

G.2 1050-1058 Pandora Avenue & 1508-1518 Cook Street: Update Report - Rezoning Application No. 00695, HD Application No. 000188 and Heritage Alteration Permit Application No. 00016 (Wellburn's Site)(North Park)

Committee received an update report dated July 23, 2020 from the Director of Sustainable Planning and Community Development regarding the Rezoning and Heritage Alteration Permit Applications for the properties located at 1050-1058 Pandora Avenue and 1508-1518 Cook Street, considered by Council at the Committee of the Whole (COTW) meeting of December 12, 2019.

Moved By Mayor Helps
Seconded By Councillor Young

Rezoning Application No. 00695

That Council instruct staff to prepare the necessary Zoning Regulation Bylaw Amendment that would authorize the proposed development outlined in Rezoning Application No. 00695 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street, that first and second reading of the Zoning Regulation Bylaw Amendment be considered by Council and a Public Hearing date be set once the following conditions are met:

1. Preparation and execution of the appropriate legal agreements in order to secure the following:
 - a. all dwelling units remain rental in perpetuity;
 - b. a Statutory Right-of-Way of 3.083m along Cook Street, to the satisfaction of the Director of Engineering and Public Works;
 - c. pay for the construction an asphalt pathway in Franklin Green Park to connect the proposed 3m wide pathway at the northwest corner of the site to an existing park pathway to the west to the satisfaction of the Director of Parks, Recreation and Facilities;
 - d. provide a bus shelter on Cook Street in accordance with the site plan dated November 22, 2019, to the satisfaction of the Director of Engineering and Public Works and the Director of Sustainable Planning and Community Development;
 - e. provide two car share vehicles, two assigned car share parking spaces on-site, 118 car share memberships, a \$100 of driving credits for each resident, and 40 electric bicycle charging stations (110V wall outlets) in the underground parkade, to the satisfaction of the Director of Engineering and Public Works;
 - f. purchase 100 transit passes through BC Transit's EcoPass program to the satisfaction of the Director of Engineering and Public Works;
 - g. permit the proposed mews to access Franklin Green Park for fire exit purposes;
 - h. maintain unrestricted public access to the mews from Cook Street and Franklin Green Park from 8AM-8PM each day;
 - i. provide a community amenity contribution in the amount of \$24,000.00 toward the Tree Reserve Fund for tree planting purposes and enhancing the city's urban forest; and
 - j. provide a community amenity contribution in the amount of \$17,800.00 toward the Local Amenity Reserve Fund for the construction of two accessible asphalt pathways within Franklin Green Park linking Cook Street and the development to the existing pathways in the park and for

- the purchase and installation of a drinking fountain for the park to the satisfaction of the Director of Parks, Recreation and Facilities.
- k. design, supply and installation of the 'New Town District' framework along both frontages, including furnishings, materials and pedestrian lights, as laid out in the City's Downtown Public Realm Plan and Streetscape Standards document to the satisfaction of the Director of Engineering and Public Works.
2. Council authorizing street-level projecting canopies and cornices over the City Right-of-Way on 1050-1058 Pandora Avenue and 1508-1518 Cook Street, provided that the applicant enters into an Encroachment Agreement in a form satisfactory to the City Solicitor and the Director of Engineering and Public Works.

Heritage Alteration Permit Application No. 00016

That Council, after giving notice and allowing an opportunity for public comment at a meeting of Council and after the Public Hearing for Rezoning Application No. 00695, if it is approved, consider the following motions:

1. "That Council authorize the issuance of Heritage Alteration Permit Application No. 00016 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street, in accordance with:
 - a. The Plans, date stamped July 15, 2020
 - b. The Conservation Plan for the Parkway Apartments, date stamped July 16, 2019.
 - c. Final plans to be generally in accordance with the plans identified above to the satisfaction of the Director of Sustainable Planning and Community Development.
 - d. Heritage Alteration Permit lapsing two years from the date of this resolution.
2. That Council approve the designation of the heritage-registered property located at 1050-1058 Pandora Avenue and 1508-1516 Cook Street, pursuant to Section 611 of the Local Government Act, as a Municipal Heritage Site, consistent with the plans dated July 15, 2020."

Committee discussed the following:

- *Proposed public access and hours through gate*
- *Removal of green roof from development plans*
- *Underground utilities along Cook Street*

Amendment:

Moved By Councillor Isitt

Seconded By Councillor Loveday

3. That staff engage BC Hydro and report back at the time for a Public Hearing on options for locating some street trees on the Cook Street frontage.

Amendment to the amendment:

Moved By Mayor Helps

Seconded By Councillor Alto

3. That staff ~~engage BC Hydro and report back at the time for a Public Hearing on options for locating some street trees on the Cook Street frontage~~ be directed to work w the applicant to achieve some form of greenery in the public realm as part of this development.

CARRIED UNANIMOUSLY

Amendment to the amendment:

Moved By Councillor Isitt
Seconded By Mayor Helps

3. That staff be directed to work with the applicant to achieve some street trees or form of greenery in the public realm on Cook Street as part of this development.

CARRIED UNANIMOUSLY

On the amendment:

3. That staff be directed to work with the applicant to achieve some street trees or form of greenery in the public realm on Cook Street as part of this development.

CARRIED UNANIMOUSLY

Amendment:

Moved By Councillor Thornton-Joe
Seconded By Councillor Alto

That the applicant consider laminated glass for sound attenuation and energy efficiency on the heritage building.

FOR (4): Councillor Alto, Councillor Dubow, Councillor Isitt, Councillor Thornton-Joe
OPPOSED (4): Mayor Helps, Councillor Loveday, Councillor Potts, Councillor Young
DEFEATED (4 to 4)

On the main motion as amended:

Rezoning Application No. 00695

That Council instruct staff to prepare the necessary Zoning Regulation Bylaw Amendment that would authorize the proposed development outlined in Rezoning Application No. 00695 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street, that first and second reading of the Zoning Regulation Bylaw Amendment be considered by Council and a Public Hearing date be set once the following conditions are met:

1. Preparation and execution of the appropriate legal agreements in order to secure the following:
 - a. all dwelling units remain rental in perpetuity;

- b. a Statutory Right-of-Way of 3.083m along Cook Street, to the satisfaction of the Director of Engineering and Public Works;
 - c. pay for the construction an asphalt pathway in Franklin Green Park to connect the proposed 3m wide pathway at the northwest corner of the site to an existing park pathway to the west to the satisfaction of the Director of Parks, Recreation and Facilities;
 - d. provide a bus shelter on Cook Street in accordance with the site plan dated November 22, 2019, to the satisfaction of the Director of Engineering and Public Works and the Director of Sustainable Planning and Community Development;
 - e. provide two car share vehicles, two assigned car share parking spaces on-site, 118 car share memberships, a \$100 of driving credits for each resident, and 40 electric bicycle charging stations (110V wall outlets) in the underground parkade, to the satisfaction of the Director of Engineering and Public Works;
 - f. purchase 100 transit passes through BC Transit's EcoPass program to the satisfaction of the Director of Engineering and Public Works;
 - g. permit the proposed mews to access Franklin Green Park for fire exit purposes;
 - h. maintain unrestricted public access to the mews from Cook Street and Franklin Green Park from 8AM-8PM each day;
 - i. provide a community amenity contribution in the amount of \$24,000.00 toward the Tree Reserve Fund for tree planting purposes and enhancing the city's urban forest; and
 - j. provide a community amenity contribution in the amount of \$17,800.00 toward the Local Amenity Reserve Fund for the construction of two accessible asphalt pathways within Franklin Green Park linking Cook Street and the development to the existing pathways in the park and for the purchase and installation of a drinking fountain for the park to the satisfaction of the Director of Parks, Recreation and Facilities.
 - k. design, supply and installation of the 'New Town District' framework along both frontages, including furnishings, materials and pedestrian lights, as laid out in the City's Downtown Public Realm Plan and Streetscape Standards document to the satisfaction of the Director of Engineering and Public Works.
2. Council authorizing street-level projecting canopies and cornices over the City Right-of-Way on 1050-1058 Pandora Avenue and 1508-1518 Cook Street, provided that the applicant enters into an Encroachment Agreement in a form satisfactory to the City Solicitor and the Director of Engineering and Public Works.
3. That staff be directed to work with the applicant to achieve some street trees or form of greenery in the public realm on Cook Street as part of this development.

Heritage Alteration Permit Application No. 00016

That Council, after giving notice and allowing an opportunity for public comment at a meeting of Council and after the Public Hearing for Rezoning Application No. 00695, if it is approved, consider the following motions:

- 1. "That Council authorize the issuance of Heritage Alteration Permit Application No. 00016 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street, in accordance with:

- a. The Plans, date stamped July 15, 2020
 - b. The Conservation Plan for the Parkway Apartments, date stamped July 16, 2019.
 - c. Final plans to be generally in accordance with the plans identified above to the satisfaction of the Director of Sustainable Planning and Community Development.
 - d. Heritage Alteration Permit lapsing two years from the date of this resolution.
2. That Council approve the designation of the heritage-registered property located at 1050-1058 Pandora Avenue and 1508-1516 Cook Street, pursuant to Section 611 of the Local Government Act, as a Municipal Heritage Site, consistent with the plans dated July 15, 2020.”

CARRIED UNANIMOUSLY

**F.1.b.c. 1050-1058 Pandora Avenue & 1508-1518 Cook Street:
Update Report - Rezoning Application No. 00695, HD
Application No. 000188 and Heritage Alteration Permit
Application No. 00016 (Wellburn's Site)(North Park)**

Moved By Councillor Young

Seconded By Councillor Alto

That Council instruct staff to prepare the necessary Zoning Regulation Bylaw Amendment that would authorize the proposed development outlined in Rezoning Application No. 00695 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street, that first and second reading of the Zoning Regulation Bylaw Amendment be considered by Council and a Public Hearing date be set once the following conditions are met:

1. Preparation and execution of the appropriate legal agreements in order to secure the following:
 - a. all dwelling units remain rental in perpetuity;
 - b. a Statutory Right-of-Way of 3.083m along Cook Street, to the satisfaction of the Director of Engineering and Public Works;
 - c. pay for the construction an asphalt pathway in Franklin Green Park to connect the proposed 3m wide pathway at the northwest corner of the site to an existing park pathway to the west to the satisfaction of the Director of Parks, Recreation and Facilities;
 - d. provide a bus shelter on Cook Street in accordance with the site plan dated November 22, 2019, to the satisfaction of the Director of Engineering and Public Works and the Director of Sustainable Planning and Community Development;
 - e. provide two car share vehicles, two assigned car share parking spaces on-site, 118 car share memberships, a \$100 of driving credits for each resident, and 40 electric bicycle charging stations (110V wall outlets) in the underground parkade, to the satisfaction of the Director of Engineering and Public Works;
 - f. purchase 100 transit passes through BC Transit's EcoPass program to the satisfaction of the Director of Engineering and Public Works;
 - g. permit the proposed mews to access Franklin Green Park for fire exit purposes;
 - h. maintain unrestricted public access to the mews from Cook Street and Franklin Green Park from 8AM-8PM each day;
 - i. provide a community amenity contribution in the amount of \$24,000.00 toward the Tree Reserve Fund for tree planting purposes and enhancing the city's urban forest; and
 - j. provide a community amenity contribution in the amount of \$17,800.00 toward the Local Amenity Reserve Fund for the construction of two accessible asphalt pathways within Franklin Green Park linking Cook Street and the

development to the existing pathways in the park and for the purchase and installation of a drinking fountain for the park to the satisfaction of the Director of Parks, Recreation and Facilities.

- k. design, supply and installation of the 'New Town District' framework along both frontages, including furnishings, materials and pedestrian lights, as laid out in the City's Downtown Public Realm Plan and Streetscape Standards document to the satisfaction of the Director of Engineering and Public Works.
2. Council authorizing street-level projecting canopies and cornices over the City Right-of-Way on 1050-1058 Pandora Avenue and 1508-1518 Cook Street, provided that the applicant enters into an Encroachment Agreement in a form satisfactory to the City Solicitor and the Director of Engineering and Public Works.
3. That staff be directed to work with the applicant to achieve some street trees or form of greenery in the public realm on Cook Street as part of this development.

Heritage Alteration Permit Application No. 00016

That Council, after giving notice and allowing an opportunity for public comment at a meeting of Council and after the Public Hearing for Rezoning Application No. 00695, if it is approved, consider the following motions:

1. "That Council authorize the issuance of Heritage Alteration Permit Application No. 00016 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street, in accordance with:
 - a. The Plans, date stamped July 15, 2020
 - b. The Conservation Plan for the Parkway Apartments, date stamped July 16, 2019.
 - c. Final plans to be generally in accordance with the plans identified above to the satisfaction of the Director of Sustainable Planning and Community Development.
 - d. Heritage Alteration Permit lapsing two years from the date of this resolution.
2. That Council approve the designation of the heritage-registered property located at 1050-1058 Pandora Avenue and 1508-1516 Cook Street, pursuant to Section 611 of the Local Government Act, as a Municipal Heritage Site, consistent with the plans dated July 15, 2020."

FOR (6): Mayor Helps, Councillor Alto, Councillor Loveday, Councillor Potts, Councillor Thornton-Joe, Councillor Young

OPPOSED (1): Councillor Dubow

CARRIED (6 to 1)



Committee of the Whole Report

For the Meeting of August 6, 2020

To: Committee of the Whole **Date:** July 23, 2020
From: Karen Hoese, Director, Sustainable Planning and Community Development
Subject: Update on Rezoning Application No. 00695, Heritage Designation Application No. 000188 and Heritage Alteration Permit Application No. 00016 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street ("Wellburn's Site")

RECOMMENDATIONS

Rezoning Application No. 00695

That Council instruct staff to prepare the necessary Zoning Regulation Bylaw Amendment that would authorize the proposed development outlined in Rezoning Application No. 00695 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street, that first and second reading of the Zoning Regulation Bylaw Amendment be considered by Council and a Public Hearing date be set once the following conditions are met:

1. Preparation and execution of the appropriate legal agreements in order to secure the following:
 - a. all dwelling units remain rental in perpetuity;
 - b. a Statutory Right-of-Way of 3.083m along Cook Street, to the satisfaction of the Director of Engineering and Public Works;
 - c. pay for the construction an asphalt pathway in Franklin Green Park to connect the proposed 3m wide pathway at the northwest corner of the site to an existing park pathway to the west to the satisfaction of the Director of Parks, Recreation and Facilities;
 - d. provide a bus shelter on Cook Street in accordance with the site plan dated November 22, 2019, to the satisfaction of the Director of Engineering and Public Works and the Director of Sustainable Planning and Community Development;
 - e. provide two car share vehicles, two assigned car share parking spaces on-site, 118 car share memberships, a \$100 of driving credits for each resident, and 40 electric bicycle charging stations (110V wall outlets) in the underground parkade, to the satisfaction of the Director of Engineering and Public Works;
 - f. purchase 100 transit passes through BC Transit's EcoPass program to the satisfaction of the Director of Engineering and Public Works;
 - g. permit the proposed mews to access Franklin Green Park for fire exit purposes;**
 - h. maintain unrestricted public access to the mews from Cook Street and Franklin**

- Green Park from 8AM-8PM each day;**
- i. **provide a community amenity contribution in the amount of \$24,000.00 toward the Tree Reserve Fund for tree planting purposes and enhancing the city's urban forest; and**
 - j. **provide a community amenity contribution in the amount of \$17,800.00 toward the Local Amenity Reserve Fund for the construction of two accessible asphalt pathways within Franklin Green Park linking Cook Street and the development to the existing pathways in the park and for the purchase and installation of a drinking fountain for the park to the satisfaction of the Director of Parks, Recreation and Facilities.**
2. Council authorizing street-level projecting canopies and cornices over the City Right-of-Way on 1050-1058 Pandora Avenue and 1508-1518 Cook Street, provided that the applicant enters into an Encroachment Agreement in a form satisfactory to the City Solicitor and the Director of Engineering and Public Works.

Heritage Alteration Permit Application No. 00016

That Council, after giving notice and allowing an opportunity for public comment at a meeting of Council and after the Public Hearing for Rezoning Application No. 00695, if it is approved, consider the following motions:

1. "That Council authorize the issuance of Heritage Alteration Permit Application No. 00016 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street, in accordance with:
 - a) The Plans, date stamped **July 15, 2020**
 - b) The Conservation Plan for the Parkway Apartments, date stamped July 16, 2019.
 - c) Final plans to be generally in accordance with the plans identified above to the satisfaction of the Director of Sustainable Planning and Community Development.
 - d) Heritage Alteration Permit lapsing two years from the date of this resolution.
2. That Council approve the designation of the heritage-registered property located at 1050-1058 Pandora Avenue and 1508-1516 Cook Street, pursuant to Section 611 of the *Local Government Act*, as a Municipal Heritage Site, consistent with the plans dated **July 15, 2020.**"

EXECUTIVE SUMMARY

The purpose of this report is to provide Council with information, analysis and recommendations on proposed revisions to the Rezoning and Heritage Alteration Permit Applications for the properties located at 1050 Pandora Avenue, known as Wellburn's or Parkway Apartments. The revisions are the result of the applicant's review of the constructability and costing of the November 22, 2019 architectural plans, which they determined was not financially viable. In response, the architect has made a series of revisions to the project design, some of which are improvements to the design and unrelated to project costs:

- the exterior cladding of the four to six storey addition to the heritage registered Parkway Apartments building has been revised from ultra-high-performance concrete panels to stucco, which is a more cost-effective cladding
- window assemblies for the addition now include a transom window, which improves operability and reduces cost

- two rental dwelling units have been added (studio + den and a one bedroom unit), which has slightly increased the gross floor area of the building from 8,412 square metres to 8,544 square metres (a difference of 132 square metres) and the overall density from 2.92:1 floor space ratio (FSR) to 2.97:1 FSR (an increase of 0.05:1 FSR)
- eight residential bicycle parking spaces have been added (six long term, two short term)
- one additional visitor parking space has been added to the underground parking level
- the sedum roof proposed for level 5 of the building has been changed to a gravel roof
- the saw tooth pattern of chamfered balconies at the northeast, north and northwest faces at the 6th storey of the building has been removed, and these portions of the facade now have a flat profile without any balcony recesses
- the garbage and recycling room for the building has been relocated from the main level to the basement level
- an additional path connection between the mews and the path system in Franklin Green Park is proposed. This connection meets fire exiting requirements for the building and improves connectivity to Cook Street. The applicant has agreed to pay for the path connection as well as purchase and pay for the installation of a new water fountain in the park as an additional community amenity contribution
- due to the existing location of the underground utilities along Cook Street, street trees cannot be accommodated along this frontage. The applicant is willing to provide a community amenity contribution of \$24,000.00 for tree planting elsewhere in the neighbourhood in order to enhance the City's urban forest.

There are no changes to the proposed rehabilitation strategy for the heritage registered Parkway Apartments Building.

Staff consider the changes to the exterior cladding and fenestration of the building to be qualitatively minor, while other changes, including public access to the mews and relocation of the garbage room a meaningful improvement to the November 2019 design (attached).

BACKGROUND

Description of Proposal

The purpose of this report is to present Council with an update regarding the Rezoning and Heritage Alteration Permit Applications for the properties located at 1050-1058 Pandora Avenue and 1508-1518 Cook Street, considered by Council at the Committee of the Whole (COTW) meeting of December 12, 2019. The proposal is to rezone from the CA-1 Zone, Pandora Avenue Special Commercial District, and R-2 Zone, Two-Family Dwelling District, to a new zone in order to increase the density and construct a new four- and six-storey addition consisting of ground floor commercial uses and residential above. There is a concurrent application to heritage designate the existing "Wellburn's" building.

The motion from the December 12, 2019, Council meeting is as follows:

Rezoning Application No. 000695

"That subject to the applicant committing to measures outlined in the District Correspondence outlining the summary of the Parkway Building flood from November 2 to December 10, Council instruct staff to prepare the necessary Zoning Regulation Bylaw Amendment that would authorize the proposed development outlined in Rezoning Application No. 00695 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street, that first and second reading of the Zoning

Regulation Bylaw Amendment be considered by Council and a Public Hearing date be set once the following conditions are met:

1. *Preparation and execution of the appropriate legal agreements in order to secure the following:*
 - a. *all dwelling units remain rental in perpetuity;*
 - b. *a Statutory Right-of-Way of 3.083m along Cook Street, to the satisfaction of the Director of Engineering and Public Works;*
 - c. *pay for the construction of an asphalt pathway in Franklin Green Park to connect the proposed 3m wide pathway at the northwest corner of the site to an existing park pathway to the west to the satisfaction of the Director of Parks, Recreation and Facilities;*
 - d. *provide a bus shelter on Cook Street in accordance with the site plan dated November 22, 2019, and to the satisfaction of the Director of Engineering and Public Works and the Director of Sustainable Planning and Community Development;*
 - e. *provide two car share vehicles, two assigned car share parking spaces on-site, 118 car share memberships, a \$100 of driving credits for each resident, and 40 electric bicycle charging stations (11 OV wall outlets) in the underground parkade, to the satisfaction of the Director of Engineering and Public Works; and*
 - f. *purchase 100 transit passes through BC Transit's EcoPass program to the satisfaction of the Director of Engineering and Public Works.*
2. *Council authorizing street-level projecting canopies and cornices over the City Right-of-Way on 1050-1058 Pandora Avenue and 1508-1518 Cook Street, provided that the applicant enters into an Encroachment Agreement in a form satisfactory to the City Solicitor and the Director of Engineering and Public Works."*

Heritage Designation Application No. 000188 and Heritage Alteration Permit Application No. 00016

"That Council, after giving notice and allowing an opportunity for public comment at a meeting of Council and after the Public Hearing for Rezoning Application No. 00695, if it is approved, consider the following motions:

1. *"That Council authorize the issuance of Heritage Alteration Permit Application No. 00016 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street, in accordance with:*
 - a. *Plans, date stamped November 22, 2019.*
 - b. *The Conservation Plan for the Parkway Apartments, date stamped July 2019.*
 - c. *Final plans to be generally in accordance with the plans identified above to the satisfaction of the Director of Sustainable Planning and Community Development.*
 - d. *Heritage Alteration Permit lapsing two years from the date of this resolution.*
2. *That Council approve the designation of the heritage-registered property located at 1050-1058 Pandora Avenue and 1508-1516 Cook Street,*

pursuant to Section 611 of the Local Government Act, as a Municipal Heritage Site, consistent with the plans dated November 22, 2019."

In the months after COTW, the applicant approached staff with proposed changes to the exterior cladding and fenestration of the building as well as slight changes to the interior configuration in order to make the project financially viable and improve the overall design and function of the building. Other proposed changes include modifications and enhanced connectivity to Franklin Green Park.

Data Table

The following data table compares the proposal with the November 19, 2019 plans and the existing CA-1 Zone, Pandora Avenue Special Commercial District. An asterisk is used to identify where the proposal is less stringent than the existing Zone.

Zoning Criteria	November 19, 2019 Plans	Revised Proposal (July 15, 2020)	Zone Standard (CA-1 Zone)
Site area (m ²) – minimum	2879	2879	n/a
Density (Floor Space Ratio) – maximum	2.92:1*	2.97:1*	2:1
Total floor area (m ²) – maximum	8412.30 *	8544.5*	5758
Height (m) – maximum	20.22 *	20.22*	15.50
Storeys – maximum	6	6	n/a
Site coverage (%) – maximum	70	70	n/a
Open site space (%) – minimum	22	22	n/a
Setbacks (m) – minimum			
Street setback (Pandora Avenue – south)	0 * (existing & Levels 1-2) 3.97 (proposed Levels 3-6)	0 * (existing & Levels 1-2) 3.97 (proposed Levels 3-6)	3
Street setback (Cook Street – east)	0 * (existing & Levels 1-2) 3.08 (proposed Levels 3-4) 13.13 (proposed Levels 5-6)	0 * (existing & Levels 1-2) 3.08 (proposed Levels 3-4) 13.13 (proposed Levels 5-6)	3
Side (North)	6.18 (proposed Levels 1-4) 7.60 (proposed	6.18 (proposed Levels 1-4) 7.60 (proposed	0 or 3

Zoning Criteria	November 19, 2019 Plans	Revised Proposal (July 15, 2020)	Zone Standard (CA-1 Zone)
Side (West)	Levels 5-6) 0 * (existing & Levels 1-2) 2.96 * (proposed Levels 3-4) 3.33 (proposed Levels 5-6)	Levels 5-6) 0 * (existing & Levels 1-2) 2.96 * (proposed Levels 3-4) 3.33 (proposed Levels 5-6)	0 or 3
Vehicle parking – minimum			
Residential	33 *	33 *	66
Commercial	5 *	5 *	17
Visitor	3 *	4 *	11
Bicycle parking stalls – minimum			
Residential – long-term	142	148	130
Residential – short-term	10	12	11
Commercial – long-term	6	12	6
Commercial – short-term	8	8	8

Community Consultation

The applicant consulted the North Park CALUC at a meeting on March 20, 2019. The CALUC Procedures for Processing Rezoning and Variance Applications state that a second community meeting is required if any one of the following changes is made:

- a change to the uses (additional uses added)
- an increase in the height
- an increase in the density or floor space ratio (FSR)
- a reduction in the setbacks or increase in site coverage equal to or greater than 20%

In this instance, although there has been an increase in density from the proposal that was presented to COTW on December 12, 2019, from 2.92:1 to 2.97:1 (FSR), the density is still less than what was presented to the CALUC on March 20, 2019 which included a density of 3.3:1. So the proposed density has not increased beyond what the CALUC originally reviewed and therefore has not triggered the requirement for a second CALUC meeting; however, on July 16, 2020, staff notified the North Park CALUC of the resubmission, in accordance with the requirements of the rezoning process.

ANALYSIS

The following sections provide an analysis of each of the proposed revisions

Window Assemblies

The window assemblies for the new addition have been changed from uninterrupted floor-to-ceiling windows that measured 8 feet in height, to windows that are 6' 2" in height, with a 1'8" tall transom window. The shorter windows are more cost effective and easier to operate for residents of the building.

The introduction of a horizontal line to the black painted window assemblies adds more visual detail to the façade, particularly the portions of the addition directly over top of the heritage building. While the simplicity of the original windows contributed to the compatibility of the building, as required by standard 11 of the *Standards and Guidelines for the Conservation of Historic Places in Canada*, in staff's opinion the overall character of the new additions is not significantly changed and they remain compatible. The additions maintain the same significant setbacks, inverted windows and balconies, which reference elements of the heritage-protected Parkway Apartments building, and which help them recede into the background.

Cladding

The applicant has revised the cladding of the building from ultra-high-performance concrete panels in a bond pattern, to a smooth stucco finish applied to an impact resistant wall system. The original panelized system emulated the bond pattern of bricks on the heritage registered Parkway Apartments building and was a thoughtful gesture that contributed to the compatibility of the additions with the heritage building. Despite the change, the smooth stucco finish simplifies the facades of the building and helps to offset the added visual detail of the transom windows. Removing the bond pattern will enhance the sculptural qualities of the building, including the light and shadow effect of the sun cast on the chamfered balconies. Staff consider the substitution of materials to be a reasonable change that enhances some of the design features of the addition.

Garbage and Recycling Room

The garbage and recycling room has been relocated from the ground floor of the building facing the pedestrian path to the park on the north edge of the property to the basement level. The applicant replaced the garbage and recycling room with a ground-oriented dwelling unit that improves the aesthetics and visual transparency of the façade viewed from the pedestrian path.

Additional Dwelling Units and Increased Gross Floor Area

The applicant has added two rental dwelling units to the building by relocating the garbage and recycling room underground and has reconfigured four of the north-facing residential units on level 6 to eliminate the large corner windows and balconies. These changes have increased the gross floor area of the building from 8,412 square metres to 8,544 square metres (a difference of 132 square metres) and the overall density has slightly increased from 2.92:1 to 2.97:1 FSR (an increase of 0.05:1 FSR), which still remains consistent with the density policies outlined in the *Official Community Plan* (OCP, 2012) and the *Downtown Core Area Plan* (DCAP, 2012)

Bicycle Parking

The applicant has added six long-term and two short-term residential bicycle parking spaces to the underground level of the building, bringing the total number of bicycle parking spaces on site up to 160, whereas 142 are required for the residential uses. The applicant has also increased the number of long-term commercial bicycle parking spaces from six to 12.

Mews Connection, Public Access and Park Improvements

BC Building Code and fire regulations requires the development to have two fire exits that egress into the adjacent Franklin Green Park. Staff have worked with the Applicant to identify the following solutions that will meet regulatory requirements and provide the following community contributions:

- Maintain unrestricted public access to the mews from Cook Street and Franklin Green Park from 8AM-8PM; and
- Provide a community amenity contribution in the amount of \$17,800.00 toward the Local Amenity Reserve Fund for the construction of two accessible asphalt pathways within Franklin Green Park linking Cook Street and development to existing pathways in the park and for the purchase and installation of a drinking fountain for the park.

The City will construct the pathway connections and install the drinking fountain within the Park. The Applicant will be required to coordinate the timing of the work with the City's Parks Department a minimum of one year in advance of requiring the work to be complete.

Street Trees

The location of existing underground utilities along Cook Street is preventing the planting of street trees along this frontage. The applicant is willing to provide \$24,000.00, which is equivalent to the planting of six trees in hardscape, which will be used to plant trees elsewhere in the neighbourhood to enhance the City's urban forest.

Tenant Assistance

All of the former tenants of the building have been successfully relocated and compensated in accordance with the Tenant Assistance Plan. The applicant prepared a series of letters (attached) with further details.

CONCLUSIONS

The applicant has made a variety of changes to the project that reflect financial and construction constraints and the longer-term investment that a rental building represents. Staff have assessed the exterior changes and concluded that the project is still consistent with heritage policies and guidelines. Other changes to the project, including public access from Cook Street to Franklin Green Park through the mews, and the relocation of the garbage room are meaningful improvements. Therefore, staff recommend that Council consider supporting the revised application and advance the proposal to a Public Hearing.

ALTERNATE MOTION

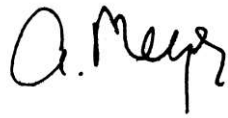
That Council decline Rezoning Application No. 00695, Heritage Designation Application No.

000188 and Heritage Alteration Permit Application No. 00016 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street

Respectfully submitted,



John O'Reilly
Senior Heritage Planner
Sustainable Planning and Community
Development Department



Karen Hoese, Director
Sustainable Planning and Community
Development Department

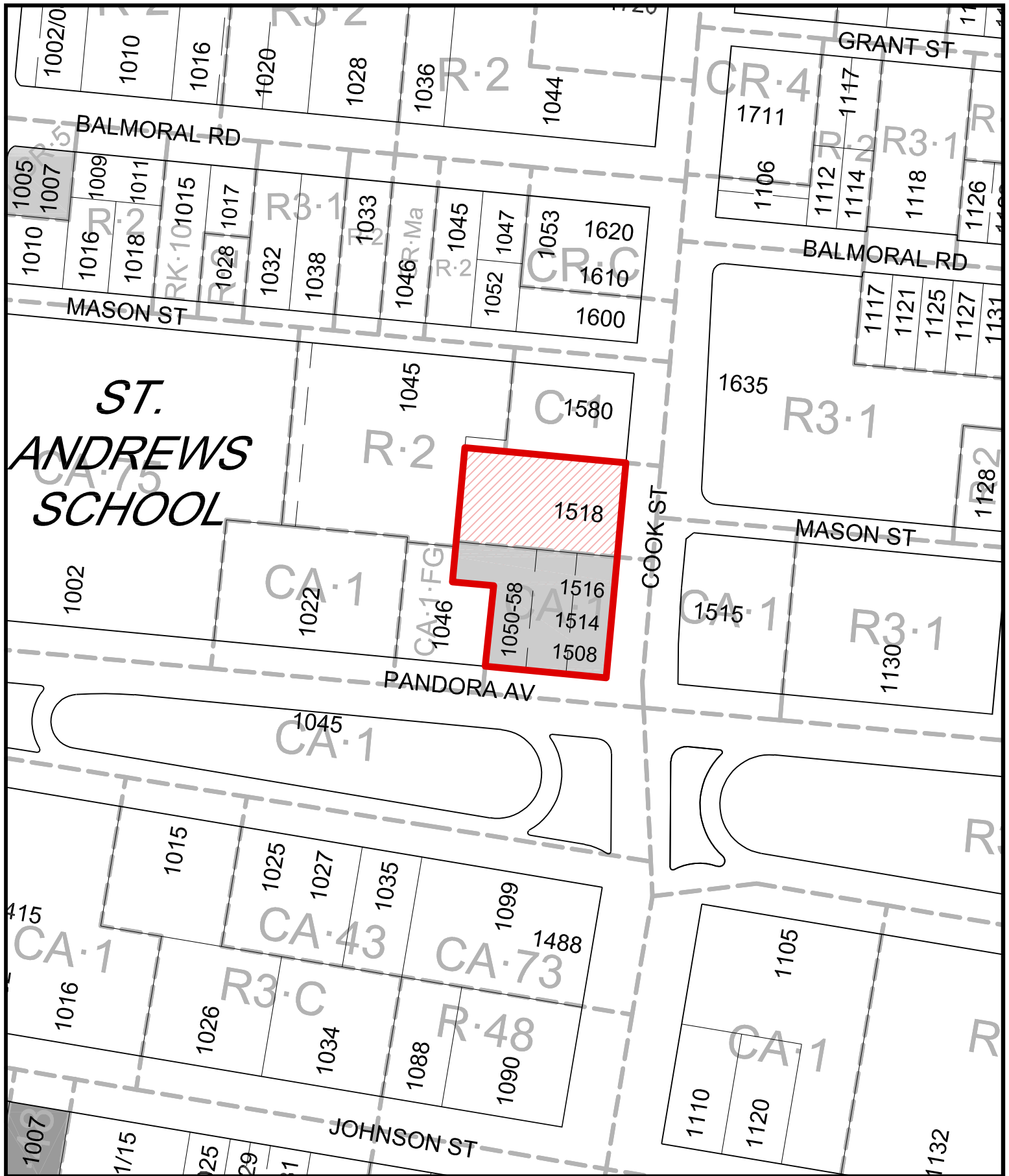
Report accepted and recommended by the City Manager:



Date: July 27, 2020

List of Attachments

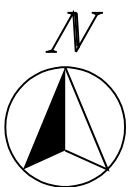
- Attachment A: Subject Map
- Attachment B: Aerial Map
- Attachment C: Plans date stamped July 15, 2020
- Attachment D: Revision letter from applicant to Mayor and Council dated July 21, 2020
- Attachment E: December 2019, February 2020 and July 2020 Letters on Tenant Assistance Measures
- Attachment F: Staff Report and Attachments for Rezoning Application No. 00695 from December 12, 2019 Committee of the Whole
- Attachment G: Staff Report and Attachments for Heritage Alteration Permit Application No. 00016 No. 00695 from December 12, 2019 Committee of the Whole
- Attachment H: Staff Report and Attachments for Heritage Designation Application No. 000188 and Heritage Alteration Permit Application No. 00016 from December 12, 2019 Committee of the Whole



1050-1058 Pandora & 1508-1518 Cook Street

Heritage Designation #000188

Heritage Alteration Permit with Variances #00016

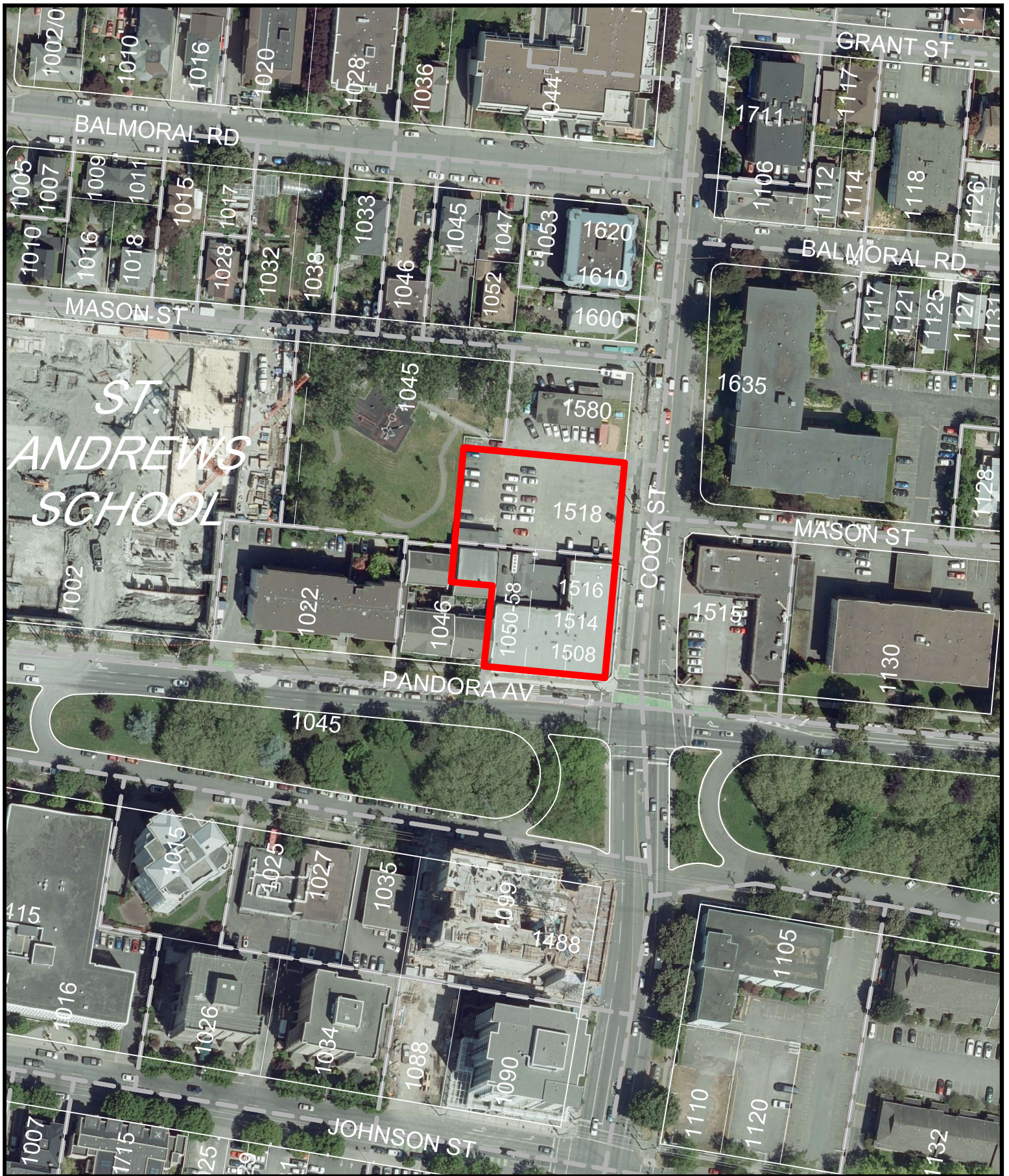


Designated

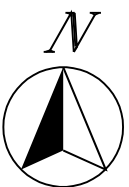



Registered





1050-1058 Pandora & 1508-1518 Cook Street
 Heritage Designation #000188
 Heritage Alteration Permit with Variances #00016





Revisions
Received Date:
July 15, 2020

MGA
ARCHITECTURE



DRAWING LIST	SCALE
A000 COVER SHEET & DRAWING LIST	NTS
A001 PROJECT DESCRIPTION & SITE LOCATION PLAN	NTS/A 1000
A002 EXISTING SITE PLAN	1:100
A003 PROPOSED SITE PLAN	1:100
A004 AVERAGE GRADE CALCULATIONS	1:100
A100 LEVEL 0 PARKING PLAN	1:100
A101 LEVEL 1 PLAN	1:100
A102 LEVEL 2 PLAN	1:100
A103 LEVEL 3 PLAN	1:100
A104 LEVEL 4 PLAN	1:100
A105 LEVEL 5 PLAN	1:100
A106 LEVEL 6 PLAN	1:100
A107 ROOF PLAN	1:100
A201 EAST & SOUTH ELEVATIONS	1:150
A202 NORTH & WEST ELEVATIONS	1:150
A203 HERITAGE ELEVATION & PLAN STUDY	1:100
A204 FACADE STUDIES	1:50
A251 STREETSCAPE ELEVATIONS	NTS
A301 SECTIONS LOOKING WEST	1:150
A302 SECTIONS LOOKING EAST	1:150
A303 SECTIONS LOOKING NORTH	1:150
A304 SECTIONS LOOKING NORTH/SOUTH	1:150
A305 SECTIONS LOOKING SOUTH	1:150
A308 STREETSCAPE CROSS SECTION	1:50
A800 PSR LEGEND	1:100
A801 PSR-LEVEL 1	1:100
A802 PSR-LEVEL 2	1:100
A803 PSR-LEVEL 3	1:100
A804 PSR-LEVEL 4	1:100
A805 PSR-LEVEL 5	1:100
A806 PSR-LEVEL 6	1:100
A811 VIEW NORTHWEST FROM CORNER OF PANDORA & COOK	NTS
A812 VIEW WEST THROUGH HERITAGE TOWN SQUARE	NTS
A813 VIEW SOUTHWEST FROM COOK STREET	NTS
A814 VIEW EAST TO COURTYARD	NTS
A815 BALCONY STUDY	NTS
A817 ADJACENT PROPERTY STUDY	NTS
A821 SOLAR IMPACT ANALYSIS	NTS
A822 ILLUMINANCE ANALYSIS	NTS



MICHAEL GREEN ARCHITECTURE
1533 W. 3RD AVENUE, VANCOUVER BC
CANADA V6J 1B8

2020-07-15	△	REVISED FOR REZONING
2020-03-20	△	REVISED FOR REZONING
2019-10-30	△	REVISED FOR REZONING
2019-09-13	△	REVISED FOR REZONING
2019-05-15	△	ISSUED FOR REZONING

DATE REVISION DESCRIPTION

PARKWAY
1001 PANDORA AVE + 1518 COOK STREET
VICTORIA, BC
2018-001

A000
COVER SHEET

PROJECT NARRATIVE

The Parkway Revitalization and Development is located at Pandora Avenue & Cook Street, at the site of what is known to the community as the Wallburns Building. Originally named Parkway Apartments, the two-story masonry building was constructed in 1911 by William Ridgway (P) Ltd. At the corner of the North Park neighbourhood, the building is a gateway feature to both the neighbourhood and the centre of Victoria.

