the proposed parking spaces overlap with the tree's critical root zone in accordance with the arborist report prepared by Talbot Mackenzie & Associates.
- 5. The Development Permit lapsing two years from the date of this resolution."

FOR (8): Mayor Helps, Councillor Alto, Councillor Coleman, Councillor Loveday, Councillor Lucas, Councillor Madoff, Councillor Thornton-Joe, and Councillor Young

OPPOSED (1): Councillor Isitt

07 May 2018

Method Built Homes Ltd.
The Garage
4566 Cordova Bay Road
Victoria, British Columbia
V8X 3V5

Attn: Mayor and Council
City of Victoria
1 Centennial Square
Victoria, British Columbia
V8W 1P6

Dear Ms. Mayor and Members of Council,

Re: 953 Balmoral Avenue – 11-unit purpose-built workforce apartment building

Further to my letters of 10 November 2017 and 20 March 2018 (enclosed herein for reference) and the Committee of the Whole meeting of 19 April 2018 and subsequent council meeting, please amend the proposal to include a Housing Agreement to provide rental accommodation for **25 years**.

I understand that a Housing Agreement was a critical issue when council considered this proposal. The vote was 4-4 at COTW and at the subsequent council meeting for this proposal to advance to public hearing. With a commitment now of a Housing Agreement, thereby securing 11 additional and much needed and workforce apartment units at the edge of the downtown core, I trust this proposal will proceed to public hearing.

With respect to the request to refine the proposal to address staff concerns regarding height, setbacks, density, site coverage, and design, please note that although Staff's feedback has evolved over the past five years with ambiguous, subjective and moving goalposts, the following can be distilled from the most recent feedback:

- 1) Reducing the height to 2-3 stories ~ 2.5 stories;
- 2) Increasing the setbacks substantially;
- 3) Decreasing the density;
- 4) Decreasing the site coverage; and
- 5) Proposing a design that retains the character of a single-family residence.

The take-away from this feedback is that Staff would likely support a proposal that looked like a single-family residence, but had increased density from the current duplex zoning; a triplex or, at best, four-plex is the likely outcome of these preferences.

Constructing such a proposal, with the cost of construction where it is in the current market, would force this developer to build stratified townhomes for sale, as opposed to building a purpose-built workforce apartment building. This is not a market that we are interested in catering to.

As a consequence of the above, of the alternatives to develop a stratified triplex (or possibly fourplex) to the satisfaction of Staff and Council, with the related development and enhanced engineering costs versus a stratified modern duplex, which the subject site is currently zoned for, we would build a modern duplex that could be complete for occupancy within six months of today. Ironically, such a duplex would have a site coverage of 0.5:1, which is more than the current proposal. Additionally, such a duplex, with a walk-out basement, would have density that is approximately 70% of the current proposal. The setbacks for such a duplex would be substantially similar, with the exception of the front yard setback, to the current proposal. Finally, depending on the final design, not subject to municipal oversight, the height would be approximately 1-1.5 stories shorter than the current proposal.

I trust the foregoing is of assistance as you consider the revised proposal.

Yours very truly,

A handwritten signature in dark ink, appearing to read 'RS'.

Rajinder S. Sahota

17 August 2018

Method Built Homes Ltd.
The Garage
4566 Cordova Bay Road
Victoria, British Columbia
V8X 3V5

Attn: Mayor and Council
City of Victoria
1 Centennial Square
Victoria, British Columbia
V8W 1P6

Dear Ms. Mayor and Members of Council,

Re: 953 Balmoral Avenue – 11-unit purpose-built workforce apartment building

I write further to the following motion from Council:

Postpone consideration of the application for 2 months and request the applicant to meet with the adjoining neighbours to explore possible consolidation of the adjoining lots.

I have had an opportunity to discuss in detail the business case for the possible consolidation of this site with the neighbouring sites as requested by Council. Unfortunately, for a number of reasons, consolidation is not feasible in the context of an affordable housing, purpose-built apartment project. As for-profit stratified condominiums to be sold at market, it may be feasible.

With respect to some of the outstanding concerns of Council when this proposal was last considered, please note the following.

1) Whether the developer considered height and setback changes. The response from staff was no.

As noted in my letter of 07 May 2018, substantial consideration was given to these issues. In fact, the reason why it has taken five years to get to this point is attributable in large part to issues of height and density. As noted in this prior letter, reducing the height and increasing the setbacks further would transform this proposal from an 11-unit, housing agreement locked, purpose-built, workforce apartment building, into (at best) a 4-unit stratified townhouse project.

As noted in my letter, the increased costs and ongoing delays associated with a rezoning and DP process, and related soft costs (professional fees) for a 4-unit stratified townhouse project outweigh the benefits of simply constructing a high-end urban oasis style private

duplex for two families (the site is already zoned for the later). As noted in my previous letter the height would in effect only be reduced by 1.5 stories, while the setback changes in this scenario would be negligible, apart from the front-yard setback.

Summary

Kindly note my previous letters to Council dated 03 April 2017, 10 November 2017, 20 March 2018, 07 May 2018, 30 May 2018, 11 June 2018, addressing outstanding questions with respect to the appropriate balance between development objectives and the provision of affordable housing in the current economic climate.

At the end of the day, as many of you accurately noted, this is a difficult decision for you to make.

From an economic perspective, as the developer, the relatively short-term return on a unique downtown duplex for two affluent families is similar to the long-term return on a larger investment in affordable rental housing, when accounting for the increased risk and capital associated with this proposal. Our goal with this proposal was to leverage what we believe to be an ideal location for affordable rental housing into something that is needed within this city.

Perhaps you are of the opinion that there is a significant profit margin in purpose-built workforce rental apartment buildings, but our analysis is that given the cost of construction and land in Victoria, this is not the case; this is why you do *not* see a proliferation of developers – outside of the non-profit societies with significant government funding like Pacifica, where I sit on the board – building out workforce rental projects in Victoria.

One thing is certain. One of two buildings will be seen on this site within the next year. In either case, the building will establish what is to come at this end of the block for the next 60 years; it will set the precedent.

The first option is the one before you, which after five years has been refined to include a 25-year housing agreement and a commitment to provide 2 of 11 units at below-market rates. This will fill what has been identified in the OCP as a glaring need within the City of Victoria; affordable rental units. It resembles, in character, what has been built at 1032 North Park, a block away, and welcomed by the majority of the neighbourhood.

The second option is to decline this proposal at which point this developer will take immediate steps build out the site as it is currently zoned, thereby providing two relatively well-off families with the opportunity to live in high-end homes in a rapidly gentrifying neighbourhood at the very edge of the downtown core; an equally attractive option from a pure ROI perspective, but one which provides no positive externalities to the community as compared with the first option.

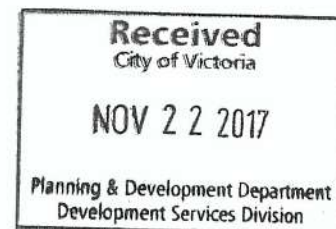
At this stage, the decision is whether or not to send this amended proposal to public hearing where you will benefit from public input. At the very least, such public input, respectful of democratic principles, should inform the ultimate decision.

Though difficult it may be, the choice is yours to make.

Yours very truly,

Rajinder S. Sahota
Enc.

10 November 2017



Method Built Homes Inc.
The Garage
4566 Cordova Bay Road
Victoria, British Columbia
V8X 3V5

Attn: Mayor and Council
City of Victoria
1 Centennial Square
Victoria, British Columbia
V8W 1P6

Dear Ms. Mayor and Members of Council,

Re: 953 Balmoral Road (the "Proposal" or "Site")

Please accept this application for the approval of a rezoning and development permit application for the Site.

The design and development of the Site began over five years ago in late October 2012. At that time, it was recommended by the former Local Area Planner Mr. Mike Wilson that this proposal proceed as a joint rezoning and development permit application and that a site-specific zone would be created for the Site. Mr. Wilson advised the following:

- 1) "[T]he highest-level plan is the Official Community Plan (OCP). The OCP designated the site as Core Residential. This designation generally envisions **multi-unit residential buildings**." (emphasis added)
- 2) "At a more detailed level, the Downtown Core Area Plan is also relevant to the site. For this site the plan envisions an **FSR of 2:1** (page 37) and a **maximum building height of 20m** (six residential stories) (page 89)." (emphasis added)
- 3) "A new zone for this site would have to contemplate up to **2:1 FSR** as per the guiding policy. It would be best to base your proposal on the guiding policies and design guidelines rather than an existing zone. **We would then craft a zone based on your proposal.**" (emphasis added)

Attached hereto at **Addendum A** is a copy of Mr. Wilson's email of 24 October 2012.

It is with this background and context that we undertook to design and develop a purpose-built **workforce rental apartment**. By way of background, we have worked diligently over the last five years with new Local Area Planner Ms. Leanne Taylor and Assistant Director of Development Services Ms. Allison Meyer on this application package.

We have undertaken three complete redesigns of this Proposal over the course of ongoing consultations and conversations with Ms. Taylor and the North Park Neighbourhood Association. In addition, we have conducted three separate CALUC meetings in relation to this Proposal.

First Concept

After initial informal discussions with the North Park Neighbourhood Association, our first concept proposed a four to six story brick Brownstone themed **workforce rental apartment** with underground parking. To offset the increased cost of underground parking for this concept, the building was designed to the property lines with no setbacks and contained an interior outdoor courtyard for the residents. Ms. Taylor expressed a concern with the density, height and setbacks of this proposal and was not overly impressed with the Brownstone concept.

Second Concept

Our second concept proposed a six-story modern **workforce rental apartment** with significant front, rear, and side yard setbacks and a **1.9:1 FSR**. As a result of much reduced density, this proposal included at grade parking at the rear of the proposal. This proposal was presented to the North Park Neighbourhood Association through a CALUC hearing. There were some concerns raised at the CALUC by one single-family residence owner to the South of the Site with respect to the height of the proposal and more generally with respect to the modern design aesthetic. Following similar concerns raised by Staff, and the economic climate at the time, we determined that reducing the proposal to four stories would render the proposal economically unviable as a workforce rental apartment building.

The owners of the adjacent properties to the East and West both support this six-story purpose built **workforce rental apartment** as the optimal proposal. Attached hereto at **Addendum B** are copies of such letters of support.

Third Concept

Given the change in the economic landscape related to housing, we determined that reducing the proposal to four stories would now be viable. We remained of the view, however, that a six-story proposal was best for the area as it had the added benefit of offering **three, 3-bedroom units** on the top two floors for **working families**. We presented this proposal once again at a CALUC and the response was generally quite positive, with the exception of the same neighbour to the South. Nevertheless, the support was not unanimous and Staff concerns with respect to height and setbacks remained. As a result, we finalized a third concept that proposed a four-story **workforce rental apartment** with significant front, rear, and side yard setbacks and with additional setbacks at the third and fourth floors as requested by Staff. This proposal envisions 11 one- and two-bedroom rental apartment units. The **FSR is 1.38:1**, well short of the 2:1 FSR referenced above.

Sister Purpose Built Rental Building

It is important to note that we recently designed, developed and completed an 11-unit purpose built **workforce rental apartment** at 1032 North Park Street ("North Park Project"), approximately one block from the Site. This project was completed in December 2016 and consists of a four-story building constructed on a 4930 square foot (458m²) site. The current Site is 7233 square foot site (672m²) or approximately **47% larger** than the site of the North Park Project.

The target renters for both the North Park Project and this Site are blue-collar, working class individuals/couples/families. The one bedrooms at the North Park Project **start at \$1100/month** and average \$1200/month; the two bedrooms start at \$1400/month and average \$1550/month. At the top end, there is one townhouse renting for **\$2000/month**.

Contrasting this Proposal with higher scale purpose built rental buildings or condominium buildings encompassing half- or full-city blocks may lead to improved design through the use of more expensive materials and/or the provisioning of better amenities for residents; however, the economics of such proposals will also inevitably lead to this becoming a condominium proposal (with stratified units for sale) and and/or a professional rental apartment with higher rents as opposed to workforce rental units.

Based on publicly available information, Hudson Walk Two, for example, rents one bedroom units that **start at \$1510/month** with an average rent much higher; two bedroom units rent for as high as **\$3095/month**. Clearly there is a demographic of professionals in Victoria who are prepared to pay these rents for apartment units in high-end purpose built rental apartments. However, these rents are **approximately 30% - 55% higher** than those we are setting and targeting, and as such make it difficult for blue-collar workers to live within

Victoria. Both our sister North Park Project and this Proposal target a workforce who provide invaluable services within Victoria.

Proposal

This application is being brought forward after careful review of the Official Community Plan and the provisions thereof dealing with the North Park Neighbourhood and the 900 block of Balmoral Road. The OCP envisages projects of a larger scale and scope than the one being proposed. For example, a 2:1 floor space ratio is envisaged in the OCP for this location. This Proposal is for an FSR of 1.38:1. Additionally, there is a need in Victoria for more **affordable** housing.

As you are no doubt aware, the North Park Neighbourhood is a rich and vibrant part of the City of Victoria. New, affordable housing, particularly an apartment building geared towards non-professional renters is precisely what this neighbourhood requires. There are other apartment projects that are targeting a more affluent demographic, but this project is vital to ensuring a diverse socio-economic mix within Victoria. This Proposal is being advanced after several re-designs over the course of nearly five years and after lengthy consultations with staff.

The only concern with this project lies with the parking ratio being proposed. There are 11 units proposed for this Proposal and servicing these units are five general parking stalls and one dedicated MODO stall. As outlined in a report from Boulevard Transportation Group, the significant transportation demand management (TDM) measures proposed are sufficient to offset the off-site parking that would otherwise be generated. Attached hereto at **Addendum C** is such report. In other words, the: (a) proximate location of the Proposal to downtown Victoria, including its immediate access to major bicycle routes, (b) substantial number of bicycle lockers provided to each unit, (c) provision of bicycles to each unit, (d) public transit passes provided to each unit, (e) public information provided to residents of the Proposal relating to TDM measures, and (f) purchase of a vehicle for the dedicated MODO stall respecting this Proposal, cumulatively offset the off-site parking demand created by this Proposal.

The need for quality, affordable housing in the City of Victoria is ever-present. Despite the provision of new high-quality, purpose-built apartments, the rents associated with those projects have been inaccessible to a significant segment of our population. The North Park Neighbourhood is an eclectic community that is looking to avoid further gentrification. This Proposal has been designed after taking into account the results of consultation with the North Park Neighbourhood Association. As a purpose-built **workforce rental apartment**, this Proposal is designed to meet the long-term needs of the local area and the City of Victoria more broadly.

There are certainly buildings being proposed and developed that offer more amenities and are generally more expensive. I recently submitted and spoke to Mayor and Council with respect to a proposed LEED GOLD office building which has broken ground at Dockside Green with an expected completion date of December 2018. However, the intended occupants of such buildings spend a lot more per square foot to occupy these spaces than the occupants of this Proposal; a Proposal that is geared for working people.

In addition to letters of support from the adjacent neighbours to the east and west of the Proposal, attached for your review at **Addendum D**, is a letters of support from a local community leader in the area of affordable housing, generated after an informal meeting held at the Parsonage Café in North Park (see **Addendum E**).

The environmental benefits of Victoria's workforce living within or adjacent to the same community within which it works cannot be overstated. By eliminating the need for single vehicle ownership, the Proposal contributes to environmental sustainability. By targeting Victoria's workforce, the Proposal contributes to social sustainability.

Summary

This Proposal has been designed specifically for this neighbourhood in accordance with the OCP, after consultation with the North Park Neighbourhood Association, and after consultation with the City of Victoria. The building's design and mass has been modified to accommodate the concerns of the NPNA. This includes a recent reduction in floors from six to four and a unit reduction from 17 to 11. Unfortunately, this has come with the elimination of three, 3-bedroom units geared towards families on the top two floors of the Proposal.

The only objective non-compliance concern is with respect to on-site parking. The provision of significant TDM measures more than offsets the potential off-site parking impact of this Proposal. This Proposal has been custom-designed to address specific needs identified by our community and warrants our strong support to move it along expeditiously to completion.

Kind regards,

Rajinder S. Sahota
Principal
Method Built Homes Inc.
www.methodbuilt.ca

ADDENDUM A

From: Rajinder Sahota [mailto:rajinder@methodbuilt.ca]
Sent: Tuesday, Oct 23, 2012 9:52 PM
To: Mike Wilson
Subject: Re: 953 Balmoral Rd.

Hi Mike,

Would you be able to give me an initial impression of your thoughts on possible development of this site and what the City would like to see here? I see from the OCP that this is likely a similar growth strategy as North Park but the current zoning may be different.

Please let me know.

Kind regards,

Rajinder Sahota

From: Mike Wilson <MWilson@victoria.ca>
To: "Rajinder Sahota" <rajinder@methodbuilt.ca>
Cc:
Bcc:
Date: Wed, 24 Oct 2012 16:07:38 +0000
Subject: RE: 953 Balmoral Rd.
Hi Raj,

With respect to planning policies for the area, the highest level plan is the Official Community Plan (OCP). The OCP designated the site as Core Residential. This designation generally envisions multi unit residential buildings. A detailed breakdown of the designation can be found here (Page 41): http://www.shapeyourfuturevictoria.ca/wp-content/uploads/2012/09/OCP_BOOK_WEB.pdf <http://www.shapeyourfuturevictoria.ca/wp-content/uploads/2012/09/OCP_BOOK_WEB.pdf>

At a more detailed level, the Downtown Core Area Plan is also relevant to the site. For this site the plan envisions an FSR of 2:1 (Page 37) and maximum building height of 20 m (six residential storeys) (page 89). See: <http://www.victoria.ca/EN/main/departments/planning-development/community-planning/downtown-plan.html> <<http://www.victoria.ca/EN/main/departments/planning-development/community-planning/downtown-plan.html>>

This property is also located within Development Permit Area 3: Core Mixed Use Residential. This DP Area provides design guidelines to be used when developing the design of the building. See Page 183/184: http://www.shapeyourfuturevictoria.ca/wp-content/uploads/2012/09/OCP_Part4_WEB.pdf <http://www.shapeyourfuturevictoria.ca/wp-content/uploads/2012/09/OCP_Part4_WEB.pdf> . Many of the relevant design guidelines are included in the Downtown Core Area Plan.

ADDENDUM A

A new zone for the site would have to contemplate up to 2:1 FSR as per the guiding policy. It would be best to base your proposal on the guiding policies and design guidelines rather than an existing zone. We would then craft a zone based on your proposal.

The foregoing is given for your convenience only and it should be clearly understood that you must satisfy yourself as to whether the existing or any proposed development would be in conformity with all applicable bylaws and policies of the City or any provincial or federal statutes or regulations.

If you require any further information please don't hesitate to give me call or e-mail.

Regards,

Mike

Mike Wilson, MCIP, RPP
Senior Planner - Urban Design
Development Services Division
Planning and Development Department
City of Victoria
1 Centennial Square, Victoria, BC, V8W 1P6
Phone: [250.361.0384](tel:250.361.0384) Fax: [250.361.0386](tel:250.361.0386)
www.victoria.ca <<http://www.victoria.ca>>

ADDENDUM B

July 24, 2017

Michael Rowe
949 Balmoral Road
Victoria, BC V8T 1A7

Mayor & Council
City of Victoria
1 Centennial Square
Victoria, BC V8W 1P6

Re: 953 Balmoral Road

I am writing to express my unqualified support for the development proposed by Method Built Homes at 953 Balmoral Road. I have owned the property adjacent to this site at 949 Balmoral Road for many years. In addition, I have owned and operated a prominent business on this block of Balmoral Road for many years.

I initially reviewed the six-story proposal Method Built Homes proposed for the site and supported that proposal. It was sensitive to the needs of the neighbourhood and was geared towards families by providing an additional three, 3-bedroom units over the top two floors. It was attractive, modern and current. The revised four-story proposal does not maximize the full potential the six-story proposal brought to the neighbourhood, and the region more generally, by offering additional accessible rental accommodations. This neighbourhood has had difficulty attracting investment capital for developments, and these proposals are welcomed.

As a result, I am fully supportive of the current four-story proposal from Method Built Homes, with the only reservation being that I would prefer the more densified six-story proposal that was an earlier concept.

Please feel free to contact me should you have any further questions.

Respectfully yours,

A handwritten signature in dark ink, appearing to read 'Michael Rowe', is written over a faint, circular official stamp.

Michael Rowe

July 23, 2017

Michael Forbes
959 Balmoral Road
Victoria, British Columbia
V8T 1A7

Mayor and Council
City of Victoria
1 Centennial Square
Victoria, British Columbia
V8W 1P6

Re: 953 Balmoral Road

I write with respect to the Development Permit application submitted for 953 Balmoral Road. I understand the application submitted is for an 11-unit rental apartment building covering 6 floors.

I own the property immediately adjacent to this proposal and to the East. I believe the No. 1000 Park neighbourhood and the 900-block of Balmoral in particular would benefit tremendously from this development. In fact, I prefer the earlier six-floor proposal as it also included 3, 3-bedroom units on the top two floors. The proposal falls within the Official Community Plan for this block and neighbourhood and would improve the area tremendously while bringing additional life and vibrancy with more residents. The block is within the downtown core and needs density to provide more affordable housing options for Victoria residents.

Please feel free to contact me should you have any further questions.

Respectfully yours,



Michael Forbes Bsc Pharm
250.882.3784

ADDENDUM C



953 BALMORAL ROAD

Parking Study

Prepared for: **Method Built**

Prepared by: **Watt Consulting Group**

Our File: **1906**

Date: **October 27, 2017**

1.0 INTRODUCTION

In November 2015, Watt Consulting Group was retained by Method Built to undertake a parking study for the proposed development at 953 Balmoral Road in the City of Victoria. The proposed development has undergone several architectural changes over the last several months. As a result, the content presented herein is an updated parking study from the report submitted on December 7, 2015.

The purpose of this study is to assess site parking demand and any off-site impacts. The study considers parking demand at representative multi-family residential sites, on-street parking conditions, and transportation demand management (TDM) programs.

1.1 LOCATION

The development site is located at 953 Balmoral Road in the City of Victoria. See [Map 1](#).

MAP 1 SUBJECT SITE



TABLE 1 VEHICLE OWNERSHIP AT REPRESENTATIVE SITES

Site	No. Units	Owned Vehicles	Ownership Rate (vehicles/unit)
975 Balmoral Road	38	10	0.26
1635 Cook Street	72	30	0.42
1035 North Park Street	79	21	0.27
1022 Pandora Avenue	40	13	0.33
1130 Pandora Avenue	45	24	0.53
1020 Pembroke Street	109	75	0.69
1630 Quadra Street	121	67	0.55
2310 Quadra Street	19	14	0.74
1017 Queens Avenue	27	11	0.41
1110 Queens Avenue	17	9	0.53
		Average	0.47

4.2 PREVIOUS STUDIES

Site observations and vehicle ownership information has been obtained for previous studies that are representative of the subject site. All study sites are rental and in representative context (i.e. location, access to transportation options) as the subject site. See **Table 2**. Results suggest average parking demand is 0.49 vehicles per unit and average vehicle ownership information is 0.50 vehicles per unit. Applied to the subject site, this results in approximately six resident vehicles.

TABLE 2 PARKING DEMAND FROM PREVIOUS STUDIES

Area	Observations	Vehicle Ownership
North Park ¹	0.56 vehicles per unit	0.53 vehicles per unit
Oak Bay ²	0.54 vehicles per unit	0.61 vehicles per unit
Outside of Downtown/North Park ³	0.39 vehicles per unit	0.37 vehicles per unit
Victoria West ⁴	0.46 vehicles per unit	0.49 vehicles per unit
Average	0.49 vehicles per unit	0.50 vehicles per unit

¹ Previous parking study completed in 2012 on North Park Street

² "The Clive" - See http://www.theclive.ca/pdfs/Clive%20Oak%20Bay%20Parking%20Study_FINAL_Feb4.pdf

³ "The Azzurro" - previous parking study completed in 2014 on Blanshard Street for affordable housing. Sites selected may have a lower parking demand

⁴ "Wilson's Walk" - previous parking study completed in 2014 on Wilson Street for affordable housing

5.0 TRANSPORTATION DEMAND MANAGEMENT

Transportation demand management (TDM) is the application of strategies and policies to influence individual travel choice, most commonly to reduce single-occupant vehicle travel. TDM measures should adopted be where possible to enhance travel options, minimize parking demand, and facilitate sustainable transportation.

The following summarizes the TDM options that are proposed and estimated impact of each in reducing parking demand.

5.1 CARSHARE

As discussed in Section 2.1, the subject site has immediate access a dedicated Modo carshare parking space / vehicle on Balmoral Road. The site plan identifies an on-site resident parking space for a Modo carshare vehicle in the future, should the on-street space be removed. The vehicle will be purchased by the proponent and memberships (valued at \$500 each) will be provided to each unit. Residents will be responsible for usage fees. With immediate access to a carshare vehicle, it is anticipated that carsharing will reduce resident parking demand.

Research has shown that carsharing programs have a significant impact on reducing vehicle ownership and thereby lowering parking demand. Below is a summary of key findings:

- One of the most comprehensive North American studies to date surveyed 6,281 households in carsharing organizations across the continent. The study found a statistically significant decrease in average vehicle ownership from 0.47 to 0.24 vehicles per household among households that joined carshare services, an approximately 50% reduction in vehicle ownership⁶.
- A study of carshare programs in the City of Toronto found that vehicle ownership rates at condominium sites without carshare vehicles was 1.07 vehicles per unit, whereas buildings with one or more carshare vehicles had significantly lower rates at 0.53 vehicles per unit, which represents a 50% reduction in vehicle ownership rates⁷.
- A 2013 study from the City of Toronto looked at the relationship between the presence of carsharing in a residential building and its impact on vehicle ownership. This was one of the first studies to examine this relationship at the building level as previous research explored impacts at the neighbourhood or city level. The study surveyed residents of buildings with and without dedicated carshare vehicles. According to the author's

⁶ Martin & Shaheen. (2011). The Impact of Carsharing on Household Vehicle Ownership. Access Magazine, Spring 2011. Available online at: http://sfpark.org/wp-content/uploads/carshare/access38_carsharing_ownership.pdf

⁷ City of Toronto. (2009). Parking Standards Review: Examination of Potential Options and Impacts of Car Share Programs on Parking Standards. Available online at: https://www1.toronto.ca/city_of_toronto/city_planning/zoning_environment/files/pdf/car_share_2009-04-02.pdf

are provided and a vehicle easily accessible, and a similar reduction of 5-10% is recommended in *Parking Management Best Practices*¹⁶.

Residents of the subject site will have access to a Modo carshare vehicle and provided with free memberships. Given the research and discussion above, it is recommended that resident parking demand will be reduced by 15% (i.e. one vehicle) as a result of proximity to the carshare vehicle and free membership.

5.2 PUBLIC TRANSIT

The subject site is well served by public transit, as follows:

- The subject site is located within walking distance of bus stops on Pandora Avenue (approximately 260m from westbound bus stops and 370m from eastbound bus stops). These stops are served by the no.27 – Gordon Head / Downtown and no.28 – Majestic / Downtown frequent routes, as well as the no.1, no.2, no.24¹⁷, and no.25¹³ local routes.
- The no.6 – Royal Oak / Downtown route offers service between downtown Victoria and Royal Oak. Bus stops on Quadra Street are approximately 180m from the subject site.
- Bus stops on Douglas Street are approximately 600m from the subject site, providing access to an additional nine transit routes with service throughout Greater Victoria.

A transit pass subsidy is proposed to facilitate transit use among residents. The proposal is to commit funds to fully subsidize one monthly transit pass for each unit over a period of three years (396 monthly passes)¹⁸. In the event that not all committed monthly passes have been acquired after three years, remaining funds will be made available to residents to purchase monthly passes beyond the three-year timeframe up to amount of the total committed budget. Uptake on similar transit pass programs has been in the range of 20%, suggesting that subsidized passes will likely be available to residents that request them well beyond the three-year timeframe. The proponent and City may wish to agree on a mechanism to commit the identified funds and ensure the program is administered as proposed.

Studies¹⁹ have found that sites with transit access and free transit passes experience approximately 10% reduced parking demand (one study suggests 5-10%, another 11%). Accordingly, it is recommended that resident parking demand will be reduced by 10%, or one vehicle (0.7, rounded) as a result of the free transit pass and proximity to transit service.