The new development proposes a 4.5 & 5 storey volume stepping back from the existing heritage building to the north & west, and from Franklin Green Park to the south & east. 105 purpose-built rental apartments are proposed with a retail / commercial space being maintained in the existing ground floor of the Wallburns Building and the addition of a cafe space in the ground floor of the new addition facing Cook Street. A news separating the historic and modern building at street level serves as the residential entrance to the building and provides access to a new living courtyard. Public access to and from Franklin Green Park is provided through a wide pedestrian walkway along the north edge of the site. One level of underground parking will be provided below the project site.

A priority of the project is to conserve the heritage value of the Wallburns building through retaining 50% of the existing volume, including the historic facade facing Parkway Ave & Cook St and the southeast wall facing the residential street, all of which are well

elements in these locations will be preserved along with any historic repairs, as required. The original use of the building will remain with opportunities for multiple retail spaces on the ground floor & residential suites above. The building will be Designated Heritage with the Heritage Registry.

The new development will be clad in a light & mid grey coloured space in between walkways with light grey finish. It will borrow elements from its historic counterpart, including the proportion & angles of the projecting window and the recessed entryways of the existing storefronts. An existing Wallburns Market stand that is currently located on the north elevation of the existing building will be reimagined on the north elevation of the new 4 storey volume, at the entrance to the parkade.

To create a strong visual connection with the surrounding context, public balconies will be provided in the living spaces of the suites, directly facing Franklin Green Park & Harris Green Park. An accessible roof deck will also be provided for all residential tenets of the building, facing onto Franklin Green Park.

PROJECT NAME

Parkway

PROJECT ADDRESS

1005 Parkway Ave + 1018 Cook Street

LEGAL DESCRIPTION

lots Tied 2, Substantial 15, Victoria V17321

PROJECT TEAM

OWNER

Pandora Cook Development Corp.

Distro Development Corp.

200-8803 Heather Street, Vancouver, BC, V6P 3T1

Primary Contact Andrew Renshaw

(604) 704-1866

AGENT

DEVELOPMENT DEVELOPMENTS CORP.

200-8803 Heather Street, Vancouver, BC, V6P 3T1

Primary Contact Mike Faj

(604) 254-7672

ARCHITECT

MGA | Michael Green Architecture

1535 West 3rd Avenue, Vancouver, BC, V6J 1J8

Arch: Michael Green

Contact Jordan Van Dijk

(604) 364-1770

PROPOSED ZONING

New Special Study Zone

Changed from R2 (Two Family Dwelling District) at 1518 Cook Street, and

C4M (Pandora Avenue Special Commercial District) at 1005 Pandora Avenue.

SITE AREA

2075 m²

AVERAGE GRADE

27.5m (Sea Level) for average grade calculation.

Note that the project ground floor is set at a grade elevation of 27.5m and

building levels are dimensioned from that elevation.

PROPOSED HEIGHT

20.22 m (mean floor average grade). Note that 22m (perpet) is excluded

from proposed height.

ALLOWABLE HEIGHT

30m / 140 steps, per OCP

APPLICABLE BUILDING CODE

BCBC 2018

STREETS FACING

Pandora Avenue to the South

Cook Street to the East

OCCUPANCY CLASSIFICATIONS

2.2.1.301, Group A, up to 8 Stories

2.2.2.301, Group E, up to 8 Stories

2.2.2.302, Group F, up to 8 Stories

2.2.2.303, Group F, up to 8 Stories

2.2.2.304, Group F, up to 8 Stories

2.2.2.305, Group F, up to 8 Stories

2.2.2.306, Group F, up to 8 Stories

2.2.2.307, Group F, up to 8 Stories

2.2.2.308, Group F, up to 8 Stories

2.2.2.309, Group F, up to 8 Stories

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2.2.2.311, Group F, up to 8 Stories

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2.2.2.357, Group F, up to 8 Stories

2.2.2.358, Group F, up to 8 Stories

2.2.2.359, Group F, up to 8 Stories

2.2.2.360, Group F, up to 8 Stories

2.2.2.361, Group F, up to 8 Stories

2.2.2.362, Group F, up to 8 Stories

2.2.2.363, Group F, up to 8 Stories

AREA CALCULATIONS

LEVEL 0 2,175.2 m²

LEVEL 1 1945.57 m²

LEVEL 2 1905.61 m²

LEVEL 3 1905.61 m²

LEVEL 4 1905.61 m²

LEVEL 5 1905.61 m²

LEVEL 6 1905.61 m²

PROPOSED FSR 8,544.5 m²

ALLOWABLE FSR 9,330 m²

PROPOSED FSR RATIO 2.97

UNIT TYPES

STUDIO 4 4%

STUDIO & DEN 8 8%

1BD 54 38%

1BD & DEN 24 22%

2BD 31 10%

2BD & DEN 4 4%

TOTAL 105

PROPOSED HEIGHT 20.22m

TOTAL RESIDENTIAL AREA 6555 m²

TOTAL RESIDENTIAL UNITS 105

TOTAL PARKING SPACES 94

94 m² (4.0m x 23.0m)

TOTAL COMMERCIAL AREA 1050 m²

880.0 m² RETAIL

170.0 m² CANE

55.6 m² OUTDOOR SEATING

ORIGINAL HERITAGE BUILDING AREA 1891.9m²

TOTAL AREA TO BE RETAINED 941.0m² (105)

TOTAL BIKE PARKING 180

160 SHORT TERM BIKE PARKING

20 LONG TERM BIKE PARKING

UNIT CALCULATIONS

QTY m² L3 L2 L3 L4 L5 L6 TOTAL

STUDIO A 56.3 1 1 2

STUDIO B 53.2 1 1 2

ST & DEN A 65.0 4 4

ST & DEN B 72.7 1 1

ST & DEN C 58.0 1 1

ST & DEN D 59.9 1 1

ST & DEN E 63.3 1 1

1BD A 57.2 1 1 1 4

1BD B 53.0 1 1 1 4

1BD C 56.1 1 1 1

1BD D 49.7 1 1

1BD E 49.3 1 1 3

1BD F 33.6 5 5 10

1BD G 47.4 1 1 1

1BD H 36.2 1 1 1 4

1BD I 31.7 1 1 1 4

1BD J 31.8 1 1 1 4

1BD K 48.5 1 1 1 4

1BD L 53.4 1 1

1BD M 45.0 1 1

1BD N 48.4 1 1

1BD O 53.5 1 1

1BD P 48.6 1 1

1BD Q 49.0 1 1

1BD R 52.8 1 1

1BD S 51.1 1 1

1BD T 45.8 1 1

1BD U 40.1 1 1

1BD V 63.3 1 1

1BD W 51.6 1 1

1BD X 62.8 1 2 2 7

1BD Y 60.0 1 1 2

1BD Z 52.8 1 1 2

1BD AA 56.0 1 1 2

1BD AB 51.0 1 1

1BD AC 28.0 1 1

1BD AD 43.0 1 1 3

1BD AE 34.8 1 1

1BD AF 42.3 1 1

1BD AG 74.1 2 1 2

1BD AH 48.7 1 1

2BD A 47.9 1 1

2BD B 68.8 1 1

2BD C 75.5 1 1

2BD D 42.3 1 1 3

2BD E 85.5 1 1

2BD F 65.4 1 1 1 3

2BD G 75.4 1 1 1

2BD H 90.8 1 1 1

2BD I 91.3 1 1 2

2BD J 75.6 1 1 1 1

TOTAL UNITS 9 21 21 21 15 15 105

PARKING CALCULATIONS

RESIDENTIAL PARKING

Parking Rate	# of Units	Required	Provided
<45m ²	6	3.0	3
45m ² -m ²	87	52.2	28
>20m ²	100	12.0	7
TOTAL RESIDENTIAL PARKING		67	38

VEHICLE PARKING

Parking Rate	# of Units	Required	Provided
Vehicle Parking	105	11	4

COMMERCIAL PARKING

Parking Rate	Total Area (m ²)	Required	Provided
Retail / Grocery	880	11	2
Restaurant	225.6	6	3
TOTAL COMMERCIAL PARKING		17	5

CARSHARE PARKING

Parking Rate	Total Area (m ²)	Required	Provided
Multi-Carshare Parking Bays	880	2	2
TOTAL PARKING		94	44

BIKE PARKING [LONG TERM]

Parking Rate	# of Units	Required	Provided
Residential	6	6	20
Commercial	99	124	128
TOTAL LONG TERM BIKE PARKING		130	148

BIKE PARKING [SHORT TERM]

FLOOR-MOUNTED RACKS	64
FLOOR-MOUNTED CARGO RACKS	40
WALL-MOUNTED RACKS	56



A002

EXISTING SITE PLAN

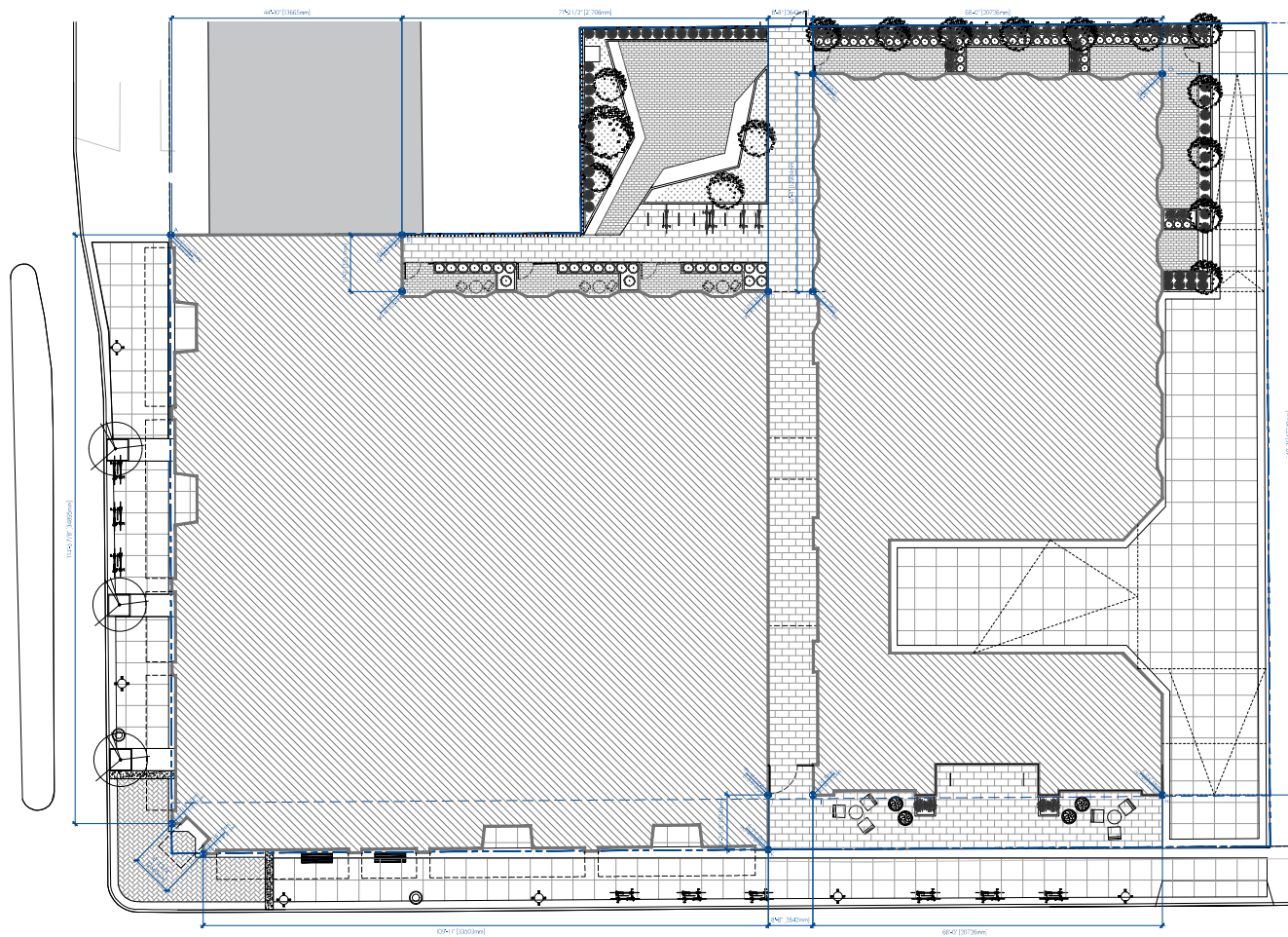
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85° 

DATE	REVISION	DESCRIPTION
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1050 PANDORA AVE + 1518 COOK STREET
VICTORIA, BC
2018-001


A003
PROPOSED SITE PLAN



MGA

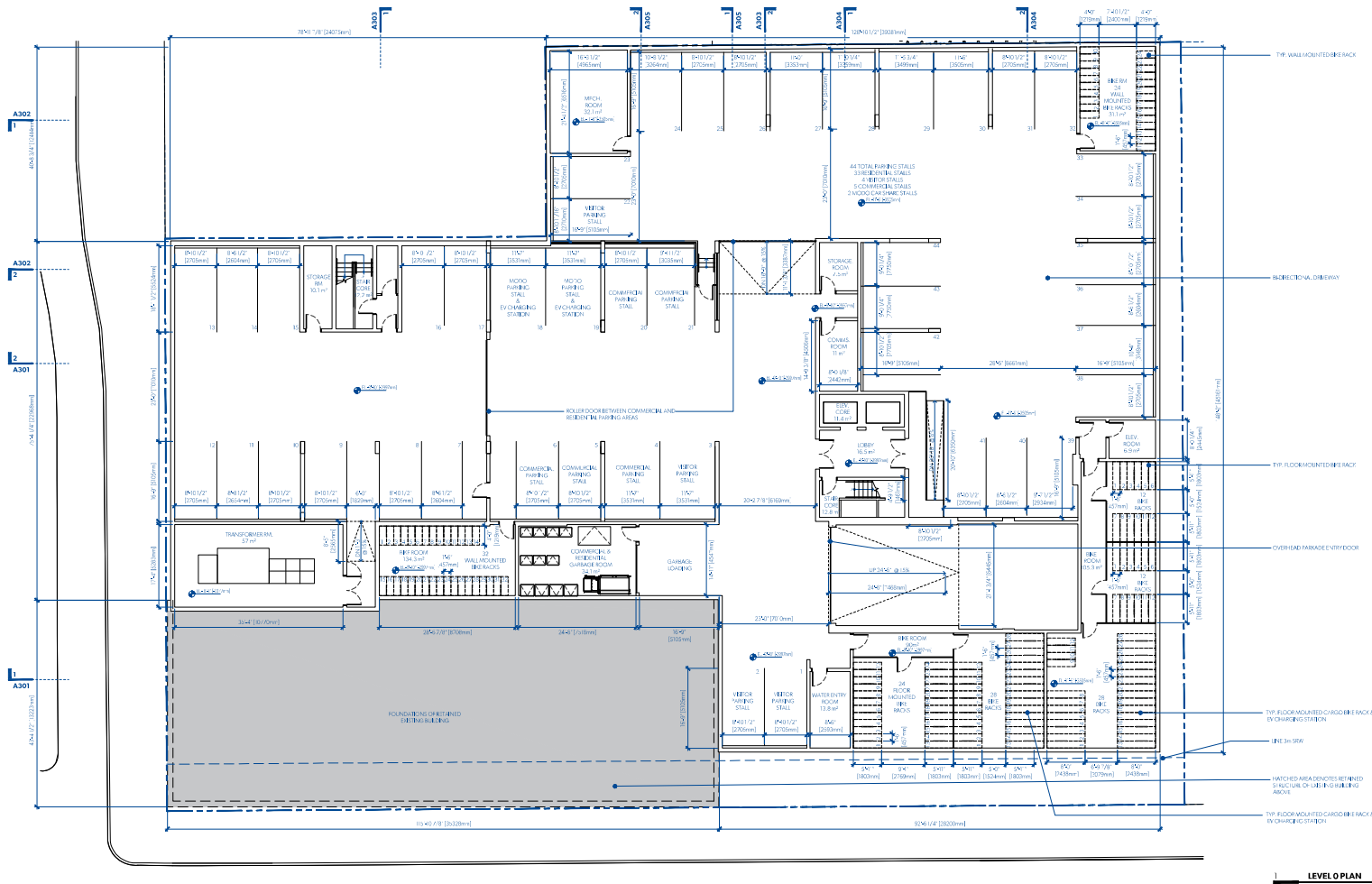
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MICHAEL GREEN ARCHITECTURE
1535 W 3RD AVENUE, VANCOUVER BC
CANADA V6J 1J8

2020-07-15		REVISED FOR REZONING
2020-03-20		REVISED FOR REZONING
2019-10-30		REVISED FOR REZONING
2019-09-13		REVISED FOR REZONING
2019-05-15		ISSUED FOR REZONING

PARKWAY

A004
AVERAGE GRADE CALC.



Location

Room	Area (sq ft)	Area (sq m)
Office	100	9.3
Meeting	50	4.6
Storage	20	1.9
Other	10	0.9
TOTAL COMMERCIAL	180	16.7

Location

Room	Area (sq ft)	Area (sq m)
Office	100	9.3
Meeting	50	4.6
Storage	20	1.9
Other	10	0.9
TOTAL COMMERCIAL	180	16.7

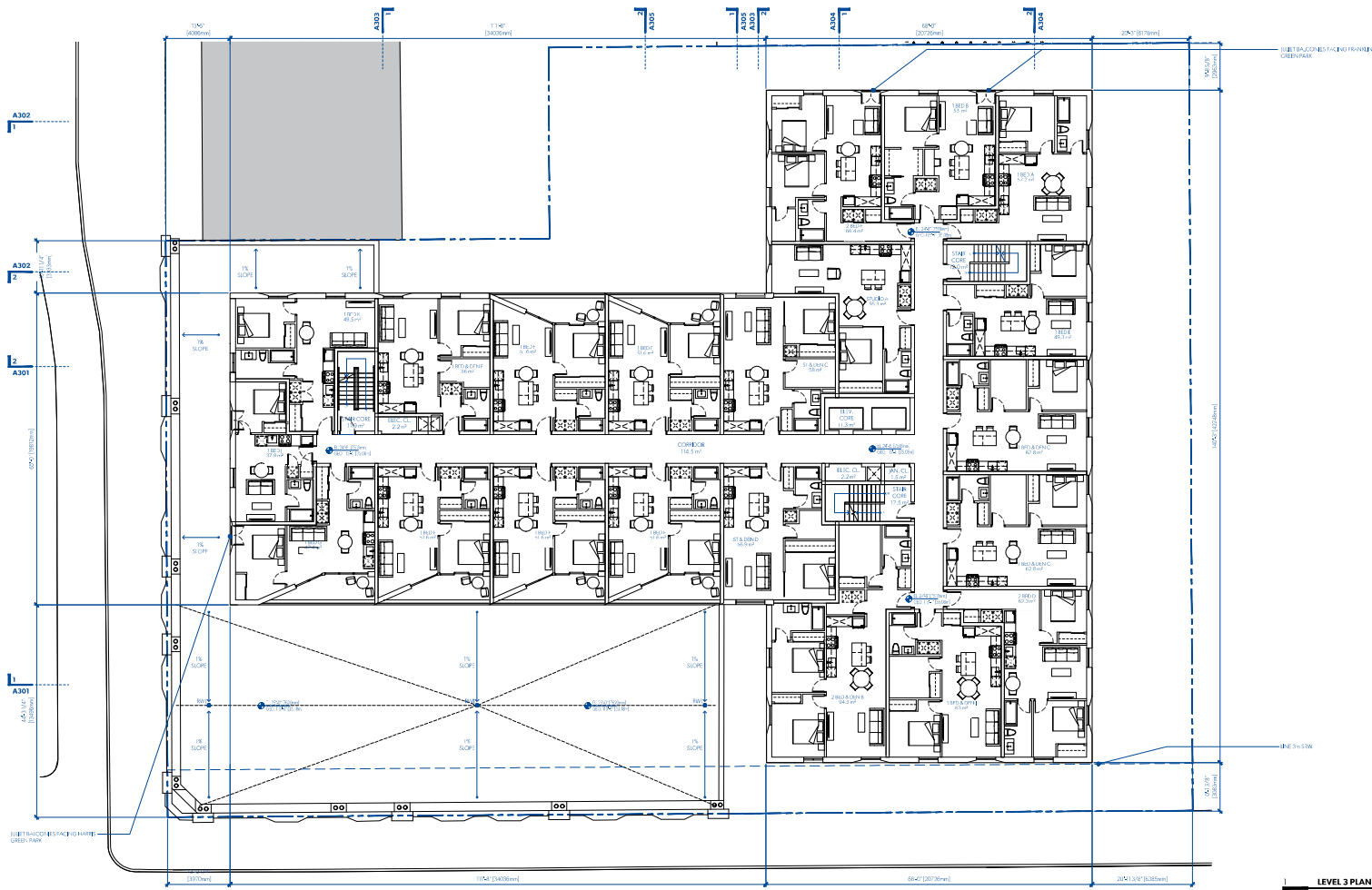
Location

Room	Area (sq ft)	Area (sq m)
Office	100	9.3
Meeting	50	4.6
Storage	20	1.9
Other	10	0.9
TOTAL COMMERCIAL	180	16.7

Location

Room	Area (sq ft)	Area (sq m)
Office	100	9.3
Meeting	50	4.6
Storage	20	1.9
Other	10	0.9
TOTAL COMMERCIAL	180	16.7

A100
LEVEL 0 PLAN



UNIT CALCULATIONS

UNIT	100	101	102	103	104	TOTAL
100-1	10.1					10.1
100-2	10.1					10.1
100-3	10.1					10.1
100-4	10.1					10.1
100-5	10.1					10.1
100-6	10.1					10.1
100-7	10.1					10.1
100-8	10.1					10.1
100-9	10.1					10.1
100-10	10.1					10.1
100-11	10.1					10.1
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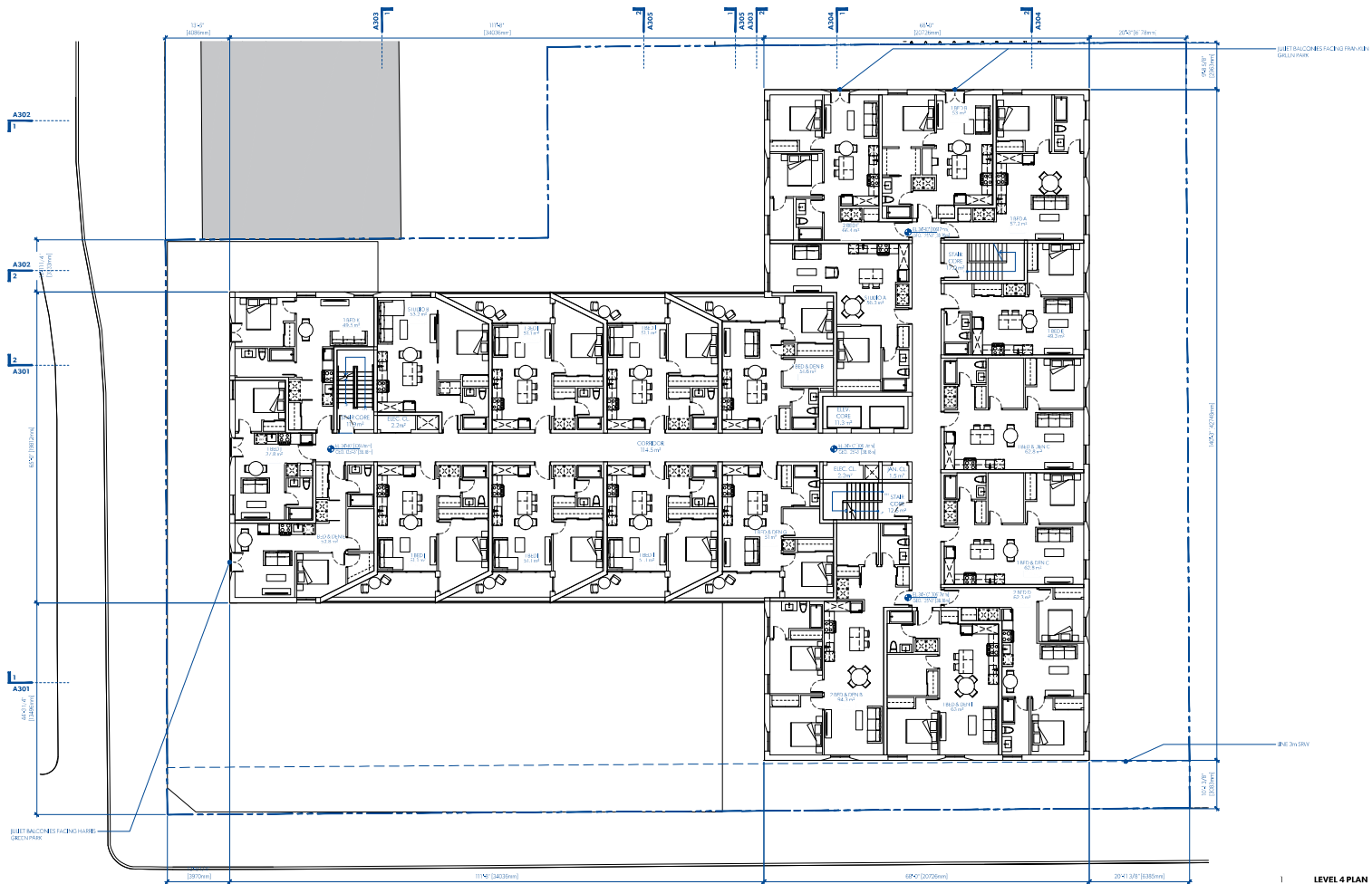
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2019-05-15	△	ISSUED FOR REZONING

PARKWAY
1001 PANDORA AVE + 1518 COOK STREET
VICTORIA BC
V8P 1G1

A103

LEVEL 3 PLAN



UNIT CALCULATIONS

UNIT	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
1001A	1001B	1001C	1001D	1001E	1001F	1001G	1001H	1001I	1001J	1001K	1001L	1001M	1001N	1001O	1001P	1001Q	1001R	1001S	1001T	1001U	1001V	1001W	1001X	1001Y	1001Z	1001AA	1001AB	1001AC	1001AD	1001AE	1001AF	1001AG	1001AH	1001AI	1001AJ	1001AK	1001AL	1001AM	1001AN	1001AO	1001AP	1001AQ	1001AR	1001AS	1001AT	1001AU	1001AV	1001AW	1001AX	1001AY	1001AZ	1001BA	1001BB	1001BC	1001BD	1001BE	1001BF	1001BG	1001BH	1001BI	1001BJ	1001BK	1001BL	1001BM	1001BN	1001BO	1001BP	1001BQ	1001BR	1001BS	1001BT	1001BU	1001BV	1001BW	1001BX	1001BY	1001BZ	1001CA	1001CB	1001CC	1001CD	1001CE	1001CF	1001CG	1001CH	1001CI	1001CJ	1001CK	1001CL	1001CM	1001CN	1001CO	1001CP	1001CQ	1001CR	1001CS	1001CT	1001CU	1001CV	1001CW	1001CX	1001CY	1001CZ	1001DA	1001DB	1001DC	1001DD	1001DE	1001DF	1001DG	1001DH	1001DI	1001DJ	1001DK	1001DL	1001DM	1001DN	1001DO	1001DP	1001DQ	1001DR	1001DS	1001DT	1001DU	1001DV	1001DW	1001DX	1001DY	1001DZ	1001EA	1001EB	1001EC	1001ED	1001EE	1001EF	1001EG	1001EH	1001EI	1001EJ	1001EK	1001EL	1001EM	1001EN	1001EO	1001EP	1001EQ	1001ER	1001ES	1001ET	1001EU	1001EV	1001EW	1001EX	1001EY	1001EZ	1001FA	1001FB	1001FC	1001FD	1001FE	1001FF	1001FG	1001FH	1001FI	1001FJ	1001FK	1001FL	1001FM	1001FN	1001FO	1001FP	1001FQ	1001FR	1001FS	1001FT	1001FU	1001FV	1001FW	1001FX	1001FY	1001FZ	1001GA	1001GB	1001GC	1001GD	1001GE	1001GF	1001GG	1001GH	1001GI	1001GJ	1001GK	1001GL	1001GM	1001GN	1001GO	1001GP	1001GQ	1001GR	1001GS	1001GT	1001GU	1001GV	1001GW	1001GX	1001GY	1001GZ	1001HA	1001HB	1001HC	1001HD	1001HE	1001HF	1001HG	1001HH	1001HI	1001HJ	1001HK	1001HL	1001HM	1001HN	1001HO	1001HP	1001HQ	1001HR	1001HS	1001HT	1001HU	1001HV	1001HW	1001HX	1001HY	1001HZ	1001IA	1001IB	1001IC	1001ID	1001IE	1001IF	1001IG	1001IH	1001II	1001IJ	1001IK	1001IL	1001IM	1001IN	1001IO	1001IP	1001IQ	1001IR	1001IS	1001IT	1001IU	1001IV	1001IW	1001IX	1001IY	1001IZ	1001JA	1001JB	1001JC	1001JD	1001JE	1001JF	1001JG	1001JH	1001JI	1001JJ	1001JK	1001JL	1001JM	1001JN	1001JO	1001JP	1001JQ	1001JR	1001JS	1001JT	1001JU	1001JV	1001JW	1001JX	1001JY	1001JZ	1001KA	1001KB	1001KC	1001KD	1001KE	1001KF	1001KG	1001KH	1001KI	1001KJ	1001KK	1001KL	1001KM	1001KN	1001KO	1001KP	1001KQ	1001KR	1001KS	1001KT	1001KU	1001KV	1001KW	1001KX	1001KY	1001KZ	1001LA	1001LB	1001LC	1001LD	1001LE	1001LF	1001LG	1001LH	1001LI	1001LJ	1001LK	1001LL	1001LM	1001LN	1001LO	1001LP	1001LQ	1001LR	1001LS	1001LT	1001LU	1001LV	1001LW	1001LX	1001LY	1001LZ	1001MA	1001MB	1001MC	1001MD	1001ME	1001MF	1001MG	1001MH	1001MI	1001MJ	1001MK	1001ML	1001MN	1001MO	1001MP	1001MQ	1001MR	1001MS	1001MT	1001MU	1001MV	1001MW	1001MX	1001MY	1001MZ	1001NA	1001NB	1001NC	1001ND	1001NE	1001NF	1001NG	1001NH	1001NI	1001NJ	1001NK	1001NL	1001NM	1001NN	1001NO	1001NP	1001NQ	1001NR	1001NS	1001NT	1001NU	1001NV	1001NW	1001NX	1001NY	1001NZ	1001OA	1001OB	1001OC	1001OD	1001OE	1001OF	1001OG	1001OH	1001OI	1001OJ	1001OK	1001OL	1001OM	1001ON	1001OO	1001OP	1001OQ	1001OR	1001OS	1001OT	1001OU	1001OV	1001OW	1001OX	1001OY	1001OZ	1001PA	1001PB	1001PC	1001PD	1001PE	1001PF	1001PG	1001PH	1001PI	1001PJ	1001PK	1001PL	1001PM	1001PN	1001PO	1001PP	1001PQ	1001PR	1001PS	1001PT	1001PU	1001PV	1001PW	1001PX	1001PY	1001PZ	1001QA	1001QB	1001QC	1001QD	1001QE	1001QF	1001QG	1001QH	1001QI	1001QJ	1001QK	1001QL	1001QM	1001QN	1001QO	1001QP	1001QQ	1001QR	1001QS	1001QT	1001QU	1001QV	1001QW	1001QX	1001QY	1001QZ	1001RA	1001RB	1001RC	1001RD	1001RE	1001RF	1001RG	1001RH	1001RI	1001RJ	1001RK	1001RL	1001RM	1001RN	1001RO	1001RP	1001RQ	1001RR	1001RS	1001RT	1001RU	1001RV	1001RW	1001RX	1001RY	1001RZ	1001SA	1001SB	1001SC	1001SD	1001SE	1001SF	1001SG	1001SH	1001SI	1001SJ	1001SK	1001SL	1001SM	1001SN	1001SO	1001SP	1001SQ	1001SR	1001SS	1001ST	1001SU	1001SV	1001SW	1001SX	1001SY	1001SZ	1001TA	1001TB	1001TC	1001TD	1001TE	1001TF	1001TG	1001TH	1001TI	1001TJ	1001TK	1001TL	1001TM	1001TN	1001TO	1001TP	1001TQ	1001TR	1001TS	1001TT	1001TU	1001TV	1001TW	1001TX	1001TY	1001TZ	1001UA	1001UB	1001UC	1001UD	1001UE	1001UF	1001UG	1001UH	1001UI	1001UJ	1001UK	1001UL	1001UM	1001UN	1001UO	1001UP	1001UQ	1001UR	1001US	1001UT	1001UU	1001UV	1001UW	1001UX	1001UY	1001UZ	1001VA	1001VB	1001VC	1001VD	1001VE	1001VF	1001VG	1001VH	1001VI	1001VJ	1001VK	1001VL	1001VM	1001VN	1001VO	1001VP	1001VQ	1001VR	1001VS	1001VT	1001VU	1001VV	1001VW	1001VX	1001VY	1001VZ	1001WA	1001WB	1001WC	1001WD	1001WE	1001WF	1001WG	1001WH	1001WI	1001WJ	1001WK	1001WL	1001WM	1001WN	1001WO	1001WP	1001WQ	1001WR	1001WS	1001WT	1001WU	1001WV	1001WW	1001WX	1001WY	1001WZ	1001XA	1001XB	1001XC	1001XD	1001XE	1001XF	1001XG	1001XH	1001XI	1001XJ	1001XK	1001XL	1001XM	1001XN	1001XO	1001XP	1001XQ	1001XR	1001XS	1001XT	1001XU	1001XV	1001XW	1001XX	1001XY	1001XZ	1001YA	1001YB	1001YC	1001YD	1001YE	1001YF	1001YG	1001YH	1001YI	1001YJ	1001YK	1001YL	1001YM	1001YN	1001YO	1001YP	1001YQ	1001YR	1001YS	1001YT	1001YU	1001YV	1001YW	1001YX	1001YY	1001YZ	1001ZA	1001ZB	1001ZC	1001ZD	1001ZE	1001ZF	1001ZG	1001ZH	1001ZI	1001ZJ	1001ZK	1001ZL	1001ZM	1001ZN	1001ZO	1001ZP	1001ZQ	1001ZR	1001ZS	1001ZT	1001ZU	1001ZV	1001ZW	1001ZX	1001ZY	1001ZZ

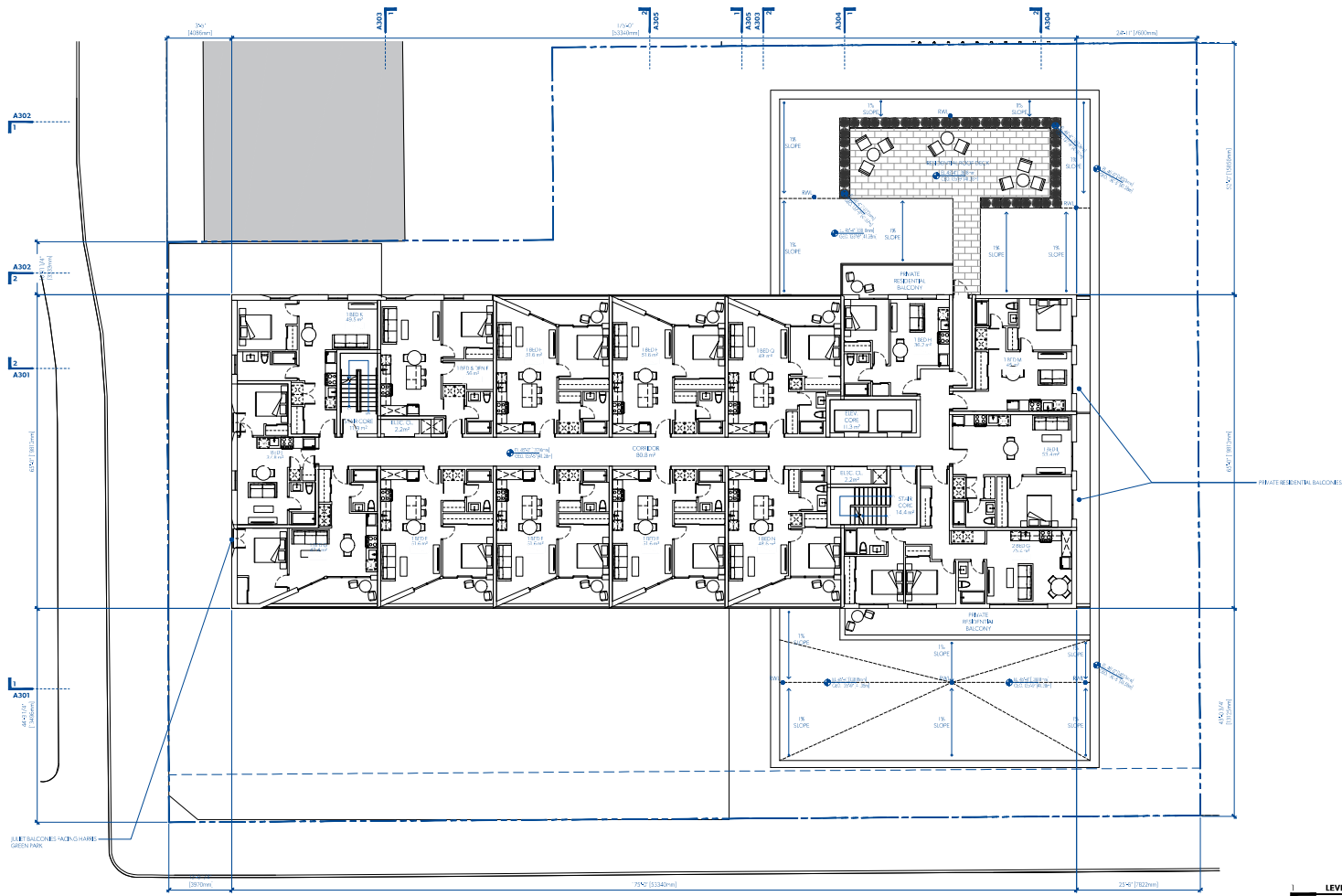
REVISIONS

DATE	REVISION	DESCRIPTION
2020-07-15	△	REVISED FOR REZONING
2020-09-30	△	REVISED FOR REZONING
2019-10-30	△	REVISED FOR REZONING
2019-09-13	△	REVISED FOR REZONING
2019-05-15	△	REVISED FOR REZONING