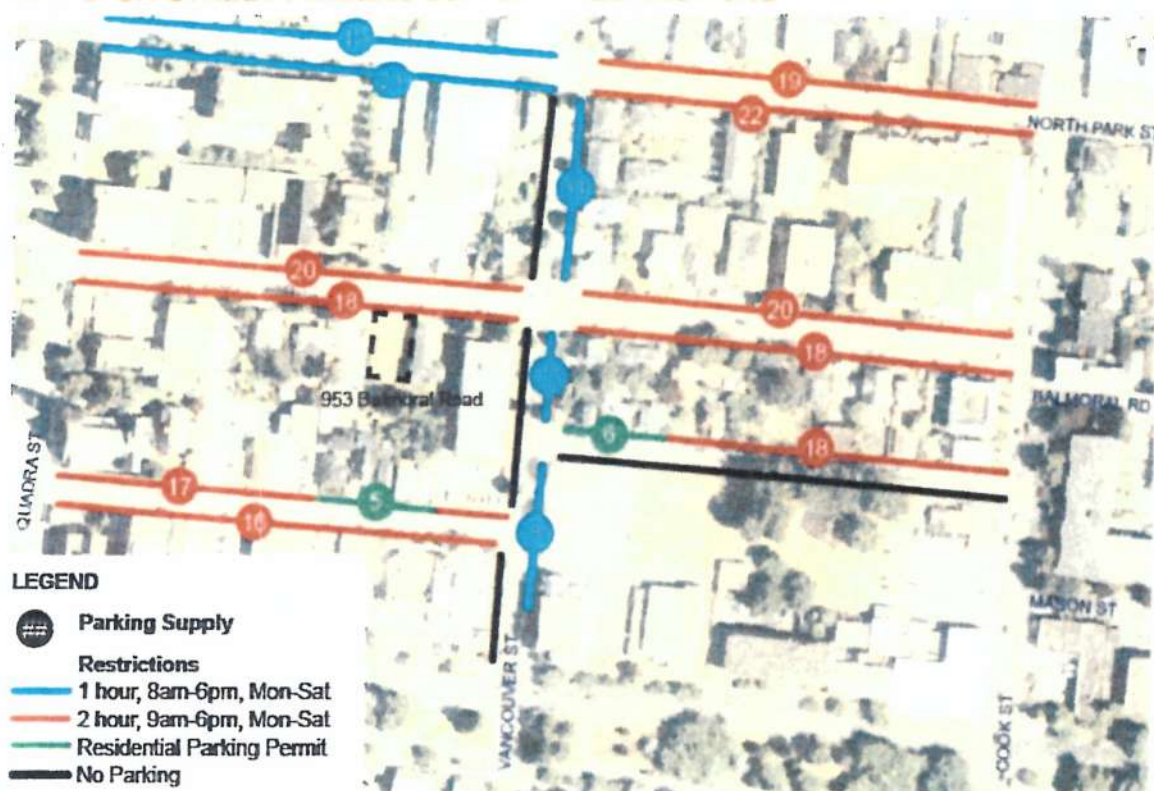
¹⁶ T Litman, *Parking Management Best Practices*, American Planning Association, 2007

¹⁷ Eastbound only (westbound routes via Yates Street)

¹⁸ Total contribution estimated at approximately \$52,000 assuming \$85 monthly pass rate. Proponent may negotiate reduced rate with BC Transit through Developer Pass Program.

¹⁹ Bort, J., *Reforming Parking Policies to Support Smart Growth, Parking Best Practices & Strategies for Supporting Transit Oriented Development*, 2007; and Tumlin, J., *Tools for Creating Vibrant, Health, and Resilient Communities*, Transportation Planning, 2012

MAP 2 ON-STREET PARKING SUPPLY + RESTRICTIONS



6.1 ON-STREET PARKING SUPPLY

A total of 243 spaces were observed. The 38 parking spaces directly adjacent the site on Balmoral Road (between Quadra Street and Vancouver Street) are restricted for a maximum stay of 2 hours from 9:00am to 6:00pm, Monday to Saturday. The majority of parking that was observed has either a 1- or 2-hour time restriction Monday to Saturday daytime, with the exception of residential permit parking adjacent specific residential properties.

6.2 ON-STREET PARKING CONDITIONS

On-street parking conditions were considered over seven observation periods. A summary of observations is provided in [Appendix B](#). Observation periods were as follows:

- Saturday October 17 at 8:15pm
- Sunday October 18 at 2:00pm
- Monday October 19 at 8:00pm
- Wednesday October 21 at 9:30pm
- Sunday October 25 at 2:00pm
- Tuesday November 17 at 10:30am²¹
- Thursday November 19 at 2:00pm⁸

²¹ Observation area limited to immediately adjacent the subject site – Balmoral Street from Quadra St to Vancouver St (both sides)

7.0 SUMMARY

The proposed development is for a four-storey residential rental building with 11 units. The proposed parking supply is five spaces; 9 spaces less than the City's parking requirement.

Expected peak resident parking demand was determined to be seven vehicles based on vehicle ownership information from representative sites and previous studies. A significant TDM program is proposed (see below) that is expected to reduce resident parking demand by two vehicles. Peak visitor parking demand is expected to be one additional vehicle. The total site parking demand is anticipated to be eight vehicles, three more than the proposed supply.

The site is expected to contribute one resident vehicle to on-street parking during the residential peak period (evenings, weekends). On-street parking on the block of Balmoral Road immediately adjacent the subject site could accommodate the expected spillover at approximately 58% occupancy (16 vacant spaces), and would not prevent others in the neighbourhood from accessing available parking. On-street parking on Balmoral Road was observed near full occupancy during weekday daytime periods when site parking demand will be met by on-site parking supply. Parking spaces must be "shared" in order to be utilized by all site users.

A comprehensive TDM program is proposed to enhance sustainable travel options and support reduced parking demand. TDM options include a Modo carshare vehicle on-site with memberships provided for each unit; monetary contribution for monthly transit passes for each unit over a three-year period; 1.4 secure bike parking spaces per unit; a free bicycle for each unit; and a travel information package provided at move-in. The TDM provisions are expected to reduce resident parking demand by approximately 25% (2 vehicles).

7.1 RECOMMENDATIONS

1. The proponent should commit to adopting the proposed TDM provisions, especially the carshare vehicle / memberships and transit passes, which are expected to reduce parking demand by 25%;
2. Parking demand is expected to exceed off-street parking supply by one vehicle during weekday evenings and weekends; and
3. The addition of one vehicle to adjacent on-street parking will not negatively impact the ability for others to access available parking.



- ① 2310 Quadra Street
- ② 1110 Queens Avenue
- ③ 1017 Queens Avenue
- ④ 1020 Pembroke Street
- ⑤ 1035 North Park Street

- ⑥ 1630 Quadra Street
- ⑦ 975 Balmoral Road
- ⑧ 1635 Cook Street
- ⑨ 1022 Pandora Avenue
- ⑩ 1130 Pandora Avenue

On-Street Parking Observations

953 Balmoral Road Parking Study

Section	Side	Restrictions	No. Parking Stalls	Saturday October 17, 8:15pm		Sunday October 18, 2:00pm		Monday October 19, 8:00pm		Wednesday October 21, 9:30pm		Sunday October 25, 2:00pm	
				Observed Vehicles	Occupancy Rate	Observed Vehicles	Occupancy Rate	Observed Vehicles	Occupancy Rate	Observed Vehicles	Occupancy Rate	Observed Vehicles	Occupancy Rate
North Park Street, Quadra to Vancouver	N	1 Hour, 8am-6pm, Mon-Sat	19	9	47%	11	58%	9	47%	5	26%	11	58%
	S		21	10	48%	12	57%	11	52%	12	57%	6	29%
North Park Street, Vancouver to Cook	N	2 Hour, 9am-6pm, Mon-Sat	19	9	47%	10	53%	6	32%	10	53%	4	21%
	S		22	9	41%	11	50%	7	32%	10	45%	8	36%
Balmoral Road, Quadra to Vancouver	N	2 Hour, 9am-6pm, Mon-Sat	20	10	50%	9	45%	5	25%	11	55%	9	45%
	S		18	7	39%	12	67%	8	44%	10	56%	10	56%
Balmoral Road, Vancouver to Cook	N	2 Hour, 9am-6pm, Mon-Sat	20	11	55%	9	45%	6	30%	6	30%	9	45%
	S		18	7	39%	13	72%	9	50%	8	44%	11	61%
Mason Street, Quadra to Vancouver	N	2 Hour, 9am-6pm, Mon-Sat	17	8	47%	6	35%	11	65%	9	53%	8	47%
		Residential Parking Permit	5	4	80%	5	100%	3	60%	7	140%	3	60%
	S	2 Hour, 9am-6pm, Mon-Sat	16	10	63%	8	50%	10	63%	11	69%	14	88%
Mason Street, Vancouver to Cook	N	Residential Parking Permit	6	2	33%	4	67%	4	67%	5	83%	5	83%
		2 Hour, 9am-6pm, Mon-Sat	18	11	61%	9	50%	11	61%	9	50%	10	56%
	S						No Parking						
Vancouver Street, North Park to Balmoral	E	1 Hour, 8am-6pm, Mon-Sat	11	8	73%	4	36%	5	45%	3	27%	4	36%
	W						No Parking						
Vancouver Street, Balmoral to Mason	E	1 Hour, 8am-6pm, Mon-Sat	5	3	60%	3	60%	2	40%	0	0%	2	40%
	W						No Parking						
Vancouver Street, Mason to Pandora	E	1 Hour, 8am-6pm, Mon-Sat	8	6	75%	5	63%	4	50%	0	0%	4	50%
	W						No Parking						
Total Occupancy			243	124	51%	131	54%	111	46%	116	48%	118	49%

ADDENDUM D

September 8, 2017

Sasha Kvakic
9-103 Wilson Street
Victoria, BC
V9A 6X1

Victoria City Mayor and Council
1 Centennial Square
Victoria, BC
V8W 1P6

Dear Mayor and Council,

I would like to register my enthusiastic support for the proposed redevelopment of 953 Balmoral Road. In the midst of a housing crisis driven by record low vacancy rates the city can ill afford to miss the opportunity to improve an underutilized space and add new rental housing units to the local market.

The only issue I have with the project is its reduced size from the originally proposed 6 story, 17 unit building down to 4 stories and 11 units. The public interest lies with encouraging the most socially responsible use of this property, which in this case is as affordable rental housing; the more units the better. Neither the current nor the original proposal are out of character with the surrounding neighbourhood, which is transitioning from a rough mix of light industrial/commercial, surface parking, and aging single family housing stock to a vibrant urban residential district on the edge of downtown. I hope that the city will embrace the opportunity presented by this project to improve the North Park neighbourhood for future generations.

Yours sincerely,



Sasha Kvakic

ADDENDUM E

953 Balmoral Road

Which purpose-built workforce rental apartment building do you prefer and why?

Open Discussion with the Developer at Parsonage Café on Saturday **05 August 2017 at 10AM**



17 units over six floors, includes three, 3-bedroom units on the top two floors



11 units over four floors

H.1.a.j 953 Balmoral Road – Rezoning Application No. 00598 and Development Permit with Variance Application No. 000506 (North Park)

Moved By Councillor Loveday
Seconded By Councillor Coleman

Rezoning Application No. 00598

That Council instruct staff to prepare the necessary Zoning Regulation Bylaw Amendment that would authorize the proposed development outlined in Rezoning Application No. 00598 for 953 Balmoral Road, that first and second reading of the Zoning Regulation Bylaw Amendment be considered by Council, and a Public Hearing date be set once the following conditions are met:

1. Preparation of the following documents, executed by the applicant to the satisfaction of City Staff:
 - a. Statutory Right-of-Way of 1.22m on Balmoral Road.
2. The applicant provide an amenity contribution in the amount of \$76,694.69 toward the Local Amenities Reserve Fund in accordance with the *City of Victoria Density Bonus Policy* to the satisfaction of City Staff.
3. Following consideration of Rezoning Application No. 00487, if approved, that Council authorize staff to prepare and enter into an Encroachment Agreement for a fee of \$750 plus \$25 per m² of exposed shored face during construction, to the satisfaction of the City staff.

Development Permit with Variance Application No. 000506

That, subject to review by the Advisory Design Panel and report back to the Committee of the Whole, that Council, after giving notice and allowing an opportunity for public comment at a meeting of Council, and after the Public Hearing for Rezoning Application No. 00598, if it is approved, consider the following motion:

“That Council authorize the issuance of Development Permit with Variance Application No. 000506 for 953 Balmoral Road, in accordance with:

1. Plans date stamped January 18, 2018
2. Development meeting all *Zoning Regulation Bylaw* requirements, except for the following variances:
 - i. reduce the required number of parking spaces from 12 to 5
 - ii. Part 3.3(10): reduce the front yard setback from 10.50m to 2.00
 - iii. Part 3.3 (10): reduce the side (east) yard setback from 6.10m to 1.52m
 - iv. Part 3.3(10): reduce the side (west) yard setback from 6.10m to 3.64m

- v. Part 3.3(4)(1): increase the site coverage from 30% to 43%
 - vi. Part 3.3(4)(6)(1): reduce the open site space from 30% to 15.30%
3. Registration of legal agreements on the property's title to secure the MODO Car Share Vehicle and parking space, car share memberships, one monthly transit pass for each unit over a period of three years (396 monthly passes), and one bicycle for each unit to the satisfaction of City Staff.
 4. Revise the landscape plan to indicate floating pavement where the proposed parking spaces overlap with the tree's critical root zone in accordance with the arborist report prepared by Talbot Mackenzie & Associates.
 5. The Development Permit lapsing two years from the date of this resolution."

FOR (8): Mayor Helps, Councillor Alto, Councillor Coleman, Councillor Loveday, Councillor Lucas, Councillor Madoff, Councillor Thornton-Joe, and Councillor Young
OPPOSED (1): Councillor Isitt

CARRIED (8 to 1)



Committee of the Whole Report

For the Meeting of September 6, 2018

To: Committee of the Whole **Date:** August 23, 2018

From: Jonathan Tinney, Director, Sustainable Planning and Community Development

Subject: Rezoning Application No. 00598 and Development Permit with Variance Application No. 000506 for 953 Balmoral Road

RECOMMENDATION

That Council decline Rezoning Application No. 00598 and Development Permit with Variance Application No. 000506 for the property located at 953 Balmoral Road.

EXECUTIVE SUMMARY

The purpose of this report is to present Council with an update on the Rezoning and Development Permit with Variance Applications for the property located at 953 Balmoral Road. The proposal is to rezone the subject property from the R-2 Zone, Two Family Dwelling District, to a new zone in order to construct a four-storey, multi-unit residential building with a density of approximately 1.38:1 floor space ratio (FSR).

Council reconsidered both applications at the Committee of the Whole meeting on June 7, 2018 and passed the following motion:

Postpone consideration of the application for 2 months and request the applicant to meet with the adjoining neighbours to explore possible consolidation of the adjoining lots.

The applicant has informed staff that an arrangement with the adjoining neighbours is not feasible and as a result, the applicant would like to proceed with the original proposal for Council's consideration (letter attached).

COMMENTS

The applicant has provided a letter dated August 17, 2018 (attached) addressing Council's motion above. The applicant has informed staff that an arrangement with the adjoining neighbours is not feasible and as a result, the applicant would like to proceed with the original proposal of a four-storey, multi-unit residential building for Council's consideration.

Staff's recommendation is to decline the Rezoning and Development Permit with Variance Applications for the same reasons discussed in the original Committee of the Whole reports (attached). The Official Community Plan encourages the logical assembly of development sites

to enable the best realization of development potential for the area. Ideally, the subject site would be consolidated with one or both of the properties on either side of it in order to realize a better site plan with fewer impacts to the adjoining properties, while achieving the overall density supported by policy. If developed on its own under the current proposal, it would compromise future redevelopment along this block of Balmoral Road.

Respectfully submitted,

Leanne Taylor
Senior Planner
Development Services Division

Jonathan Tinney, Director
Sustainable Planning and Community
Development Department

Report accepted and recommended by the City Manager:

Date: _____

List of Attachments

- Attachment A: Letter to Mayor and Council dated August 17, 2018
- Attachment B: Committee of the Whole Report dated May 24, 2018
- Attachment C: Committee of the Whole Report dated April 19, 2018
- Attachment D: Minutes from the Committee of the Whole meeting dated June 7, 2018
- Attachment E: Minutes from the Council meeting dated April 26, 2018.



Committee of the Whole Report For the Meeting of June 7, 2018

To: Committee of the Whole **Date:** May 24, 2018

From: Jonathan Tinney, Director, Sustainable Planning and Community Development

Subject: Rezoning Application No. 000506 and Development Permit with Variance Application No. 000506 for 953 Balmoral Road

RECOMMENDATION

That Council decline Rezoning Application No. 00598 and Development Permit with Variance Application No. 000506 for the property located at 953 Balmoral Road.

EXECUTIVE SUMMARY

The purpose of this report is to present Council with an update regarding the Rezoning and Development Permit with Variance Applications for the property located at 953 Balmoral Road. The proposal is to rezone the property from the R-2 Zone, Two Family Dwelling District, to a new zone in order to construct a four-storey, multi-unit building with a density of approximately 1.38:1 floor space ratio (FSR).

Council considered both applications at the Committee of the Whole meeting on April 18, 2018 and passed the following motion:

Rezoning Application No. 00598

"That Council direct staff to work with the applicant to refine the proposal to encourage a better fit with the current neighbourhood context and to minimize potential negative impacts associated with a piecemeal approach to development in this area."

Development Permit with Variances Application No. 000506

"That Council:

- 1. Direct staff to work with the applicant to revise the proposal to comply with the design guidelines and*
 - i. minimize the impact of the east side yard setback by reducing the requested variance and by introducing additional design interventions to mitigate potential concerns related to privacy and overlook*
 - ii. reduce the site coverage and increase the open site space in order to provide private open space and high quality soft landscaping.*

- iii. *provide a landscaping strip along the side and rear property lines to screen the parking.*
 - iv. *address Council's issue of the lack of affordability in this application and revisit discussions of entering into a housing agreement.*
2. *Refer the application to the Advisory Design Panel and report back to the Committee of the Whole following a review by the panel."*

COMMENTS

The applicant has provided a letter dated May 7, 2018 (attached) addressing Council's motion above. The applicant is willing to enter a Housing Agreement ensuring that the proposed 11 dwelling units would remain as rental housing for a 25-year term. In the letter, the applicant has indicated that it is not feasible from their perspective to make any design and onsite landscaping changes to the current proposal as required in Council's motion.

Staff's recommendation is to decline the Rezoning and Development Permit with Variance Applications for the same reasons discussed in the original Committee of the Whole reports (attached). The OCP encourages the logical assembly of development sites to enable the best realization of development potential for the area. Ideally, the subject site would be consolidated with one or both of the properties on either side of it in order to realize a better site plan with fewer impacts to the adjoining properties, while achieving the overall density supported by policy. If developed on its own under the current proposal, it would compromise future redevelopment along this block of Balmoral Road.

Respectfully submitted,

Leanne Taylor
Senior Planner
Development Services

Jonathan Tinney, Director
Sustainable Planning and Community
Development Department

Report accepted and recommended by the City Manager:

Date: _____

List of Attachments

- Attachment A: Letter to Mayor and Council
- Attachment B: Committee of the Whole Reports dated April 19, 2018.



Committee of the Whole Report For the Meeting of March 15, 2018

To: Committee of the Whole **Date:** February 22, 2018

From: Jonathan Tinney, Director, Sustainable Planning and Community Development

Subject: Rezoning Application No. 00598 for 953 Balmoral Road

RECOMMENDATION

That Council decline Rezoning Application No. 00598 for the property located at 953 Balmoral Road.

LEGISLATIVE AUTHORITY

In accordance with Section 479 of the *Local Government Act*, Council may regulate within a zone the use of land, buildings and other structures, the density of the use of the land, building and other structures, the siting, size and dimensions of buildings and other structures; as well as, the uses that are permitted on the land, and the location of uses on the land and within buildings and other structures.

In accordance with Section 482 of the *Local Government Act*, a zoning bylaw may establish different density regulations for a zone, one generally applicable for the zone and the others to apply if certain conditions are met.

EXECUTIVE SUMMARY

The purpose of this report is to present Council with information, analysis and recommendations for a Rezoning Application for the property located at 953 Balmoral Road. The proposal is to rezone the property from the R-2 Zone, Two Family Dwelling District, to a new zone in order to construct a four-storey multi-unit building with a density of approximately 1.38:1 floor space ratio (FSR).

The following points were considered in assessing this application:

- the subject property is designated Core Residential in the *Official Community Plan*, 2012 (OCP), which supports a diverse range of housing types including low and mid-rise multi-unit residential buildings
- the *Downtown Core Area Plan* (DCAP) identifies this property within the Residential Mixed-Use District, which supports multi-residential development up to six-storeys and a floor space ratio up to 2:1
- the OCP encourages the logical assembly of development sites to enable the best

realization of development envisioned for the area. The proposed site area is approximately 671.5m², which is a standard lot size for a duplex. The property to the west is an existing parking lot tied to a building on a different lot and there is a rooming house to the east. Given the existing neighbourhood context and the site's redevelopment potential, land assembly with the adjacent properties is strongly encouraged.

- the site being only 672m² cannot comfortably support a development at this proposed density, size and scale without significantly impacting the development potential of adjacent properties and achieving the densities that are supported in DCAP.
- the applicant is proposing to construct purpose-built rental; however they are unwilling to register a Housing Agreement to ensure that the building remains rental in perpetuity, or for a given time period. Instead the applicant notes that Council approval to strata title the building in the future would be required if the vacancy rate is less than 4%.

BACKGROUND

Description of Proposal

This Rezoning Application is to allow a four-storey multi-unit building with a density of approximately 1.38:1 floor space ratio (FSR).

The following differences from the existing R-2 Zone, Two Family Dwelling District, are related to increasing the floor space ratio, floor area, reducing setbacks and open site space requirements, and increasing the site coverage.

Affordable Housing Impacts

The applicant proposes the creation of 11 new residential rental units which would increase the overall supply of housing in the area. The applicant is proposing to construct purpose-built rental; however they are unwilling to register a Housing Agreement to ensure that the building remains rental in perpetuity, or for a given time period. Instead the applicant notes that Council approval to strata title the building in the future would be required if the vacancy rate is less than 4%.

Sustainability Features

The applicant has not identified any sustainability features associated with this proposal.

Active Transportation Impacts

The application proposes 16 Class 1 (secure and enclosed) and seven Class 2 (one bike rack) bicycle parking spaces to support active transportation.

Public Realm Improvements

No public realm improvements are proposed in association with this Rezoning Application.

Accessibility Impact Statement

The British Columbia Building Code regulates accessibility as it pertains to buildings.

Land Use Context

The area is characterized by a mix of commercial, institutional and residential land uses.

Existing Site Development and Development Potential

The site is presently a vacant lot. Under the current R-2 Zone, the property could be developed as a duplex.

Data Table

The following data table compares the proposal with the existing R-2 Zone, Two Family Dwelling District, as well as the R3-1 Zone, Multiple Dwelling District, which is seen as a comparable zone as it anticipates similar uses at a similar density. However, there are still numerous aspects of the proposal that would still not meet this zone's requirement. An asterisk is used to identify where the proposal is less stringent than the R3-1 Zone.

Zoning Criteria	Proposal	Existing R-2 Zone	Zone Standard R3-1 Zone
Site area (m ²) - minimum	671.50*	555.00	920.00
Density (Floor Space Ratio) - maximum	1.38:1*	0.50:1	1.20:1
Total floor area (m ²) - maximum	929.50*	280.00	805.80
Lot width (m) - minimum	15.48	15.00	n/a
Height (m) - maximum	12.19	7.60	18.50
Storeys - maximum	4.00	2	6
Site coverage % - maximum	43.00*	40.00	30.00
Open site space % - minimum	15.30*	30.00	30.00
Setbacks (m) – minimum:			
Front	2.00*	7.50	10.50
Rear	10.85	15.20	6.10
Side (east)	1.52*	1.55	6.10
Side (west)	3.64*	3.00	6.10
Parking – minimum			
Existing Schedule C	5*	14	12
Proposed Schedule C	5*	8 (7 residential and 1 visitor)	8 (7 residential and 1 visitor)
Bicycle parking stalls - minimum	16	11	11

Zoning Criteria	Proposal	Existing R-2 Zone	Zone Standard R3-1 Zone
Class 1 Class 2	6	6	6

Community Consultation

Consistent with the *Community Association Land Use Committee (CALUC) Procedures for Processing Rezoning and Variance Applications*, the applicant has consulted the North Park CALUC at a community meeting held on June 7, 2017. At this meeting, the applicant presented a proposal for a six-storey multi-unit residential building consisting of approximately 17 rental dwelling units. Minutes from the June 7, 2017 CALUC meeting are attached to this report. On June 15, 2017, the applicant submitted a rezoning application for a four-storey multi-unit residential building which caused some confusion; therefore, a second community meeting was held on August 15, 2017 to present the four-storey option. Minutes from the second community meeting are attached to this report.

ANALYSIS

Official Community Plan

The subject property is designated Core Residential in the *Official Community Plan, 2012* (OCP), which supports a diverse range of housing types including low and mid-rise multi-unit residential buildings. The subject property is within Development Permit Area 3(HC): Core Mixed-use Residential, which encourages higher density residential development on the edge of the Central Business District.

The OCP also encourages the logical assembly of development sites to enable the best realization of development potential for the area. The site area of the subject property is 671.5m², which is a standard lot size for a duplex. The property to the west (949 Balmoral Road) is an existing parking lot tied to a building on a different lot and there is a rooming house located on the property to the east (959 Balmoral Road). Given the existing neighbourhood context and development potential, land assembly with the adjacent properties is strongly encouraged. All three properties have similar lot areas and lot widths. This approach would avoid mid-block, piecemeal development and achieve higher density residential development more consistent with the policies and objectives in the OCP. The property on the corner of Balmoral Road and Vancouver Street (one property to the east the subject site) is a large site which is occupied by a four-storey apartment building and could easily be redeveloped on its own in the future. Additionally, there are a number of scenarios that could see the lots to the west of the subject site being consolidated and redeveloped. Ideally, the subject site would be consolidated with one or both of the properties on either side of it in order to realize a better site plan with fewer impacts to the adjoining properties while achieving the overall density supported by policy.

If developed on its own, the subject property could handle some additional residential density; however, this would still compromise future redevelopment along this block of Balmoral Road and limit the future redevelopment of the area.

Density Bonus Policy

In October 2016 Council adopted the *City of Victoria Density Bonus Policy*, which would apply to this proposal. The Policy identifies an amenity contribution target (fixed rate target) for standard rezoning of properties designated "Core Residential (less than 30,000ft² of bonus density)" in

the OCP of \$129.17 per m². Based on the bonus density calculation, the applicant would be required to provide an amenity contribution in the amount of \$76,694.69 towards the Local Amenities Reserve Fund and to the satisfaction of City Staff.

Downtown Core Area Plan

The subject property is within the Residential Mixed-Use District in the *Downtown Core Area Plan* (DCAP), which supports multi-residential development up to six-storeys and a floor space ratio up to 2:1. The proposal for a four-storey multi-unit residential building with a FSR of 1.38:1 complies with the policies outlined in DCAP; however, staff have concerns with the overall design of the proposal, which will be discussed further in the concurrent report associated with the Development Permit with Variance Application.

Tree Preservation Bylaw and Urban Forest Master Plan

There are no impacts to public trees with this application; however, there is one Horse chestnut tree protected by a Tree Preservation Bylaw on the neighbouring property at 959 Balmoral Road. The applicant provided an arborist report (attached) prepared by Talbot Mackenzie & Associates, which includes a tree assessment and tree impact mitigation measures. The report concluded that the tree may be impacted by the proposed construction; however, the impacts would be minor if floating pavement is installed where the proposed parking spaces overlap with the tree's critical root zone. Pruning would be required to lift the lower canopy above the nearest parking space at the property line and may be required to provide clearance for building construction.

The applicant is not proposing to plant additional trees on the subject property.

Statutory Right-of-Way

A Statutory Right-of-Way (SRW) of 1.22m is required on Balmoral Road in order to achieve the standard width of a secondary collector street of 20.0m in the Highway Access Bylaw. The applicant will provide the SRW and has shown it on the site plan.

Regulatory Considerations

Proposing a four-storey building on a lot with a site area of 671.5m² is tight and compromises the site planning with respect to providing sufficient landscaping and open site space, and will also impact the relationship with adjacent properties in the short-term and influence the redevelopment of those lots in the future.

By comparison, the standard R3-1 Zone requires a minimum lot area of 920.00m² and allows a maximum FSR of 1.2:1 for a four storey building. The zone also incorporates larger setbacks to allow for some breathing room between neighbouring buildings. If the subject property were consolidated with adjoining lots, the lot area would be approximately 2081.77m² which is a similar lot area to the nearby property at 975 Balmoral Road occupied by a four-storey apartment building. A larger site area could easily accommodate a six-storey building with adequate open site space, landscaping, appropriate setbacks, and underground parking. It would also provide more options for site planning and building footprint, and reduce impacts on the Horse chestnut tree. Allowing the subject property to redevelop on its own would limit the redevelopment potential of adjacent lots in the future.

CONCLUSIONS

The proposal to construct a four-storey multi-unit building consisting of 11 rental units is consistent with the OCP and DCAP with respect to the proposed land use and density. The subject property is suitable for some additional higher density residential development, although, preferably through a land assembly with adjacent properties to enable the best realization of development potential. Staff recommend for Council's consideration that this Rezoning Application is declined.

ALTERNATE MOTION 1 (Amend Proposal)

That Council direct staff to work with the applicant to refine the proposal to encourage a better fit with the current neighbourhood context and to minimize potential negative impacts associated with a piecemeal approach to development in this area.

ALTERNATE MOTION 2 (advance application as presented)

That Council instruct staff to prepare the necessary Zoning Regulation Bylaw Amendment that would authorize the proposed development outlined in Rezoning Application No. 00598 for 953 Balmoral Road, that first and second reading of the Zoning Regulation Bylaw Amendment be considered by Council, and a Public Hearing date be set once the following conditions are met:

1. Preparation of the following documents, executed by the applicant to the satisfaction of City Staff:
 - a. Statutory Right-of-Way of 1.22m on Balmoral Road.
2. The applicant provide an amenity contribution in the amount of \$76,694.69 toward the Local Amenities Reserve Fund in accordance with the *City of Victoria Density Bonus Policy* to the satisfaction of City Staff.
3. Following consideration of Rezoning Application No. 00487, if approved, that Council authorize staff to prepare and enter into an Encroachment Agreement for a fee of \$750 plus \$25 per m² of exposed shored face during construction, to the satisfaction of the City staff.