PARKWAY

1001 PANDORA AVE + 1018 COOK STREET
VICTORIA, BC
V8P 1G1

A104
LEVEL 4 PLAN



UNIT CALCULATIONS

UNIT	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																														
1000.1	1000.2	1000.3	1000.4	1000.5	1000.6	1000.7	1000.8	1000.9	1001.0	1001.1	1001.2	1001.3	1001.4	1001.5	1001.6	1001.7	1001.8	1001.9	1002.0	1002.1	1002.2	1002.3	1002.4	1002.5	1002.6	1002.7	1002.8	1002.9	1003.0	1003.1	1003.2	1003.3	1003.4	1003.5	1003.6	1003.7	1003.8	1003.9	1004.0	1004.1	1004.2	1004.3	1004.4	1004.5	1004.6	1004.7	1004.8	1004.9	1005.0	1005.1	1005.2	1005.3	1005.4	1005.5	1005.6	1005.7	1005.8	1005.9	1006.0	1006.1	1006.2	1006.3	1006.4	1006.5	1006.6	1006.7	1006.8	1006.9	1007.0	1007.1	1007.2	1007.3	1007.4	1007.5	1007.6	1007.7	1007.8	1007.9	1008.0	1008.1	1008.2	1008.3	1008.4	1008.5	1008.6	1008.7	1008.8	1008.9	1009.0	1009.1	1009.2	1009.3	1009.4	1009.5	1009.6	1009.7	1009.8	1009.9	1010.0	1010.1	1010.2	1010.3	1010.4	1010.5	1010.6	1010.7	1010.8	1010.9	1011.0	1011.1	1011.2	1011.3	1011.4	1011.5	1011.6	1011.7	1011.8	1011.9	1012.0	1012.1	1012.2	1012.3	1012.4	1012.5	1012.6	1012.7	1012.8	1012.9	1013.0	1013.1	1013.2	1013.3	1013.4	1013.5	1013.6	1013.7	1013.8	1013.9	1014.0	1014.1	1014.2	1014.3	1014.4	1014.5	1014.6	1014.7	1014.8	1014.9	1015.0	1015.1	1015.2	1015.3	1015.4	1015.5	1015.6	1015.7	1015.8	1015.9	1016.0	1016.1	1016.2	1016.3	1016.4	1016.5	1016.6	1016.7	1016.8	1016.9	1017.0	1017.1	1017.2	1017.3	1017.4	1017.5	1017.6	1017.7	1017.8	1017.9	1018.0	1018.1	1018.2	1018.3	1018.4	1018.5	1018.6	1018.7	1018.8	1018.9	1019.0	1019.1	1019.2	1019.3	1019.4	1019.5	1019.6	1019.7	1019.8	1019.9	1020.0	1020.1	1020.2	1020.3	1020.4	1020.5	1020.6	1020.7	1020.8	1020.9	1021.0	1021.1	1021.2	1021.3	1021.4	1021.5	1021.6	1021.7	1021.8	1021.9	1022.0	1022.1	1022.2	1022.3	1022.4	1022.5	1022.6	1022.7	1022.8	1022.9	1023.0	1023.1	1023.2	1023.3	1023.4	1023.5	1023.6	1023.7	1023.8	1023.9	1024.0	1024.1	1024.2	1024.3	1024.4	1024.5	1024.6	1024.7	1024.8	1024.9	1025.0	1025.1	1025.2	1025.3	1025.4	1025.5	1025.6	1025.7	1025.8	1025.9	1026.0	1026.1	1026.2	1026.3	1026.4	1026.5	1026.6	1026.7	1026.8	1026.9	1027.0	1027.1	1027.2	1027.3	1027.4	1027.5	1027.6	1027.7	1027.8	1027.9	1028.0	1028.1	1028.2	1028.3	1028.4	1028.5	1028.6	1028.7	1028.8	1028.9	1029.0	1029.1	1029.2	1029.3	1029.4	1029.5	1029.6	1029.7	1029.8	1029.9	1030.0	1030.1	1030.2	1030.3	1030.4	1030.5	1030.6	1030.7	1030.8	1030.9	1031.0	1031.1	1031.2	1031.3	1031.4	1031.5	1031.6	1031.7	1031.8	1031.9	1032.0	1032.1	1032.2	1032.3	1032.4	1032.5	1032.6	1032.7	1032.8	1032.9	1033.0	1033.1	1033.2	1033.3	1033.4	1033.5	1033.6	1033.7	1033.8	1033.9	1034.0	1034.1	1034.2	1034.3	1034.4	1034.5	1034.6	1034.7	1034.8	1034.9	1035.0	1035.1	1035.2	1035.3	1035.4	1035.5	1035.6	1035.7	1035.8	1035.9	1036.0	1036.1	1036.2	1036.3	1036.4	1036.5	1036.6	1036.7	1036.8	1036.9	1037.0	1037.1	1037.2	1037.3	1037.4	1037.5	1037.6	1037.7	1037.8	1037.9	1038.0	1038.1	1038.2	1038.3	1038.4	1038.5	1038.6	1038.7	1038.8	1038.9	1039.0	1039.1	1039.2	1039.3	1039.4	1039.5	1039.6	1039.7	1039.8	1039.9	1040.0	1040.1	1040.2	1040.3	1040.4	1040.5	1040.6	1040.7	1040.8	1040.9	1041.0	1041.1	1041.2	1041.3	1041.4	1041.5	1041.6	1041.7	1041.8	1041.9	1042.0	1042.1	1042.2	1042.3	1042.4	1042.5	1042.6	1042.7	1042.8	1042.9	1043.0	1043.1	1043.2	1043.3	1043.4	1043.5	1043.6	1043.7	1043.8	1043.9	1044.0	1044.1	1044.2	1044.3	1044.4	1044.5	1044.6	1044.7	1044.8	1044.9	1045.0	1045.1	1045.2	1045.3	1045.4	1045.5	1045.6	1045.7	1045.8	1045.9	1046.0	1046.1	1046.2	1046.3	1046.4	1046.5	1046.6	1046.7	1046.8	1046.9	1047.0	1047.1	1047.2	1047.3	1047.4	1047.5	1047.6	1047.7	1047.8	1047.9	1048.0	1048.1	1048.2	1048.3	1048.4	1048.5	1048.6	1048.7	1048.8	1048.9	1049.0	1049.1	1049.2	1049.3	1049.4	1049.5	1049.6	1049.7	1049.8	1049.9	1050.0	1050.1	1050.2	1050.3	1050.4	1050.5	1050.6	1050.7	1050.8	1050.9	1051.0	1051.1	1051.2	1051.3	1051.4	1051.5	1051.6	1051.7	1051.8	1051.9	1052.0	1052.1	1052.2	1052.3	1052.4	1052.5	1052.6	1052.7	1052.8	1052.9	1053.0	1053.1	1053.2	1053.3	1053.4	1053.5	1053.6	1053.7	1053.8	1053.9	1054.0	1054.1	1054.2	1054.3	1054.4	1054.5	1054.6	1054.7	1054.8	1054.9	1055.0	1055.1	1055.2	1055.3	1055.4	1055.5	1055.6	1055.7	1055.8	1055.9	1056.0	1056.1	1056.2	1056.3	1056.4	1056.5	1056.6	1056.7	1056.8	1056.9	1057.0	1057.1	1057.2	1057.3	1057.4	1057.5	1057.6	1057.7	1057.8	1057.9	1058.0	1058.1	1058.2	1058.3	1058.4	1058.5	1058.6	1058.7	1058.8	1058.9	1059.0	1059.1	1059.2	1059.3	1059.4	1059.5	1059.6	1059.7	1059.8	1059.9	1060.0	1060.1	1060.2	1060.3	1060.4	1060.5	1060.6	1060.7	1060.8	1060.9	1061.0	1061.1	1061.2	1061.3	1061.4	1061.5	1061.6	1061.7	1061.8	1061.9	1062.0	1062.1	1062.2	1062.3	1062.4	1062.5	1062.6	1062.7	1062.8	1062.9	1063.0	1063.1	1063.2	1063.3	1063.4	1063.5	1063.6	1063.7	1063.8	1063.9	1064.0	1064.1	1064.2	1064.3	1064.4	1064.5	1064.6	1064.7	1064.8	1064.9	1065.0	1065.1	1065.2	1065.3	1065.4	1065.5	1065.6	1065.7	1065.8	1065.9	1066.0	1066.1	1066.2	1066.3	1066.4	1066.5	1066.6	1066.7	1066.8	1066.9	1067.0	1067.1	1067.2	1067.3	1067.4	1067.5	1067.6	1067.7	1067.8	1067.9	1068.0	1068.1	1068.2	1068.3	1068.4	1068.5	1068.6	1068.7	1068.8	1068.9	1069.0	1069.1	1069.2	1069.3	1069.4	1069.5	1069.6	1069.7	1069.8	1069.9	1070.0	1070.1	1070.2	1070.3	1070.4	1070.5	1070.6	1070.7	1070.8	1070.9	1071.0	1071.1	1071.2	1071.3	1071.4	1071.5	1071.6	1071.7	1071.8	1071.9	1072.0	1072.1	1072.2	1072.3	1072.4	1072.5	1072.6	1072.7	1072.8	1072.9	1073.0	1073.1	1073.2	1073.3	1073.4	1073.5	1073.6	1073.7	1073.8	1073.9	1074.0	1074.1	1074.2	1074.3	1074.4	1074.5	1074.6	1074.7	1074.8	1074.9	1075.0	1075.1	1075.2	1075.3	1075.4	1075.5	1075.6	1075.7	1075.8	1075.9	1076.0	1076.1	1076.2	1076.3	1076.4	1076.5	1076.6	1076.7	1076.8	1076.9	1077.0	1077.1	1077.2	1077.3	1077.4	1077.5	1077.6	1077.7	1077.8	1077.9	1078.0	1078.1	1078.2	1078.3	1078.4	1078.5	1078.6	1078.7	1078.8	1078.9	1079.0	1079.1	1079.2	1079.3	1079.4	1079.5	1079.6	1079.7	1079.8	1079.9	1080.0	1080.1	1080.2	1080.3	1080.4	1080.5	1080.6	1080.7	1080.8	1080.9	1081.0	1081.1	1081.2	1081.3	1081.4	1081.5	1081.6	1081.7	1081.8	1081.9	1082.0	1082.1	1082.2	1082.3	1082.4	1082.5	1082.6	1082.7	1082.8	1082.9	1083.0	1083.1	1083.2	1083.3	1083.4	1083.5	1083.6	1083.7	1083.8	1083.9	1084.0	1084.1	1084.2	1084.3	1084.4	1084.5	1084.6	1084.7	1084.8	1084.9	1085.0	1085.1	1085.2	1085.3	1085.4	1085.5	1085.6	1085.7	1085.8	1085.9	1086.0	1086.1	1086.2	1086.3	1086.4	1086.5	1086.6	1086.7	1086.8	1086.9	1087.0	1087.1	1087.2	1087.3	1087.4	1087.5	1087.6	1087.7	1087.8	1087.9	1088.0	1088.1	1088.2	1088.3	1088.4	1088.5	1088.6	1088.7	1088.8	1088.9	1089.0	1089.1	1089.2	1089.3	1089.4	1089.5	1089.6	1089.7	1089.8	1089.9	1090.0	1090.1	1090.2	1090.3	1090.4	1090.5	1090.6	1090.7	1090.8	1090.9	1091.0	1091.1	1091.2	1091.3	1091.4	1091.5	1091.6	1091.7	1091.8	1091.9	1092.0	1092.1	1092.2	1092.3	1092.4	1092.5	1092.6	1092.7	1092.8	1092.9	1093.0	1093.1	1093.2	1093.3	1093.4	1093.5	1093.6	1093.7	1093.8	1093.9	1094.0	1094.1	1094.2	1094.3	1094.4	1094.5	1094.6	1094.7	1094.8	1094.9	1095.0	1095.1	1095.2	1095.3	1095.4	1095.5	1095.6	1095.7	1095.8	1095.9	1096.0	1096.1	1096.2	1096.3	1096.4	1096.5	1096.6	1096.7	1096.8	1096.9	1097.0	1097.1	1097.2	1097.3	1097.4	1097.5	1097.6	1097.7	1097.8	1097.9	1098.0	1098.1	1098.2	1098.3	1098.4	1098.5	1098.6	1098.7	1098.8	1098.9	1099.0	1099.1	1099.2	1099.3	1099.4	1099.5	1099.6	1099.7	1099.8	1099.9	1100.0	1100.1	1100.2	1100.3	1100.4	1100.5	1100.6	1100.7	1100.8	1100.9	1101.0	1101.1	1101.2	1101.3	1101.4	1101.5	1101.6	1101.7	1101.8	1101.9	1102.0	1102.1	1102.2	1102.3	1102.4	1102.5	1102.6	1102.7

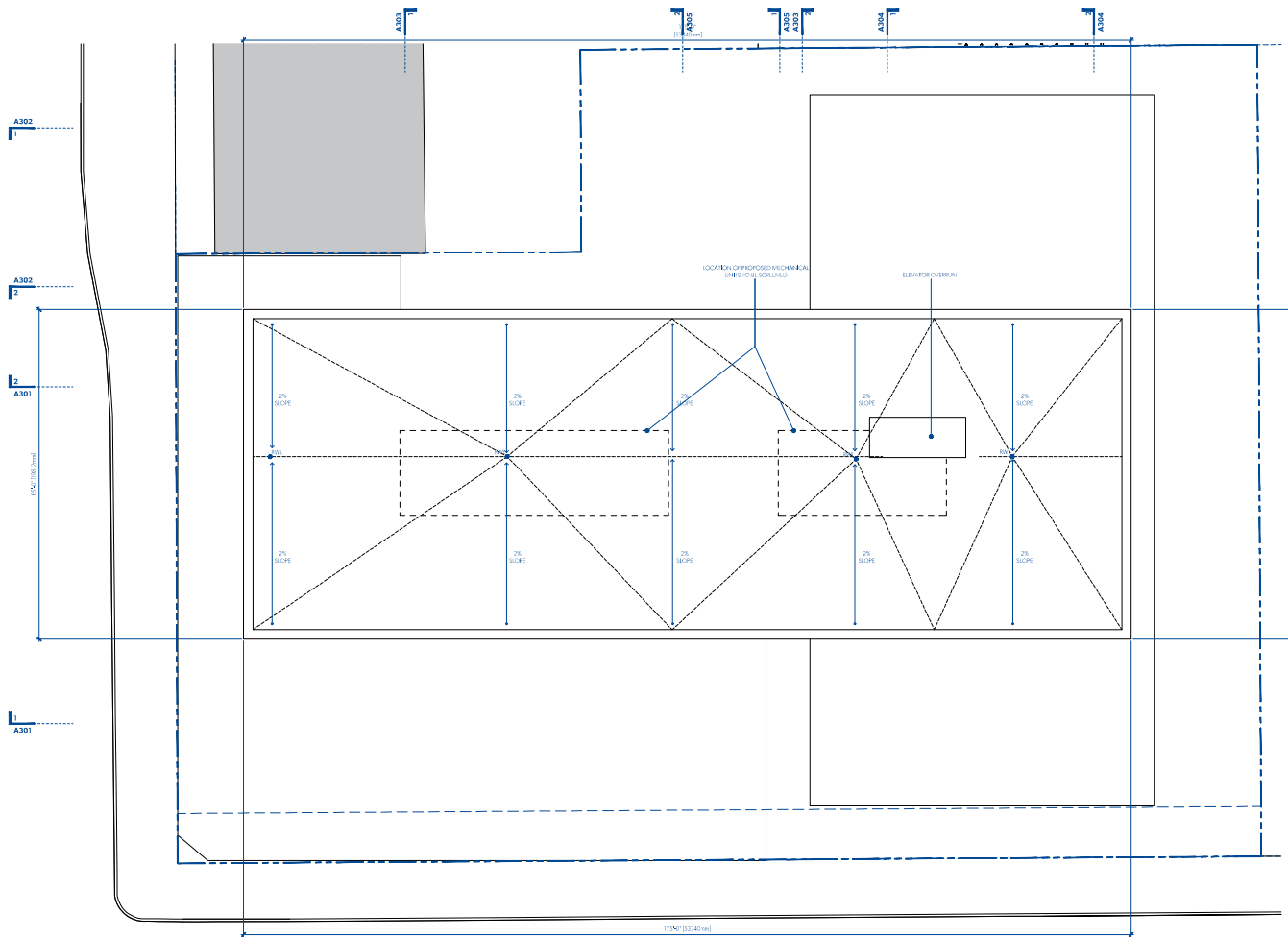


85°N

DATE	REVISION	DESCRIPTION
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1050 PANDORA AVE + 1518 COOK STREET
VICTORIA, BC
2018-001

A106
LEVEL 6 PLAN



ROOF PLAN
100

MGA



MICHAEL GREEN ARCHITECTURE
1533 W. 3RD AVENUE, VANCOUVER BC
CANADA V6J 1B8



2020/07/15	△	REVISED FOR REZONING
2020/03/20	△	REVISED FOR REZONING
2019/10/30	△	REVISED FOR REZONING
2019/09/13	△	REVISED FOR REZONING
2019/05/15	△	ISSUED FOR REZONING

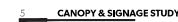
DATE REVISION DESCRIPTION

PARKWAY

1001 PARKWAY AVE + 1518 COOK STREET
VICTORIA, BC
2018/001

A107
ROOF PLAN







1 PANDORA AVENUE STREETScape
A251 1/12



2 COOK STREET STREETScape
A251 1/12

MGA
ARCHITECTURE



MICHAEL GREEN ARCHITECTURE
1535 W. 3RD AVENUE, VANCOUVER BC
CANADA V6J 1B8

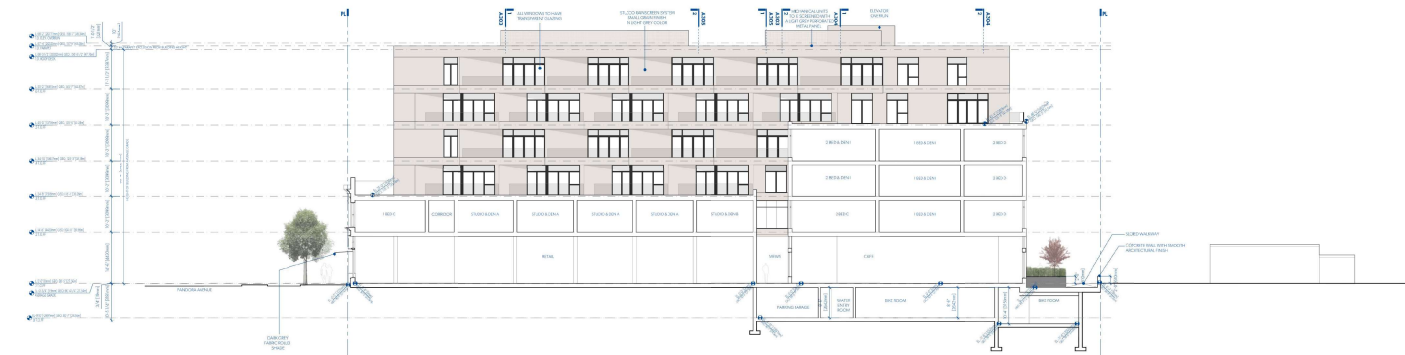
2020/07/15	△	REVISED FOR REZONING
2020/03/00	△	REVISED FOR REZONING
2019/10/30	△	REVISED FOR REZONING
2019/09/13	△	REVISED FOR REZONING
2019/05/15	△	REVISED FOR REZONING

DATE REVISION DESCRIPTION

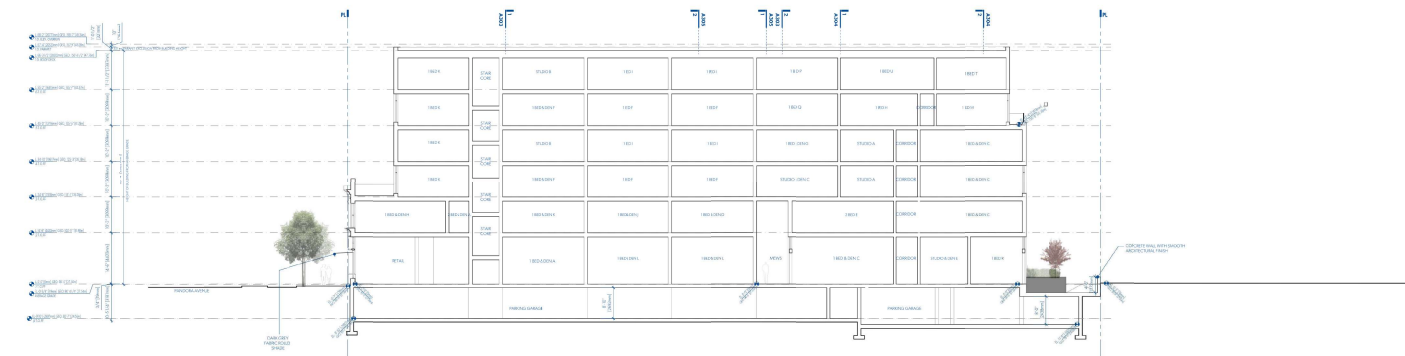
PARKWAY

1000 PANDORA AVE + 1518 COOK STREET
VICTORIA, BC
2019/001

A251
STREETSCAPE ELEVATIONS



1 SECTION LOOKING WEST THROUGH EXISTING BUILDING & NEW 4 STOREY VOLUME
A301 1/500



2 SECTION LOOKING WEST THROUGH NEW 6 STOREY VOLUME
A301 1/500

MGA
MICHAEL GREEN ARCHITECTURE



MATERIALITY



LIGHT GREY & MOSSY GREEN GLAZED, STUCCO-CHAMFERED WALL SYSTEM IN A 4-STOREY VOLUME



TRANSPARENT GLAZING WITH DARK GREY FRAMES



EXISTING WHITE CLASTO BRICK



MOSSY GREEN PAINTED, CAST-IN-PLACE CONCRETE WITH CIRCULAR PALETTE



DARK BALCONIES WITH A GREEN WALL ON A LANDSCAPE WINDOW AND A GREEN WALL ON A LANDSCAPE WINDOW

MICHAEL GREEN ARCHITECTURE
1535 W. 3RD AVENUE, VANCOUVER BC
CANADA V6J 1B8

2020-07-15	△	REVISED FOR REZONING
2020-03-20	△	REVISED FOR REZONING
2019-10-30	△	REVISED FOR REZONING
2019-09-13	△	REVISED FOR REZONING
2019-05-15	△	ISSUED FOR REZONING

DATE REVISION DESCRIPTION

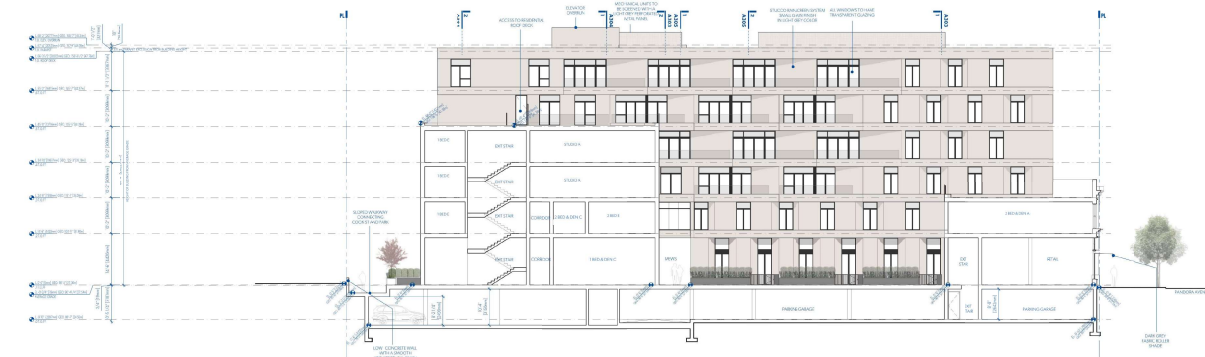
PARKWAY

1050 PANDORA AVE + 1518 COOK STREET
VICTORIA, BC
2019-02-01

A301
SECTIONS



1 SECTION LOOKING EAST THROUGH NEW 4 STOREY VOLUME
A302 1/500



2 SECTION LOOKING EAST THROUGH NEW 4 STOREY VOLUME & EXISTING BUILDING
A302 1/500

MGA



MATERIALITY



LIGHT GREY & MEDIUM GREY COLOURED STUCCO OR MASONRY FINISH SYSTEM IN A & B BUILDING VOLUMES



TRANSPARENT GLAZING WITH DARK GREY FRAMES



EXISTING WHITE CLASTIC BRICK



DARK GREY MASONRY OR STONE FINISH SYSTEM IN C BUILDING VOLUME



DARK GREY MASONRY OR STONE FINISH SYSTEM IN D BUILDING VOLUME



DARK GREY MASONRY OR STONE FINISH SYSTEM IN E BUILDING VOLUME

MICHAEL GREEN ARCHITECTURE
1535 W 3RD AVENUE, VANCOUVER, BC
CANADA V6J 1J8

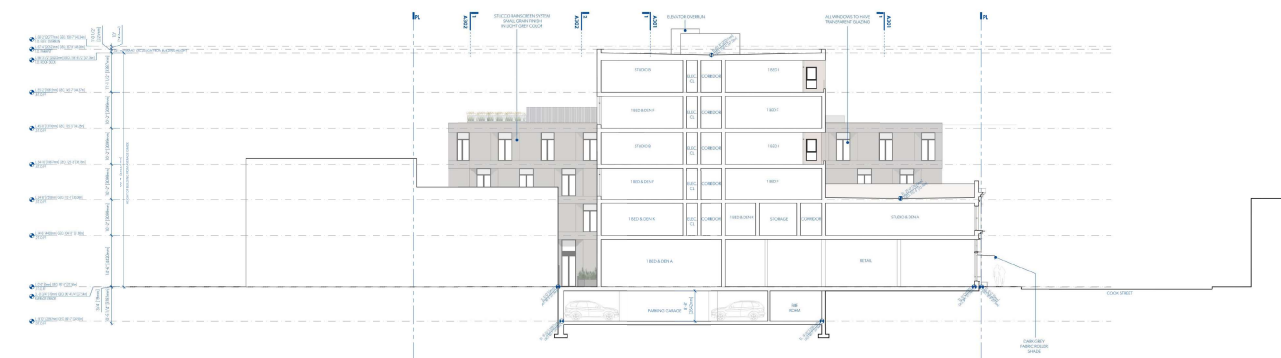
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2020-03-20	△	REVISED FOR REZONING
2019-10-30	△	REVISED FOR REZONING
2019-09-13	△	REVISED FOR REZONING
2019-05-15	△	ISSUED FOR REZONING

DATE REVISION DESCRIPTION

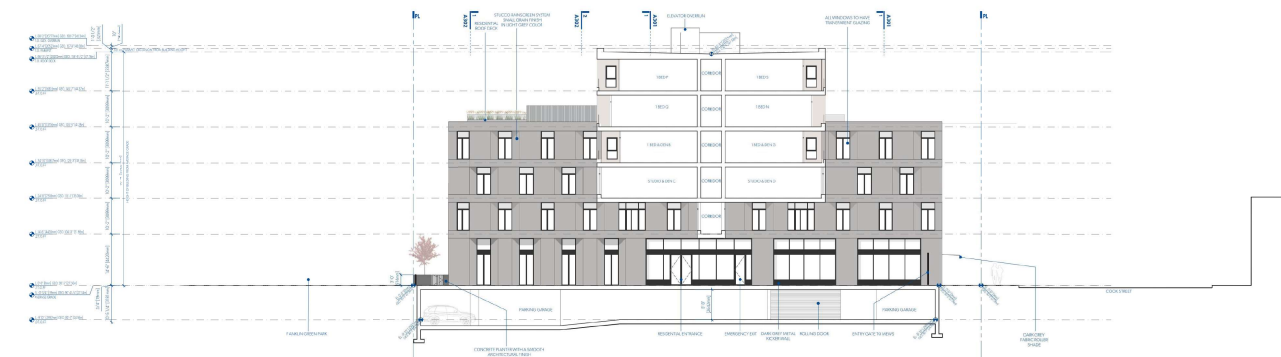
PARKWAY

1009 PANDORA AVE + 1518 COOK STREET
VICTORIA, BC
2019-02

A302
SECTIONS



1 SECTION LOOKING NORTH THROUGH EXISTING BUILDING & NEW 6 STOREY VOLUME
A303 1/500



2 SECTION LOOKING NORTH THROUGH NEW 6 STOREY VOLUME
A303 1/500

MGA
MICHAEL GREEN ARCHITECTURE



MATERIALITY



LIGHT GREY & MEDIUM GREY COLOURED, STEEL CHANNEL IN WALL SYSTEM IN A BATHROOM



TRANSPARENT GLAZING WITH DARK GREY FRAMES



EXISTING WHITE CLASTIC BRICK



DARK GREY PANELS WITH A CIRCULAR PATTERN, RESISTANT TO THE FACE OF THE PANEL



DARK BALCONIES WITH A CIRCULAR PATTERN, RESISTANT TO THE FACE OF THE PANEL

MICHAEL GREEN ARCHITECTURE
1535 W. 3RD AVENUE, VANCOUVER BC
CANADA V6J 1B8

2020-07-15	△	REVISED FOR REZONING
2020-03-20	△	REVISED FOR REZONING
2019-11-30	△	REVISED FOR REZONING
2019-10-13	△	REVISED FOR REZONING
2019-05-15	△	ISSUED FOR REZONING

DATE REVISION DESCRIPTION

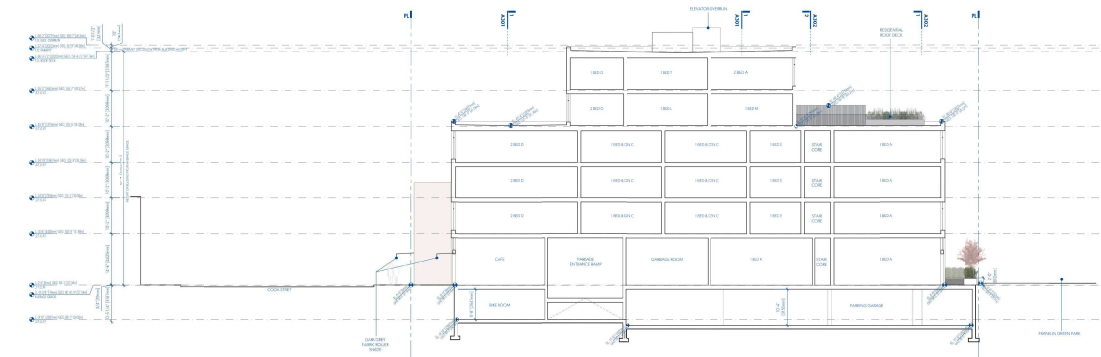
PARKWAY

1050 PANDORA AVE + 1518 COOK STREET
VICTORIA, BC
2018-01

A303
SECTIONS



1 SECTION LOOKING NORTH THROUGH NEW 4 & 6 STOREY VOLUME
A304 1/500



2 SECTION LOOKING SOUTH THROUGH NEW 4 & 6 STOREY VOLUME
A304 1/500

MGA
MICHAEL GREEN ARCHITECTURE



MATERIALITY



LIGHT GREY & MEDIUM GREY COLOURED, STUCCO OR PLASTER FINISH SYSTEM IN A 6-8' SQUARE TILES



TRANSPARENT GLAZING WITH DARK GREY FRAMES



TEXTURED WHITE PLASTER BRICK



DARK GREY HORIZONTAL SIDING, RESISTANT TO WEATHER & CORROSION



DARK GREY HORIZONTAL SIDING, RESISTANT TO WEATHER & CORROSION

MICHAEL GREEN ARCHITECTURE
1535 W. 3RD AVENUE, VANCOUVER, BC
CANADA V6J 1B8

2020-07-15	△	REVISED FOR REZONING
2020-03-20	△	REVISED FOR REZONING
2019-11-30	△	REVISED FOR REZONING
2019-09-13	△	REVISED FOR REZONING
2019-05-15	△	ISSUED FOR REZONING

DATE	REVISION	DESCRIPTION
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PARKWAY

1050 PANDORA AVE + 1518 COOK STREET
VICTORIA, BC
2019-02-01

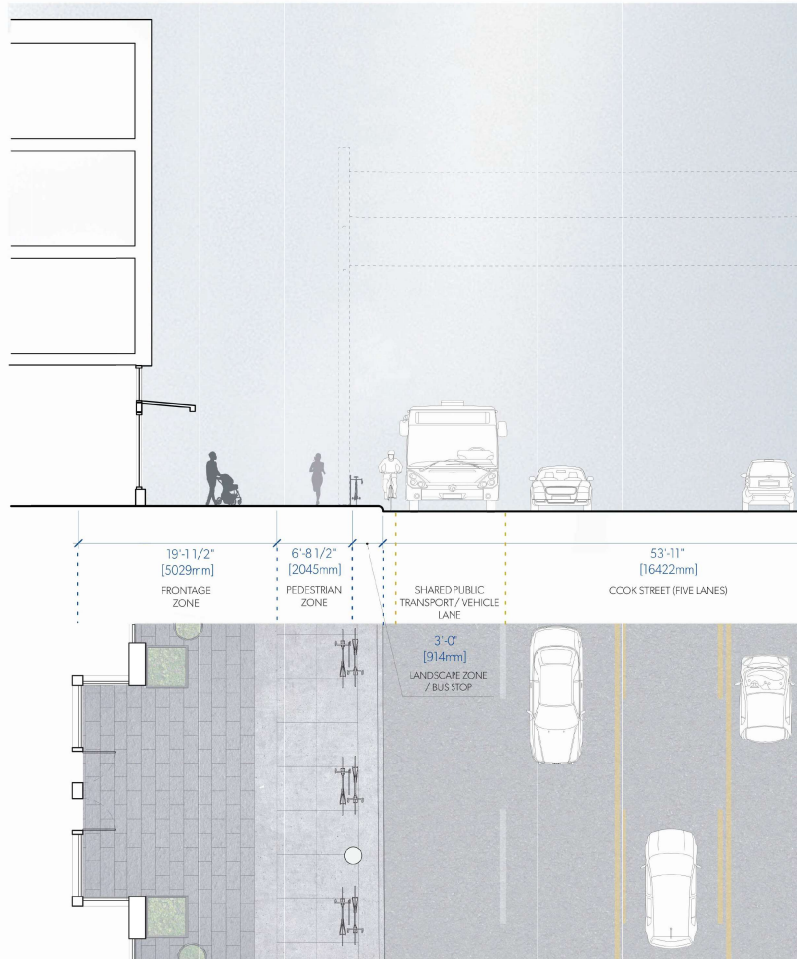
A304
SECTIONS



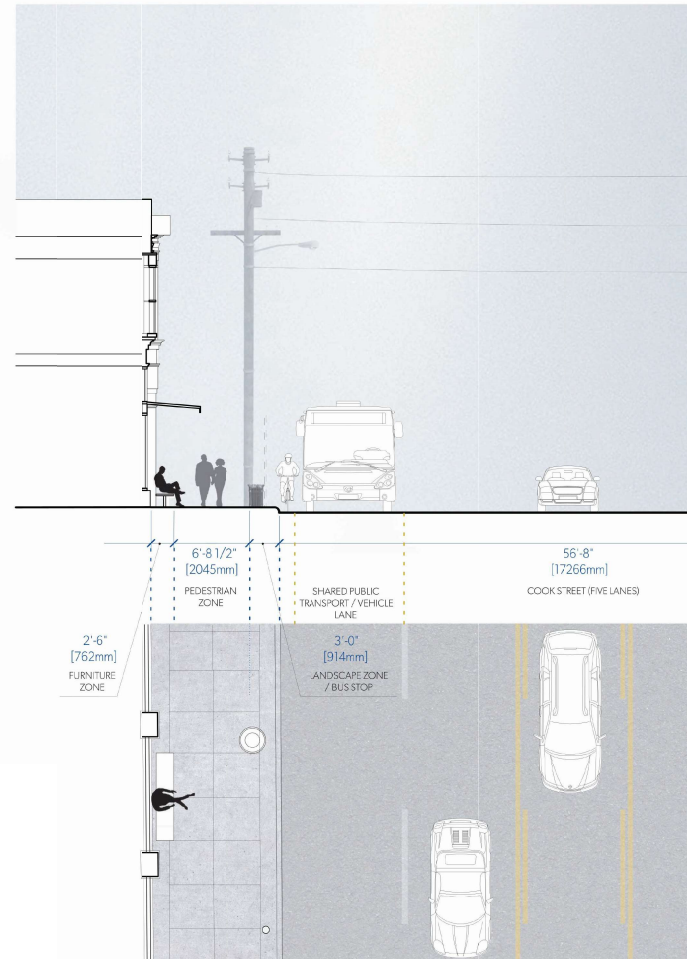
JULY BALCONIES WITH CLEARGLASS ON LIVINGSPACE WINDOWS FACING FRANKLIN GREEN PARK & HARRIS GREEN PARK.