Respectfully submitted,

Leanne Taylor
Senior Planner
Development Services Department

Jonathan Tinney, Director
Sustainable Planning and Community
Development Department

Report accepted and recommended by the City Manager:

Date: _____

List of Attachments:

- Attachment A: Subject Map
- Attachment B: Aerial Map
- Attachment C: Plans date stamped January 18, 2018

- Attachment D: Package from applicant date stamped November 22, 2017 including Letter To Mayor and Council, correspondence, and Parking Study dated October 27, 2017, prepared by Watt Consulting Group
- Attachment E: Community Association Land Use Committee Comments dated June 7, 2017 and August 15, 2017
- Attachment F: Arborist Report prepared by Talbot Mackenzie & Associates.

Heather McIntyre

From: Deanna Bhandar [REDACTED]
Sent: July 15, 2019 11:03 AM
To: Victoria Mayor and Council
Cc: Leanne Taylor; Kathy Stinson
Subject: RE: Cool Aid Society: 959 Balmoral Road property
Attachments: Letter_Cool Aid_953-959 Balmoral_Development Impact Analysis.pdf

Dear Mayor and Council,

I write to you regarding the property that Cool Aid owns at 959 Balmoral Road which is presently occupied by a rooming house operated as affordable rental housing. There is currently a Rezoning and Development Permit application before Council for our neighbouring property - 953 Balmoral Road. The proposal is to create an 11-unit market rental building over 4 storeys. While we strongly support the efforts of private developers to create more rental housing supply in Victoria, we do have significant concerns regarding the impacts of this development proposal on the livability for our current tenants, and the considerable limitations that this development, with its list of requested variances, will cause for the potential future redevelopment of our property at 959 Balmoral. I have attached an impact assessment of 953 Balmoral Road's proposed development on our property which has been prepared by Paul Hammond of Low Hammond Rowe Architects. The impact assessment clearly outlines the gravity of our neighbour's development proposal on our property and I hope that Council will give this analysis serious consideration as you deliberate on this proposal.

Under the Core Residential designation in the OCP, both our property and the neighbouring 953 Balmoral Road are envisioned to achieve a density of 2.0 floor area ratio for properties with a minimum lot size of 920 square meters. If these two properties were consolidated and the full development potential of this land was realized, 40 – 50 units of housing could be created in a very economical project which would adhere to all zoning and site requirements, without the need for variances. The most recent developments in the area have achieved an increase in market rental housing. While this market rental housing will be an asset to the community, it will not alleviate the needs of our very low income residents, nor does it fully realise the community's potential for more diverse and equitable housing stock. There have not been any new affordable rental or supportive housing units created in the North Park neighbourhood to increase options for people experiencing homelessness in the immediate area; our neighbourhood's most vulnerable residents. This gentrification of the downtown core and surrounding neighbourhoods will further displace low income people and people at risk of homelessness. Providing housing options across the spectrum is needed to build inclusive communities.

In summary, it's very difficult for us to be in this position of opposing our neighbour's development proposal as our general position is to strongly advocate for any new rental housing in our community. We have reached out to our neighbour to discuss our concerns regarding his application. Unfortunately, he did not have time to meet with us and hear these concerns. We would respectfully ask that you consider the impacts of the 953 Balmoral Road development proposal on our property located at 959 Balmoral Road. We echo the remarks and concerns found both in the Staff Reports (dated March 15, 2018; June 7, 2018; September 6, 2018 and July 4, 2019), and the attached design review from Low Hammond Rowe Architects, which succinctly shows the impact that this development proposal and its requested variances would have on our property. Should you have any questions or require further information, please don't hesitate to contact us.

We thank Mayor and Council for their ongoing support of Cool Aid, and for their dedication to improving the equity and diversity of housing stock in our community.

Yours Sincerely,

Deanna Bhandar

Deanna Bhandar MSc
Director, Real Estate Development



Victoria Cool Aid Society



Victoria, BC V8W 1N9



Together we will end homelessness.

Victoria Cool Aid Society acknowledges the Lekwungen and W̱SÁNEĆ peoples of the Songhees and Esquimalt Nations, on whose traditional territories we build homes, lives, and community. HÍSW̱ḴE.



08 May 2019

**LOW
HAMMOND
ROWE
ARCHITECTS**

Ms Deanna Bhandar, MSc.
Director, Real Estate Development
Victoria Cool Aid Society
101-749 Pandora Avenue
Victoria, BC, V8W 1N9

Re: DRAFT Impact Analysis of Proposed Development at 953 Balmoral Rd on Cool Aid Owned Property
953 ~ 959 Balmoral Road, Victoria BC

Dear Deanna,

We were asked by Victoria Cool Aid Society (VCAS) to analyse the proposed redevelopment of 953 Balmoral Road, to review potential impacts on the adjacent 959 Balmoral Rd property, which contains a one storey rooming house for low income people, owned by VCAS.

The proposed development at 953 Balmoral Rd, represented by Rezoning Application No. 00598 and Development Permit with Variance Application No. 000506 (North Park), consists of a four-storey rental building with 11 units. Our analysis considers this proposed development relative to three categories:

1. Comparison to requirements of existing R2 Zone, standard R3-1 Zone & the OCP
2. Impact of this development on adjacent Rooming House at 959 Balmoral Rd
3. Potential limits to future development of adjacent Cool Aid owned property

Summary:

The following analysis shows that the proposed development, currently R-2 Zone, does not meet the requirements of R3-1 Zone in density, site coverage, site area, setbacks, or open site space for similar sized developments; has significant impacts on the current Cool Aid Rooming House building; and would greatly limit future Cool Aid development of the 959 Balmoral Rd property, unless the same or similar rezoning concessions are made to this property as well.

Analysis:

1. Comparison to requirements of existing Zone, comparable Zone and the OCP:

While the proposed development meets the intensions of the OCP and R3-1 Zone, whereby increased density and number of storeys are considered, beyond the minimal R2 Zone, the size and quantity of requested variances, as compared to R3-1 zone, will have considerable negative impact on the 959 property.

- a. According to the Committee of the Whole Report prepared by Development Services, dated February 22, 2018, recommendations were made to decline the application, arguing that the site is too small to accommodate such a large development, considering the following required variances to the 'comparable zone' R3-1, Multiple Dwelling District:

- Setbacks (6.1m reduced to 1.52m),
- Lot coverage (30% increased to 43%),
- Site open space (30% reduced to 15.3%)
- Small lot size (671.5m² down from 920m² envisioned as minimum for R3-1 Zone).

The report further highlights the intentions explicit in the Official Community Plan for Core Residential Areas: *"The OCP encourages the logical assembly of development sites to enable the best realization of development potential for the area"*.

It is clear that this proposed development is too large for the available site area, which is why such variances are required. Critically, it is the adjacent property at 959 Balmoral Rd that will suffer the most harmful impacts from these variances. To quote the above noted Report; *"Proposing a four-storey building on a lot with a site area of 671.5m² is tight and compromises the site planning..., and will also impact the relationship with adjacent properties in the short-term and influence the redevelopment of those lots in the future"*.

- b. At the subsequent Committee of the Whole meeting on June 7, 2018, a motion was passed to postpone consideration of the application for 2 months and requested the applicant to meet with adjoining neighbours to explore possible consolidation of the adjoining lots. As no agreement between neighbouring property owners was reached, the applicant proceeded to Committee of the Whole, with the same request for variances to the Development Permit application.

Similarly, a second Committee of the Whole Report was prepared, dated August 23, 2018, again recommending the application be declined by Council, considering no substantial change was made to the variances being requested.

2. Impact of this development on adjacent Rooming House at 959 Balmoral Rd:

The most significant impact of this development on 959 Balmoral is the four-storey building located only 1.52 m from the east property line. The proposed development will dwarf over the 1-1/2 storey Rooming House, which will block out the sun to the west façade, year-round. The Rooming house has 12 lower windows and upper main level windows on the west façade, which would be blocked by the proposed building, running the length of the Rooming House. The attached sketch images illustrate the impact:

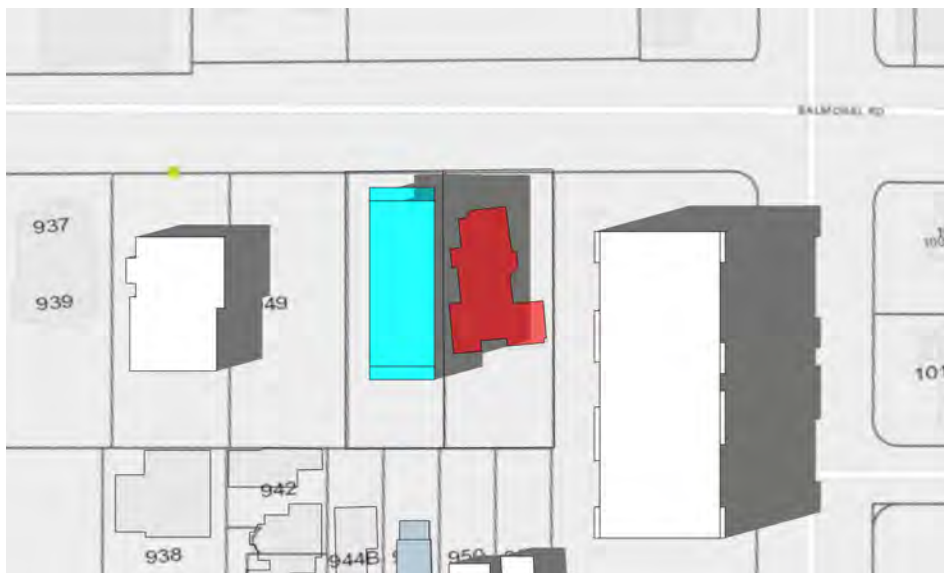


Fig.1 - Sun study



Fig. 2 – Aerial View from North West

We note that the DP submission shows the Rooming House situated parallel to the west property line, on the floor plans, although the site plans seems to accurately depict the location. However, the renderings certainly downplay the proximity of the two buildings. The following sketch images show the approximate relationship of the proposed development to the existing Rooming House, as viewed from Balmoral:

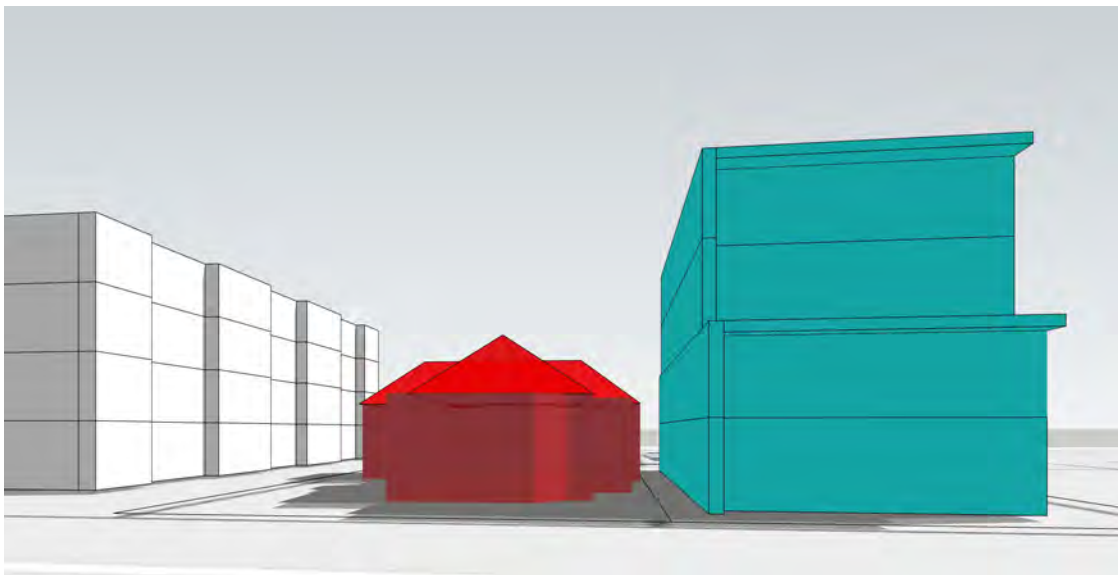


Fig. 3 – Sketch Elevation Viewed from Balmoral

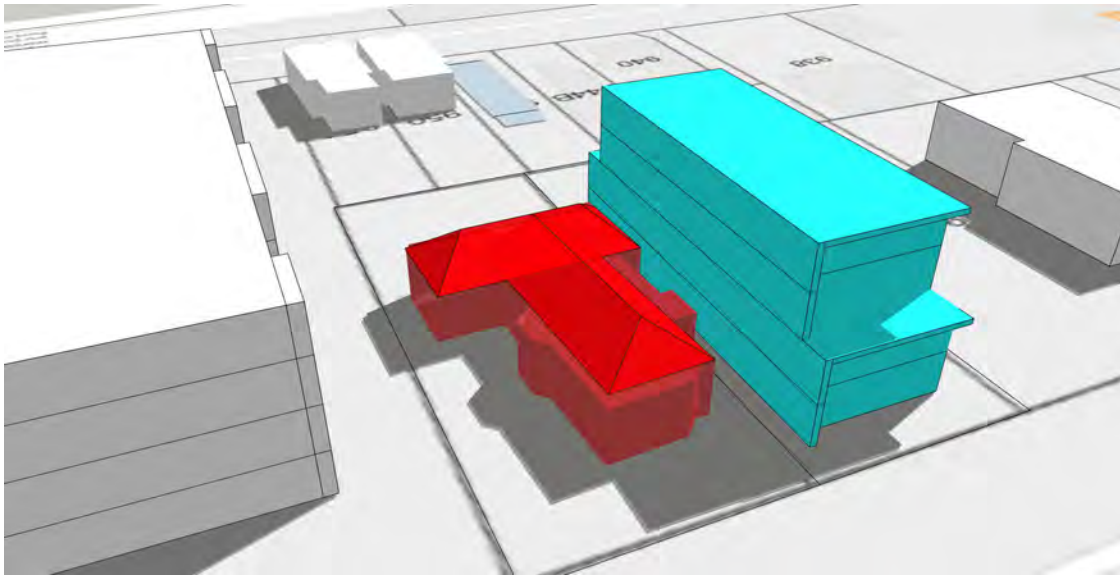


Fig. 4 – Sketch Aerial View from North East

3. Impact of this development on potential future development of 959 Balmoral Rd:

The most significant impact this proposed development would have on future development of the adjacent property at 959 Balmoral Road, would be if the same rezoning and development permit variances were not also extended to the 959 Balmoral property. Without such guarantee, the limitation would be severe. Competing with setbacks, building height, density and open space issues would limit the development to a townhouse scale, which would again be dwarfed by the proposed development at 953 Balmoral.

- a. The following sketch diagrams illustrate what would be possible if the same size/scale development were to be implemented on the 959 Balmoral property. The building would be located in the same position as the 953 development, 1.52m from the east property line, allowing for separation between the two buildings. Driveway access would be limited to the west side of the property, but the limited setback on the east side would be adjacent to a four-storey apartment, which has higher density. Concerns for overlook and limiting distance issues may have a negative impact on successfully realizing the necessary development variances required to make the development viable.



Fig. 5 – Site plan showing similar scale development on 953 and 959 Balmoral Rd



Fig. 6 – Aerial Perspective showing similar scale development on 953 and 959 Balmoral Rd

- b. An alternative development that could benefit both properties, while still maintaining independence, would be to build to a shared zero-lot line in the centre of both sites. Such a development would allow the proposed limited 1.52m setback to be removed at the shared property line and added to the outer property lines. This would put much needed distance between the two developments and adjacent properties, facilitating space for an increased naturalized buffer.

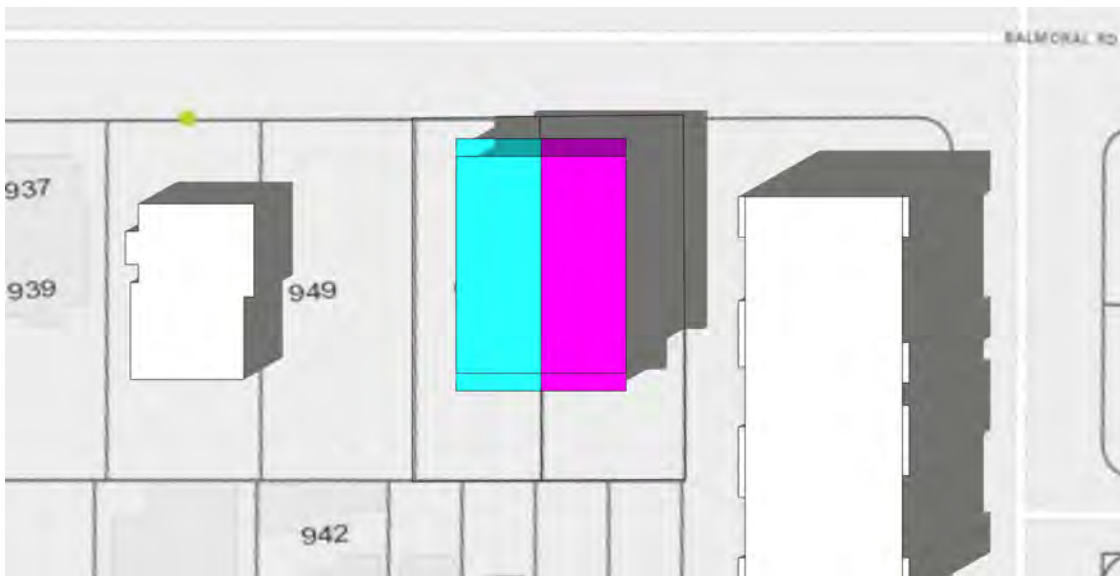


Fig. 7 – Site plan showing similar zero lot line developments on 953 and 959 Balmoral Rd

The difficulty of successfully developing this option involves the timing between developments and obtaining approvals from Council to develop these two sites independently. Similar to development option 3b noted above. Figure 8 illustrates this option in perspective view.

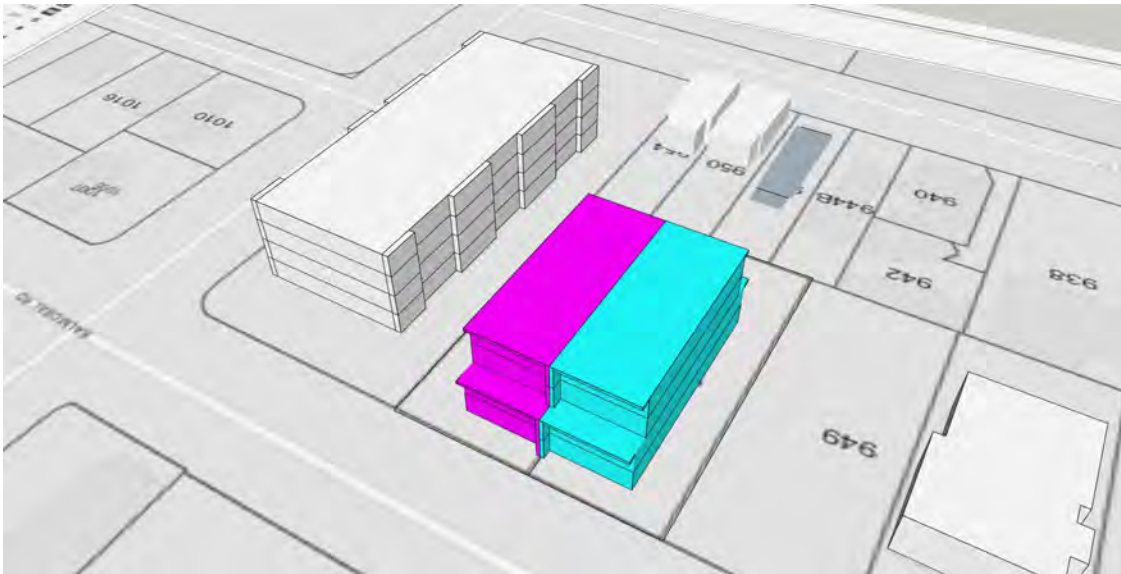


Fig. 8 – Aerial view showing similar zero lot line developments on 953 and 959 Balmoral Rd

In conclusion, the proposed development for 953 would have significant negative impacts on the existing Rooming House as well as on future development of your 959 Balmoral Road property. The only equitable way to mitigate these negative impacts would be for Council to grant the same rezoning and development permit variances to your property, at the same time of approving them for proposed development on 953 Balmoral Road.

Alternatively, as recommended by Development Services, future developments for both sites should either be reduced in scale, height and footprint, or developed together on a consolidated site.

Yours truly,

LOW HAMMOND ROWE ARCHITECTS INC

Paul Hammond, Architect AIBC, MRAIC, CPHD
Principal

[Redacted]
[Redacted]

cc. Kathy Stinson, CEO, Victoria Cool Aid Society

15 July 2019

Method Built Homes Ltd.
The Garage
4566 Cordova Bay Road
Victoria, British Columbia
V8X 3V5

Attn: Mayor and Council
City of Victoria
1 Centennial Square
Victoria, British Columbia
V8W 1P6

Dear Ms. Mayor and Members of Council,

Re: 953 Balmoral Avenue: 11-unit purpose-built workforce apartment building – Rezoning and DP

Further to my letter of 06 March 2019, and Staff's report of 04 July 2019 (the "Report"), please note the following.

On Page 2 of the Report, Staff note the following:

In accordance with Council's motion above, on October 24, 2018, the Advisory Design Panel reviewed the proposal and provided a recommendation that the applicant make changes related to the massing and side yard setbacks, and provide design consideration to the landscaping, privacy of ground-oriented units, front entryways and balconies on the upper units (minutes attached). In response to the ADP's comments and recommendation, the applicant made some revisions to the landscaping by adding some landscaping in the front yard, a landscaping strip on the east side of the building and the surface parking area and a trellis system along the rear property line. The applicant also added glazed front doors to soften the front entryways. There are no changes related to the massing and side yard setbacks.

While this tells part of the story with respect to massing, it does not tell the entire story. Attachment C to the Report, are the Minutes from the 24 October 2018 Advisory Design Panel (the "ADP") meeting. At Page 4, the ADP discussed the following:

- *opportunity to shift the building's massing, possibly by adding one storey, to reduce and adjust the building footprint, mitigate the impact of a long eastern wall to neighbours, and maintain the proposed density*

As noted at paragraph 1 of my 06 March 2019 letter, the issue of re-distributing the existing massing was discussed, but it was discussed in the context of re-distributing the massing upwards to increase the number of levels of the proposed apartment building. This is something that initially supported approximately 3-4 years ago, but have since strenuously opposed. As a result, reducing the footprint and re-distributing the massing to higher floors was not considered a feasible option.

Yours very truly,



Rajinder S. Sahota

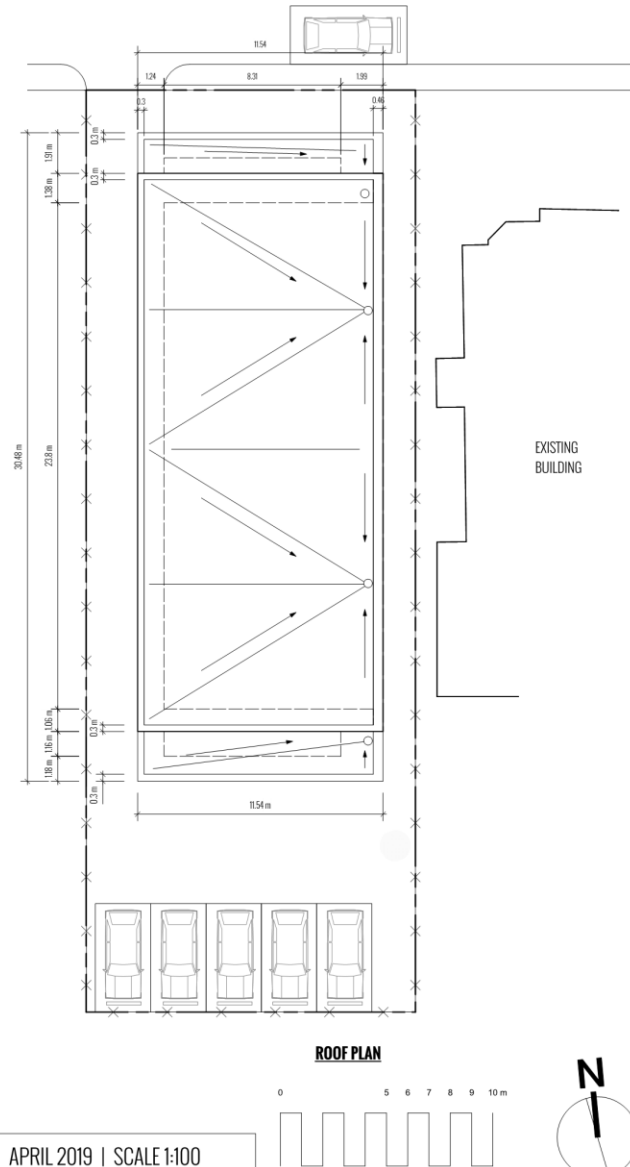
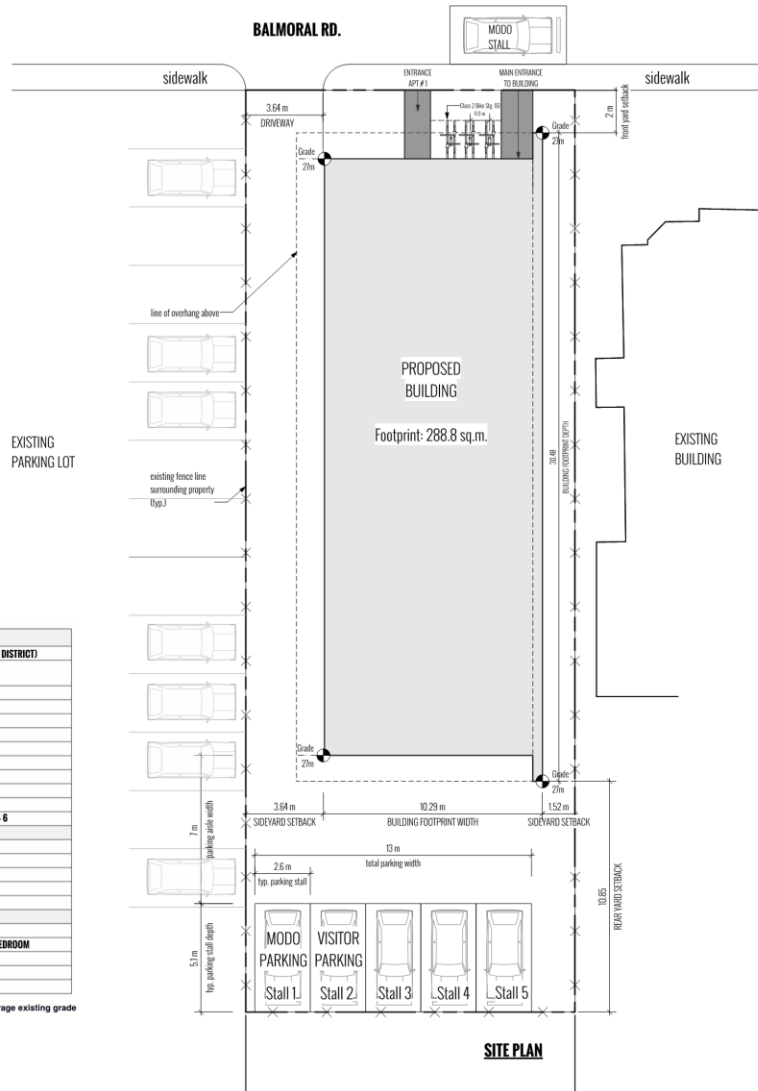
Rezoning Application No. 00598
and Development Permit with
Variance Application No. 000506
for 953 Balmoral Road

Subject Property – 953 Balmoral Road



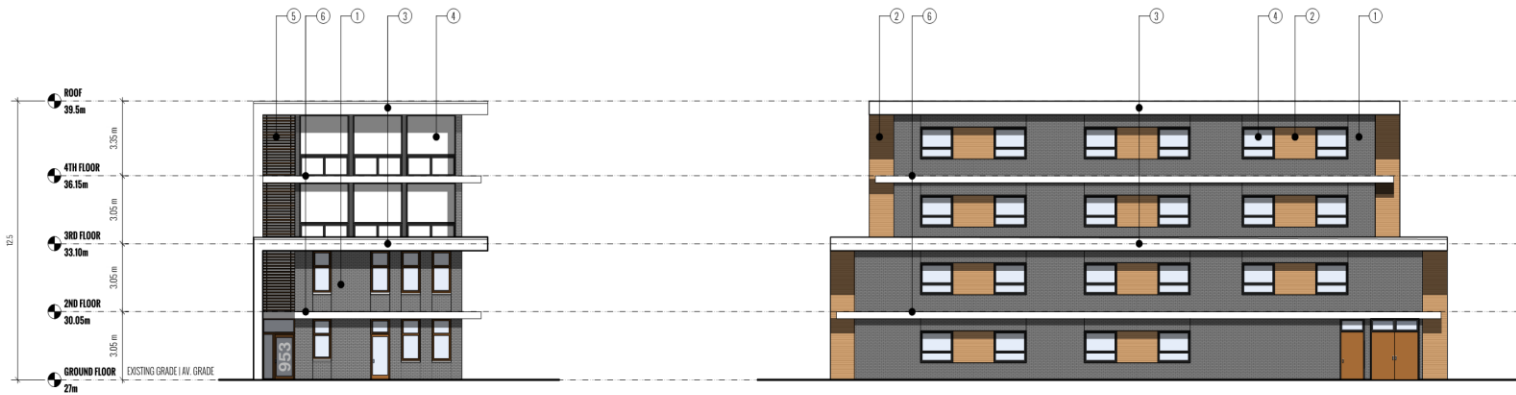
PROJECT INFORMATION TABLE	
Zone (existing)	R.2 (TWO FAMILY DWELLING DISTRICT)
Proposed zone or site specific zone If unsure, state "new zone"	NEW ZONE
Site area (m ²)	671.5 m ²
Total floor area (m ²)	929.5 m ²
Commercial floor area (m ²)	7180
Floor space ratio	929.5 / 671.5 = 1.38:1
Site coverage (%)	288.8 / 671.5 = 43%
Open site space (%)	103 / 671.5 = 15.3%
Height of building (m)	12.5m
Number of storeys	4
Parking stalls (number) on site	5
Bicycle parking number (Class 1 and Class 2)	Class 1 - 24 Class 2 - 6
Building Setbacks (m)	
Front yard	2.0m
Rear yard	10.85m
Side yard (indicate which side)	EAST 1.52m
Side yard (indicate which side)	WEST 3.64m
Combined side yards	5.16m
Residential Use Details	
Total number of units	11
Unit type, e.g., 1 bedroom	9 - ONE BEDROOM, 2 - TWO BEDROOM
Ground-orientated units	2
Minimum unit floor area (m ²)	47.1 m ²
Total residential floor area (m ²)	558.3m ²