1050 PANDORA AVE + 1518 COOK STREET
VICTORIA, BC
2018-001

A305



1 STREETScape SECTION & PLAN THRU COMMERCIAL



2 STREETScape SECTION & PLAN THRU CAFE



MATERIALITY



LIGHT GREY & MOSSY COLOURED STEEL CHANNELS WITH RAIL SYSTEM IN A BOTTOM RAMP



TRANSPARENT GLAZING WITH DARK GREY FRAMES



EXISTING WHITE GLAZED BRICK



DARK GREY PAINTED STEEL CHANNELS ON LANDSCAPE WITH DARK GREY RAIL SYSTEM



DARK GREY PAINTED STEEL CHANNELS ON LANDSCAPE WITH DARK GREY RAIL SYSTEM

MICHAEL GREEN ARCHITECTURE
1515 W 3RD AVENUE, VICTORIA BC
CANADA V8W 3B8

2020-07-15	△	REVISED FOR REZONING
2020-03-20	△	REVISED FOR REZONING
2019-10-30	△	REVISED FOR REZONING
2019-09-13	△	REVISED FOR REZONING
2019-05-15	△	ISSUED FOR REZONING

DATE	REVISION	DESCRIPTION
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PARKWAY

1050 PANDORA AVE + 1518 COOK STREET
VICTORIA, BC
2018-201

A308
STREETScape CROSS SECTIONS

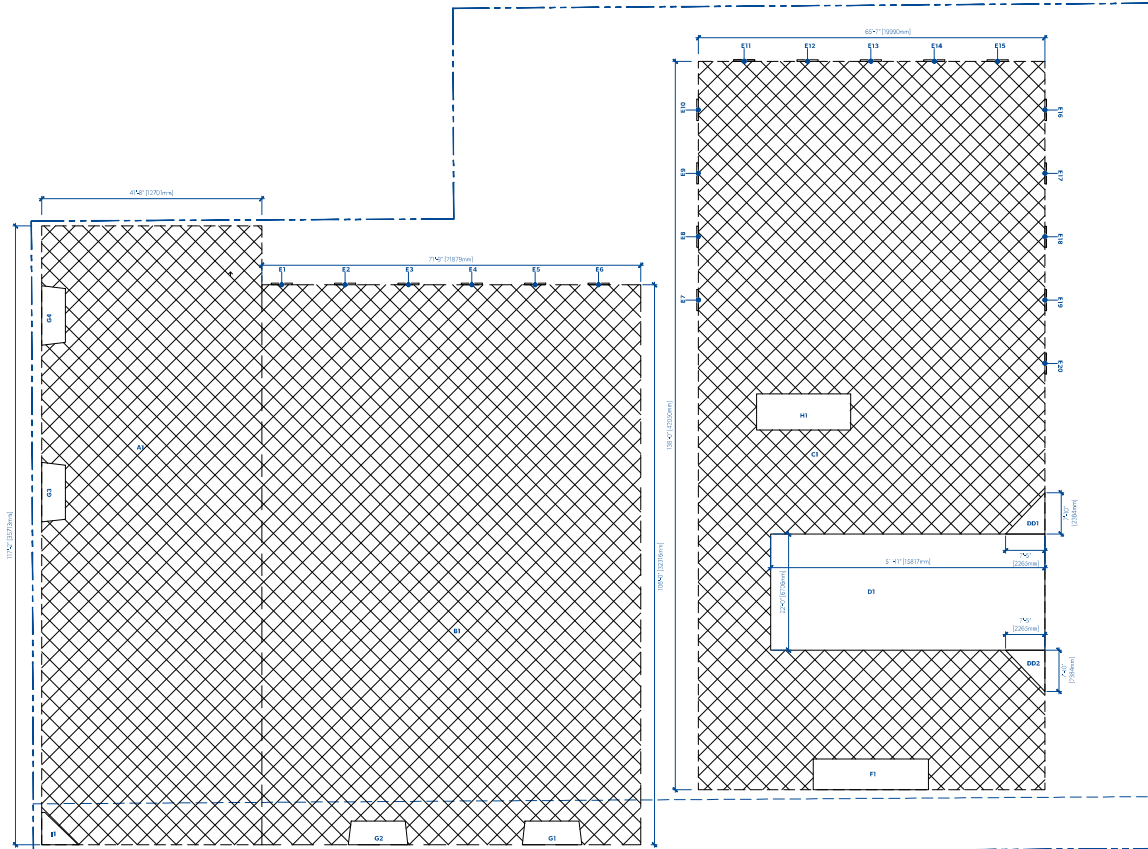


TABLE 1

ITEM	DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	TOTAL PRICE
1	CONCRETE	m ³	100	150	15000
2	STEEL	kg	500	20	10000
3	PAVING	m ²	200	50	10000
4	LANDSCAPING	m ²	100	100	10000
5	UTILITIES	m	50	200	10000
6	CONCRETE	m ³	50	150	7500
7	STEEL	kg	250	20	5000
8	PAVING	m ²	100	50	5000
9	LANDSCAPING	m ²	50	100	5000
10	UTILITIES	m	25	200	5000
11	CONCRETE	m ³	25	150	3750
12	STEEL	kg	125	20	2500
13	PAVING	m ²	50	50	2500
14	LANDSCAPING	m ²	25	100	2500
15	UTILITIES	m	12.5	200	2500
16	CONCRETE	m ³	12.5	150	1875
17	STEEL	kg	62.5	20	1250
18	PAVING	m ²	25	50	1250
19	LANDSCAPING	m ²	12.5	100	1250
20	UTILITIES	m	6.25	200	1250
21	CONCRETE	m ³	6.25	150	937.5
22	STEEL	kg	31.25	20	625
23	PAVING	m ²	12.5	50	625
24	LANDSCAPING	m ²	6.25	100	625
25	UTILITIES	m	3.125	200	625
26	CONCRETE	m ³	3.125	150	468.75
27	STEEL	kg	15.625	20	312.5
28	PAVING	m ²	6.25	50	312.5
29	LANDSCAPING	m ²	3.125	100	312.5
30	UTILITIES	m	1.5625	200	312.5
31	CONCRETE	m ³	1.5625	150	234.375
32	STEEL	kg	7.8125	20	156.25
33	PAVING	m ²	3.125	50	156.25
34	LANDSCAPING	m ²	1.5625	100	156.25
35	UTILITIES	m	0.78125	200	156.25
36	CONCRETE	m ³	0.78125	150	117.1875
37	STEEL	kg	3.90625	20	78.125
38	PAVING	m ²	1.5625	50	78.125
39	LANDSCAPING	m ²	0.78125	100	78.125
40	UTILITIES	m	0.390625	200	78.125
41	CONCRETE	m ³	0.390625	150	58.59375
42	STEEL	kg	1.953125	20	39.0625
43	PAVING	m ²	0.78125	50	39.0625
44	LANDSCAPING	m ²	0.390625	100	39.0625
45	UTILITIES	m	0.1953125	200	39.0625
46	CONCRETE	m ³	0.1953125	150	29.296875
47	STEEL	kg	0.9765625	20	19.53125
48	PAVING	m ²	0.390625	50	19.53125
49	LANDSCAPING	m ²	0.1953125	100	19.53125
50	UTILITIES	m	0.09765625	200	19.53125
51	CONCRETE	m ³	0.09765625	150	14.6484375
52	STEEL	kg	0.48828125	20	9.765625
53	PAVING	m ²	0.1953125	50	9.765625
54	LANDSCAPING	m ²	0.09765625	100	9.765625
55	UTILITIES	m	0.048828125	200	9.765625
56	CONCRETE	m ³	0.048828125	150	7.32421875
57	STEEL	kg	0.244140625	20	4.8828125
58	PAVING	m ²	0.09765625	50	4.8828125
59	LANDSCAPING	m ²	0.048828125	100	4.8828125
60	UTILITIES	m	0.0244140625	200	4.8828125
61	CONCRETE	m ³	0.0244140625	150	3.662109375
62	STEEL	kg	0.1220703125	20	2.44140625
63	PAVING	m ²	0.048828125	50	2.44140625
64	LANDSCAPING	m ²	0.0244140625	100	2.44140625
65	UTILITIES	m	0.01220703125	200	2.44140625
66	CONCRETE	m ³	0.01220703125	150	1.8310546875
67	STEEL	kg	0.06103515625	20	1.220703125
68	PAVING	m ²	0.048828125	50	2.44140625
69	LANDSCAPING	m ²	0.0244140625	100	2.44140625
70	UTILITIES	m	0.01220703125	200	2.44140625
71	CONCRETE	m ³	0.01220703125	150	1.8310546875
72	STEEL	kg	0.06103515625	20	1.220703125
73	PAVING	m ²	0.048828125	50	2.44140625
74	LANDSCAPING	m ²	0.0244140625	100	2.44140625
75	UTILITIES	m	0.01220703125	200	2.44140625
76	CONCRETE	m ³	0.01220703125	150	1.8310546875
77	STEEL	kg	0.06103515625	20	1.220703125
78	PAVING	m ²	0.048828125	50	2.44140625
79	LANDSCAPING	m ²	0.0244140625	100	2.44140625
80	UTILITIES	m	0.01220703125	200	2.44140625
81	CONCRETE	m ³	0.01220703125	150	1.8310546875
82	STEEL	kg	0.06103515625	20	1.220703125
83	PAVING	m ²	0.048828125	50	2.44140625
84	LANDSCAPING	m ²	0.0244140625	100	2.44140625
85	UTILITIES	m	0.01220703125	200	2.44140625
86	CONCRETE	m ³	0.01220703125	150	1.8310546875
87	STEEL	kg	0.06103515625	20	1.220703125
88	PAVING	m ²	0.048828125	50	2.44140625
89	LANDSCAPING	m ²	0.0244140625	100	2.44140625
90	UTILITIES	m	0.01220703125	200	2.44140625
91	CONCRETE	m ³	0.01220703125	150	1.8310546875
92	STEEL	kg	0.06103515625	20	1.220703125
93	PAVING	m ²	0.048828125	50	2.44140625
94	LANDSCAPING	m ²	0.0244140625	100	2.44140625
95	UTILITIES	m	0.01220703125	200	2.44140625
96	CONCRETE	m ³	0.01220703125	150	1.8310546875
97	STEEL	kg	0.06103515625	20	1.220703125
98	PAVING	m ²	0.048828125	50	2.44140625
99	LANDSCAPING	m ²	0.0244140625	100	2.44140625
100	UTILITIES	m	0.01220703125	200	2.44140625



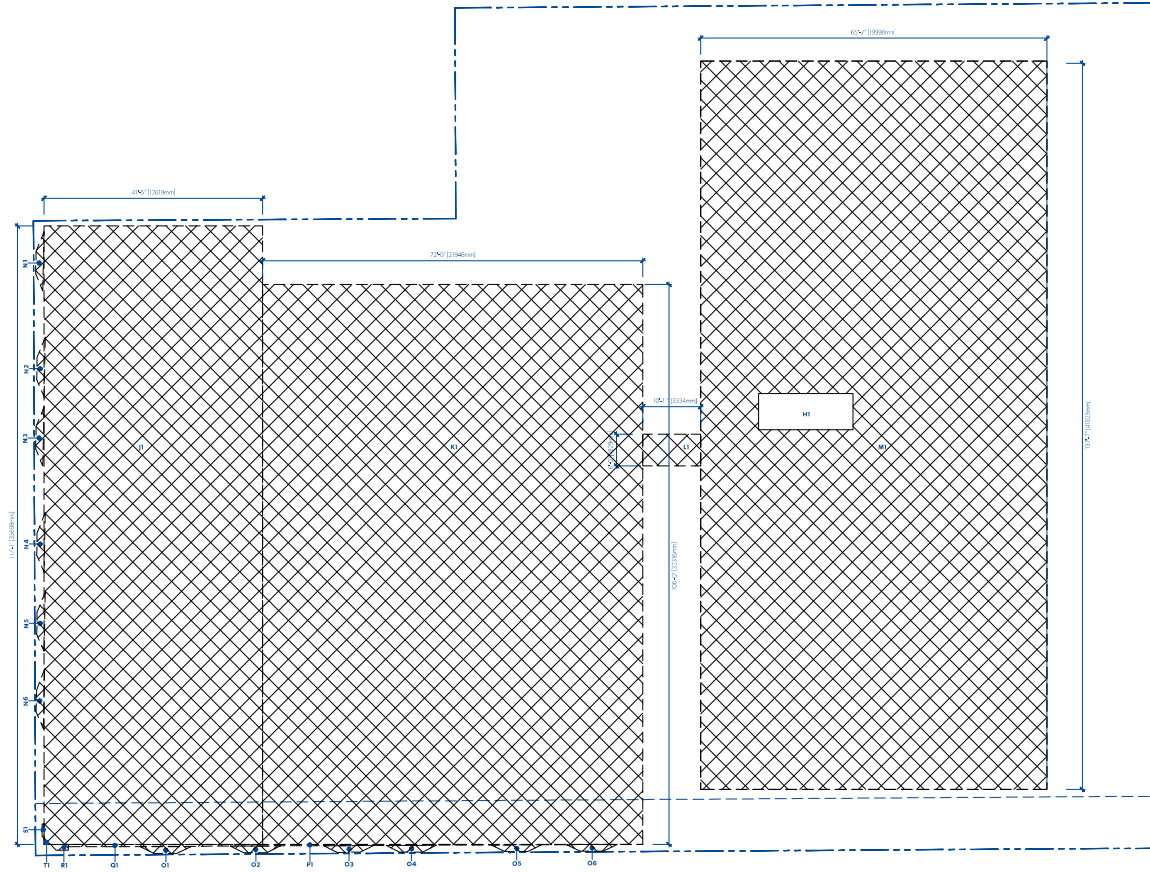
MICHAEL GREEN ARCHITECTURE
1535 W. 3RD AVENUE, VANCOUVER, BC
CANADA V6J 1B8

DATE	REVISION	DESCRIPTION
2020/07/15	△	REVISED FOR ZONING
2020/09/30	△	REVISED FOR ZONING
2019/06/30	△	REVISED FOR ZONING
2019/09/13	△	REVISED FOR ZONING
2019/05/15	△	ISSUED FOR ZONING

PARKWAY
1001 PARKWAY AVE + 1018 COOK STREET
VICTORIA, BC
2019/06/30

FSR LEVEL 1
1:500

A801
FSR LEVEL 1



NO.	DESCRIPTION	QTY	UNIT	PRICE	TOTAL
1	CEILING	100.00	SQ. FT.	1.00	100.00
2	FLOOR	100.00	SQ. FT.	1.00	100.00
3	WALL	100.00	SQ. FT.	1.00	100.00
4	DOOR	100.00	SQ. FT.	1.00	100.00
5	WINDOW	100.00	SQ. FT.	1.00	100.00
6	MECHANICAL	100.00	SQ. FT.	1.00	100.00
7	ELECTRICAL	100.00	SQ. FT.	1.00	100.00
8	PLUMBING	100.00	SQ. FT.	1.00	100.00
9	PAINT	100.00	SQ. FT.	1.00	100.00
10	FINISH	100.00	SQ. FT.	1.00	100.00
11	LANDSCAPE	100.00	SQ. FT.	1.00	100.00
12	TOTAL	100.00	SQ. FT.	1.00	100.00



MICHAEL GREEN ARCHITECTURE
1533 W 3RD AVENUE, VANCOUVER BC
CANADA V6J 1B8

DATE	REVISION	DESCRIPTION
2020-07-15	△	REVISED FOR RECONING
2020-03-20	△	REVISED FOR RECONING
2019-09-30	△	REVISED FOR RECONING
2019-03-13	△	REVISED FOR RECONING
2019-03-13	△	ISSUED FOR RECONING

PARKWAY
1050 PARKWAY AVE + 1518 COOK STREET
VICTORIA, BC
2019-001

1 FSR LEVEL 2
MGA 1000

A802
FSR LEVEL 2

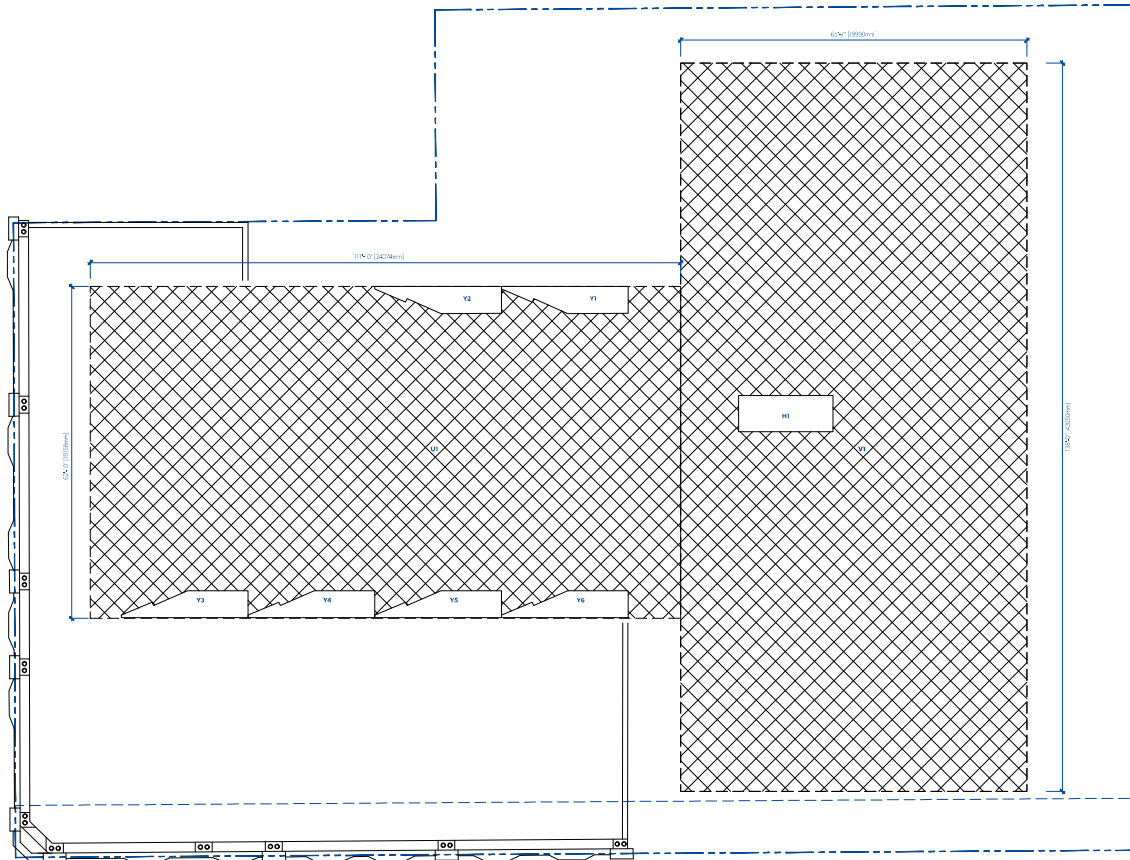


TABLE 1		TABLE 2	
ITEM	DESCRIPTION	ITEM	DESCRIPTION
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3	3.00	3	3.00
4	4.00	4	4.00
5	5.00	5	5.00
6	6.00	6	6.00
7	7.00	7	7.00
8	8.00	8	8.00
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11	11.00	11	11.00
12	12.00	12	12.00
13	13.00	13	13.00
14	14.00	14	14.00
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18	18.00	18	18.00
19	19.00	19	19.00
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21	21.00	21	21.00
22	22.00	22	22.00
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25	25.00	25	25.00
26	26.00	26	26.00
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42	42.00	42	42.00
43	43.00	43	43.00
44	44.00	44	44.00
45	45.00	45	45.00
46	46.00	46	46.00
47	47.00	47	47.00
48	48.00	48	48.00
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95	95.00	95	95.00
96	96.00	96	96.00
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MGA



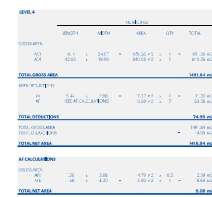
MICHAEL GREEN ARCHITECTURE
1533 W 3RD AVENUE, VANCOUVER BC
CANADA V6J 1B8

DATE	REVISION	DESCRIPTION
2020-07-15	△	REVISED FOR RETAINING
2020-03-20	△	REVISED FOR RETAINING
2019-03-20	△	REVISED FOR RETAINING
2019-03-13	△	REVISED FOR RETAINING
2019-03-15	△	ISSUED FOR RETAINING

PARKWAY
1050 PARKWAY AVE + 1518 COOK STREET
VICTORIA, BC
V8N 1C1

A803
FSR LEVEL 3

1
FSR LEVEL 3
A803 1/100








MGA



MICHAEL GREEN ARCHITECTURE
1535 W 3RD AVENUE, VANCOUVER BC
CANADA V6J 1J8



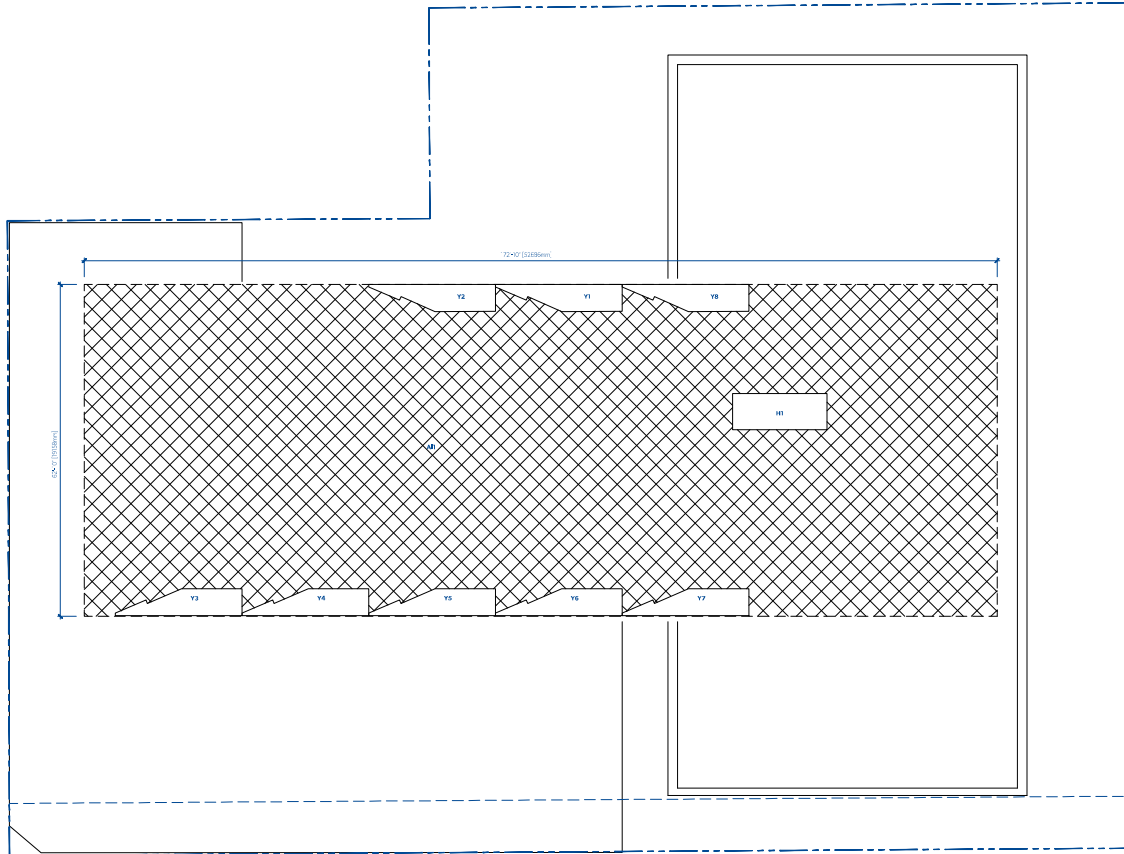
2020-07-15		REVISED FOR REZONING
2020-03-20		REVISED FOR REZONING
2019-10-30		REVISED FOR REZONING
2019-09-13		REVISED FOR REZONING
2019-05-15		ISSUED FOR REZONING

DATE	REVISION	DESCRIPTION
------	----------	-------------

PARKWAY

1050 PANDORA AVE + 1518 COOK STREET
VICTORIA, BC
2018-001

A804
FSR LEVEL 4



LEVEL 5		TOTAL AREA	
DESCRIPTION	AREA	AREA	AREA
1	12' x 10' (2288mm)	120.00	120.00
2	12' x 10' (2288mm)	120.00	120.00
3	12' x 10' (2288mm)	120.00	120.00
4	12' x 10' (2288mm)	120.00	120.00
5	12' x 10' (2288mm)	120.00	120.00
6	12' x 10' (2288mm)	120.00	120.00
7	12' x 10' (2288mm)	120.00	120.00
8	12' x 10' (2288mm)	120.00	120.00
9	12' x 10' (2288mm)	120.00	120.00
10	12' x 10' (2288mm)	120.00	120.00
11	12' x 10' (2288mm)	120.00	120.00
12	12' x 10' (2288mm)	120.00	120.00
13	12' x 10' (2288mm)	120.00	120.00
14	12' x 10' (2288mm)	120.00	120.00
15	12' x 10' (2288mm)	120.00	120.00
16	12' x 10' (2288mm)	120.00	120.00
17	12' x 10' (2288mm)	120.00	120.00
18	12' x 10' (2288mm)	120.00	120.00
19	12' x 10' (2288mm)	120.00	120.00
20	12' x 10' (2288mm)	120.00	120.00
21	12' x 10' (2288mm)	120.00	120.00
22	12' x 10' (2288mm)	120.00	120.00
23	12' x 10' (2288mm)	120.00	120.00
24	12' x 10' (2288mm)	120.00	120.00
25	12' x 10' (2288mm)	120.00	120.00
26	12' x 10' (2288mm)	120.00	120.00
27	12' x 10' (2288mm)	120.00	120.00
28	12' x 10' (2288mm)	120.00	120.00
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41	12' x 10' (2288mm)	120.00	120.00
42	12' x 10' (2288mm)	120.00	120.00
43	12' x 10' (2288mm)	120.00	120.00
44	12' x 10' (2288mm)	120.00	120.00
45	12' x 10' (2288mm)	120.00	120.00
46	12' x 10' (2288mm)	120.00	120.00
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52	12' x 10' (2288mm)	120.00	120.00
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70	12' x 10' (2288mm)	120.00	120.00
71	12' x 10' (2288mm)	120.00	120.00
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82	12' x 10' (2288mm)	120.00	120.00
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87	12' x 10' (2288mm)	120.00	120.00
88	12' x 10' (2288mm)	120.00	120.00
89	12' x 10' (2288mm)	120.00	120.00
90	12' x 10' (2288mm)	120.00	120.00
91	12' x 10' (2288mm)	120.00	120.00
92	12' x 10' (2288mm)	120.00	120.00
93	12' x 10' (2288mm)	120.00	120.00
94	12' x 10' (2288mm)	120.00	120.00
95	12' x 10' (2288mm)	120.00	120.00
96	12' x 10' (2288mm)	120.00	120.00
97	12' x 10' (2288mm)	120.00	120.00
98	12' x 10' (2288mm)	120.00	120.00
99	12' x 10' (2288mm)	120.00	120.00
100	12' x 10' (2288mm)	120.00	120.00

FSR LEVEL 5
1:1000

MGA
ARCHITECTURE



MICHAEL GREEN ARCHITECTURE
1533 W 3RD AVENUE, VANCOUVER BC
CANADA V6J 1B8

- 2020-07-15 REVISED FOR REDLINING
- 2020-03-20 REVISED FOR REDLINING
- 2019-03-20 REVISED FOR REDLINING
- 2019-03-20 REVISED FOR REDLINING
- 2019-03-20 REVISED FOR REDLINING

DATE REVISION DESCRIPTION

PARKWAY

1050 PARKWAY AVE + 1518 COOK STREET
VICTORIA, BC
2019-03-20

A805
FSR LEVEL 5



1 VIEW NORTHWEST FROM CORNER OF PANDORA & COOK
A811

MGA
ARCHITECTURE



MICHAEL GREEN ARCHITECTURE
1535 W. 3RD AVENUE, VANCOUVER BC
CANADA V6J 1B8

2020-07-15	△	REVISED FOR REZONING
2020-09-20	△	REVISED FOR REZONING
2019-10-30	△	REVISED FOR REZONING
2019-09-13	△	REVISED FOR REZONING
2019-05-15	△	ISSUED FOR REZONING

DATE REVISION DESCRIPTION

PARKWAY

1001 PANDORA AVE + 1518 COOK STREET
VICTORIA BC
2018-001

A811
3D RENDERS



3 VIEW WEST THROUGH RESIDENTIAL NEWS
DATE: 11/11/2019

MGA
ARCHITECTURE



MICHAEL GREEN ARCHITECTURE
1533 W. 3RD AVENUE, VANCOUVER BC
CANADA V6J 1B8

2020-07-15	△	REVISED FOR REZONING
2020-09-20	△	REVISED FOR REZONING
2019-10-30	△	REVISED FOR REZONING
2019-09-13	△	REVISED FOR REZONING
2019-05-15	△	ISSUED FOR REZONING

DATE REVISION DESCRIPTION

PARKWAY

1001 PARKWAY AVE + 1518 COOK STREET
VICTORIA BC
2018-001

A812
3D RENDERS



1 VIEW SOUTHWEST FROM COOK STREET
A813

MGA
ARCHITECTS



MICHAEL GREEN ARCHITECTURE
1535 W. 3RD AVENUE, VANCOUVER BC
CANADA V6J 1B8

2020-07-15	△	REVISED FOR REZONING
2020-09-20	△	REVISED FOR REZONING
2019-10-30	△	REVISED FOR REZONING
2019-09-13	△	REVISED FOR REZONING
2019-05-15	△	ISSUED FOR REZONING

DATE REVISION DESCRIPTION

PARKWAY

1001 PARKWAY AVE + 1518 COOK STREET
VICTORIA BC
2018-001

A813
3D RENDERS



VIEW WEST ALONG PUBLIC WALKWAY

MGA



MICHAEL GREEN ARCHITECTURE
1533 W. 3RD AVENUE, VANCOUVER BC
CANADA V6J 1B8

2020-07-15	△	REVISED FOR REZONING
2020-09-30	△	REVISED FOR REZONING
2019-10-30	△	REVISED FOR REZONING
2019-09-13	△	REVISED FOR REZONING
2019-05-15	△	ISSUED FOR REZONING

DATE REVISION DESCRIPTION

PARKWAY

1001 PARKDAVE AVE + 1518 COOK STREET
VICTORIA, BC
2018-001

A814
3D RENDERS



MICHAEL GREEN ARCHITECTURE
1535 W. 3RD AVENUE, VANCOUVER BC
CANADA V6J 1B8

2020-07-15	△	REVISED FOR REZONING
2020-09-20	△	REVISED FOR REZONING
2019-10-30	△	REVISED FOR REZONING
2019-09-13	△	REVISED FOR REZONING
2019-05-15	△	ISSUED FOR REZONING

DATE REVISION DESCRIPTION

PARKWAY

1001 PARKWAY AVE + 1518 COOK STREET
VICTORIA, BC
2018-001

A815
3D RENDERS



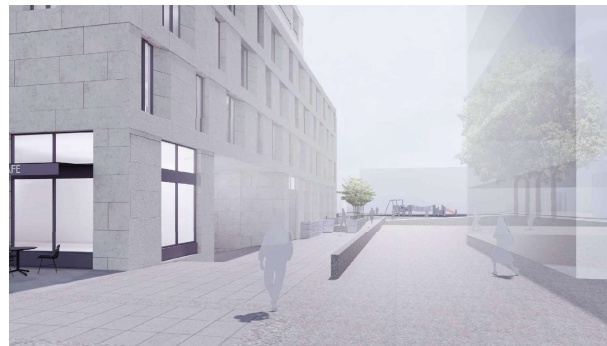
1
A817 ADJACENT PROPERTY STUDY | VIEW WEST ACROSS COOK STREET



3
A817 ADJACENT PROPERTY STUDY | VIEW EAST ACROSS FRANKLIN GREEN PARK



2
A817 ADJACENT PROPERTY STUDY | VIEW SOUTHWEST DOWN COOK STREET



4
A817 ADJACENT PROPERTY STUDY | VIEW WEST ALONG NORTH WALKWAY

MGA
ARCHITECTURE



MICHAEL GREEN ARCHITECTURE
1533 W. 3RD AVENUE, VANCOUVER BC
CANADA V6J 1B8

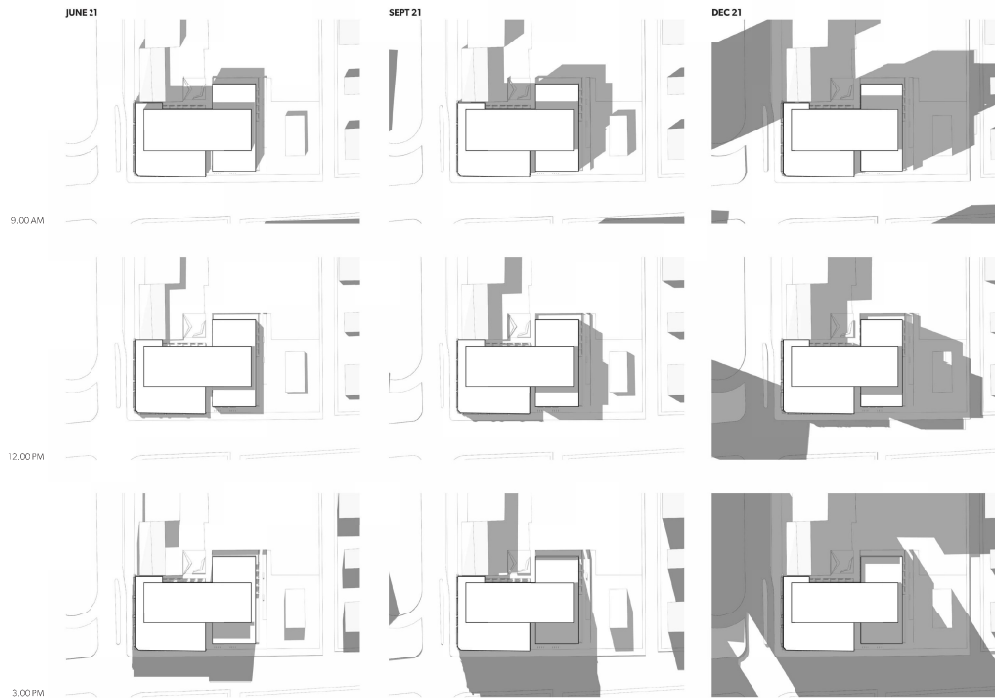
2020-07-15	△	REVISED FOR REZONING
2020-09-20	△	REVISED FOR REZONING
2019-10-30	△	REVISED FOR REZONING
2019-09-13	△	REVISED FOR REZONING
2019-05-15	△	ISSUED FOR REZONING

DATE REVISION DESCRIPTION

PARKWAY

1001 PANDORA AVE + 1518 COOK STREET
VICTORIA BC
2018-001

A817
ADJACENT PROPERTY
STUDY

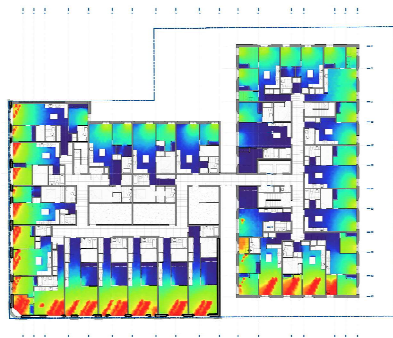


2020-07-15	△	REBID FOR REZONING
2020-03-20	△	REBID FOR REZONING
2019-10-30	△	REBID FOR REZONING
2019-09-13	△	REBID FOR REZONING
2019-05-15	△	ISSUED FOR REZONING

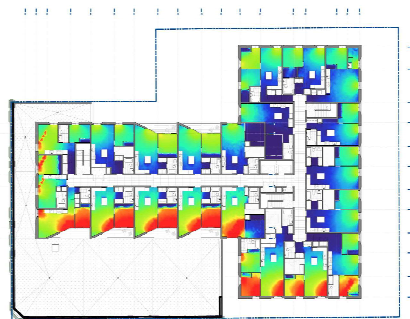
DATE	REVISION	DESCRIPTION
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PARKWAY

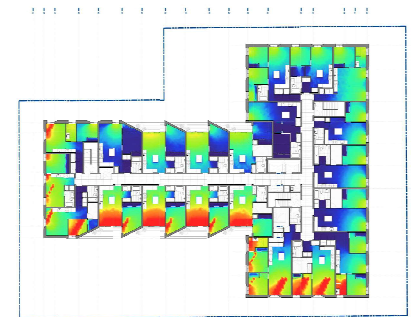
1050 PARKWAY AVE - 1518 COOK STREET
VICTORIA, BC
V8K 0G1



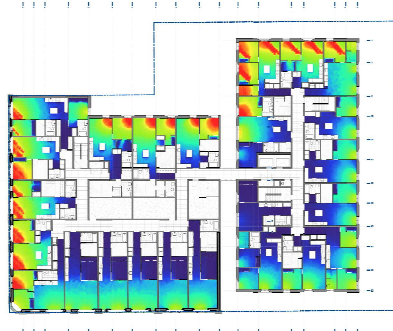
LEVEL 2, 9:00 AM, 14.50 (LUX)



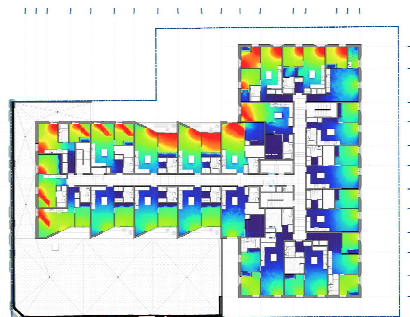
LEVEL 3, 9:00 AM, 24.67 (LUX)



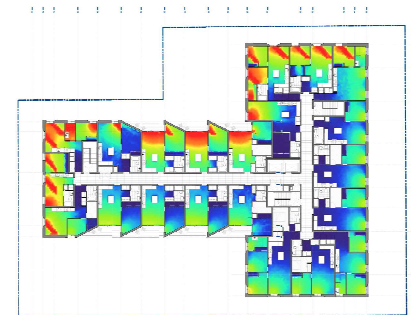
LEVEL 4, 9:00 AM, 34.83 (LUX)



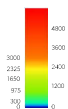
LEVEL 2, 3:00 PM, 14.50 (LUX)



LEVEL 3, 3:00 PM, 24.67 (LUX)



LEVEL 4, 3:00 PM, 34.83 (LUX)



MGA



MICHAEL GREEN ARCHITECTURE
1535 W. 3RD AVENUE, VANCOUVER BC
CANADA V6J 1B8



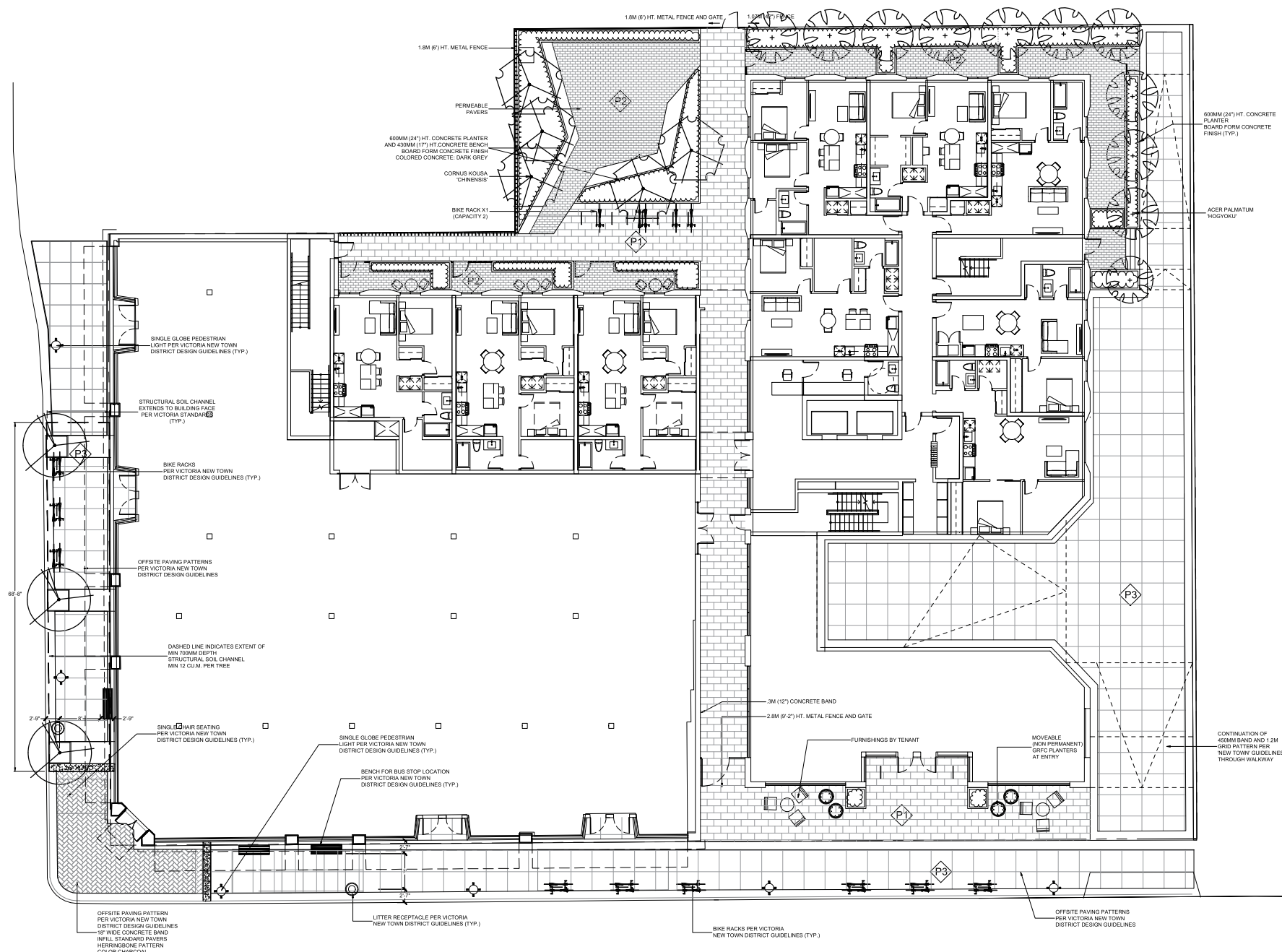
2020/07/15	△	REVISED FOR REDONING
2020/10/20	△	REVISED FOR REDONING
2018/09/10	△	REVISED FOR REDONING
2019/05/15	△	ISSUED FOR REDONING