Average Existing Grade Calculation: $27.5 + 27.6 + 26.5 + 26.7 = 108.3 / 4 = 27.0$ average existing grade
Proposed Average Grade: $27 + 27 + 27 + 27 = 108 / 4 = 27m$ average proposed grade



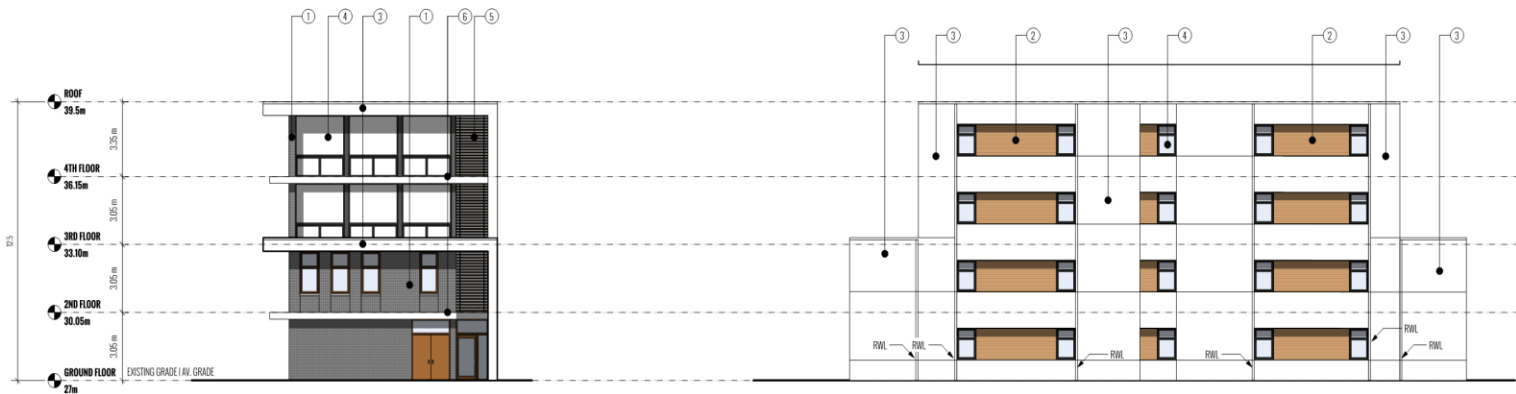
SITE PLAN & ROOF PLAN | APARTMENTS @ 953 BALMORAL | SCHEMATIC DESIGN FOR DEVELOPMENT PERMIT & REZONING APPLICATION | APRIL 2019 | SCALE 1:100

Proposed Site Plan



NORTH ELEVATION

WEST ELEVATION



SOUTH ELEVATION

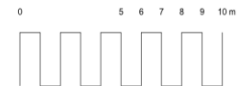
EAST ELEVATION

MATERIALS

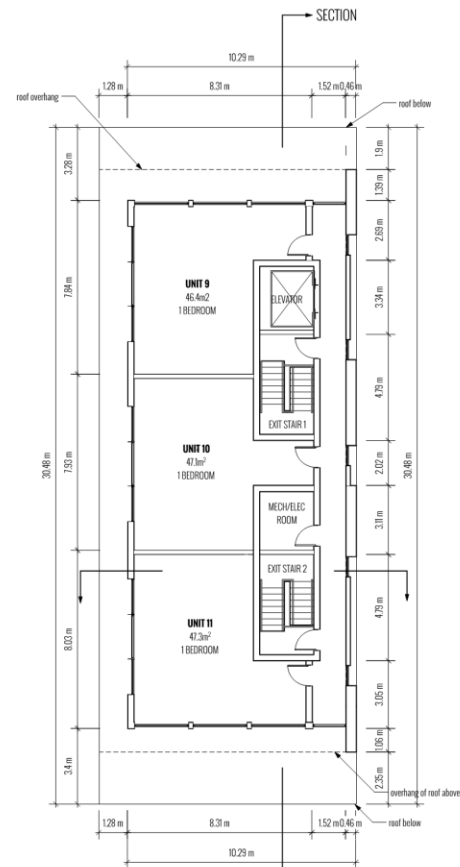
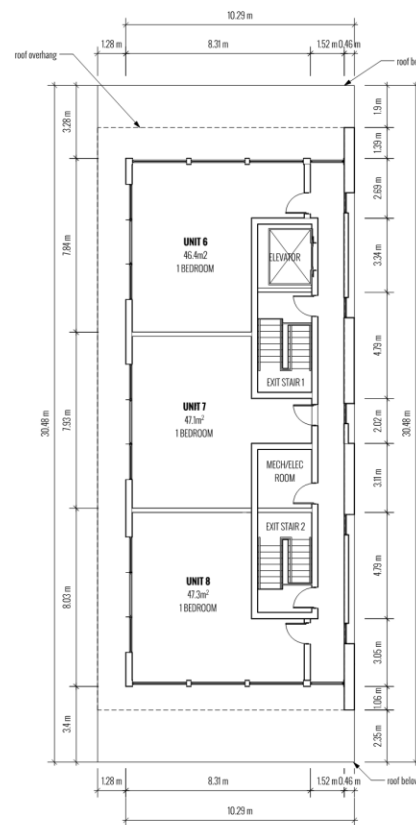
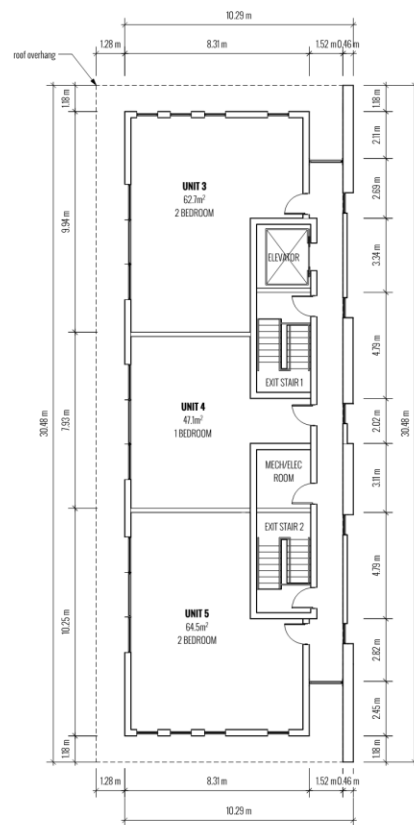
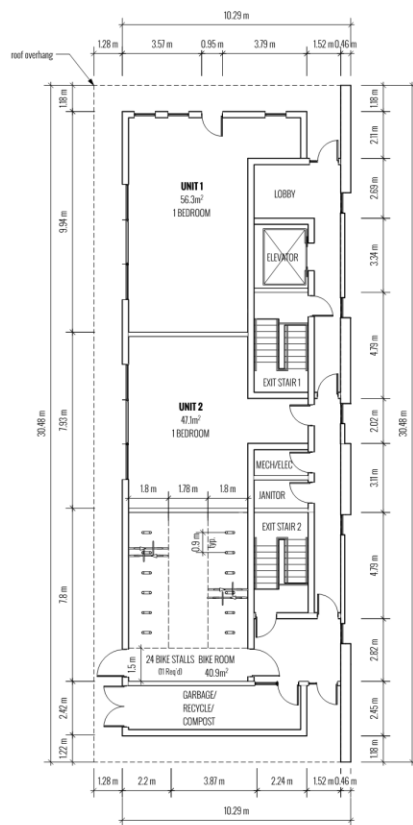
- ① Variable Charcoal Brick
- ② Stained Wood Siding
- ③ Stucco
- ④ Glazing
- ⑤ Screen
- ⑥ Brise Soleil

**COAST
+ BEAM**

ELEVATIONS | APARTMENTS @ 953 BALMORAL | SCHEMATIC DESIGN FOR DEVELOPMENT PERMIT & REZONING APPLICATION | APRIL 2019 | SCALE 1:100



Proposed Elevation Plans



COAST + BEAM

FLOOR PLANS | APARTMENTS @ 953 BALMORAL | SCHEMATIC DESIGN FOR DEVELOPMENT PERMIT & REZONING APPLICATION | APRIL 2019 | SCALE 1:100



Proposed floor plans



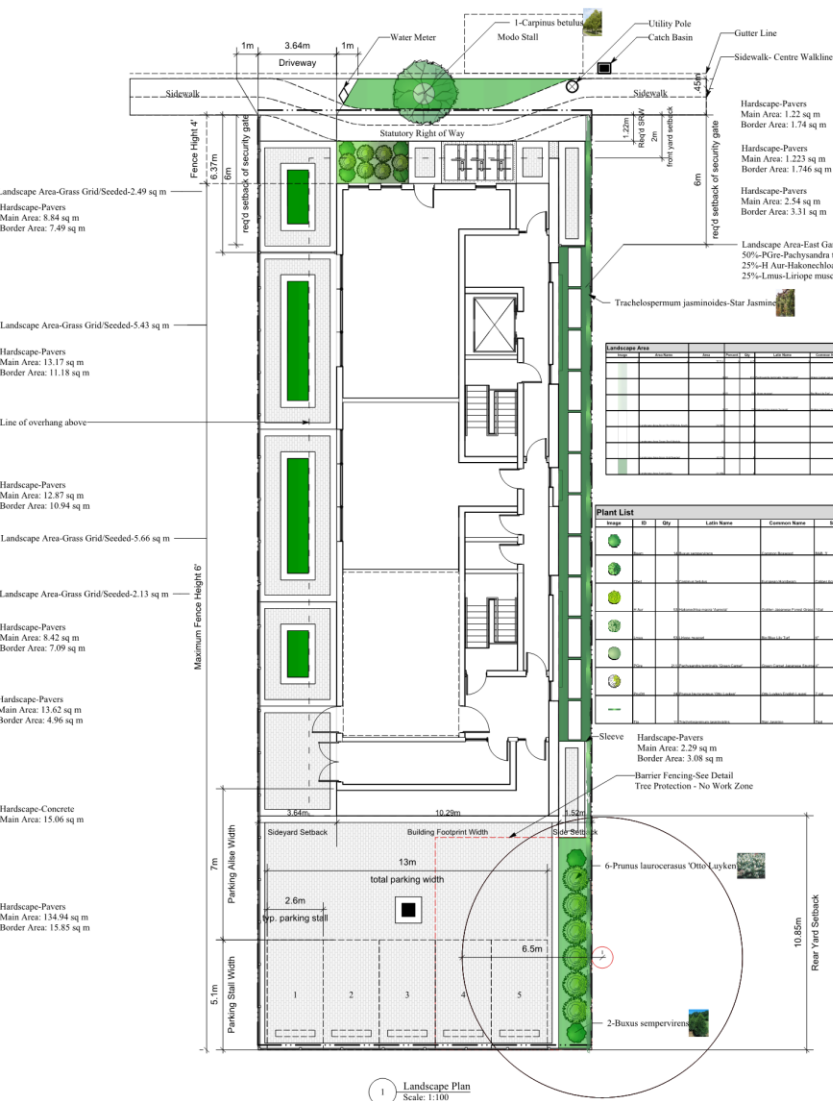


FIGURE 1: RENDERING OF NORTHWEST VIEW OF BUILDING

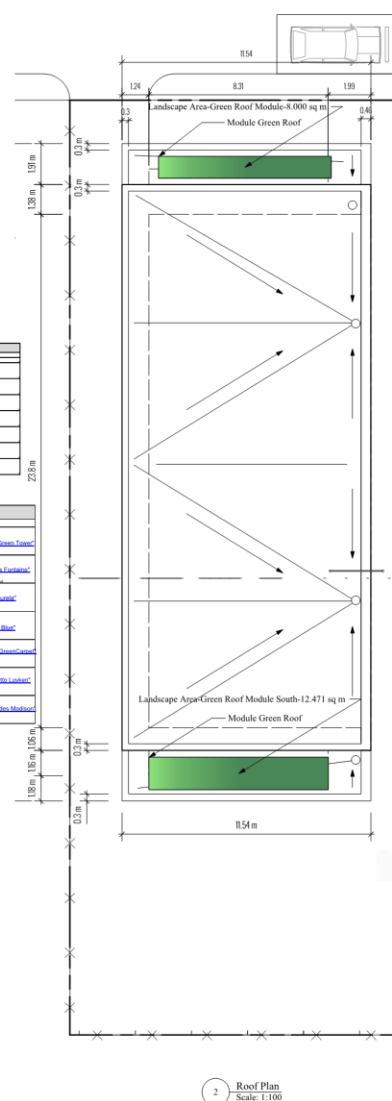


FIGURE 2: RENDERING OF NORTH VIEW OF BUILDING

Proposed landscape plan



953 Balmoral / Tree Management						
Image	ID	Botanical Name	Common Name	DBH	Action	Date Assessed
	1	<i>Prunus laurocerasus</i>	Laurel	2.1m	Retain - Arborist work	January 19, 2019
	2	<i>Prunus laurocerasus</i>	Laurel	2.1m	Retain - Arborist work	January 19, 2019
	3	<i>Prunus laurocerasus</i>	Laurel	2.1m	Retain - Arborist work	January 19, 2019
	4	<i>Prunus laurocerasus</i>	Laurel	2.1m	Retain - Arborist work	January 19, 2019
	5	<i>Prunus laurocerasus</i>	Laurel	2.1m	Retain - Arborist work	January 19, 2019
	6	<i>Prunus laurocerasus</i>	Laurel	2.1m	Retain - Arborist work	January 19, 2019
	7	<i>Prunus laurocerasus</i>	Laurel	2.1m	Retain - Arborist work	January 19, 2019
	8	<i>Prunus laurocerasus</i>	Laurel	2.1m	Retain - Arborist work	January 19, 2019
	9	<i>Prunus laurocerasus</i>	Laurel	2.1m	Retain - Arborist work	January 19, 2019
	10	<i>Prunus laurocerasus</i>	Laurel	2.1m	Retain - Arborist work	January 19, 2019