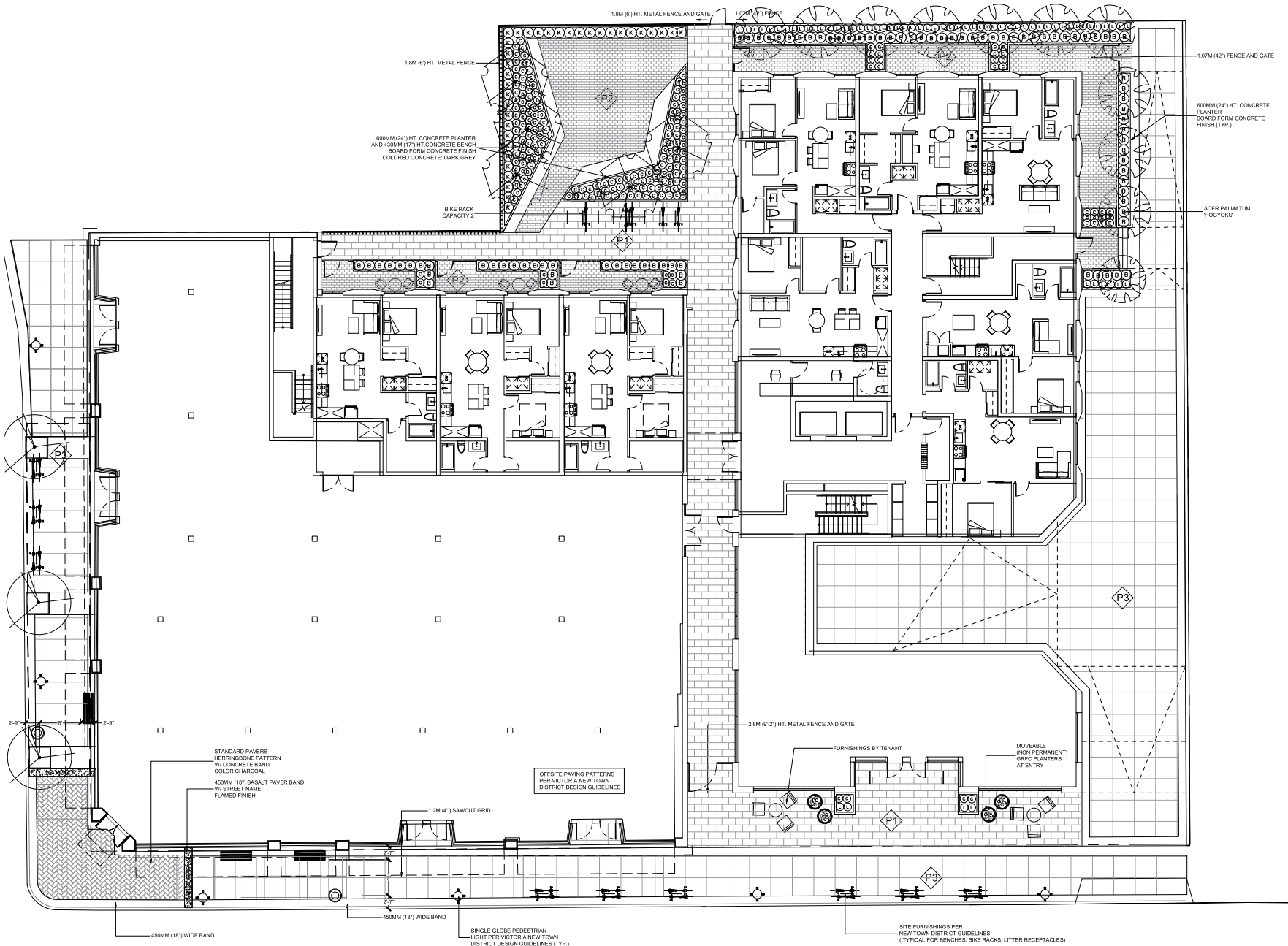
DATE REVISION DESCRIPTION

PARKWAY

1050 PANDORA AVE + 1518 COOK STREET
VICTORIA, BC
2018/001

A822
Illuminance Analysis





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pmg
LANDSCAPE
ARCHITECTS

Suite C100 - 4185 Still Creek Drive
Burnaby, British Columbia V5C 6G9
p. 604 294-0011 • f. 604 294-0022

SEAL:

NO.	DATE	REVISION DESCRIPTION	DR.
1	20 JUL 14	ISSUING FOR SUBMISSION	BA
2	20 JUN 15	REV. PER CLIENT COMMENTS	BA
3	20 FEB 16	ISSUED FOR TENDER	BA
4	15 DEC 15	ISSUED FOR BP	BA
5	15 NOV 16	100% BP SET	BA
6	15 NOV 16	50% CS SET	BA
7	15 OCT 17	NEW GRADING FLOOR PLAN	DD
8	15 OCT 17	REVISION	DD
9	15 OCT 17	NEW SITE PLANTING REQUEST	DD
10	15 OCT 17	50% CS SET	BA
11	15 SEP 17	REVISION	BA
12	21 AUG 19	REV. PER CITY/CLIENT COMMENTS	BA
13	15 JUL 19	100% BP SUBMISSION	BA

CLIENT:

PROJECT:

**PARKWAY
MIXED USE DEVELOPMENT**

**1050 PANDORA AVENUE
VICTORIA, BC**

DRAWING TITLE:

**SHRUB
PLAN**

DATE: 19 JUL 10 DRAWING NUMBER:

SCALE: 1:100

DRAWN: BA

DESIGN: BA

CHKD:

L2

OF 9

18240-13.ZIP

PMG PROJECT NUMBER:

18240

FURNISHINGS LEGEND		
KEY	QTY	DESCRIPTION
	4	OFFSITE: SINGLE SEAT CHAIR WITH BACK HERITAGE CHAIR W/ BACK, ALUMINUM FRAME, OAK SLATS GLOSSY BLACK (RAL 9017) PER NEW TOWN GUIDELINES
	2	OFFSITE: TYPE A WOOD BENCH WITH BACKREST HERITAGE BENCH, ALUMINUM FRAME, OAK SLATS GLOSSY BLACK (RAL 9017) PER NEW TOWN GUIDELINES
	7	OFFSITE: BIKE RACK CAPACITY 2 STANDARD BIKE RACK, GLOSSY BLACK (RAL 9017) PER NEW TOWN GUIDELINES
	7	ONSITE: BIKE RACK CAPACITY 2 MAGLIN MBRS00-S, GLOSS BLACK
	9	TREE GRATE W/ FRAME DOBNEY FOUNDRY ST-48
	4	BARKMAN NEWPORT ROUND 38D22H, OFRC PLANTER, COLOR EBONY
	2	LITTER RECEPTACLE



CALAMAGROSTIS ACUTIFLORA
'KARL FOERSTER'



BUXUS M. KOREANA



CAREX OSHIMENSIS



LAVENDULA 'MUNSTEAD'



CALAMAGROSTIS BRACHYTRICHA

ROOF LANDSCAPE LEGEND	
KEY	DESCRIPTION
	SEDUM TILE - ETERA 'COLOR MAX'
	SEDUM TILE - ETERA 'ALL SEASONS'
	SEDUM TILE - ETERA 'BLUE MIX'
	BASALT GRAVEL - 50MM CLEAR 770635 20-30MM
	AUTUMN GOLD PEBBLE - NORTHWEST LANDSCAPE SUPPLY 770635 20-30MM
	MEXICAN SUNBURST PEBBLE - NORTHWEST LANDSCAPE SUPPLY MSB1375 25-75MM
	DAZZLING WHITE PEBBLE - NORTHWEST LANDSCAPE SUPPLY 770745 20-30MM



ACER PALMATUM 'HOGYOKU'



CORNUS K. CHINENSIS



ACER GINNALA

PLANT SCHEDULE				PMG PROJECT NUMBER: 18240
KEY	QTY	BOTANICAL NAME	COMMON NAME	PLANTED SIZE / REMARKS
TREE	11	ACER PALMATUM 'HOGYOKU'	JEWEL JAPANESE MAPLE	3M HT. B&B UPRIGHT FORM
SHRUB	5	CORNUS KOUSA 'CHINENSIS'	CHINESE KOUSA DOGWOOD	3M HT. TREE FORM, B&B
SHRUB	3	STREET TREE	COORDINATE WITH PARKS DEPT	-
B	90	BUXUS MICROPHYLLA KOREANA	KOREAN BOXWOOD	#3 POT, 40CM
K	37	CALAMAGROSTIS ACUTIFLORA 'KARL FOERSTER'	FEATHER REED GRASS	#1 POT
C	216	CAREX OSHIMENSIS 'EVERGOLD'	JAPANESE SEDGE	#1 POT
PERENNIAL	L	LA VANDULA ANGUSTIFOLIA 'MUNSTEAD'	ENGLISH LAVENDER, COMPACT, VIOLET-BLUE	#2 POT
NOTES: * PLANT SIZES IN THIS LIST ARE SPECIFIED ACCORDING TO THE BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD, LATEST EDITION. CONTAINER SIZES SPECIFIED AS PER CNLA STANDARD. BOTH PLANT SIZE AND CONTAINER SIZE ARE THE MINIMUM ACCEPTABLE SIZES. * REFER TO SPECIFICATIONS FOR DEFINED CONTAINER MEASUREMENTS AND OTHER PLANT MATERIAL REQUIREMENTS. * SEARCH AND REVIEW MAKE PLANT MATERIAL AVAILABLE FOR OPTIONAL REVIEW BY LANDSCAPE ARCHITECT AT SOURCE OF SUPPLY. AREA OF SEARCH TO INCLUDE LOWER MAINLAND AND FRASER VALLEY. * SUBSTITUTIONS OBTAIN WRITTEN APPROVAL FROM THE LANDSCAPE ARCHITECT PRIOR TO MAKING ANY SUBSTITUTIONS. TO THE SPECIFIED MATERIAL. UNAPPROVED SUBSTITUTIONS WILL BE REJECTED. ALLOW A MINIMUM OF FIVE DAYS PRIOR TO DELIVERY FOR REQUEST TO SUBSTITUTE. SUBSTITUTIONS ARE SUBJECT TO BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD. DEFINITION OF CONDITIONS OF AVAILABILITY: * ALL LANDSCAPE MATERIAL AND WORKMANSHIP MUST MEET OR EXCEED BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD LATEST EDITION. * ALL PLANT MATERIAL MUST BE PROVIDED FROM CERTIFIED DISEASE FREE NURSERY. * BIO-SOLIDS NOT PERMITTED IN GROWING MEDIUM UNLESS AUTHORIZED BY LANDSCAPE ARCHITECT.				

CONTRACTOR TO PROVIDE DESIGN-BUILD HIGH EFFICIENCY IRRIGATION SYSTEM THAT MEETS CURRENT IABC STANDARDS. DEVELOPER AND/OR LANDSCAPE ARCHITECT TO REVIEW DESIGN PRIOR TO INSTALLATION.



1.8M (6') HT. PERIMETER FENCE AND GATE AT COURTYARD
2.2M (9'-2") HT. FENCE AND GATE AT ENTRANCE TO MEWS



1.07M (42") HT. FENCE AND GATES AT PATIOS



P1 - BARKMAN BROADWAY 24X12"



P2 - EXPOCRETE AQUAROC PERMEABLE PAVER

MATERIALS LEGEND	
KEY	DESCRIPTION
	BARKMAN BROADWAY PAVES 24X12", COLOR GREY
	EXPOCRETE AQUAROC STANDARD SIZE, PERMEABLE PAVES COLOR GREY
	450MM (18") CONCRETE BAND W/ 1.2M SAWCUT GRID PER VICTORIA NEW TOWN GUIDELINES



G1 - 'COLOR MAX'



G2 - 'ALL SEASONS'



G3 - 'BLUE MAX'



G4 - 50MM CLEAR BASALT GRAVEL



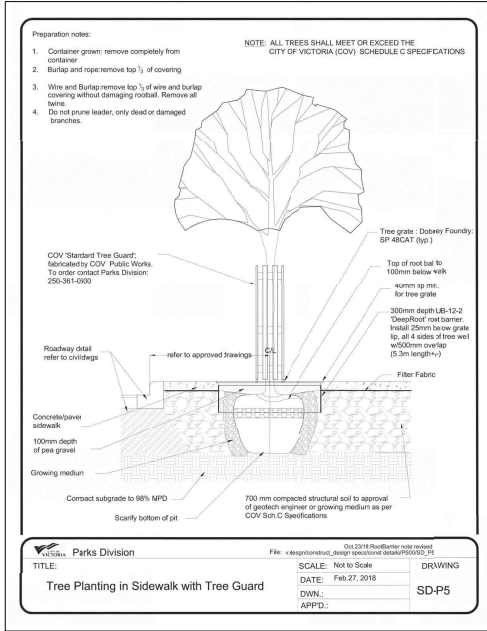
G5 - AUTUMN GOLD PEBBLE



G6 - MEXICAN SUNBURST PEBBLE



G7 - DAZZLING WHITE PEBBLE



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ARCHITECTS
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p. 604 294-0111 f. 604 294-0022

SEAL:

NO.	DATE	REVISION DESCRIPTION	DR.
12	20 JUL 14	REVISION 20 SUBMISSION	BA
13	20 JUN 15	REV PER CLIENT COMMENTS	BA
14	20 FEB 16	ISSUED FOR TENDERS	BA
15	15 DEC 15	ISSUED FOR BP	BA
16	15 DEC 16	ISSUED FOR BP	BA
17	15 NOV 16	50% CD SET	BA
18	15 OCT 17	NEW GRADING FLOOR PLAN	CO
19	15 OCT 17	REVISION	CO
20	15 OCT 17	NEW SITE PLANNING REQUEST	CO
21	15 OCT 17	60% CD SET	BA
22	15 SEP 17	REVISION	BA
23	21 AUG 19	REV PER CITY/CLIENT COMMENTS	BA
24	15 JUL 19	50% BP SUBMISSION	BA

CLIENT:

PROJECT:

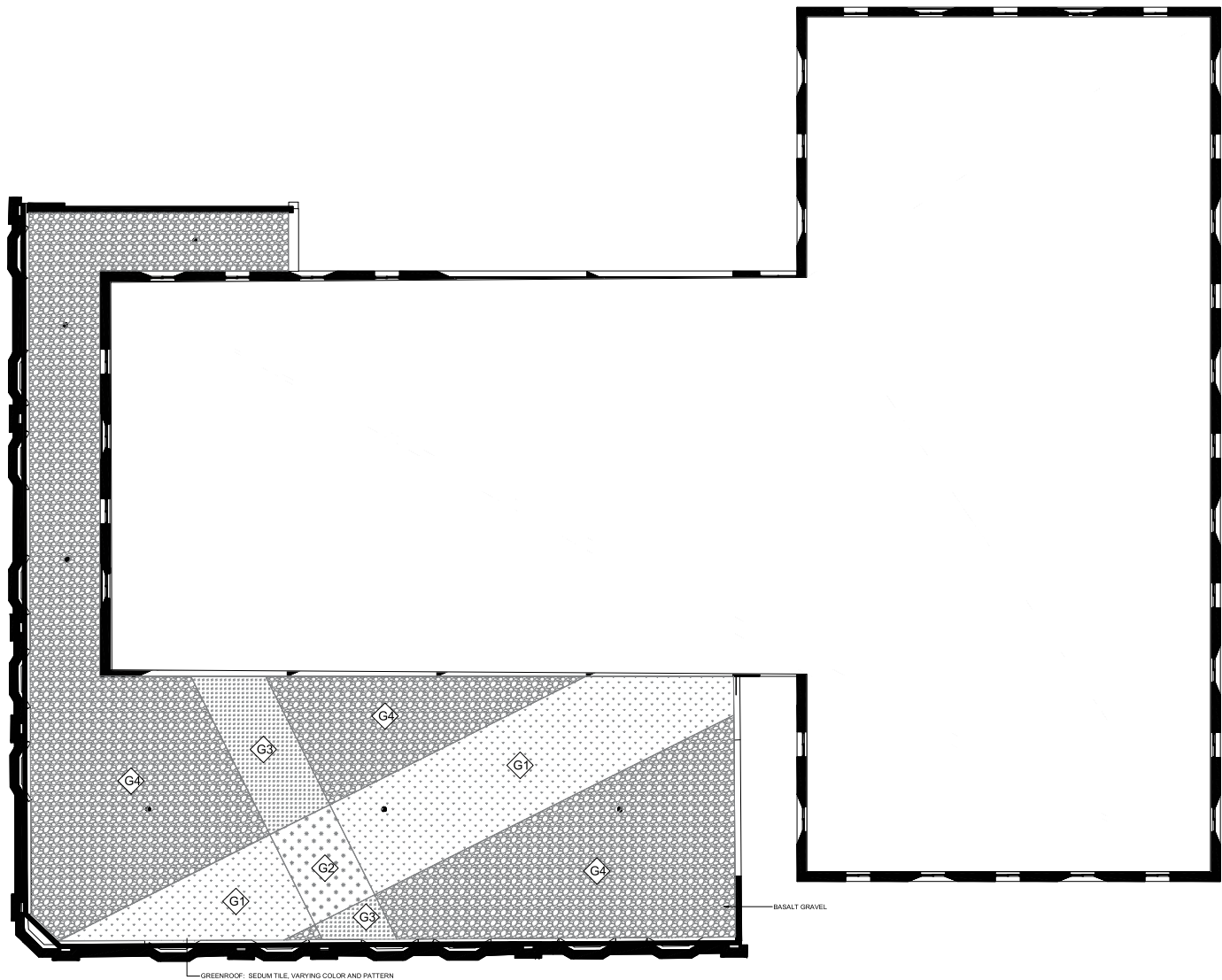
**PARKWAY
MIXED USE DEVELOPMENT**
**1050 PANDORA AVENUE
VICTORIA, BC**

DRAWING TITLE:
**LANDSCAPE
DETAILS**

DATE: 19 JUL 10 DRAWING NUMBER:
SCALE: AS SHOWN
DRAWN: BA
DESIGN: BA
CHKD: OF 9

PMG PROJECT NUMBER: 18240

18240-13.ZIP



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p. 604 294-0011 : f. 604 294-0022

SEAL:

NO.	DATE	REVISION DESCRIPTION	DR.
13	30 JUL 14	FITTING FOR SUBMISSION	BA
12	20 JUN 15	REV. PER CLIENT COMMENTS	BA
11	20 FEB 05	ISSUED FOR TENDERS	BA
10	15 DEC 05	ISSUED FOR BP	BA
9	10 NOV 08	100% BP SET	BA
8	10 NOV 08	50% CD SET	BA
7	10 OCT 11	NEW GRADING FLOOR PLAN	DD
6	10 OCT 11	REWORK	DD
5	10 OCT 11	NEW SITE PLANNING REQUEST	DD
4	10 OCT 11	60% CD SET	BA
3	10 SEP 17	REWORK	BA
2	21 AUG 19	REV. PER CITY/CLIENT COMMENTS	BA
1	10 JUL 19	10% BP SUBMISSION	BA

CLIENT:

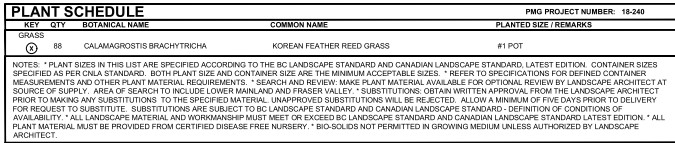
PROJECT:

**PARKWAY
MIXED USE DEVELOPMENT**
**1050 PANDORA AVENUE
VICTORIA, BC**

DRAWING TITLE:
**LEVEL 3
ROOF LANDSCAPE**

DATE: 19 JUL 10 DRAWING NUMBER:
SCALE: 1:100
DRAWN: BA
DESIGN: BA
CHKD: OF 9

SEAL:



NO.	DATE	REVISION DESCRIPTION	DR
13	10.10.14	REWORKING RESUBMISSION	BA
12	20.09.14	REV PER CLIENT COMMENTS	BA
11	20.05.03	ISSUED FOR TENDER	BA
10	18.05.03	ISSUED FOR BP	BA
9	10.NOV.28	90% BP SET	BA
8	10.NOV.04	90% CP SET	BA
7	18.03.23	NEW GROUND FLOOR PLAN	CD
6	18.03.22	REVISION	CD
5	18.03.21	NEW SITE LAYOUT REQUEST	CD
4	18.03.03	60% CP SET	CD
3	15.05.07	REWORKING	BA
1	21.04.10	REV PER CITYCLINT COMMENTS	BA
0	10.01.29	30% BP SUBMISSION	BA

CLIENT:

PROJECT:

PARKWAY

MIXED USE DEVELOPMENT

1050 PANDORA AVENUE
VICTORIA, BC

DRAWING TITLE:

LEVEL 5 ROOF LANDSCAPE

DATE: 19.JUL.10 DRAWING NUMBER:

SCALE: 1:100

DRAWN: 8A

DESIGN: BA

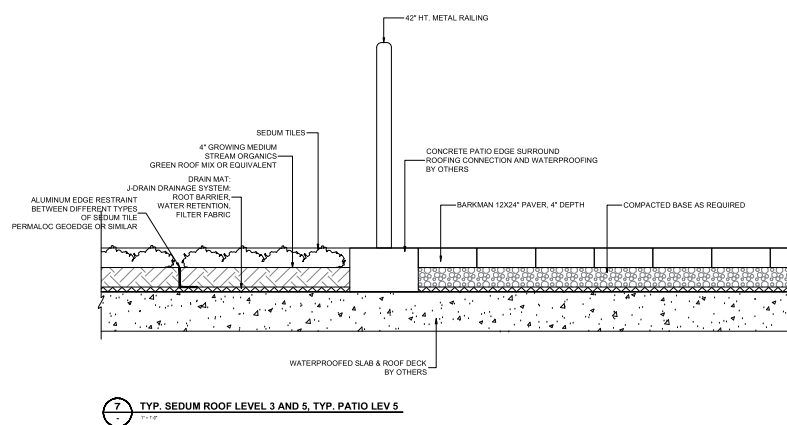
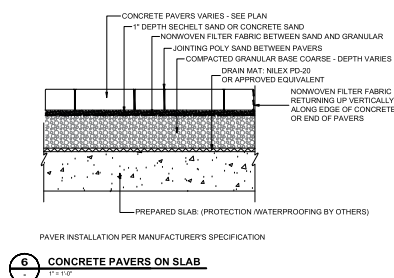
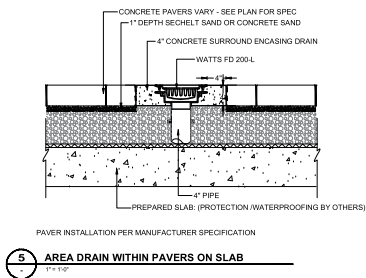
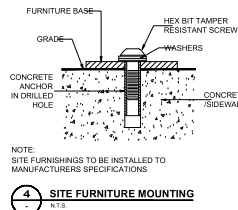
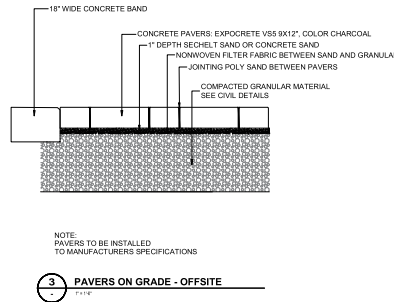
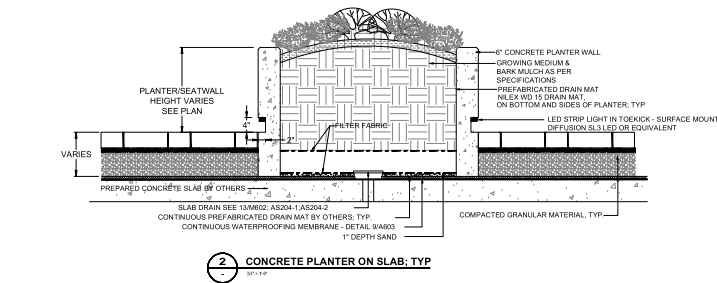
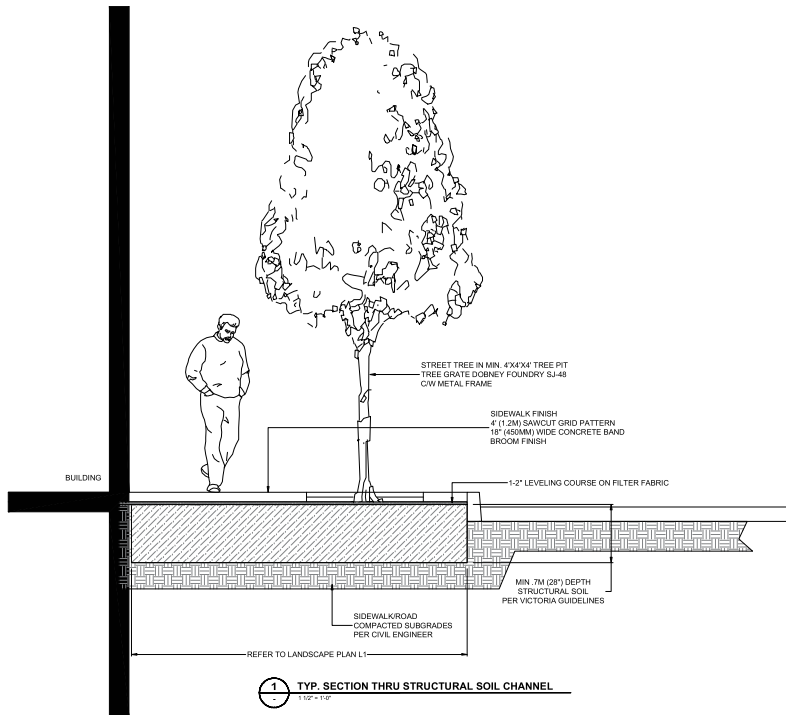
CHKD:

OF 9

18240-13.ZIP

PMG PROJECT NUMBER:

8240



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SEAL:

NO.	DATE	REVISION DESCRIPTION	DR.
12	20 JUL 14	REVISION: SUBMISSION	BA
13	20 JUL 14	REV. PER CLIENT COMMENTS	BA
11	20 FEB 05	ISSUED FOR TENDERS	BA
10	15 DEC 05	ISSUED FOR BP	BA
9	10 NOV 08	ISSUED FOR BP	BA
8	10 NOV 08	50% CD SET	BA
7	10 OCT 03	REVISION	CD
6	10 OCT 03	NEW GROUND FLOOR PLAN	CD
5	10 OCT 03	NEW SITE PLANNING REQUEST	CD
4	10 OCT 03	50% CD SET	BA
3	10 SEP 07	REVISION	BA
2	21 AUG 19	REV. PER CIVIL COMMENTS	BA
1	10 JUL 29	50% BP SUBMISSION	BA

NO. DATE REVISION DESCRIPTION DR.

CLIENT:

PROJECT:

**PARKWAY
MIXED USE DEVELOPMENT**
1050 PANDORA AVENUE
VICTORIA, BC

DRAWING TITLE:
**LANDSCAPE
DETAILS AND SECTIONS**

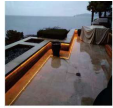
DATE: 19 JUL 10 DRAWING NUMBER:
SCALE: AS SHOWN
DRAWN: BA
DESIGN: BA
CHKD: **L6**
OF 9

LIGHTING LEGEND

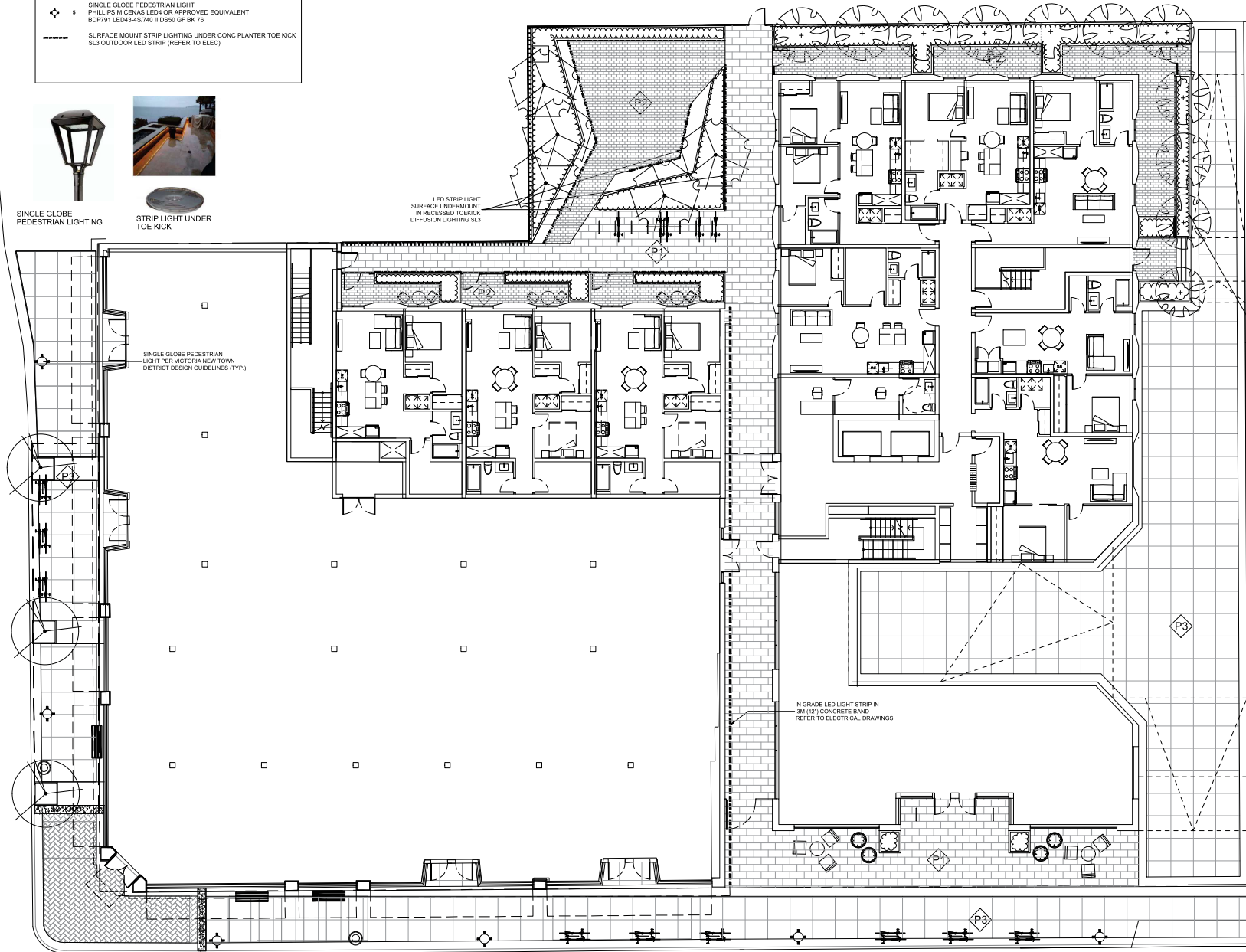
KEY	DESCRIPTION
5	SINGLE GLOBE PEDESTRIAN LIGHT PHILLIPS MICENAS LED4 OR APPROVED EQUIVALENT B0P791 LED43-45/140 II D350 GF BK 76
	SURFACE MOUNT STRIP LIGHTING UNDER CONC PLANTER TOE KICK SL3 OUTDOOR LED STRIP (REFER TO ELEC)



SINGLE GLOBE
PEDESTRIAN LIGHTING



STRIP LIGHT UNDER
TOE KICK



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SEAL:

12	20 JUL 14	REVISION SUBMISSION	BA
13	20 JUN 20	REV PER CLIENT COMMENTS	BA
14	20 FEB 20	ISSUED FOR TENDERS	BA
15	15 DEC 05	ISSUED FOR BP	BA
16	10 NOV 08	100% BP SET	BA
17	10 NOV 08	50% CS SET	BA
18	10 OCT 21	NEW GRADING FLOOR PLAN	DD
19	10 OCT 21	REVISION	DD
20	10 OCT 21	NEW SITE PLANNING REQUEST	DD
21	10 OCT 21	60% CS SET	BA
22	10 SEP 21	REVISION	BA
23	21 AUG 19	REV PER CITY/CLIENT COMMENTS	BA
24	10 JUL 20	100% BP SUBMISSION	BA

NO. DATE REVISION DESCRIPTION DR.

CLIENT:

PROJECT:

**PARKWAY
MIXED USE DEVELOPMENT**

**1050 PANDORA AVENUE
VICTORIA, BC**

DRAWING TITLE:
**LIGHTING
LAYOUT PLAN**

DATE: 19 JUL 10 DRAWING NUMBER:

SCALE: 1:100

DRAWN: BA

DESIGN: BA

CHKD:

L7

OF 9

18240-13.ZIP

PMG PROJECT NUMBER:

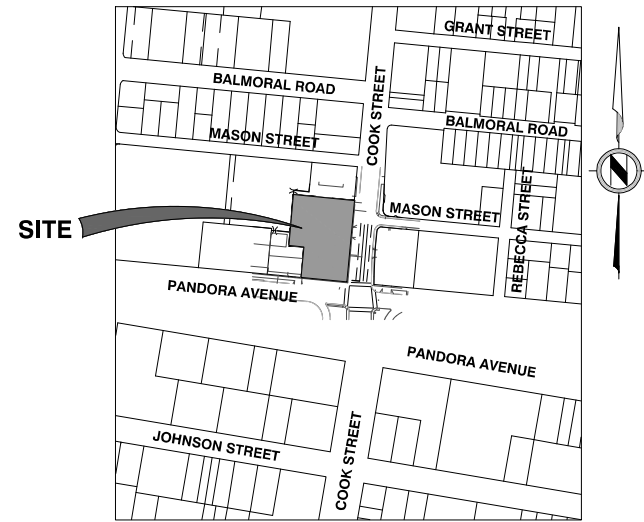
18240



APLIN MARTIN

ENGINEERING ARCHITECTURE PLANNING SURVEYING

Aplin & Martin Consultants Ltd.
#104 - 6596 Applercross Road, Nanaimo, BC, Canada V9V 0A4
Tel: (778) 841-0484, Fax: (604) 597-9061, Email: general@aplinmartin.com



SITE LOCATION PLAN

SCALE 1:2000

DRAWING INDEX

18-010-01	COVER SHEET
18-010-02	KEY PLAN & GENERAL NOTES
18-010-03	SERVICING & GRADING PLAN
18-010-04	TRUCK TURNING PLAN

CLIENT:

DISTRICT GROUP

SUITE 200 - 8809 HEATHER STREET, VANCOUVER, BC V6P 3T1
PH. 604-322-5762

NOT FOR CONSTRUCTION

PROJECT:

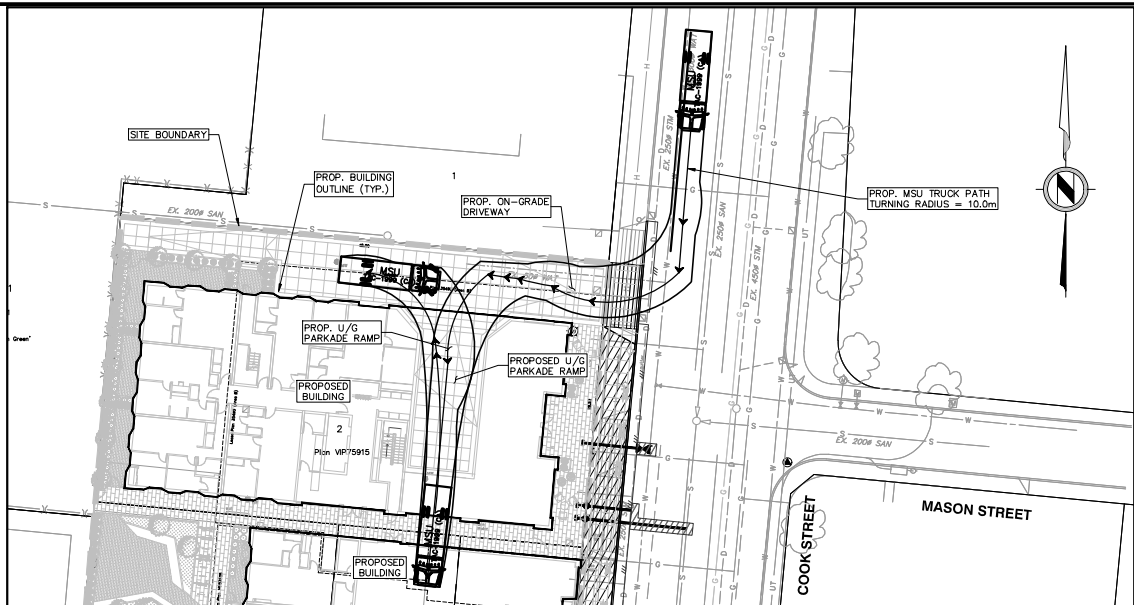
PARKWAY - MIXED-USE DEVELOPMENT

1050 PANDORA AVENUE & 1518 COOK STREET, VICTORIA, BC

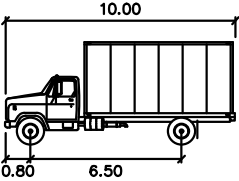
MUNICIPAL PROJECT No. XXX

APLIN & MARTIN PROJECT No. 18-010

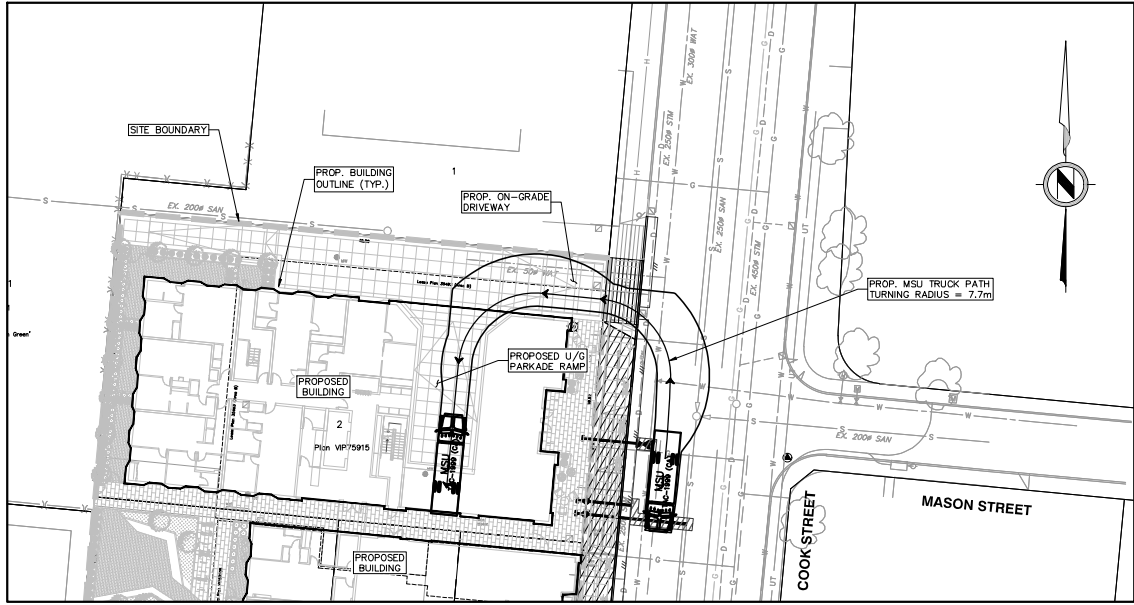
DATE	TIME	LOCATION	OFFSITE	PRODUCTION	DWPS 18-010	02 KEY PLAN	GENERAL NOTES	GENERAL NOTES
14	2020	2:27pm	M:\2018\18-010\TMC\					



TURN ROUTE 1 - DIRECT ENTRY



MSU	meters
Width	: 2.60
Track	: 2.60
Lock to Lock Time	: 6.0
Steering Angle	: 40.2



TURN ROUTE 1 - BACK-IN ENTRY

NOT FOR CONSTRUCTION

NOTICE TO CONTRACTOR

IT IS THE RESPONSIBILITY OF THE CONTRACTOR'S SURVEYOR TO VERIFY THAT ALL LEGAL SURVEY DIMENSIONS SHOWN ON THE ENGINEER'S DRAWINGS AGREE WITH THOSE ON THE REGISTERED LEGAL SURVEY PLAN. SHOULD THERE BE ANY DISCREPANCIES, THEN IMMEDIATELY NOTIFY THE ENGINEER OF RECORD.

LEGAL DESCRIPTION: SITE PLAN OF AMENDED LOT 14 (DO 106561), LOTS 15 & 16, LOT 2, PLAN VP75915 OF SUBURBAN LOT 15				
B.M. MONUMENT NO. 16-64A ELEVATION: 27.355m				
LOCATED AT COOK STREET & PANDORA AVENUE				
REV. NO.	DESCRIPTION	DR	CH	DATE
04	ISSUED FOR 60% BP SUBMISSION	VG	SL	15-OCT-19
05	ISSUED FOR 90% BP SUBMISSION	VG	SL	06-NOV-19
06	ISSUED FOR 100% BP SUBMISSION	VG	SL	25-NOV-19
07	TREE REMOVED ON COOK STREET	CL	SL	20-MAR-20
08	REVISED FOR REZONING	JP	SL	15-JUL-20

APLIN MARTIN
ENGINEERING ARCHITECTURE PLANNING SURVEYING

Aplin & Martin Consultants Ltd.
#104 • 6596 Applecross Road, Nanaimo, BC, Canada V9W 0A4
Tel (779) 941-0484, Fax (804) 597-9561, Email: general@aplinmartin.com

CLIENT: **DISTRICT GROUP**
SUITE 200 • 8809 HEATHER STREET, VANCOUVER, BC V6P 3T1
PH: 604-322-5762

PROJECT: **PARKWAY - MIXED USE DEVELOPMENT**
1050 PANDORA AVENUE & 1518 COOK STREET, VICTORIA BC



The location of existing underground utilities are shown on the drawings as indicated. They have not been independently verified by the owner or its representative. The contractor shall determine the exact location of all existing utilities before commencing work, and agree to be fully responsible for any and all damages which might be occasioned by the contractor's failure to locate and preserve any and all underground utilities.

TITLE: TRUCK TURNING PLAN		DESIGN: VG CHECK: SL DRAWN: VG/CL APPR: SL	
PROJECT NO. .		A & M FILE: 18-010	
DRAWING NO. .		DRAWING DATE: FEBRUARY 2019	
SCALE: .		SHEET NO. 04 OF 04	
HORIZ. 1:250		REV. 08	
VERT. N/A			
A & M DRAWING NO. 18-010-04			



July 21, 2020

City of Victoria
1 Centennial Square
via email

Re: Parkway Apartments Development - 1050 Pandora Avenue & 1518 Cook Street

PID#s 003-785-565 & 025-763-601

Legal Descriptions:

Lot 16, Suburban Lot 15, Victoria City
Lot 2, Suburban Lot 15, Victoria City, Plan VIP75915

To Mr. John O'Reilly,

This letter is written as rationale to support the revised and submitted development drawings for the project referenced above.

Since the project's initial presentation to Committee of the Whole, the design development process was advanced into the working drawing stage. Throughout this process several key details were identified as technically unfeasible, or presented solutions that increased project cost to a point where the project became financially unviable.

As presented by the team at Michael Green Architecture, the proposed changes included:

- 1) Revised Exterior Cladding System – The initially specified Ultra High Performance Concrete Panel presented some technical challenges relating to the attachment system to a wood frame structure. The primary concern with this product selection was the shrinkage factor of a wood frame structure in combination with a very heavy and rigid concrete panel system. Working in conjunction with local contractors to develop the proposed system led to significant challenges that deemed the system as not viable for this particular application. As an alternative a stucco rainscreen system is being proposed in a similar texture and colour.
- 2) Revisions to Fenestration – The revisions to the doors and windows are also a technical challenge. The initially conceptualized vinyl windows and sliding glass doors did not meet code compliant testing standards and the cost to upgrade the window and door package to an aluminum system proved to be too costly for the project to be viable. To resolve this, the design team introduced a horizontal transom across all of the upper level windows so that testing standards can be achieved, and there is no reduction in the overall percentage of

glazing initially presented. The proportion and size of this transom directly relates to the proportion of the storefront transom windows in the existing building.

- 3) The residential units on level 6, facing north, were revised to eliminate the large corner windows and balconies. This revision was contemplated to simplify the building structure in a cost saving effort, and to better utilize floor area. The result of this change generated an additional residential rental unit while maintaining the characteristics of the south end of the 6 storey building mass.
- 4) Relocation of the Garbage Room to the Parkade – The garbage room relocated to the parkade in an effort to make access more convenient for use by the commercial tenants and the patio suites. The relocation of this room also facilitated the addition of 1 residential rental unit on the ground floor.
- 5) Relocation of the Transformer Room – This revision was undertaken after further consultation with BC Hydro dictated that the primary hydro service will be fed from Pandora Avenue.
- 6) Landscaping – Several changes have been proposed to the landscaping in an effort to find cost savings for the project. The most significant of these changes is to change the level 5 sedum roof to a multi-colored gravel roof around the patio area. This change offers some modest savings for the project and simplifies ongoing maintenance for the building operator while still maintaining the roof top amenity space for tenants with overlook to the Franklin Green Park.

Other landscape revisions included substituting the cast in place retaining wall planters for pre-cast concrete planters, and the substitution of interlocking pavers for stamped concrete surfacing that maintains a very similar appearance.

In summary of the changes I would like to highlight that the efforts undertaken to develop these changes were done so with careful consideration and with respect to the initial design intent of the proposal while still developing more sensible detailing to make this a technically and financially viable project. We thank you for your consideration of the revised proposal.

Respectfully,



Christopher M. Supeene
Owner's Agent / Project Manager



PACIFIC COVE
PROPERTY MANAGEMENT LTD.

December 12, 2019

Dear Mayor & Council:

The attached letter was provided to the residents of 1054 Pandora on December 10, 2019.

In summary, all four remaining tenants are being provided with the following:

- Three to six months' rent, depending on the length of their tenancy as dictated by the Tenant Assistance Policy. Rent is calculated based on the greater of current rent or CMHC average for the unit size;
- An additional two months' rent;
- A moving allowance of \$500-\$750, depending on the size of the unit; and
- The services of a relocation consultant, who to date has shared more than 65 comparable Victoria listings.

In addition, upon signing the Mutual Agreement to End Tenancy these tenants will receive:

- A full refund of their security deposits, plus interest where applicable; and
- A return of their rent from the date of the incident (November 26, 2019) onwards.

Under Section 44 (1)(e) of the Residential Tenancy Act, the tenancies ended on November 26, 2019. The tenants have been provided hotel accommodation until December 31, 2019, 35 days beyond that which is required under the Residential Tenancy Act.

Regards,

Lee Rennison
Pacific Cove Property Management Ltd.
604-230-7116



PACIFIC COVE
PROPERTY MANAGEMENT LTD.

December 10, 2019

WITHOUT PREJUDICE

To the Residents of 1054 Pandora Street:

Thank you for bearing with us as we have worked to diagnose the issue at the building at 1054 Pandora.

After extensive work on the site, together with our consultants, have determined that the building at 1054 Pandora will not be habitable again. The disintegration and collapse of the sewer line more than 3 metres below ground, would require in excess of six months to replace or repair. We apologize for the inconvenience and disruption this has caused.

We understand that most of you have made use of the hotel and/or other accommodation choices we have provided since late November. We are happy to continue to provide hotel or other accommodation up to December 31, 2019. We hope that by this time, many of you will have had success in your search for alternate accommodation. We also hope that you have made contact with your insurers to discuss your respective situations.

Based on the date of the incident, and in accordance with Section 44 (1)(e) of the Residential Tenancy Act, your tenancies ended on November 26, 2019.

Thank you to those of you that have made use of the services of Lee Murphy, our relocation consultant, and to those tenants who have not yet returned Lee's correspondence we encourage you to make use of his services. Lee will continue to provide you with listings for alternate permanent accommodation and is available seven days a week to help you as you search for your next home. Lee will be able to provide you with a Mutual Agreement to End Tenancy. This document acknowledges the end of your tenancy and allows us to release your security deposit and pro-rated rent, from November 26, the date of the incident. You will note that we did not cash your December rent cheques.

The compensation that is in accordance with the City of Victoria's Tenant Assistance policy plus the additional monetary compensation discussed during your meetings with Lee Murphy is still to be provided to you. As you know, the compensation you have been offered is well in excess of the compensation required by the City of Victoria Tenant Assistance Policy. The monetary compensation includes a lump sum payment of:

- Three to six months' rent depending on the length of your tenancy as dictated by the Tenant Assistance Policy;

- An additional two months' rent;
- A moving allowance of \$500-\$750, depending on the size of your unit; and
- A full refund of your security deposit, plus interest where applicable.

Please contact Lee Murphy if you wish to sign a Mutual Agreement to End Tenancy and receive this compensation. Lee can be reached at 604-313-3412.

We will have the building open until February 1, 2020 for you to retrieve your belongings, after which time, the building will be boarded up pending development.

We apologize again for the inconvenience this has caused, and we thank you for your cooperation during these stressful and unforeseen circumstances.

Regards,
Kevin Kuypers
Pacific Cove Property Management Ltd.
236-882-2512



PACIFIC COVE
PROPERTY MANAGEMENT LTD.

February 20, 2020

Dear Mayor & Council:

This letter is intended to follow up on our letter dated December 12, 2019.

As of December 12th, there were four remaining tenants at the building at 1054 Pandora. Since then, three of the four remaining tenants have agreed to vacate. Each of them received the following:

- Twelve months' rent, calculated based on the greater of current rent or CMHC average for the unit size;
- A moving allowance of \$1000;
- A full refund of their security deposits, plus interest where applicable;
- A return of their rent from the date of the incident (November 26, 2019) onwards; and
- The services of a relocation consultant.

The building has been declared uninhabitable by the Victoria Fire Department and has been vacated and secured. There is one remaining tenant, who no longer lives at the building, but from whom we have not reached an agreement to end tenancy. This tenant has been offered, through his lawyer, the same compensation outlined above. He did not respond to our offer (attached).

He has since been provided a cheque for the following, based on the City of Victoria Tenant Assistance Policy:

- Six months' rent, calculated based on the greater of current rent or CMHC average for the unit size;
- A moving allowance of \$750;
- A full refund of his security deposit, plus interest;
- A return of his rent from the date of the incident (November 26, 2019); and
- The services of a relocation consultant.

We have an RTB hearing scheduled with this tenant on April 3, 2020. At the hearing, we intend to ask the arbitrator to declare his tenancy ended.

Regards,

Lee Rennison
Pacific Cove Property Management Ltd.
604-230-7116



PACIFIC COVE
PROPERTY MANAGEMENT LTD.

July 20, 2020

Dear Mayor & Council:

This letter is intended to follow up on our letter dated February 20, 2020.

As of the February 20, 2020 letter, with the exception of one tenant, each tenant in the building at 1054 Pandora Street had been successfully relocated and compensated on financial terms equal to or higher than the Victoria Tenant Assistance Policy requirements.

As of today's date, the building has been vacated. The one final tenant has received and cashed a cheque for assistance in accordance with Victoria's Tenant Assistance Policy Requirements, as follows:

- Six months' rent, calculated based on the greater of current rent or CMHC average for the unit size;
- A moving allowance of \$750;
- A full refund of his security deposit, plus interest;
- A return of his rent from the date of the incident (November 26, 2019).

After a Residential Tenancy Branch hearing on June 1, 2020, the arbitrator ruled that the Landlord was already in possession of the rental property, and therefore declined to issue an order of possession. We consider the matter closed.

With respect to the commercial tenant in the building, Wellburns Market vacated the building on or around November 29, 2019. The lease for this tenant subsequently expired.

Regards,

Lee Rennison
Pacific Cove Property Management Ltd.
604-230-7116



Committee of the Whole Report

For the Meeting of December 12, 2019

To: Committee of the Whole **Date:** November 28, 2019

From: Andrea Hudson, Acting Director, Sustainable Planning and Community Development

Subject: Rezoning Application No. 00695 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street (Wellburn's Site)

RECOMMENDATION

That Council instruct staff to prepare the necessary Zoning Regulation Bylaw Amendment that would authorize the proposed development outlined in Rezoning Application No. 00695 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street, that first and second reading of the Zoning Regulation Bylaw Amendment be considered by Council and a Public Hearing date be set once the following conditions are met:

1. Preparation and execution of the appropriate legal agreements in order to secure the following:
 - a. all dwelling units remain rental in perpetuity;
 - b. a Statutory Right-of-Way of 3.083m along Cook Street, to the satisfaction of the Director of Engineering and Public Works;
 - c. pay for the construction of an asphalt pathway in Franklin Green Park to connect the proposed 3m wide pathway at the northwest corner of the site to an existing park pathway to the west to the satisfaction of the Director of Parks, Recreation and Facilities;
 - d. provide a bus shelter on Cook Street in accordance with the site plan dated November 22, 2019, and to the satisfaction of the Director of Engineering and Public Works and the Director of Sustainable Planning and Community Development;
 - e. provide two car share vehicles, two assigned car share parking spaces on-site, 118 car share memberships, a \$100 of driving credits for each resident, and 40 electric bicycle charging stations (110V wall outlets) in the underground parkade, to the satisfaction of the Director of Engineering and Public Works; and
 - f. purchase 100 transit passes through BC Transit's EcoPass program to the satisfaction of the Director of Engineering and Public Works.
2. Council authorizing street-level projecting canopies and cornices over the City Right-of-Way on 1050-1058 Pandora Avenue and 1508-1518 Cook Street, provided that the applicant enters into an Encroachment Agreement in a form satisfactory to the City Solicitor and the Director of Engineering and Public Works.

LEGISLATIVE AUTHORITY

In accordance with Section 479 of the *Local Government Act*, Council may regulate within a zone the use of land, buildings and other structures, the density of the use of the land, building and other structures, the siting, size and dimensions of buildings and other structures as well as the uses that are permitted on the land and the location of uses on the land and within buildings and other structures.

In accordance with Section 482 of the *Local Government Act*, a zoning bylaw may establish different density regulations for a zone, one generally applicable for the zone and the others to apply if certain conditions are met.

In accordance with Section 483 of the *Local Government Act*, Council may enter into a Housing Agreement which may include terms agreed to by the owner regarding the occupancy of the housing units and provided such agreement does not vary the use of the density of the land from that permitted under the zoning bylaw.

EXECUTIVE SUMMARY

The purpose of this report is to present Council with information, analysis and recommendations for a Rezoning Application for the property located at 1050-1058 Pandora Avenue and 1508-1518 Cook Street. The proposal is to rezone from the CA-1 Zone, Pandora Avenue Special Commercial District, and R-2 Zone, Two-Family Dwelling District, to a new zone in order to increase the density and construct a new four- and six-storey mixed-use addition consisting of ground floor commercial uses and residential above.

The following points were considered in assessing this application:

- The *Official Community Plan* (OCP, 2012) Urban Place Designation for the subject properties is Core Residential, which supports mixed-use buildings from three up to approximately 20 storeys and a density of up to approximately 5.5:1 floor space ratio (FSR). The proposal complies with the land use policies outlined in the OCP.
- The applicant is proposing approximately 103 rental units and a mix of unit types, including ground-oriented and some larger two-bedroom dwelling units.
- The applicant is proposing to retain, rehabilitate and heritage-designate approximately 50% of the existing building (Wellburn's building).
- The Downtown Core Area Plan (DCAP) designates the subject properties within the Residential Mixed-Use District, which supports mixed-use buildings with active commercial and retail uses along Pandora Avenue. The Plan supports a density up to 5.5:1 FSR and heights up to ten storeys for the properties located at 1050-1058 Pandora Avenue and 1508 Cook Street, and a density up to 2:1 FSR and heights up to six storeys for the property located at 1518 Cook Street. The applicant is proposing a maximum height of six storeys and an overall density of 2.92:1 FSR. The proposal complies with the land use policies outlined in DCAP.
- The DCAP supports new development that conserves and enhances the form, character and features of heritage buildings and the proposal complies with this policy direction.

BACKGROUND

Description of Proposal

This Rezoning Application is to rezone from the CA-1 Zone, Pandora Avenue Special Commercial District, and R-2 Zone, Two-Family Dwelling District, in order to increase the overall density to 2.92:1 floor space ratio (FSR), retain 50% of the existing heritage-registered building and construct a new four- and six-storey mixed-use addition consisting of ground floor

commercial uses and residential above. The applicant is proposing approximately 103 rental dwelling units and 1054m² of commercial floor area, which would accommodate ground floor uses such as restaurants, retail, offices, financial institutions and personal services.

The following differences from the standard zones are being proposed and would be accommodated in the new zone:

- increasing the height and density
- reducing the required number of parking spaces.

Affordable Housing Impacts

The applicant proposes the creation of 103 new rental residential units which would increase the overall supply of housing in the area. A Housing Agreement is also being proposed which would ensure that the dwelling units remain rental in perpetuity.

Tenant Assistance Policy

The proposal is to redevelop an existing building which would result in a loss of 11 existing residential rental units. Consistent with the Tenant Assistance Policy, the applicant has provided a Tenant Assistance Plan which is attached to this report.

Sustainability Features

The following sustainability features are associated with this application:

- Step 1 of the BC Energy Step Code and reduce energy use by at least 10% compared to ASHRAE 90.1 2007
- tapered overhangs of balconies to prevent summer solar gain and maximize winter solar exposure
- light wood framing as primary structural material
- 50% retention of the existing building for its exterior assemblies, structural systems and finishes where appropriate
- permeable surface treatment
- addition of 13 new trees on-site (there are currently no trees on-site).

Active Transportation Impacts

The applicant is proposing 134 long-term and 18 short-term bicycle parking spaces. Forty long-term bicycle parking spaces would be designed for cargo and electric bikes.

Public Realm Improvements

The applicant is willing to pay for the construction of an asphalt pathway in Franklin Green Park in order to connect the proposed 3m wide pathway at the northwest corner of the site to an existing park pathway to the west to the satisfaction of the Director of Parks, Recreation and Facilities.

Accessibility Impact Statement

The British Columbia Building Code regulates accessibility as it pertains to buildings. The proposed ground level dwelling units, courtyard and the pathways surrounding the proposed buildings are designed to be accessible.

Land Use Context

The area is characterized by a mix of residential and commercial uses. The subject properties also adjoin Franklin Green Park.

Existing Site Development and Development Potential

The site is presently a two-storey, mixed-use building with ground floor commercial uses and residential above, and a surface parking lot. Under the current CA-1 Zone, the property could be developed into an approximately five-storey, mixed-use building with ground floor commercial and residential uses at a density of 2:1 FSR. The existing surface parking lot is currently zoned R-2 and the property could be developed as a duplex.

Data Table

The following data table compares the proposal with the existing CA-1 Zone, Pandora Avenue Special Commercial District. An asterisk is used to identify where the proposal is less stringent than the existing Zone.

Zoning Criteria	Proposal	Zone Standard (CA-1 Zone)
Site area (m ²) – minimum	2879	n/a
Density (Floor Space Ratio) – maximum	2.92:1 *	2:1
Total floor area (m ²) – maximum	8412.30 *	5758
Height (m) – maximum	20.22 *	15.50
Storeys – maximum	6	n/a
Site coverage (%) – maximum	70	n/a
Open site space (%) – minimum	22	n/a
Setbacks (m) – minimum		
Street setback (Pandora Avenue – south)	0 * (existing & Levels 1-2) 3.97 (proposed Levels 3-6)	3
Street setback (Cook Street – east)	0 * (existing & Levels 1-2) 3.08 (proposed Levels 3-4) 13.13 (proposed Levels 5-6)	3
Side (North)	6.18 (proposed Levels 1-4) 7.60 (proposed Levels 5-6)	0 or 3
Side (West)	0 * (existing & Levels 1-2) 2.96 * (proposed Levels 3-4) 3.33 (proposed Levels 5-6)	0 or 3

Zoning Criteria	Proposal	Zone Standard (CA-1 Zone)
Vehicle parking – minimum		
Residential	33 *	66
Commercial	5 *	17
Visitor	3 *	10
Bicycle parking stalls – minimum		
Residential – long-term	128	127
Residential – short-term	10	10
Commercial – long-term	6	6
Commercial – short-term	8	8

Community Consultation

Consistent with the *Community Association Land Use Committee (CALUC) Procedures for Processing Rezoning and Variance Applications*, the applicant has consulted the North Park CALUC at a Community Meeting held on March 20, 2019. The meeting minutes dated March 20, 2019 are attached to this report.

ANALYSIS

Official Community Plan

Land Use, Height and Density

The *Official Community Plan* (OCP, 2012) Urban Place Designation for the subject properties is Core Residential, which supports mixed-use buildings from three up to approximately 20 storeys and a density of up to approximately 5.5:1 FSR. The applicant is proposing a six-storey, mixed-use building with an overall density of 2.92 FSR, which is consistent with the policy direction. The OCP envisions higher density mixed-use development on Pandora Avenue and encourages new population and housing growth within walking distance of North Park Village and within portions of the neighbourhood designated Core Residential.

Built Form

The OCP encourages high quality architecture, landscape and urban design to enhance the visual identity and appearance of the city and supports new infill and building additions that respond to context through sensitive and innovative design. The OCP also supports new additions that conserve and enhance heritage properties in such a way that avoids demolition.

The applicant is proposing to retain, rehabilitate and heritage-designate approximately 50% of the existing building, which also includes the entire historical façade facing Pandora Avenue and Cook Street and a portion for the north side wall. The rehabilitation efforts being proposed by the applicant will ensure that the Wellburn's building remains a prominent architectural feature in the development. A new four- and six-storey volume is being proposed that is designed to be modern yet subordinate to the heritage building. The applicant feels that a maximum height of six storeys is an appropriate scale to ensure that the new addition responds well to the heritage context and Franklin Green Park, even though the OCP and DCAP policies support more height and density at this location.

The proposed T-shape building allows for a large west-facing courtyard to be shared and activated by the many residents in the building and commercial patrons during business hours. A mews/breezeway is also being proposed, which would separate the historic and modern buildings at street level and provide access to the interior courtyard and Franklin Green Park. Access to the park would also be provided on the north side of the building.

The OCP encourages active land uses adjacent to parks where appropriate, for animation, surveillance and to encourage park use. The applicant is proposing ground floor units that face the park, but where the public and private space is clearly defined through fencing and soft landscaping. There are upper storey dwelling units with Juliet balconies that overlook the park and provide additional surveillance in the area.

In the Urban Core, animating the pedestrian realm is strongly encouraged through urban design considerations such as sidewalk cafes, street furniture, etc. The applicant is proposing to activate the ground floor of the existing heritage building by reinstating recessed commercial entryways and storefront windows to match the original building design. A ground-level coffee shop with outdoor seating is being proposed in the new four-storey addition to foster neighbourhood vibrancy and strengthen the human-scale design of the proposed building.

Housing Type and Tenure

The OCP supports a range of housing types, forms and tenures across the City and within neighbourhoods to meet the needs of residents at different life stages, and to facilitate ageing in place. The OCP also encourages a mix of residents, including households with children, by increasing opportunities for innovative forms of ground-oriented multi-residential housing. The applicant is proposing to provide 103 rental dwelling units in perpetuity, as well as a mix of unit types, including some ground-oriented units and some larger two-bedroom units. At this time, the proposal contains the following unit mix:

Unit Type	Number of Units
Studio	4
Studio + Den	7
One Bedroom	53
One Bedroom + Den	24
Two Bedroom	11
Two Bedroom + Den	4

Downtown Core Area Plan

The Downtown Core Area Plan (DCAP) designates the subject properties within the Residential Mixed-Use District, which supports mixed-use buildings with active commercial and retail uses along Pandora Avenue. The Plan supports a density up to 5.5:1 FSR and heights up to ten storeys for the properties located at 1050-1058 Pandora Avenue and 1508 Cook Street, and a height up to six storeys and a density up to 2:1 FSR for the property located at 1518 Cook Street. The DCAP also supports new development that conserves and enhances the form, character and features of heritage buildings. The applicant is proposing to preserve all the character-defining elements of the existing heritage building. The additions would also incorporate some architectural features that are visible in the heritage building, such as a running bond pattern of the glazed white brick, the rhythm, proportion and angles of the projecting oriel windows, and the recessed entryways on the existing storefronts. The proposal complies with the policy direction outlined in DCAP.

Tree Preservation Bylaw and Urban Forest Master Plan

There are no existing trees within the proposed development area or on the street frontages. A total of nine new street trees in grates within the sidewalk are proposed (three on Pandora Avenue and six on Cook Street). The street tree species will be determined by Parks at the building permit stage. Tree training and maintenance will require careful attention by the City to raise the tree crowns above the fabric roller shades, maintain clearance from buildings, provide space for buses on Cook Street, and maintain offsets from overhead electrical wires.

The proposed pathway connection from Cook Street to Franklin Green Park will necessitate park upgrades to ensure accessible access from the development into the park. Currently, there is a new community garden bed where the path is planned to enter Franklin Green.

Density Bonus Policy

The proposal is exempt from the current Inclusionary Housing and Amenity Contribution Policy as the applicant would be providing 100% purpose-built rental housing for the life of the building.

Regulatory Considerations

Statutory Right-of-Way

A Statutory Right-of-Way (SRW) of 4.94m on Cook Street would be recommended as a condition of rezoning to enhance facilities for walking, cycling, public transit and boulevards. Given that the existing Wellburn's building will be heritage-designated and remain in situ, setting back the new addition by 4.94m would interrupt a continuous building frontage, reduce the amount of sunlight exposure on the north side of the Wellburn's building and expand the amount of blank, north-facing wall. For these reasons, the applicant is proposing an SRW of 3.083m along Cook Street. A reduction in the amount of SRW would likely result in no on-street parking along the Cook Street frontage in the future. There is currently no on-street parking fronting the subject properties on Cook Street.

Proposed New Zone

The new zone would include a provision to allow for the creation of one or more air space parcels, which are essentially volumetric parcels. Air space parcel subdivisions are a common approach, especially for mixed-use/mixed ownership buildings to allow for the transfer of title of different components of a development.

Reduction in Vehicle Parking

The applicant is proposing 43 parking spaces for the development (41 parking spaces allocated to residents and uses within the building, and two car share parking spaces). Schedule C: Off-street Parking requires 94 parking spaces, so there would be a parking shortfall of 51 parking spaces. There are challenges with providing underground parking due to the structural integrity of the heritage building and inability to excavate underneath the existing building.

Given the magnitude of the parking shortfall, the applicant engaged WATT Consulting Group to complete a Parking Study for this development. To help offset some of the parking shortfall, the applicant is proposing a variety of Transportation Demand Management (TDM) measures which staff support, including the following:

- purchasing two car share vehicles with assigned parking spaces on-site
- 118 car share memberships (one car share membership per dwelling unit) for the life of the building along with usage credits for each membership
- 100 transit passes
- 40 cargo and electric charging bicycle parking spaces.

Staff recommends that a covenant is registered on title to secure the TDM measures outlined above to the satisfaction of the Director of Engineering and Public Works.

The applicant has also integrated a new bus shelter into the architecture of the building on Cook Street, which is considered a frequent transit route. Other considerations for a parking variance include the site's WalkScore of 97 – "Walker's Paradise: Daily errands do not need a car." The site also abuts the Pandora separated bike lane and is in close proximity to planned bicycle network infrastructure. The OCP supports variances from the *Zoning Regulation Bylaw* to enable and support heritage conservation; therefore, staff recommend embedding the parking requirements into the new zone.

Encroachment Agreement

A number of street-level canopies and cornices are also proposed along 1050-1058 Pandora Avenue and 1508-1518 Cook Street, which project above the City Right-of-Way. These are encouraged in the Guidelines to provide pedestrian weather protection and welcoming streetscapes. In order to facilitate these canopies, the applicant is required to enter into an Encroachment Agreement with the City. Appropriate wording is included in the recommendation for Council's consideration.


CONCLUSIONS

The proposal complies with the applicable land use policies and further advances many other policies and objectives outlined in the OCP and the Downtown Core Area Plan, such as retaining and rehabilitating an existing heritage building; providing 103 rental dwelling units in perpetuity as well as a mix of unit types; providing pedestrian connections and active land uses adjacent to Franklin Green Park; and animating the pedestrian realm. A reduction in parking at this location is supportable given the proposed TDM measures and the site's proximity to frequent transit service and bicycle infrastructure. Staff recommend for Council's consideration that the application proceed to a Public Hearing.

ALTERNATE MOTION

That Council decline Rezoning Application No. 00695 for the property located at 1050-1058 Pandora Avenue and 1508-1518 Cook Street.

Respectfully submitted,



Leanne Taylor
Senior Planner
Development Services Division



Andrea Hudson, Acting Director
Sustainable Planning and Community
Development Department

Report accepted and recommended by the City Manager:



Date:

Dec 4, 2019

List of Attachments

- Attachment A: Subject Map
- Attachment B: Aerial Map
- Attachment C: Plans dated/date stamped November 22, 2019
- Attachment D: Letter from applicant to Mayor and Council dated October 30, 2019
- Attachment E: Community Association Land Use Committee Comments dated March 20, 2019
- Attachment F: Parking Study prepared by WATT Consulting dated October 29, 2019
- Attachment G: Letter from MODO dated October 28, 2019
- Attachment H: Letter from applicant regarding TDM measures dated October 30, 2019
- Attachment I: Tenant Assistance Plan
- Attachment J: Conservation Plan, dated July 2019
- Attachment K: Photographs
- Attachment L: Minutes of the Heritage Advisory Panel, October 8, 2019
- Attachment M: Minutes of the Advisory Design Panel, October 9, 2019.



Committee of the Whole Report

For the Meeting of December 12, 2019

To: Committee of the Whole **Date:** November 29, 2019

From: Andrea Hudson, Acting Director, Sustainable Planning and Community Development

Subject: Heritage Designation Application No. 000188 and Heritage Alteration Permit Application No. 00016 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street

RECOMMENDATIONS

That Council, after giving notice and allowing an opportunity for public comment at a meeting of Council and after the Public Hearing for Rezoning Application No. 00695, if it is approved, consider the following motions:

1. "That Council authorize the issuance of Heritage Alteration Permit Application No. 00016 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street, in accordance with:
 - a) Plans, date stamped November 22, 2019.
 - b) The Conservation Plan for the Parkway Apartments, date stamped July 2019.
 - c) Final plans to be generally in accordance with the plans identified above to the satisfaction of the Director of Sustainable Planning and Community Development.
 - d) Heritage Alteration Permit lapsing two years from the date of this resolution.
2. That Council approve the designation of the heritage-registered property located at 1050-1058 Pandora Avenue and 1508-1516 Cook Street, pursuant to Section 611 of the *Local Government Act*, as a Municipal Heritage Site, consistent with the plans dated November 22, 2019."

LEGISLATIVE AUTHORITY

In accordance with Sections 617 and 618 of the Local Government Act, Council may issue a Heritage Alteration Permit which may be subject to terms consistent with the purpose of the heritage protection of the property, including: (i) conditions respecting the sequencing and timing of construction, (ii) conditions respecting the character of the alteration or action to be authorized, including landscaping and the siting, form, exterior design and finish of buildings and structures and (iii) security. Council may refuse to issue a Heritage Alteration Permit for an action that, in the opinion of Council, would not be consistent with the purpose of the heritage protection of the property.

In accordance with Section 611 of the *Local Government Act*, Council may designate real property, in whole or in part, as protected property.

EXECUTIVE SUMMARY

The purpose of this report is to provide Council with information, analysis and recommendations for a Heritage Designation Application and a Heritage Alteration Permit Application for the property located at 1050-1058 Pandora Avenue and 1508-1518 Cook Street. The approvals for designation and heritage alterations would apply to the heritage-registered mixed-use building formerly known as the Parkway Apartments and later Wellburn's Market, which is located at the corner of Cook Street and Pandora Avenue. The building was built in 1911 and contributes to the historic character of Victoria's North Park Neighbourhood.

The proposal is for the redevelopment of the site, including the construction of a new four-storey building on the surface parking lot at 1518 Cook Street and construction of a four-storey addition to the Parkway Apartments Building, which will be renovated. The proposed development would contain 103 purpose-built rental units with retail commercial space on the ground floor. The development would result in a major restoration and rehabilitation of the street-facing elevations of the Parkway Apartments Building based on the original architectural plans for the building. Fifty percent of the interior structure of the building would be conserved and seismically upgraded.

The development requires a Rezoning and a Heritage Alteration Permit. The applicant has also proposed to heritage designate the Parkway Apartments Building as part of the application.

The development conserves a substantial amount of the original building, includes significant façade enhancements, and is thoughtfully detailed and positioned relative to the heritage building to achieve a compatible visual relationship. The applications are consistent with the *Official Community Plan* (OCP, 2012) which supports new development that enhances heritage property. The applications are consistent with the *Downtown Core Area Plan* (DCAP, 2011) heritage policies under chapter 7, which supports new development that enhances the form, character and features of heritage property and encourages new development to conserve the façades of heritage property. The applications are consistent with the *Standards and Guidelines for the Conservation of Historic Places in Canada* (2010) which encourages new additions that achieve visual compatibility through a balance between imitation and pointed contrast.

The designation of this building is generally consistent with Section 8: "Placemaking (Urban Design and Heritage)" and Section 21: "Neighbourhood Directions" of the *Official Community Plan* (OCP, 2012), the *North Park Neighbourhood Directions*, the *North Park Neighbourhood Plan* (1996) and the *Victoria Heritage Thematic Framework*.

The Heritage Advisory Panel (HAPL) reviewed the application at its October 8, 2019 meeting. HAPL recommended approval of the designation and the heritage alteration permit, with consideration of changes including an increased upper storey setback of 4 metres from Pandora Avenue, conservation of portions of the building not covered by the new construction and conservation of interior features. The applicant has increased the proposed setback in accordance with HAPL's recommendation and will be salvaging trim and furnishings from existing units. Conservation of a whole unit is not feasible due to the presence of hazardous materials that must be removed as part of this project.

The Advisory Design Panel (ADP) reviewed the Heritage Alteration Permit Application at their meeting on October 9, 2019, and recommended approval with some revisions. The applicant has revised the design in response to the most significant comments.

BACKGROUND

Description of Proposal

The applications propose 103 units of purpose-built rental housing and ground floor commercial uses within a development comprised of the renovated Parkway Apartments Building with a four-storey addition and a new four-storey building at 1518 Cook Street. The applicant has submitted a rezoning application to permit the increased density, height and reduced parking requirements, a Heritage Alteration Permit Application to authorize alterations to the heritage building, and a Heritage Designation Application to ensure the long-term protection of the Parkway Apartments Building.

The proposal includes the following major design components:

- construction of a new four-storey building on 1518 Cook Street with a 2.6 metre wide pedestrian walkway between the south sidewall and the conserved north sidewall of the Parkway Apartments Building
- construction of a four-storey addition to the Parkway Apartments Building set back 13.3 metres from the east elevation facing Cook Street, 4 metres from the south elevation facing Pandora Avenue, and 3.3 metres from the west elevation facing the adjacent property
- comprehensive rehabilitation and restoration of the Parkway Apartments Building ground floor glazing, transom windows and bulkheads based on the original architectural plans for the building
- restoration of the second-storey windows and parapet of the Parkway Apartments Building
- creation of a residential courtyard on the interior of the site accessed by a walkway from Cook Street between the conserved heritage building and the new four-storey building
- provision of 154 long-term bicycle parking spaces and 18 short-term bicycle parking spaces
- provision of 44 underground parking spaces.

The following data table compares the proposal with the existing CA-1 Zone, Pandora Avenue Special Commercial District. An "L" used before a number refers to the level of the building to which the measurement applies. An asterisk is used to identify where the proposal is less stringent than the existing zone. A double asterisk is used to identify where the existing building is legal non-conforming.

Zoning Criteria	Proposal	Zone Standard (CA-1 Zone)
Site area (m ²) - minimum	2879	n/a
Density (Floor Space Ratio) - maximum	2.92:1*	2:1
Total floor area (m ²) - maximum	8412.30*	5758
Height (m) - maximum	20.22*	15.50
Storeys - maximum	6	n/a
Site coverage (%) - maximum	70	n/a
Open site space (%) - minimum	22	n/a
Setbacks (m) - minimum		
Street setback (Pandora Ave. - south)	0* (existing & L1-2) / 3.97 (proposed L3-6)	3
Street setback (Cook St. - east)	0* (existing & L1-2) / 3.08 (proposed L3-4) / 13.13 (proposed L5-6)	3
Side (north)	6.18 (proposed L1-4) / 7.60 (proposed L5-6)	0 or 3
Side (west)	0* (existing & L1-2) / 2.96* (proposed L3-4) / 3.33 (proposed L5-6)	0 or 3
Vehicle parking - minimum		
Residential	33*	66
Commercial	5*	17
Visitor	3*	10
Bicycle parking stalls - minimum		
Residential - long-term	154	127
Residential - short-term	18	10
Commercial - long-term	6	6
Commercial - short-term	8	8

Description of Historic Place

A full description of the historic place is included in the attached Conservation Plan. The Parkway Apartments Building is significant for its association with the Edwardian-era development of Victoria and for its unique commercial architectural style by architect William Ridgway Wilson. The building was constructed during a pre WWI real estate boom and featured ground-floor commercial uses with residential uses above. William Ridgway Wilson designed many significant buildings in the city, including Gyppeswyk, now part of the Art Gallery of Greater Victoria (1889), St. John the Divine Anglican Church (1912), and the Victoria Drill Hall, now the Bay Street Armoury (1913-1915). One of the earliest tenants of the building was the Wellburn's Market on the ground floor, which still occupies the building today. Character-defining elements of the building are paraphrased below:

- location
- siting
- mix of uses
- commercial form, scale and massing and L-shaped plan
- masonry construction details, including red brick in a common bond on the rear and side elevations and the white glazed brick used on the main façades
- Edwardian era design features including its: commercial and residential design with full-height storefront on the ground floor; brick piers extending from grade to parapet delineating storefront bays; decorative pressed metal cornices at the storefront and roof levels; regular arrangement of fenestration on the upper floor; flat parapet punctuated by raised portions which align with brick piers on Cook Street and Pandora Avenue façades
- original windows including: multi light wood storefront transoms spanning each storefront bay of the Pandora Avenue and Cook Street façades; single assembly hung wood frame and sash windows with parged brick sills on rear façade; single and double assembly hung wooden-sash windows, oriel windows with hung wood frame and sash windows on either side of a fixed multi-light wood frame and sash windows on the second storey of the Pandora Avenue and Cook Street façades; half-moon leaded glass window with wood frame on Cook Street façade; three panel wood doors with wood transom windows on rear façade.

Community Consultation

Consistent with the *Community Association Land Use Committee (CALUC) Procedures for Processing Rezoning and Variance Applications*, the applicant has consulted the North Park CALUC at a Community Meeting held on March 20, 2019. The meeting minutes, dated March 20, 2019, are attached to this report.

ANALYSIS

The following sections provide a summary of the application's consistency with the relevant City policies, regulations and policies.

Official Community Plan

The proposed alterations outlined in the application are consistent with the OCP and advance the following policies:

- 8.6 *Conserve and enhance the heritage value, character and special features of areas, districts, streetscapes, cultural landscapes and individual properties throughout the city.*

- 8.11 *Determine the heritage value of areas, districts, streetscapes, cultural landscape and individual properties using the Victoria Heritage Thematic Framework as identified in Figure 12.*
- 8.43 *Encourage high quality architecture, landscape and urban design to enhance the visual identity and appearance of the City*
- 8.49 *Continue to support new additions that conserve and enhance heritage property, as consistent with the National Standards and Guidelines for the Conservation of Historic Places in Canada.*
- 8.50 *Encourage new development to avoid the demolition of heritage property, or one or more of its façades.*
- 8.51 *Continue to give consideration to tools available under legislation to protect or conserve heritage property including, but not limited to: heritage designation bylaws.*

The development preserves and enhances the landmark qualities of the Parkway Apartments Building through significant façade improvements, the strategic positioning of new construction and high quality new architecture that does not compete for attention with the heritage building. The project includes major façade improvements to the Parkway Apartments Building, including the restoration of missing transom windows, re-introduction of ground storey retail glazing across the entire façade and re-animation of the partially vacant ground floor with new commercial uses. Details of the new building and addition respond directly to the heritage building through a matching colour palette, matching horizontal window dimensions and chamfered window edges. The inverted semi-hexagonal shape of the windows on the front elevation of the new building match the proportions of the projecting semi-hexagonal (oriel) windows of the heritage building. The new building is also setback over 3 metres further from the street than the heritage building, which exposes its sidewall and the front wall of the building to pedestrians. Designation of the building furthers the City's objective of ensuring the long-term conservation of heritage property through heritage designation.

Downtown Core Area Plan

The development conserves and restores the significant physical features of the heritage-registered building and the overall design conserves its heritage value as a landmark in the neighbourhood. This reflects the DCAP's vision of a balance between sensitive new development and heritage conservation.

The DCAP includes the following relevant objectives for heritage conservation in the downtown:

1. Retain, protect and improve real property with aesthetic, historic, scientific, cultural, social or spiritual value and heritage character as a benefit to the public.
2. Development and heritage conservation are balanced through sensitive new infill and property additions that respond to the heritage value and character of Downtown Core Area Districts.

The DCAP contains the following relevant policies for the conservation of heritage properties and districts in the downtown:

- 7.3. Conserve heritage values of the Downtown Core Area and its character-defining elements, such as individual buildings, collections of buildings, streetscapes, structures and features.
- 7.18. Support new development that conserves and enhances the form, character and features of heritage property and areas, where controlled and regulated in the Downtown Core Area.
- 7.22. Encourage new development to avoid the demolition of a protected heritage property, or one or more of its facades, where the exterior alterations are assessed to significantly damage the heritage value of the property or an area, and where facade retention is feasible in conjunction with other policies for Downtown Core Area Districts in this Plan.