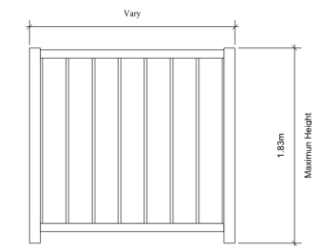


- General Notes**
- All Work shall be equal in all respects to good construction practices and shall conform to current Zoning, By-Laws and Regulations pertaining to the Governing Authority having jurisdiction and the British Columbia Building Code 2012, (BCBC 2012).
 - All materials, finishes, manufactured items, and equipment shall be installed in full accordance with the supplier's or manufacturer's written recommendations and instructions of these documents, whichever is more stringent.
 - Engineering shall be required for several aspects of the work on this project. Engineering specifications shall take precedence.
 - All layouts shall be confirmed by a registered B.C. Land Surveyor. All setbacks shall be confirmed by Owner/Builder and any modifications are to be made on site. Conformity of these plans to the actual site is the responsibility of the Owner/Builder.
 - Tree Protected - All work shall be conducted in such manner not to compromise the health or vigor of any protected tree. Work shall be conducted as per the Arborist Guidelines. All excavation with in the Critical Root Zone (CRZ) will be conducted with an air spade or as instructed by an arborist.
- Softscape and Tree Retention**
- Boulevard Tree subject to the City of Victoria Schedule C Supplemental Specifications for Street Trees and Irrigation / Victoria Subdivision and Development Servicing. Boulevard Tree shall be planted a minimum distance of 0.45m from a municipal sidewalk or road. Root guard shall be installed. All work shall be subject to inspections and comply with the City of Victoria Supplementary Specifications for Street Trees and Irrigation Schedule C.
 - Boulevard shall be planted with turf subject to the approval.
 - Recommend that the Irrigation System be designed by a Certified Irrigation Designer, and installed as per the Certified Irrigation Design. The Contractor installing the Irrigation System should be certified and install the specified system. The system should have a rain sensor with a Solar Sync and Rain Shut off as part of the irrigated controller system to ensure optimum controlled watering.
 - Landscape Areas have been created for the purpose of planting. The Areas will be planting using Plant Material based on percentages allocated and at a density per area. The Plant material is accounted for in the Planting Schedule.
 - Mulch should be applied to a depth of approximately 5cm.
 - The barrier shall be removed. All material shall be removed.
 - Designer may substitute plant material as required. Substitution of plant stock may be necessary due to the availability, size, condition or designer preference at any time during the planning or implementation process of the project. The substitution of plant material shall comply with the intent and spirit of the conceptual design.
 - Excavation of the subgrade below the rootball of a tree shall be kept to a minimum to prevent settling. The root ball should be set at a depth conducive to the finished grade.
 - The Contractor is responsible for the health and vigor of the plant material and provide adequate protection while the material is on site. Store plant material in the shade and provide water as required.
 - The placement of plant material on site may vary from the planting plan and schedule. The arrangement of plant material shall be determined at the time of planting and shall be configured as a best fit. Changes may occur to the placement of plant material due to variations of sub plant material, site conditions or as a result of planting stock substitutions.
 - All planting stock shall be planted in growing medium as required ensuring adequate depth.
 - All planting material shall be acquired from Certified Horticultural Suppliers.
 - All planting stock shall be planted in growing medium as required ensuring adequate depth.
 - Preparing Roots: All container stock shall have the roots checked for girdling and loosed as required prior to planting.
 - All container stock shall have the roots checked for girdling and loosed as required prior to planting.
 - All trees and shrubs shall be stacked and tied at the time of planting if required.
 - All stock shall be checked to ensure good condition prior to planting. Any stock indicating poor condition, health or vigor shall not be planted and reported to Supervisor and Designer.
 - The Designer may substitute stock as required and as a result of availability, stock conditions, size requirements or other constraints. Substitutions made by the designer shall be in context and consistent with the intent of the plan.
 - The soil used for the Green Roof shall be 1 part 3/8" lava stone and 4 parts planting medium as specified.
 - All planting material shall be acquired from Certified Horticultural Suppliers.
 - Planting Medium-Landscape
 - Percentage of Dry Weight of Total Growing Medium
 - Coarse Gravel - 19-40 ml - 0- 3%
 - All Gravel - 3-19 ml - 0-10%
 - Silt - 30-70%
 - Sand - <0.002 ml 15-50%
 - Clay - <0.002 ml 15-50%
 - Clay and Sand Combined / Maximum 60%
 - Organic Content Coastal - 2-10%
 - Acidity PH 4.5-7.0
 - Structural soil composite shall be composed of growing medium and clear crush granular components in accordance with the following recommended base ratio of materials. Ensure sufficient moisture (25% to 75% of field capacity) to provide a homogeneous mixture with consistent properties throughout the composite soil. Post moss shall not be used in the preparation of structural soil.
 - Soil Component
 - Proportion by Weight
 - Growing medium 15% to 20% dry weight
 - Clear Crush (25mm to 75mm clear crush 80% to 85% dry weight
 - Hydrogel Stabilizer* 0.01% to 0.02%
 - *Hydrogel Stabilizer is applied as a soil tackifier to ensure even distribution and blending of the component materials. Refer to manufacturer specifications for appropriate mixing proportions.
 - Growing medium proportion for use as a component in structural soil shall conform to Topsoil and Grading (32 91 21 MMCD 2009 Supplemental).
 - Clear crush gravel proportion for use as a component in structural soil shall conform to Aggregates and Granular Materials (31 05 17 MMCD 2009) specifications. Gravel gradation shall consist of 25mm to 75mm clear crush washed rock free of any foreign elements or materials.
 - Structural soil installation shall conform to Excavating, Trenching and Backfilling (31 23 01 MMCD 2009) specifications.
 - DeepRoot UB 24-2 shall be installed as per manufacturer's specifications.
 - All Landscape work shall comply with the latest edition of the Bc Landscape Standard.
 - Tree Protected - All work shall be conducted in such manner not to compromise the health or vigor of any protected tree. Work shall be conducted as per the Arborist Guidelines. All excavation with in the Critical Root Zone (CRZ) will be conducted with an air spade or as instructed by an arborist.

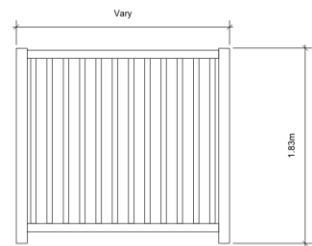
No	Date	Appr	Revision Notes
1	2018-12-06	LM	L1 Softscape and Tree Retention Plan New Sheet Submission
2	2019-04-23	LM	Removal Vise
3	2019-05-06	LM	Revised Softscape

No	Date	Issue Notes
1	2018-12-06	Issued
2	2019-04-24	Sheets Revised / As requested
3	2019-05-06	Revised Softscape

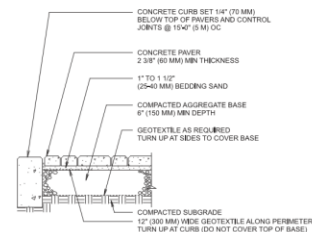
Errors and Omissions	
Designer	Will not be responsible for the costs incurred to owner or Contractor(s) through errors or omissions on plans or drawing specifications after building permits are issued, contract or agreement to commence work are made.
Contractor	Do not scale drawing. The contractor / owner shall check all dimensions and aspects of the plan, and make written agreement prior to construction. Notify the designer of any discrepancies before work commences. All materials and workmanship to meet the requirements of the current issue of the British Columbia Building Code 2012, Municipal By-Laws. Road Plan to conform with any Professional Engineering Drawings or Reports, Geotechnical Drawings or Reports or Legal Surveys and other related documents.
Part	This drawing is the intellectual property of the designer and is to be used exclusively for the project shown. Any reproduction in whole, or in part, shall only be permitted with the written consent and authorization from Larry Myers of Affresco Living Design.
All work shall comply with the British Columbia Building Code 2012 and all Municipal By-Laws.	
All Windows, Doors and Skylights shall comply with BCBC 2012 Part 9.	
Contractor to all Fire and Life provisions of the BCBC 2012.	
Locate and Install all Smoke Detectors in accordance with BCBC Part 9.	
Affresco	
Living Design	
Design Firm	
Larry Myers Affresco Living Design 79-450 Parklands Drive, Victoria, BC, V8A 1Y9 affrescolivingdesign@gmail.com	
exterior	
interior	
environment	
Project Title	
Apartments @ 953 Balmoral	
Sheet Title	
Softscape And Tree Retention Plan	
Notes	
For Development Application Only	
Project Manager	Project Engineer
LM	LM
LM	LM
May 6, 2019	L-2
2019/Balmoral	



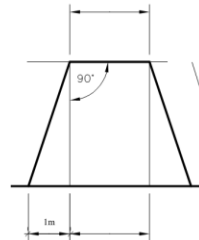
1 Privacy Fence
L-3



2 Fence-Typical
L-3



3 Privacy Gate
L-3

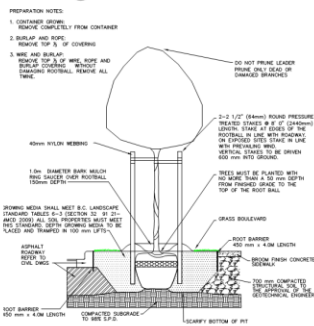


TYPE 'A'

5 Driveway-Type 'A'
L-3

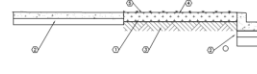
- NOTES:
- THICKNESS OF AGGREGATE BASE WILL VARY WITH SUBGRADE CONDITIONS AND CLIMATE.
 - COLDER CLIMATES MAY REQUIRE THICKER BASES.
 - CONCRETE PAVERS SHOULD BE PLACED ON A CEMENT TREATED BASE IF SOIL IS EXTREMELY WEAK OR CONSTANTLY SATURATED. PAVERS CAN BE OVERLAPPED OR PLACED ON EXISTING ASPHALT OR CONCRETE DRIVEWAYS.
 - CONSULT FOR TECH SPEC 2 FOR GUIDELINES ON SPECIFICATIONS FOR BASE MATERIALS, SUBGRADE SOIL AND BASE COMPACTION.
 - PLASTIC, STEEL, ALUMINUM OR PRECAST CONCRETE, EDGING MAY BE USED.

4 Permeable Pavers
L-3



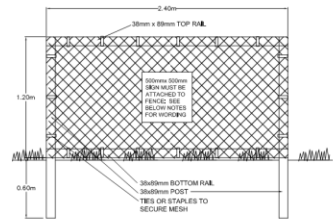
- NOTES:
- ALL TREES SHALL MEET OR EXCEED THE CITY OF VICTORIA TREE REGULATIONS.
 - ROOT BARRIERS REQUIRED ON 800 x 200 x 10mm OR 45 DEGREES BY THE DIRECTION OF FENCE.
 - ROOT BARRIERS REQUIRED PENETRATING TREE SPECIES AND/OR 800mm.

6 Tree Planting in Boulevard
L-3



- NOTES:
- APPROVED, COMPACTED GROWING MEDIUM PLACED AS PER MMCO 32 BY 21 TABLE 3A.
 - FOR CURB, SIDEWALK, ASPHALT, DUCT AND ROAD BASE, REFER TO MMCO AND CITY OF VICTORIA SUPPLEMENTARY STANDARDS.
 - APPROVED SUBGRADE TO MMCO AND CITY OF VICTORIA SUPPLEMENTARY STANDARDS.
 - SOD OR SEED, AS SPECIFIED TO MMCO AND CITY OF VICTORIA SUPPLEMENTARY STANDARDS.
 - SOD NOT TO BE REINFORCED WITH MESH.

7 Sod - Boulevard Detail
L-3



- TREE PROTECTION FENCING
- FENCE WILL BE CONSTRUCTED USING 38mm X 88mm WOOD FRAME; TOP, BOTTOM AND POSTS * USE ORANGE SNOWFENCING MESH AND SECURE THE WOOD FRAME WITH 20P TIES OR GALVANIZED STAPLES.
 - ATTACH A 300mm X 500mm SIGN WITH THE FOLLOWING WORKING WARNING: TREE PROTECTION AREA, THIS SIGN MUST BE AFFIXED ON EVERY FENCE OR AT LEAST EVERY 10 LINEAR METERS.
 - IN ROCKY AREAS, METAL POSTS (T-BAR OR REBAR) DRILLED INTO ROCK WILL BE ACCEPTED.

8 Tree Protection Fencing
L-3

No.	Date	Appr.	Revision Notes
1	2019-11-29	LM	L-3, Detail New Sheet Submission
2	2019-04-12	LM	Remove Trellis / Silva Cell Detail
3	2019-05-06	LM	Revised

No.	Date	Issue Notes
1	2018-12-06	Issued
2	2019-04-24	Detail Sheet Updated
3	2019-05-06	Sheet Revised

Errors and Omissions:	
Designer: Will not be responsible for the costs incurred to correct or Contractor(s) through errors or omissions on plans or drawing specifications after building permit(s) are issued, contracts or agreements to commence work are made.	
Contractor: Do not scale drawing. The contractor / owner shall check all dimensions and aspects of this plan, and make written agreement prior to construction. Notify the designer of any discrepancies before work commences. All materials and workmanship to meet the requirements of the current issue of the British Columbia Building Code 2012, Municipal by-law, Road Panel in compliance with the Professional Engineering Drawings or Reports, Geotechnical Drawings or Reports or Legal Surveys and other related documents.	
This drawing is the intellectual property of the designer and is to be used exclusively for the project shown. Any reproduction in whole, or in part, shall only be permitted with the written consent and authorization from Larry Myers of Allsource Living Designs.	
All work shall comply with the British Columbia Building Code 2012 and all Municipal By-Laws.	
All Windows, Doors and Skylights shall comply with BCBC 2012 Part 9, Clause 9.04. Fire and Life provisions of the BCBC 2012, Part 9.	
Locate and Install all Smoke Detectors in accordance with BCBC Part 9.	

Allsource Living Design	
Design Firm	Larry Myers Allsource Living Design 79-850 Parklands Drive, Victoria, BC, V9A 7L9 allsourcelivingdesigns@gmail.com allsourcelivingdesigns.ca
exterior	230-893-1082 landscaping

Project Title	
Apartments @ 953 Balmoral	
Sheet Title	
Detail	
Scale	
For Development Application Only	
Drawn By	Project ID
Checked By	Scale
Approved By	LM
Drawn By	LM
Date	May 6, 2019
Sheet No.	L-3
Project No.	2019/Balmoral

Proposed fence and gate diagrams



**COAST
+ BEAM**

Proposed streetscape