The development balances new development and heritage conservation through a sensitive property addition set back over 13 metres from the heritage building's Cook Street elevation and 4 metres from the Pandora Avenue elevation. The new building north of the heritage building is strategically sited to preserve views of the heritage building approaching south along Cook Street. The new development conserves the form of the building from many vantage points and the simplified detailing and colour scheme does not compete for attention with it. As discussed above, the development conserves the main façades of the building, a sidewall, a substantial amount of the interior structure and includes significant and meaningful enhancements to the heritage character of the building through the restoration of the main façades.

Advisory Design Guidelines for Buildings, Signs and Awnings

The *Advisory Design Guidelines for Buildings, Signs and Awnings* (2006) states that the overall design of new signage and awnings should enhance the architecture of the building, respond to its significant features, be simple and not busy and include complimentary colours. The applicant has proposed retractable fabric canopies along the ground floor of the building between the storefront and transom, which would reveal the transom window for the first time in decades. Signage could be applied directly to the canopy rather than the building. Historically, the building featured retractable canopies that could expose the transom when not in use. Currently the building features non-retractable, yellow canopies across the entire frontage that cover the transom window, which has been mostly boarded over.

The guidelines advise that new construction should complement any flanking heritage building and consider detailing, a comprehensive design approach and appropriate street relationship. The guidelines suggest that a contemporary solution can be quite acceptable in this context provided it is sympathetic. In staff's opinion, the combination of setbacks, cladding, colour scheme and window pattern and detailing are sympathetic to the Parkway Apartments Building.

Standards and Guidelines for the Conservation of Historic Places in Canada

The proposal is consistent with the *Standards and Guidelines for the Conservation of Historic Places in Canada* (the Standards and Guidelines). The applicant proposes to preserve and retain virtually all intact character-defining elements on the primary elevations, construct a compatible addition to the heritage building and provide a new storefront that replicates the proportions of the original and reinstates missing entrances and glazing. The applicant also proposes to restore the wood transom window across the whole frontage. The conservation

approach is primarily rehabilitation; however, it also includes a significant amount of preservation and restoration. Rehabilitation is defined in the Standards and Guidelines as the sensitive adaptation of an historic place for a continuing or compatible contemporary use while protecting its heritage value.

The proposed new construction meets Standard 11 of the Standards and Guidelines, which applies to rehabilitation projects. Standard 11 states that any new additions should conserve heritage value and character-defining elements, while being physically and visually compatible with, subordinate to and distinguishable from the historic place. The position of the addition over the heritage building with its substantial setback from Cook Street and Pandora Avenue conserve the form, scale and massing of the Parkway Apartments Building, preserving its prominence at the corner of Pandora Avenue and Cook Street from many vantage points. The addition is subordinate to the historic building through its colour palette, design details and significant setbacks. The modern, simplified details of the addition make it clearly distinguishable. The inverted bays on the new building and the matching horizontal widths of windows is a clear reference to the historic building.

Statement of Significance

A Statement of Significance describing the historic place, outlining its heritage value and identifying its character-defining elements can be found on page 7 of the attached Conservation Plan.

Resource Impacts

Heritage designation of 1050-1058 Pandora Avenue and 1508-1516 Cook Street will enable the property owner to apply for heritage grants from the Victoria Civic Heritage Trust and for a tax incentive from City Council to assist with the cost of seismic upgrading.

Heritage Advisory Panel

The application was reviewed by the Heritage Advisory Panel (HAPL) at its October 8, 2019 meeting. The minutes are attached. HAPL provided the following recommendation for Council's consideration:

That the Heritage Advisory Panel recommend that Council approve the designation of the heritage-registered property located at 1050-1058 Pandora Avenue and 1508-1516 Cook Street, pursuant to Section 611 of the *Local Government Act*, as a Municipal Heritage Site.

That the Heritage Advisory Panel recommend that Council approve Heritage Alteration Permit with Variances Application No. 00016 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street with the following changes:

- a) Setback from Pandora Avenue be increased to a minimum of at least 4m so that the separation is clearly visible
- b) Conserve the portion of the old building that will not be covered by the new building
- c) Consider retention of interior features where feasible, preferably in situ or relocated within the development.

The applicant increased the setback of the addition to the Pandora Avenue elevation to 4 metres in response. Conservation of the structure of the building not covered by the addition is not possible due to the location of the underground parking garage. The building does

contain original finishes including wainscoting and built in furniture. The applicant explored conservation of a full unit; however, the hazardous materials removal strategy requires extensive interior renovations. As an alternative, the applicant intends to salvage and reuse safe interior components within the renovated building.

Advisory Design Panel

The Advisory Design Panel (ADP) reviewed the Heritage Alteration Permit Application at their meeting on October 9, 2019. The minutes are attached. They provided the following recommendation for Council's consideration:

That the Advisory Design Panel recommend to Council that Heritage Alteration Permit with Variances Application No. 00016 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street be approved with the following changes:

- increase the building setback facing Pandora Avenue to 4m from the street
- revise the second floor units that face the mews to improve liveability
- reconsider the windowless interior dens and bedrooms
- resolve the proposed materials
- consider adding usable roof spaces
- consider revising the articulation of the four-storey building's façade facing Franklin Green Park, and consider the addition of balconies or Julietts
- reconsider the width of the alleyway (mews) to increase accessibility.

In response to ADP's recommendations above, the applicant made the following changes:

- increased the building setback facing Pandora Avenue to 4m from the street
- re-oriented windows to allow for more daylight penetration
- added a west-facing roof deck on the fifth storey for residents in the building
- added Juliet balconies on the west elevation overlooking Franklin Green Park
- removed planters and short-term bicycle parking stalls from the mews and relocated to the residential courtyard.
- resolved exterior materials

CONCLUSIONS

The development conserves a substantial amount of the original building, includes significant façade enhancements, and is thoughtfully detailed and positioned relative to the heritage building to achieve a compatible visual relationship. The applications are consistent with the *Official Community Plan* and *Downtown Core Area Plan* which both support new development that enhances heritage property. The applications are consistent with the *Standards and Guidelines for the Conservation of Historic Places in Canada* and are particularly successful in meeting the requirements that a new addition be compatible, distinguishable and subordinate. The proposed designation is for a neighbourhood landmark in North Park featuring unique materials and an exceptional design by a notable historic architect. In staff's opinion, the designation and the heritage alteration permit are both worthy of Council's approval.

ALTERNATE MOTION

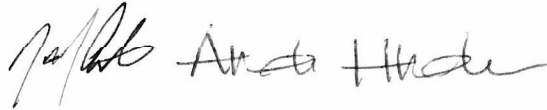
Option 1

That Council decline Heritage Designation Application No. 000188 and Heritage Alteration Permit Application No. 00016 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street

Respectfully submitted,



John O'Reilly, Senior Heritage Planner
Development Services Division



Andrea Hudson, Acting Director
Sustainable Planning and Community
Development Department

Report accepted and recommended by the City Manager:



Date: Dec 4, 2019

List of Attachments

- Attachment A: Subject Map
- Attachment B: Aerial Map
- Attachment C: Plans dated/date stamped November 22, 2019
- Attachment D: Letter from applicant to Mayor and Council dated October 30, 2019
- Attachment E: Community Association Land Use Committee Comments dated March 20, 2019
- Attachment F: Parking Study prepared by WATT Consulting dated October 29, 2019
- Attachment G: Letter from MODO dated October 28, 2019
- Attachment H: Letter from applicant regarding TDM measures dated October 30, 2019
- Attachment I: Tenant Assistance Plan
- Attachment J: Conservation Plan, dated July 2019
- Attachment K: Photographs
- Attachment L: Minutes of the Heritage Advisory Panel, October 8, 2019
- Attachment M: Minutes of the Advisory Design Panel, October 9, 2019.

2.2 Heritage Alteration Permit with Variance Application No. 00016 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street

The City is considering a Heritage Alteration Permit with Variance Application to retain 50% of the existing heritage-registered building and construct a new four- and six-storey mixed-use addition consisting of ground floor commercial uses and residential above. The applicant is proposing to heritage-designate the existing building.

Applicant meeting attendees:

MICHAEL GREEN	MGA MICHAEL GREEN ARCHITECTURE INC
MARIE-CLAIRE BLIGH	MGA MICHAEL GREEN ARCHITECTURE INC
JESSICA GIBSON	DISTRICT PROPERTIES

Leanne Taylor provided the Panel with a brief introduction of the application and the areas that Council is seeking advice on, including the following:

- the relationship between the heritage building and new addition
- the application of building materials
- the parapet height
- any other aspects of the proposal on which ADP chooses to comment.

Michael Green and Marie-Claire Bligh provided the Panel with a detailed presentation of the site and context of the proposal.

The Panel asked the following questions of clarification:

- was more of a reveal between the heritage building and the new addition considered?
 - a variety of approaches were considered, but the proposal feels the cleanest, most appropriate solution
 - the true view line and pedestrian experience were paramount
 - the simplicity of the proposed design is its strength
- why do no balconies face the park?
 - Juliet balconies face the park to relate to the Cook Street façade
 - the four-storey portion was deliberately simplified
- are no balconies proposed on the four-storey building?
 - correct
- is the new building's roof a green roof?
 - no; the sedum roof is only on the heritage building roof
- was a different use considered for a more public/private transition onto Franklin Green, for more eyes on the street and park?
 - other uses were considered early on, but they presented considerable challenges
 - the community feedback has shown desire for 24hr activation, which is not readily satisfied by a commercial component
- is the connection to Franklin Green accessible?
 - yes, pedestrians would descend the vehicle ramp to the garage level then ascend to the park level
 - the ramp is very gentle and the site is accessible throughout
- how are rooms labelled as dens regulated by the City?
 - Leanne Taylor noted that the City does not regulate buildings' interior layouts

- what relation is there between the number of units and number of required vehicle parking stalls?
 - Leanne Taylor clarified that the application proposes a significant parking variance, and that the OCP does support parking variances for applications which include heritage conservation and preservation
- are parking requirements tied to the number of bedrooms?
 - no
- is the heritage building restricted in how much additional weight it can carry?
 - the heritage building will require seismic upgrading regardless of its existing structure
 - the proposal will not put a new load onto the roof
- what is the rationale for the building entry location?
 - earlier iterations proposed the new addition closer to Cook Street; however, the response from the heritage community was to push the addition as far back as possible and to save as much as possible of the heritage building
 - with four public faces to the building, many factors are considered in selecting an entry point; the proposed entry is at a logical mid-point
 - the entry location creates an interesting space with a café component
- are the buried units on the second level the best solution?
 - there are challenges with the depth of the building coupled with heritage retention
- portions of the mews are 2.9m wide; what is the proposed use in this location and was additional accessibility considered?
 - the mews will be fully accessible and open during the daytime, and gated at night only for residents' use
 - the existing brick wall will be rehabilitated
- does the brick wall need to be preserved as a blank wall?
 - this was desired from a heritage standpoint
- will all the units be rental?
 - yes, they will be secured as rental.

Panel members discussed:

- appreciation for the restoration of the heritage building
- need to resolve the relation between the proposed additions and the heritage building
- support for an increased building setback along Pandora Avenue
- the proposal would benefit from an additional 2 storeys facing Pandora Avenue for a more interesting articulation of building massing
- the four-storey portion facing Franklin Green is very simple, with a big-city feeling
- appreciation for the inset windows relating to the existing heritage windows
- opportunity for balconies or outdoor space on the four-storey building
- appreciation for the six-storey building's articulation and balconies
- concern for the liveability of the two second floor units facing the mews and for the many units with closets as bedrooms
- concern with the interior angles created by the proposed balconies, which may limit the useable interior space
- appreciation for the proposed sedum roof
- opportunity to add a green roof component to the top roof as well to improve views from adjacent buildings and to help conceal the rooftop mechanical equipment
- missed opportunity for a common amenity space for residents on the stepped roofs

- need to fully resolve the materiality; opportunity for an additional level of materiality
- appreciation for the proposal's minimal design
- desire for a richer material on the lower building to lend a more solid appearance
- CPTED concerns with the narrow laneway and with the recessed entries along Pandora Avenue
- the need for the laneway to remain clear of clutter from planter boxes, seating, etc.
- need to resolve the building's northern façade facing Franklin Green
- appreciation for the two access points to the park
- need to avoid locking Franklin Green in the middle of the block.

Motion:

It was moved by Jessi-Anne Reeves, seconded by Elizabeth Balderston, that the Advisory Design Panel recommend to Council that Heritage Alteration Permit with Variance Application No. 00016 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street be approved with the following changes:

- increase the building setback facing Pandora Avenue to 4m from the street
- revise the 2nd floor units that face the mews to improve liveability
- reconsider the windowless interior dens and bedrooms
- resolve the proposed materials
- consider adding usable roof spaces
- consider revising the articulation of the four-storey building's façade facing Franklin park, and consider the addition of balconies or juliets
- reconsider the width of the alleyway (mews) to increase accessibility.

Carried (5:3)

For: Elizabeth Balderston, Brad Forth, Jason Niles, Jessi-Anne Reeves, Stefan Schulson (Chair)

Opposed: Pamela Madoff, Carl-Jan Rupp, Karen Sander.

3. ADJOURNMENT

The Advisory Design Panel meeting of October 9, 2019 was adjourned at 2:20 pm.

Stefan Schulson, Chair

3. **1050-1058 Pandora Avenue and 1508-1518 Cook Street**
Heritage Designation Application No. 000188
Heritage Alteration Permit Application with Variances No. 00016

Attendees: Marie-Claire Bligh and Michael Green (Michael Green Architecture), Chelsea Dunk (Donald Luxton & Associates), Jessica Gibson and Michael Nygren (District Group)

John O'Reilly provided a brief introduction. Marie-Claire Bligh, Michael Green and Chelsea Dunk presented.

Panel Questions and Comments

- To confirm, the applicant is requesting an increase in allowable density and floor area? John O'Reilly: Yes, that is part of the rezoning application. The Panel is welcome to comment on how the density translates to built form.
- How much of the façade will be retained? John O'Reilly: The portion of the façade shown on slide 10 (along Cook Street and Pandora Avenue and the sidewall currently facing the parking lot) will be retained and held in place; new construction will be built behind the façade; 50% of the internal structure will be conserved.
- Why is only 50% of the internal structure being conserved? Marie-Claire Bligh: This is due to the cost of seismic upgrading and the parking requirement. The 50% will be used for retail below and residential above.
- Will the Wellburn's sign on the sidewall be retained? Marie-Claire Bligh: The retention of the sign is not ruled out. A lot of repair is needed for the sidewall and the black paint will be removed in keeping with the "lightness" of the new construction. Michael Green: We recognize the significance of the sign to the neighbourhood.
- What type of canopy will be used? Marie-Claire Bligh: The canopy will be retractable, dark grey on the heritage façades on Pandora Avenue and Cook Street and on the café facing Cook Street, with a small area for signage on the vertical front. Heritage guidelines will be followed for the signage.
- The historical reason for recessed entries is so that the door can swing out without interfering with the sidewalk. This also allows for fire exit and more interior floor area. Marie-Claire: The original doors swung in, but the proposed doors will swing out.
- Transom lights are usually vertically, not horizontally, articulated; traditional design usually has a void on the centre, not a solid on the centre. The proposed double windows have a solid on the centre which the applicant may want to reconsider.
- There is not a lot of parking. Is the City okay with that? Leanne Taylor: There is a significant parking variance. The applicant is providing a comprehensive program of transportation demand management measures which will help to offset the shortfall.
- What can be done about the damaged glazed brick as mentioned in the Conservation Plan? Chelsea Dunk: There are masonry techniques that can repair the glazed surface of the bricks. There is some overpainting and general staining and some repointing will be required. Some bricks may have to be replaced with material that is compatible in appearance and brick dimension. The intent is not for the building to look brand new. The aim is to retain much of the original fabric in situ.
- Could the remaining rooftop of the heritage building be a green roof? Marie-Claire Bligh: There will be landscaped cedar roofs on levels 3 and 5 which will be inaccessible to tenants, but accessible for maintenance.
- What elements of the interior structure will be retained? Michael Green: Some residential features will be removed to increase the floor area. However, these features will be reused in more public areas.

- Is it possible to keep one residential unit as is? Marie-Claire Bligh: That will be considered.
- As part of the demolition, could some bricks and windows be saved for future repair or repurposing? Michael Green: Yes, this absolutely could be done for the bricks, but it is tougher to do for the windows due to difficulty in removal, their condition and energy issues.
- What is being heritage designated? John O'Reilly: Heritage designation is for the conserved façades. Pamela Madoff: If the heritage alteration permit with variances application is not approved by Council, the heritage designation would likely not go forward.
- The building is a real gem and deserves heritage designation due in part to its interesting capitals; recessed oriel windows; slightly sloping parapet; and its unique deftness of design. The North Park neighbourhood has very few heritage commercial buildings.
- According to the policies and guidelines for rooftop additions and depending on the depth of the excavation, one to two storey buildings should not have a rooftop addition. City guidelines govern the visibility of additions from the street. The proposed setback from Pandora Avenue is 2.2m, but the City's policies recommend a minimum setback of 4m. The proposed height, floor space and FSR variances are already very generous thus it is reasonable to request a greater setback on Pandora Avenue.
- John O'Reilly: The *Downtown Core Area Plan* allows 5.5:1 density. A setback of 4m is recommended in Old Town, but not in North Park.
- The composition does not highlight or showcase the heritage building, and it is not setback enough from Pandora Avenue. To rectify this, the size of the development could be reduced. The proposal is requesting an almost 50% increase in density.
- Could volume be moved over into the existing parking lot to increase the setback on Pandora Avenue? Leanne Taylor: Staff can explore this idea with the applicant; however, issues regarding the driveway and building separation may arise.
- Why only retain the façade and nothing else? There are benefits in retaining the internal structure, i.e. it can be restored in the future. More than 50% of the heritage building could be saved.
- If the new building is setback from the heritage façade, it will become more subordinate (as per Standard 11).
- The proposed new construction is distinguishable from the old and reads as a backdrop to the existing building. The additions are staggered and uniform. The addition incorporates elements of the heritage building, e.g. window height, oriel windows, etc. The complete separation of the heritage building from the four-storey addition provides balance.

Moved

Seconded

That the Heritage Advisory Panel recommend that Council approve the designation of the heritage-registered property located at 1050-1058 Pandora Avenue and 1508-1516 Cook Street, pursuant to Section 611 of the Local Government Act, as a Municipal Heritage Site.

Carried (unanimous)

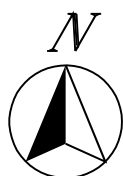
Moved

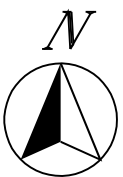
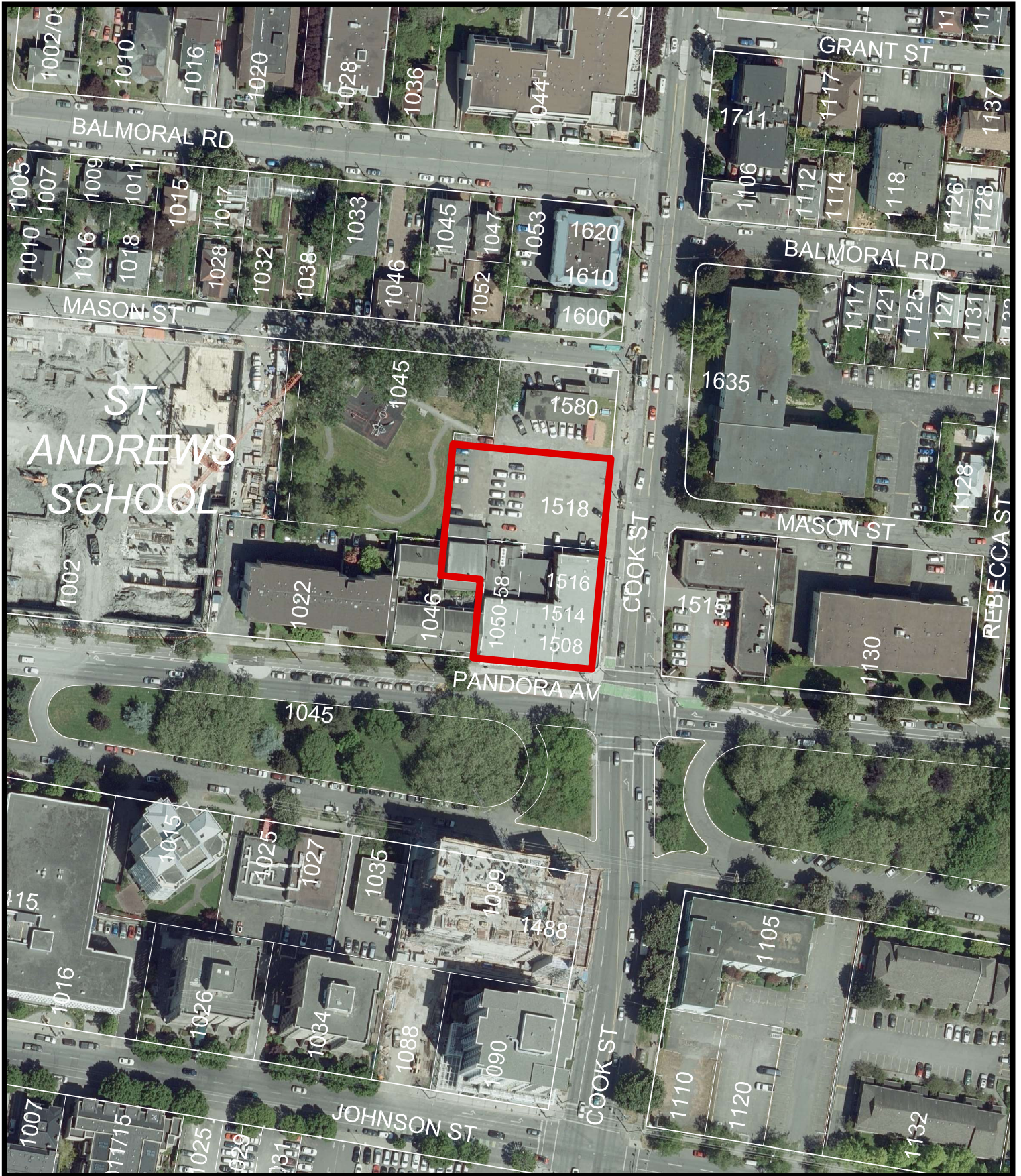
Seconded

That the Heritage Advisory Panel recommend that Council approve Heritage Alteration Permit with Variances Application No. 00016 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street with the following changes:

- a) Setback from Pandora Avenue be increased to a minimum of at least 4m so that the separation is clearly visible
- b) Conserve the portion of the old building that will not be covered by the new building
- c) Consider retention of interior features where feasible, preferably in situ or relocated within the development.

Carried (7 in favour, 1 opposed)





1050-1058 Pandora & 1508-1518 Cook Street
Rezoning No.00695



DRAWING LIST	SCALE
A000 COVER SHEET DRAWING LIST	NAT
A001 PROJECT DESCRIPTION & SITE LOCATION PLAN	NAT (1:100)
A002 EXISTING SITE PLAN	1:100
A003 PROPOSED SITE PLAN	1:100
A004 AVERAGE FLOOR CALCULATIONS	1:100
A005 LEVEL SHOWN PLAN	1:100
A006 FSR PLAN	1:100
A007 EXIST PLAN	1:100
A008 LEVEL PLAN	1:100
A009 EXIST PLAN	1:100
A010 LEVEL PLAN	1:100
A011 FLOOR PLAN	1:100
A012 EXIST PLAN	1:100
A013 LEVEL PLAN	1:100
A014 EXIST PLAN	1:100
A015 LEVEL PLAN	1:100
A016 EXIST PLAN	1:100
A017 FLOOR PLAN	1:100
A018 EAST ELEVATION	1:100
A019 NORTH & WEST ELEVATIONS	1:100
A020 HERITAGE ELEVATION (NORTH)	1:100
A021 FACADE STORES	1:100
A022 STREETSCAPE ELEVATIONS	NAT
A023 SECTION LOOKING WEST	1:100
A024 SECTION LOOKING EAST	1:100
A025 SECTION LOOKING NORTH	1:100
A026 SECTION LOOKING NORTH SOUTH	1:100
A027 SECTION LOOKING SOUTH	1:100
A028 STRATEGIC CROSS SECTION	1:100
A029 FSR ELEVATION	1:100
A030 FSR ELEVATION	1:100
A031 FSR ELEVATION	1:100
A032 FSR ELEVATION	1:100
A033 FSR ELEVATION	1:100
A034 FSR ELEVATION	1:100
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A061 FSR ELEVATION	1:100
A062 FSR ELEVATION	1:100



Received
City of Victoria
NOV 22 2019
Planning & Development Department
Development Services Division

# of units	Required	Provided
1	2.5	3
2	5	6
3	7.5	9
4	10	12
5	12.5	15
6	15	18
7	17.5	21
8	20	24
9	22.5	27
10	25	30
11	27.5	33
12	30	36
13	32.5	39
14	35	42
15	37.5	45
16	40	48
17	42.5	51
18	45	54
19	47.5	57
20	50	60
21	52.5	63
22	55	66
23	57.5	69
24	60	72
25	62.5	75
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92	230	276
93	232.5	279
94	235	282
95	237.5	285
96	240	288
97	242.5	291
98	245	294
99	247.5	297
100	250	300

BIKE PARKING | CALCULATED
4500-2
4500-2
4500-2
4500-2

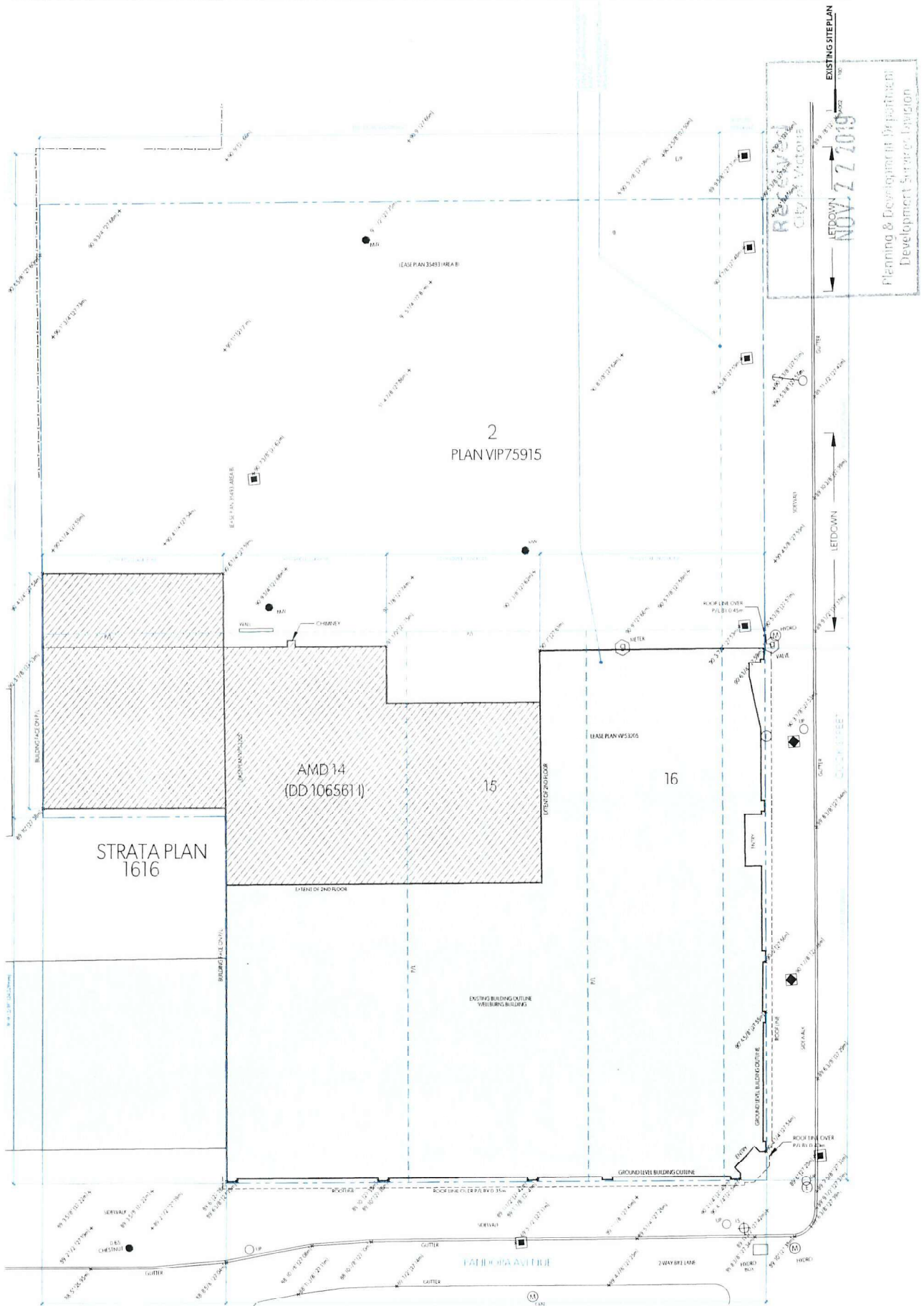
VISITOR PARKING
Visitor Parking
Commercial Parking

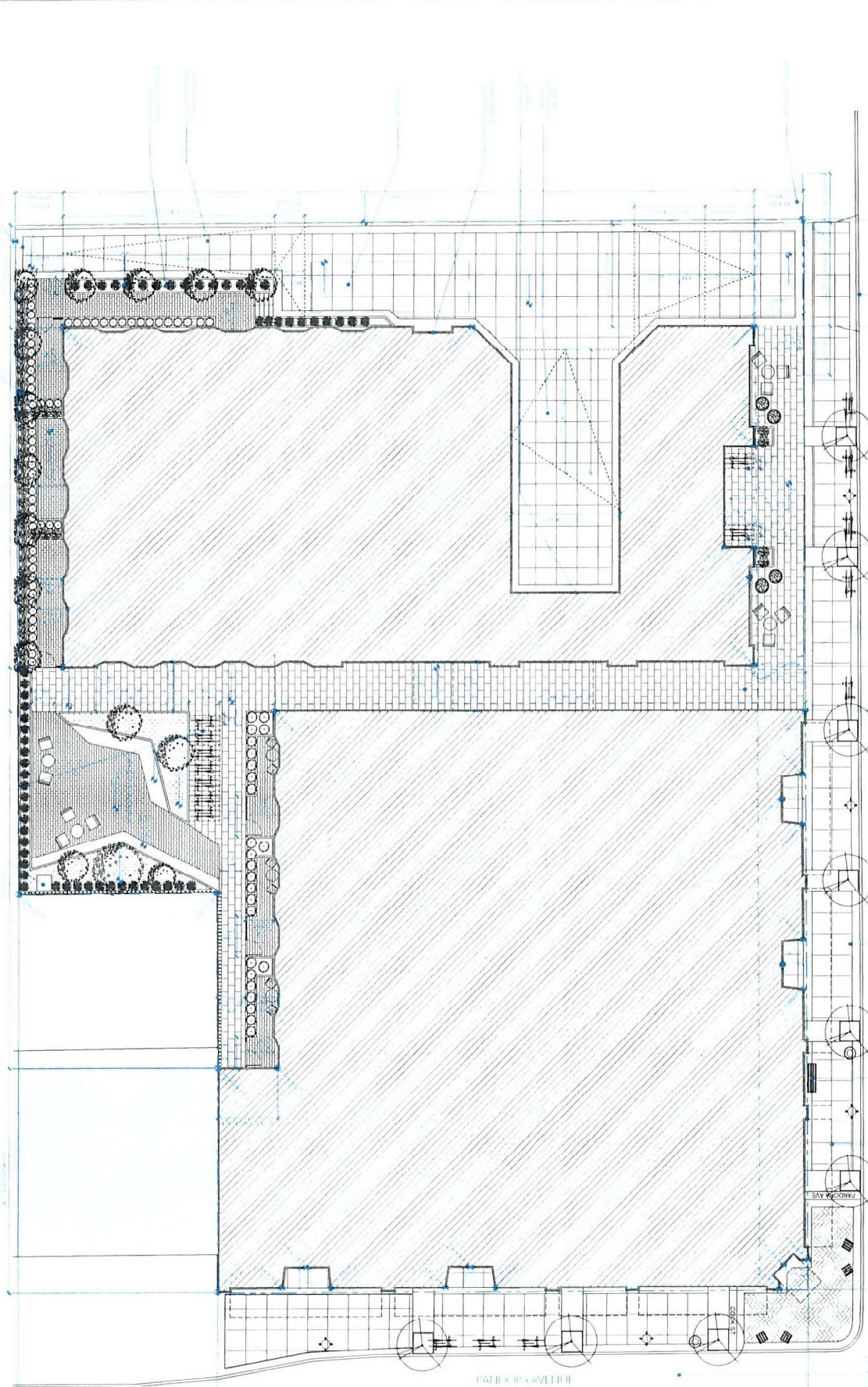
CARSHARE PARKING
Moped, Carshare Parking
Restaurant

BIKE PARKING | LONG-TERM
Restaurant
Regular, Grocery

BIKE PARKING | SHORT-TERM
Restaurant
Regular, Grocery





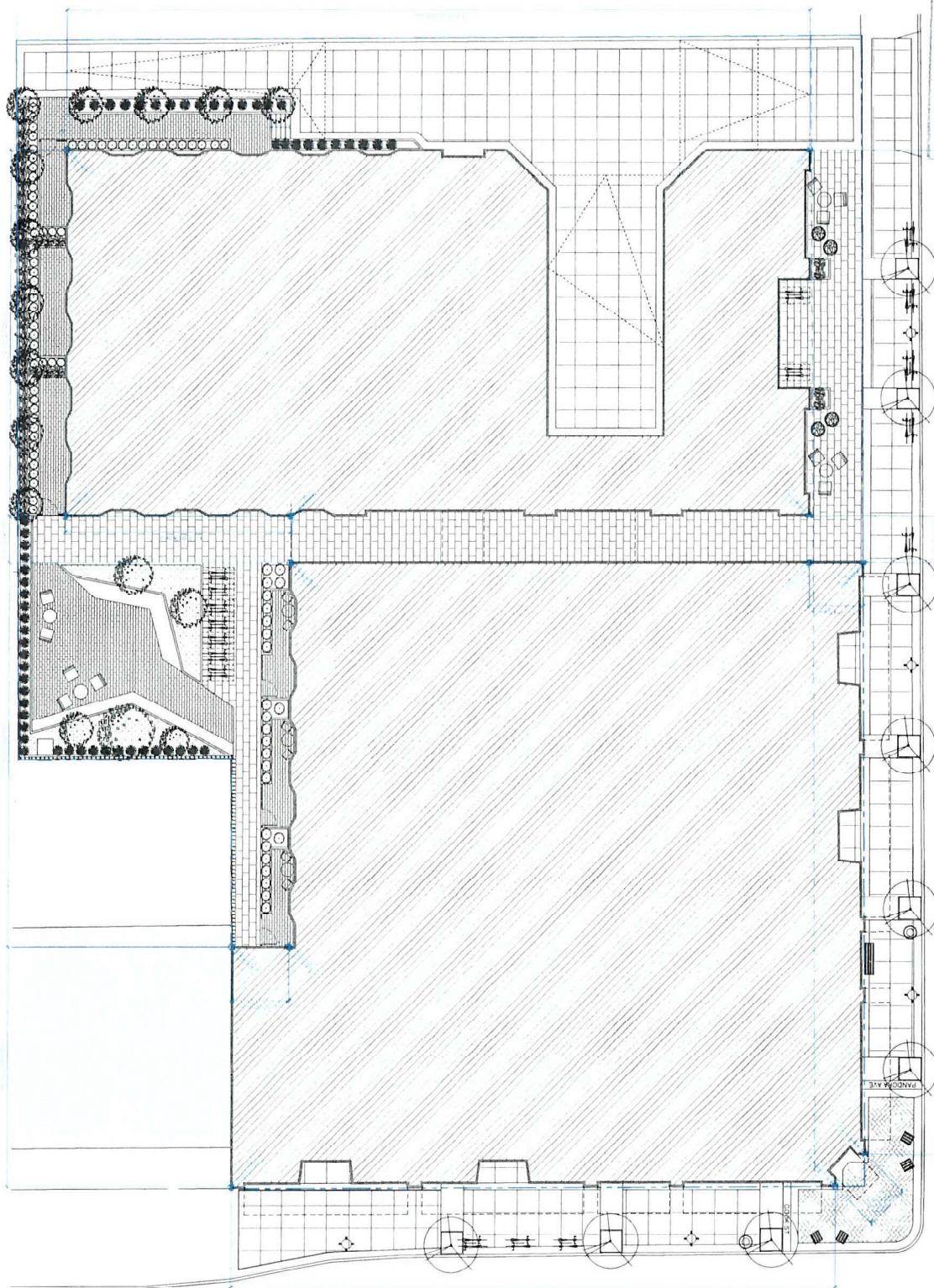


PROPOSED SITE PLAN

Received
 City of Victoria

NOV 27 2019

Planning & Development Department
 Development Services Division



AVERAGE GRADE CALC.

Received

NOV 22 1967

Planning & Development Department
Development Services Division

MGA

DATE	2018.10.26	REVISION	REVISION 1
DATE	2018.10.26	REVISION	REVISION 2
DATE	2018.10.26	REVISION	REVISION 3
DATE	2018.10.26	REVISION	REVISION 4
DATE	2018.10.26	REVISION	REVISION 5
DATE	2018.10.26	REVISION	REVISION 6
DATE	2018.10.26	REVISION	REVISION 7
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DATE	2018.10.26	REVISION	REVISION 10
DATE	2018.10.26	REVISION	REVISION 11
DATE	2018.10.26	REVISION	REVISION 12
DATE	2018.10.26	REVISION	REVISION 13
DATE	2018.10.26	REVISION	REVISION 14
DATE	2018.10.26	REVISION	REVISION 15
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DATE	2018.10.26	REVISION	REVISION 17
DATE	2018.10.26	REVISION	REVISION 18
DATE	2018.10.26	REVISION	REVISION 19
DATE	2018.10.26	REVISION	REVISION 20

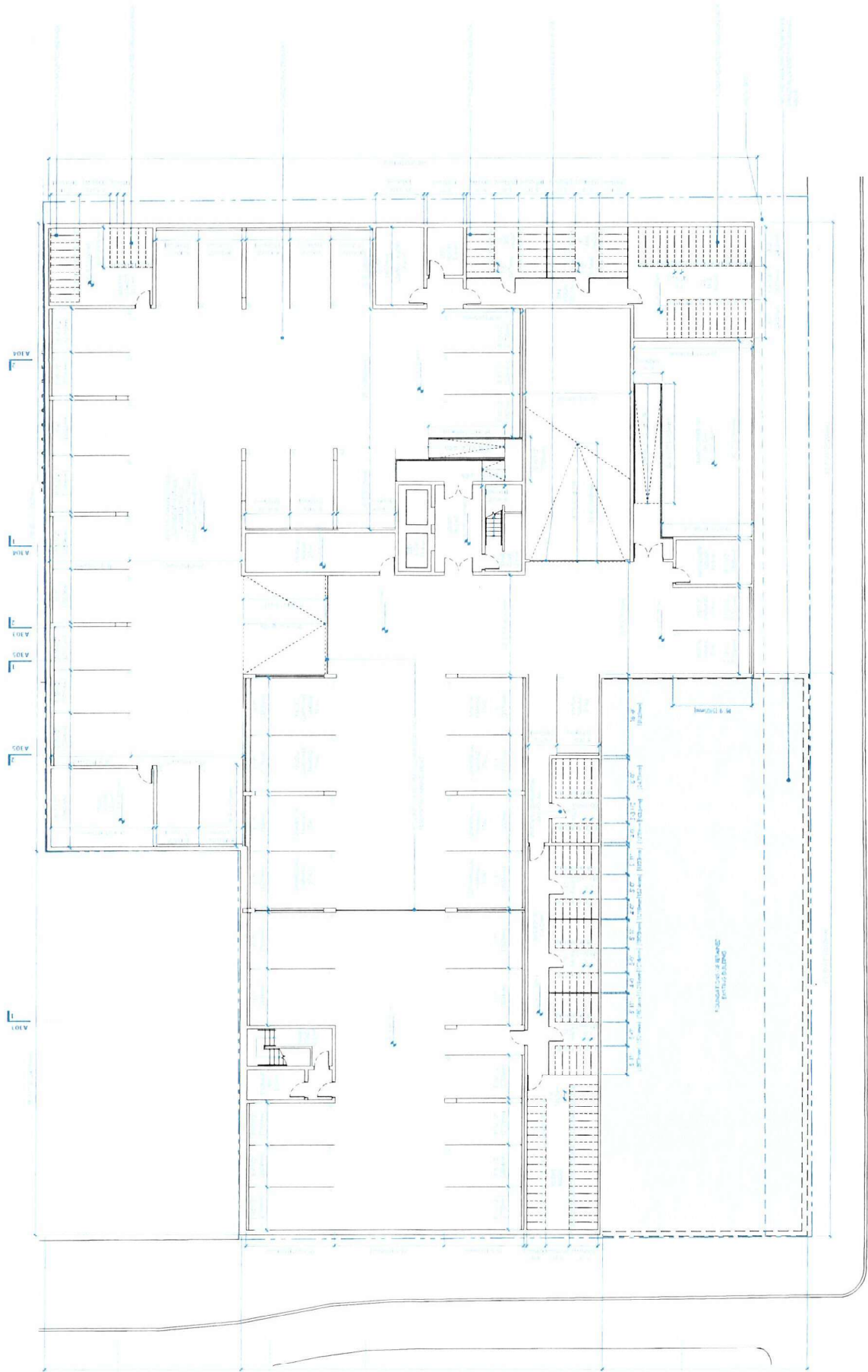
MICHAEL GREEN ARCHITECTURE
3330 ROAD 100, SUITE 100
VICTORIA, BC V8N 1A1

2018.10.26 REVISION 1
2018.10.26 REVISION 2
2018.10.26 REVISION 3
2018.10.26 REVISION 4
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2018.10.26 REVISION 14
2018.10.26 REVISION 15
2018.10.26 REVISION 16
2018.10.26 REVISION 17
2018.10.26 REVISION 18
2018.10.26 REVISION 19
2018.10.26 REVISION 20

PARKWAY
1000 PARKWAY AVE. 115 & 116 STREET
VICTORIA, BC
V8N 1A1

A100

LEVEL PLAN



Received
City of Victoria
NOV 22 2019
Planning & Development Department
Development Services Division

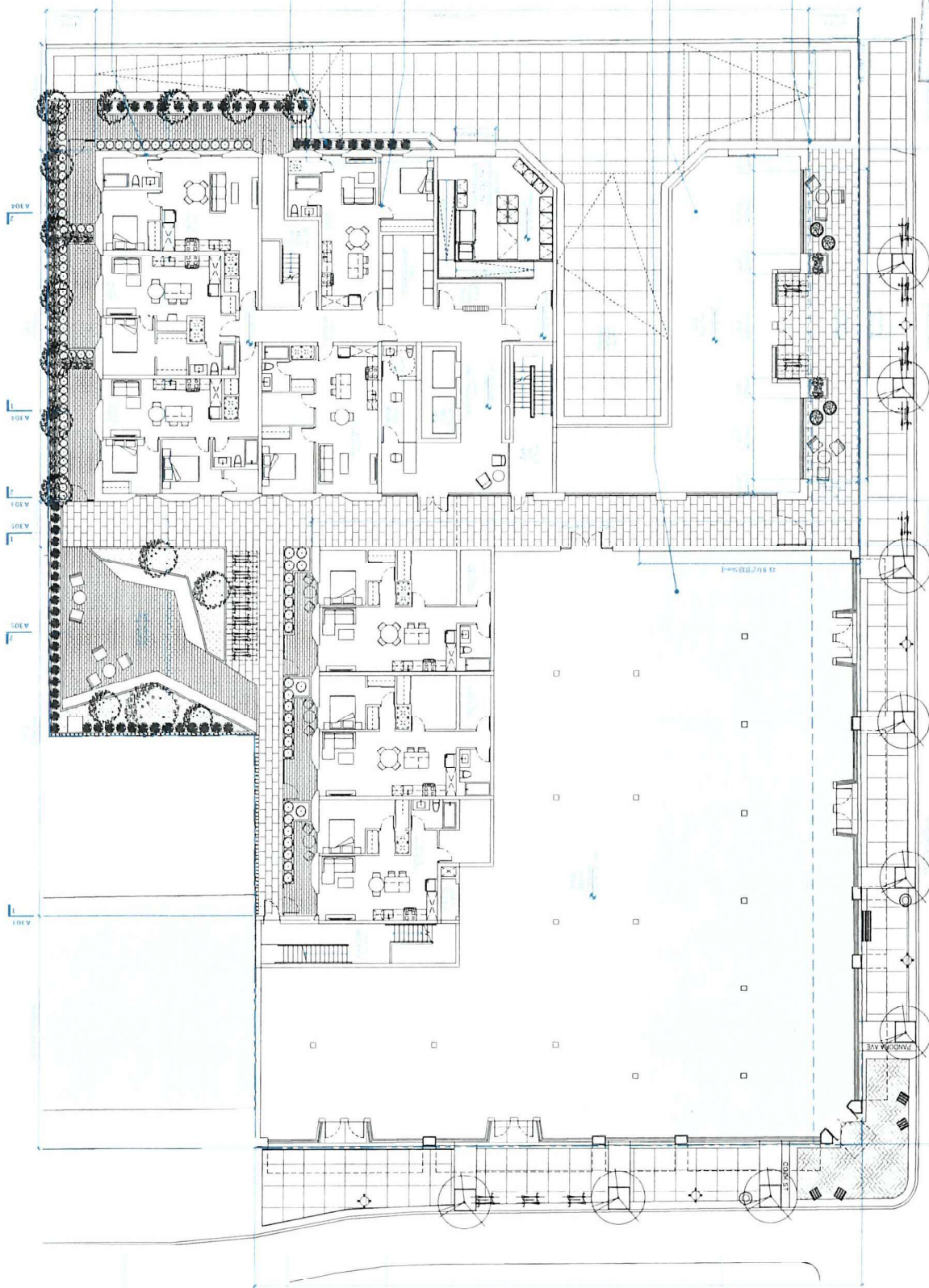
[illegible]

MICHAEL GREEN ARCHITECTURE
1535 W 3RD AVENUE VANCOUVER BC
CANADA V6J 1B8

DATE	REASON	DESCRIPTION
2019/10/30	△	REVISED FOR REZONING
2019/09/13	△	REVISED FOR REZONING
2019/05/15	△	ISSUED FOR REZONING

PARKWAY
1050 PANDORA AVE • 15 B COOK STREET
VICTORIA BC
2018-0011

A101



Received 2008 10 10
LEVEL PLAN
City of Victoria

NOV 27 1969

Planning & Development Department
Development Services Division

NO.	DATE	DESCRIPTION
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3	2010.10.30	REVISION FOR REZONING
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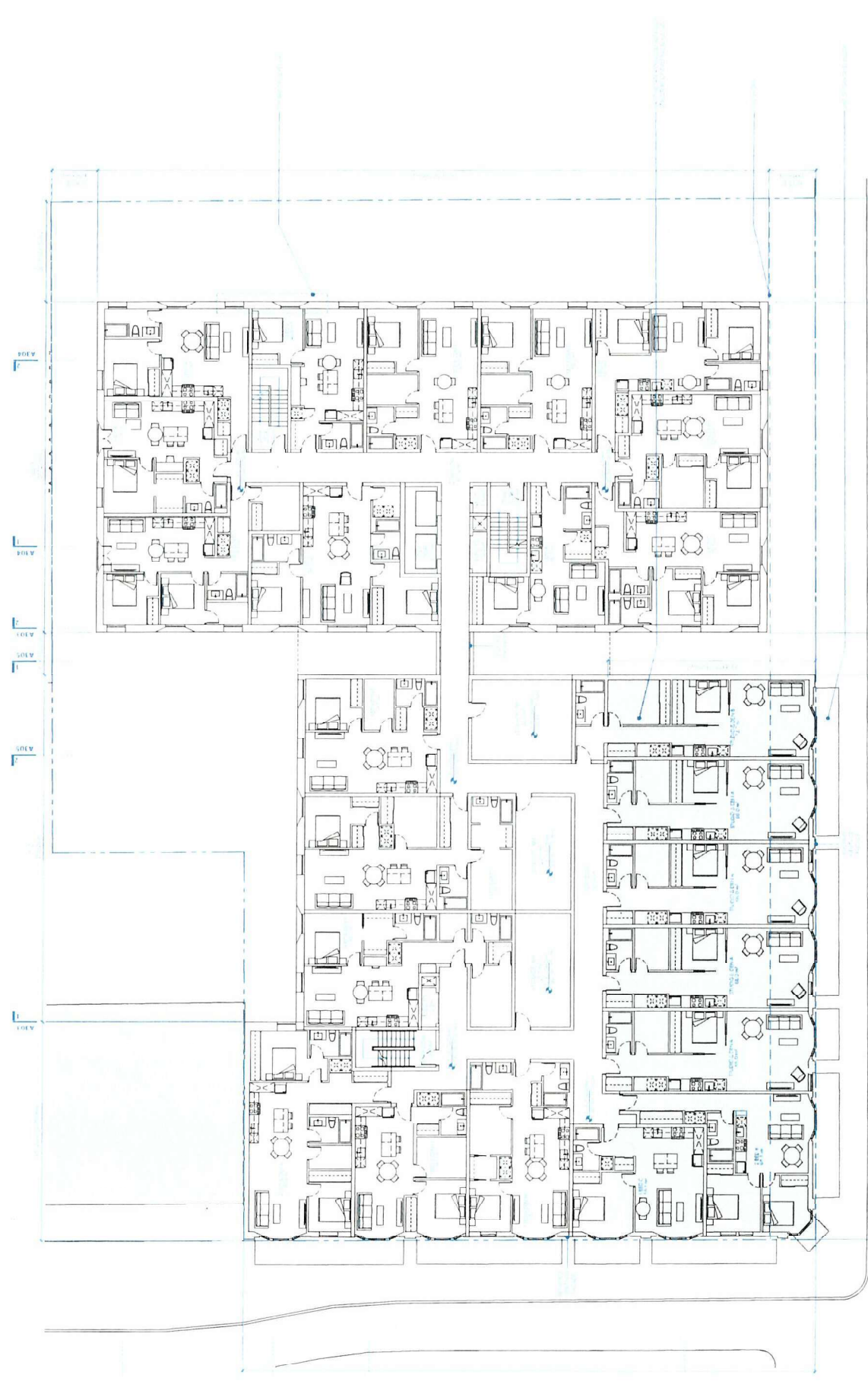
MICHAEL GREEN ARCHITECTURE
1000 PANDORA AVE. #100
VICTORIA, BC
CANADA V8P 1A8

2010.10.30 REVISION FOR REZONING
2010.10.30 REVISION FOR REZONING
2010.10.30 REVISION FOR REZONING
DATE REVISION DESCRIPTION

PARKWAY
1000 PANDORA AVE. #100
VICTORIA, BC
CANADA V8P 1A8

A102

LEVEL 2 PLAN



LEVEL 2 PLAN
A102

NOV 22 2013

Planning & Development Department
Development Services Division

MEASUREMENTS

NO.	DESCRIPTION	UNIT	QTY	UNIT PRICE	TOTAL
1	CEILING	SQ. FT.	12,000	1.25	15,000.00
2	FLOOR	SQ. FT.	12,000	1.25	15,000.00
3	WALL	SQ. FT.	1,000	1.50	1,500.00
4	DOOR	EA.	10	150.00	1,500.00
5	WINDOW	EA.	20	75.00	1,500.00
6	STAIR	SQ. FT.	500	3.00	1,500.00
7	ELEVATOR	EA.	1	1,000.00	1,000.00
8	HALLWAY	SQ. FT.	1,000	1.25	1,250.00
9	RESTROOM	EA.	5	200.00	1,000.00
10	KITCHEN	SQ. FT.	500	2.50	1,250.00
11	LAUNDRY	SQ. FT.	500	2.50	1,250.00
12	MECHANICAL	SQ. FT.	500	2.50	1,250.00
13	STORAGE	SQ. FT.	500	2.50	1,250.00
14	ENTRY	SQ. FT.	500	2.50	1,250.00
15	LOBBY	SQ. FT.	500	2.50	1,250.00
16	CONFERENCE	SQ. FT.	500	2.50	1,250.00
17	OFFICE	SQ. FT.	500	2.50	1,250.00
18	RECEPTION	SQ. FT.	500	2.50	1,250.00
19	TRAINING	SQ. FT.	500	2.50	1,250.00
20	MEETING	SQ. FT.	500	2.50	1,250.00
21	WORKSHOP	SQ. FT.	500	2.50	1,250.00
22	LABORATORY	SQ. FT.	500	2.50	1,250.00
23	CLUBHOUSE	SQ. FT.	500	2.50	1,250.00
24	GYMNASIUM	SQ. FT.	500	2.50	1,250.00
25	POOL	SQ. FT.	500	2.50	1,250.00
26	SAUNA	SQ. FT.	500	2.50	1,250.00
27	SPRINKLER	SQ. FT.	500	2.50	1,250.00
28	SMOKE	SQ. FT.	500	2.50	1,250.00
29	ALARMS	SQ. FT.	500	2.50	1,250.00
30	PAINT	SQ. FT.	500	2.50	1,250.00
31	GLASS	SQ. FT.	500	2.50	1,250.00
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36	LANDSCAPE	SQ. FT.	500	2.50	1,250.00
37	DRIVEWAY	SQ. FT.	500	2.50	1,250.00
38	PARKING	SQ. FT.	500	2.50	1,250.00
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46	TELEPHONE	SQ. FT.	500	2.50	1,250.00
47	TELEVISION	SQ. FT.	500	2.50	1,250.00
48	COMPUTER	SQ. FT.	500	2.50	1,250.00
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54	INTERCOM	SQ. FT.	500	2.50	1,250.00
55	STAIRCASE	SQ. FT.	500	2.50	1,250.00
56	ELEVATOR	SQ. FT.	500	2.50	1,250.00
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63	WORKSHOP	SQ. FT.	500	2.50	1,250.00
64	LABORATORY	SQ. FT.	500	2.50	1,250.00
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66	GYMNASIUM	SQ. FT.	500	2.50	1,250.00
67	POOL	SQ. FT.	500	2.50	1,250.00
68	SAUNA	SQ. FT.	500	2.50	1,250.00
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76	FOUNDATION	SQ. FT.	500	2.50	1,250.00
77	ROOF	SQ. FT.	500	2.50	1,250.00
78	LANDSCAPE	SQ. FT.	500	2.50	1,250.00
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98	ELEVATOR	SQ. FT.	500	2.50	1,250.00
99	LOBBY	SQ. FT.	500	2.50	1,250.00
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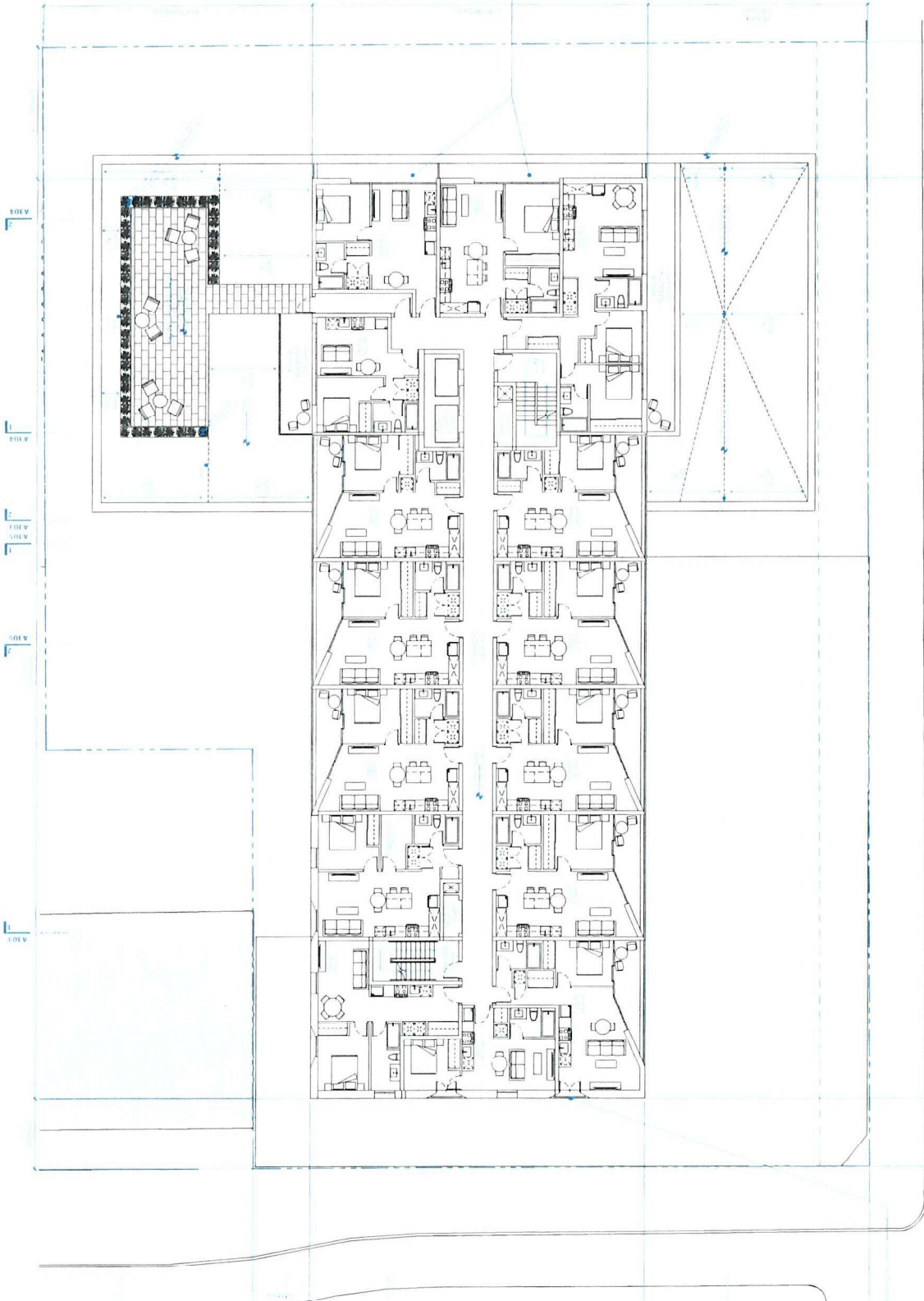
MICHAEL GREER ARCHITECTURE
1000 WEST 10TH AVENUE
CANADA, AB T2T 0N1

REVISIONS
2010-10-20 REVISION FOR REVISION
2010-09-10 REVISION FOR REVISION
2010-08-15 REVISION FOR REVISION
DATE: REVISION DESCRIPTION

PARKWAY
1000 WEST 10TH AVENUE
CANADA, AB T2T 0N1

A105

LEVELS PLAN

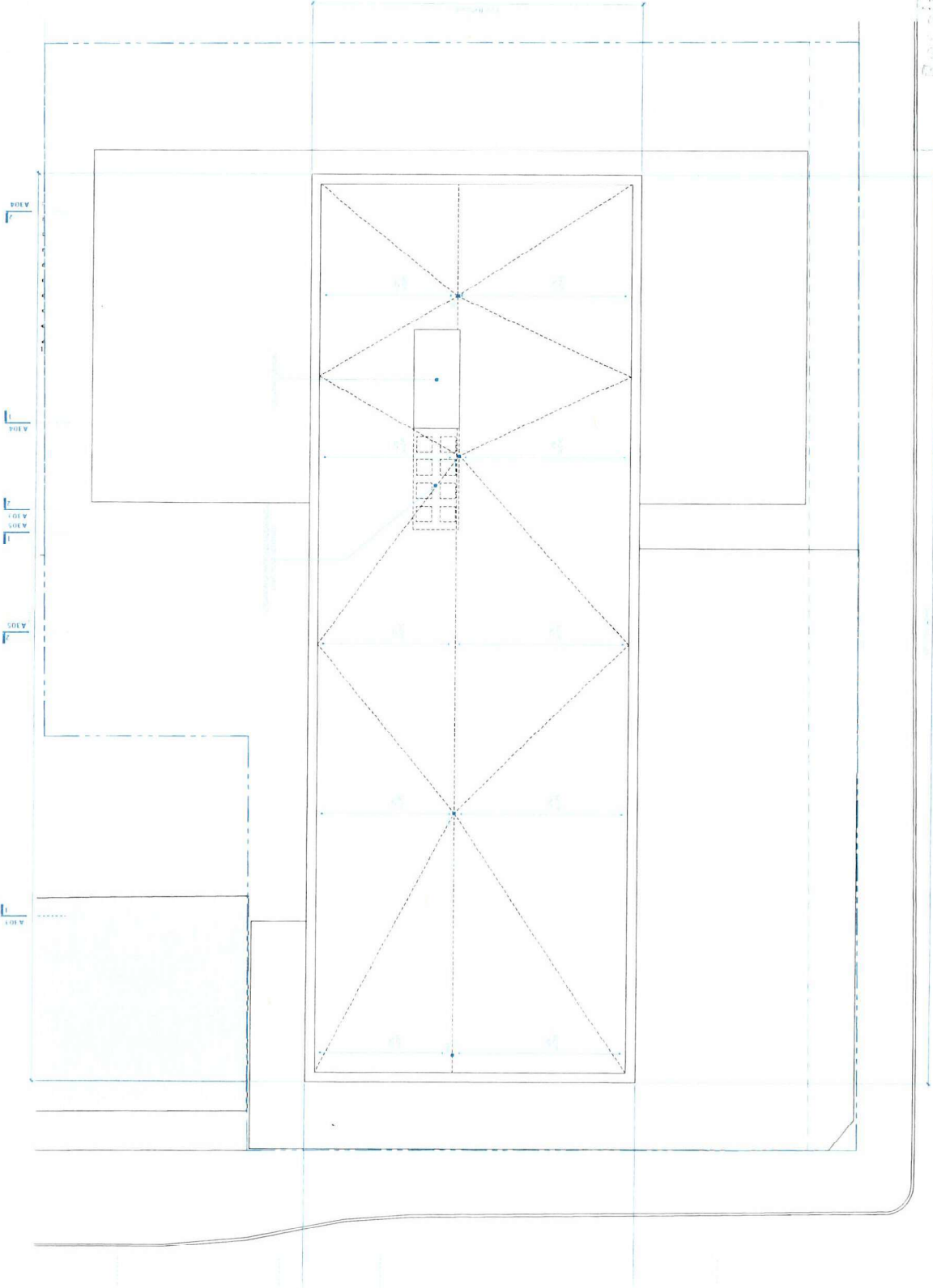


LEVELS PLAN

1000 WEST 10TH AVENUE
CANADA, AB T2T 0N1

NOV 22 2010

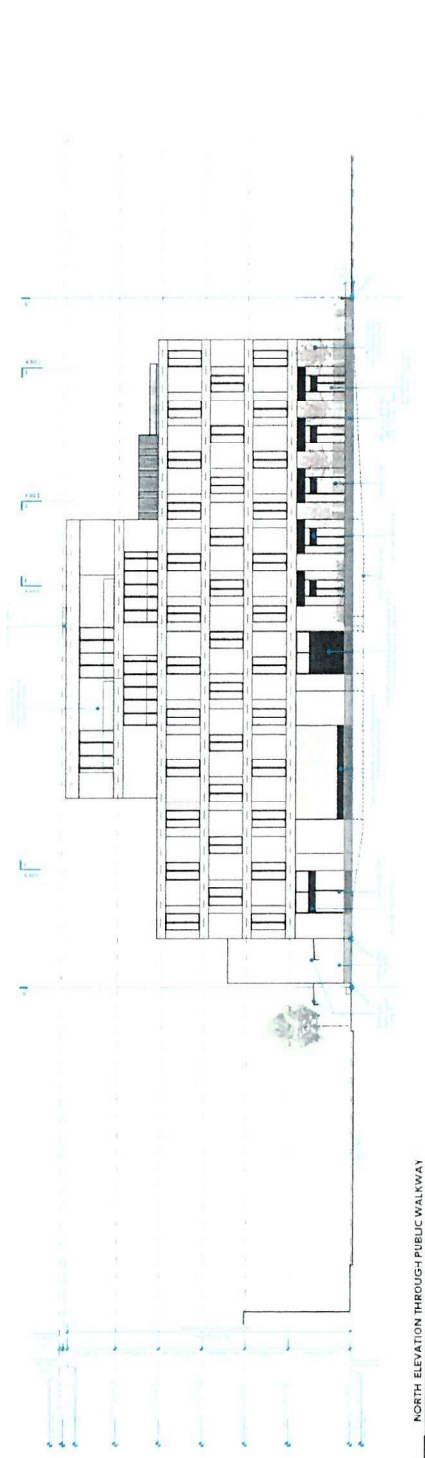
Planning & Development Department
Permitting Services Division



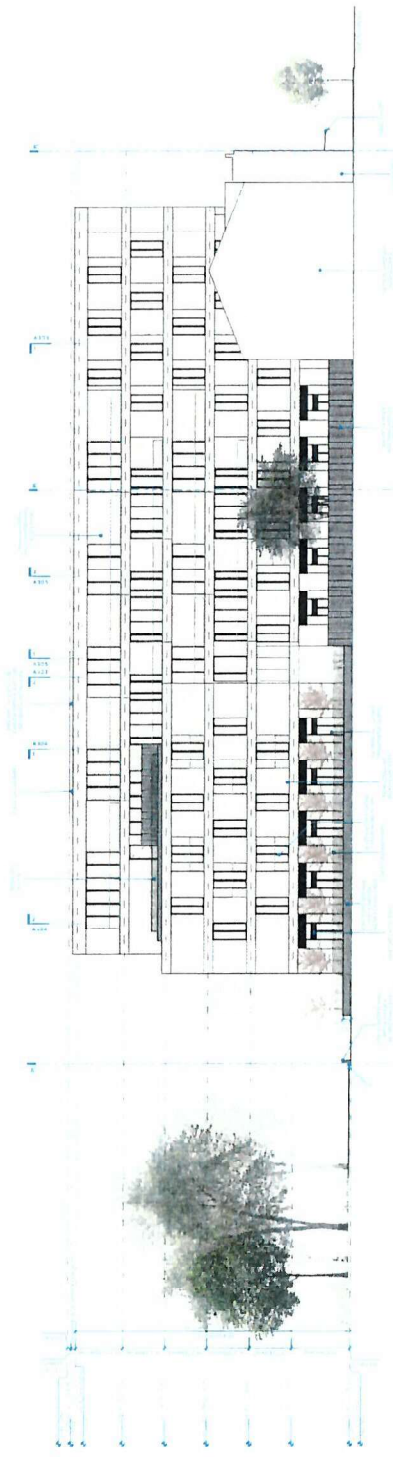
Project: A107
City of Victoria

NOV 22 2019

Planning & Development Division
Development Services Branch



NORTH ELEVATION THROUGH PUBLIC WALKWAY



WEST ELEVATION

MGA MATERIALITY

NOTES



MICHAEL GREEN ARCHITECTURE
2220 BROADVIEW, #100
CHICAGO, IL 60614

TEST	DATE	TIME
1. 100% PASS	10/10/10	10:10
2. 100% PASS	10/10/10	10:10
3. 100% PASS	10/10/10	10:10
4. 100% PASS	10/10/10	10:10
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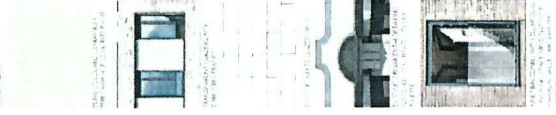
PARKWAY
1030 PANDORA AVE + PARKWAY TRIST
VICTORIA BC
2018-2011

A202

Received
City of Victoria
NOV 22 2019

MGA

MATERIALITY



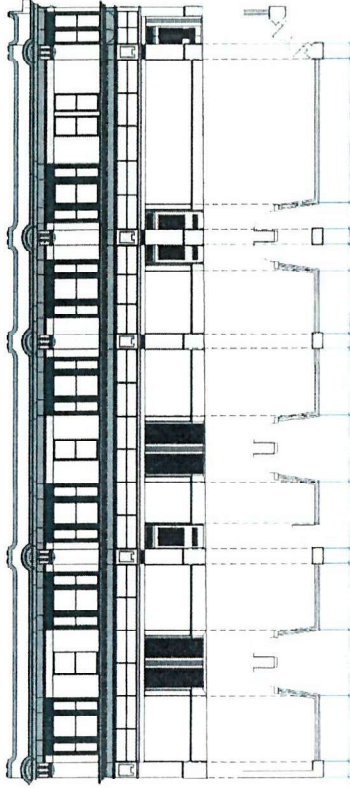
MICHAEL GREEN ARCHITECTURE
1001 N. 1ST AVE. SUITE 100
DENVER, CO 80202
TEL: 303.733.1111
WWW.MGA-ARCHITECT.COM

2ND FLOOR
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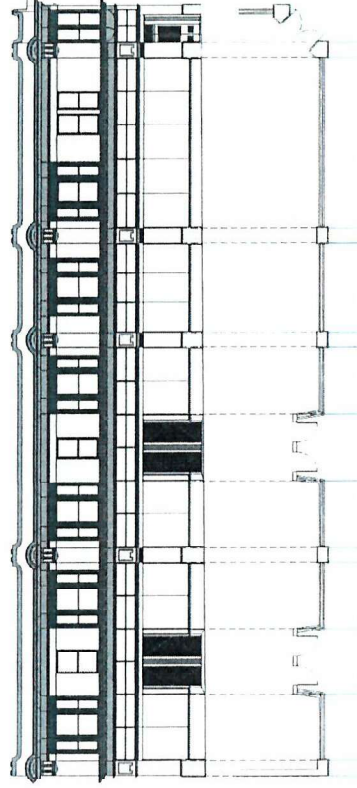
PARKWAY

1001 N. 1ST AVE. SUITE 100
DENVER, CO 80202
TEL: 303.733.1111
WWW.MGA-ARCHITECT.COM

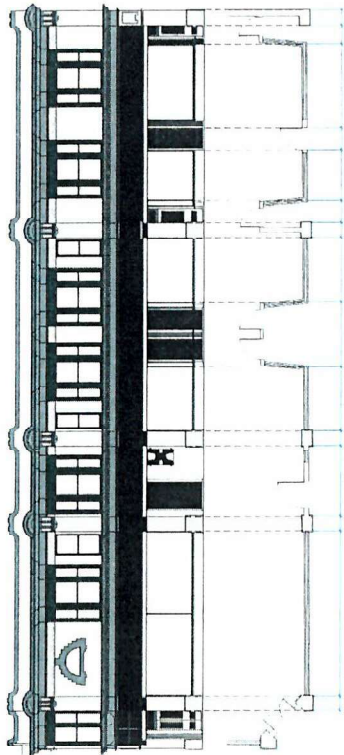
A203
HERITAGE ELEVATION STL



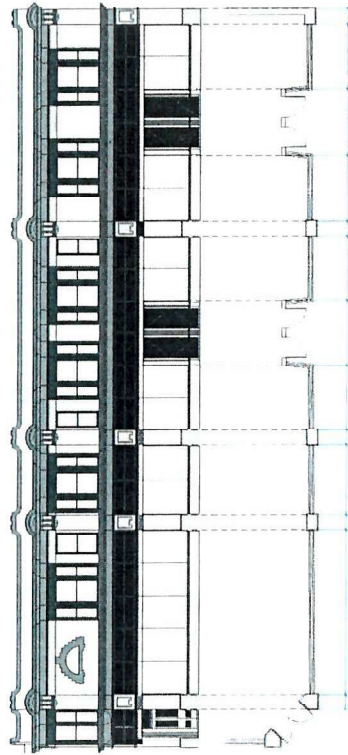
3 PANDORA AVE ORIGINAL ELEVATION STUDY



4 PANDORA AVE PROPOSED ELEVATION STUDY



COOK ST ORIGINAL ELEVATION STUDY



COOK ST PROPOSED ELEVATION STUDY

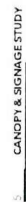


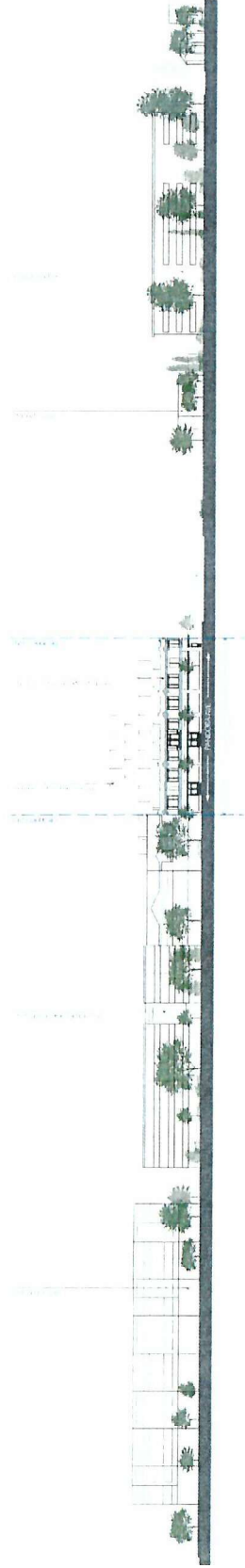
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PARKWAY

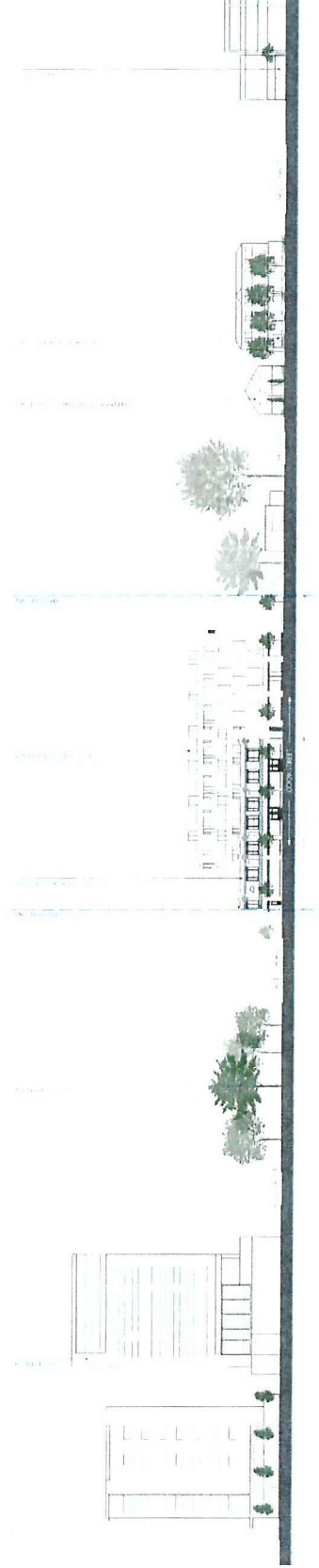
0040-4009/96/0000-0000\$05.00/0
© 1996 CIBA-GEIGY
SCIENCE

A204
FACADE STUDIES





PANDORA AVENUE STREETSCAPE



COOK STREET STREETSCAPE

Approved by
Planning & Development
November 22, 2013

MICHAEL GREEN ARCHITECTURE
1000 SEASIDE AVENUE, SUITE 200
VICTORIA, BC V8M 1A1
TEL: 250-363-1111

2013.01.02
2013.01.02
2013.01.02
DATE: 01.01.13

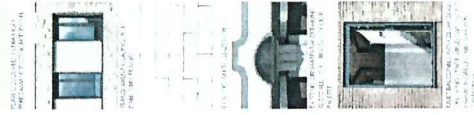
PARKWAY

1000 SEASIDE AVENUE, SUITE 200
VICTORIA, BC V8M 1A1
2013.01.02

A251
STREETSCAPE ELEVATION

MGA

MATERIALITY



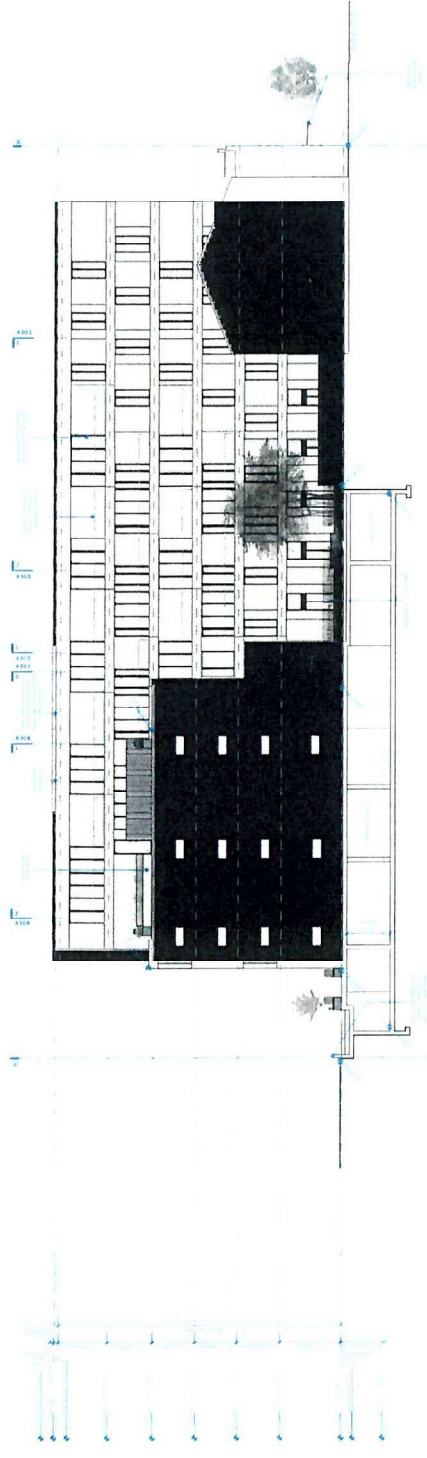
MICHAEL GREEN ARCHITECTURE
 1000 BAYVIEW AVE. SUITE 1000
 SCARBOROUGH, ONTARIO M1S 1B5
 TEL: (416) 291-1111
 WWW.MGAARCHITECTURE.COM

23-0-0-0000 NEW BUILDING
 23-0-0-0-0000 NEW BUILDING
 23-0-0-0-0000 NEW BUILDING
 DATE: 2019.11.22
 DRAWN: T. HARTMAN

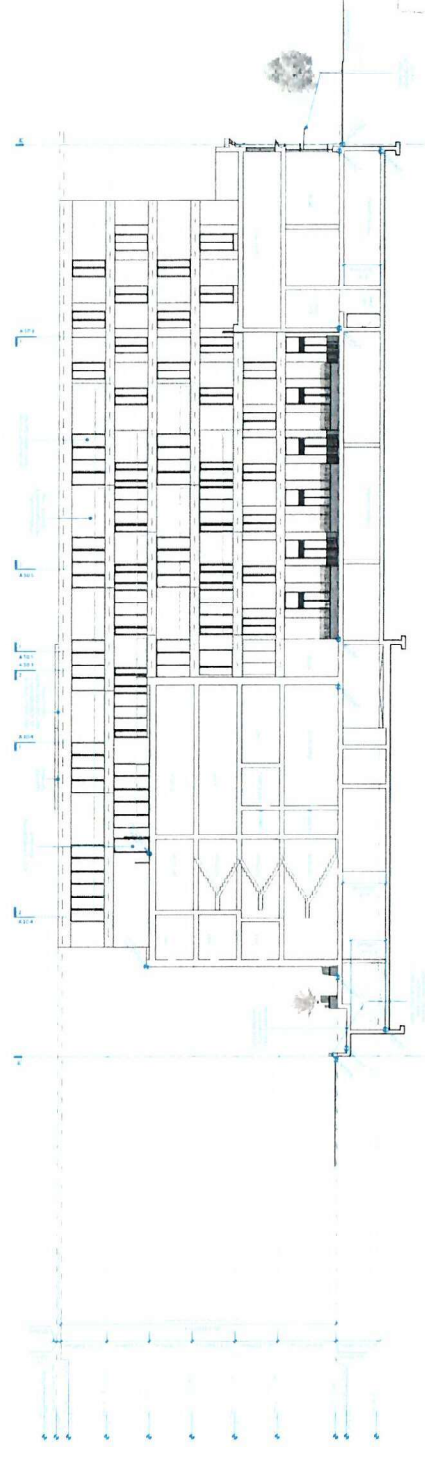
PARKWAY

1000 BAYVIEW AVE. SUITE 1000
 SCARBOROUGH, ONTARIO M1S 1B5
 TEL: (416) 291-1111
 WWW.MGAARCHITECTURE.COM

A302
 SECTIONS

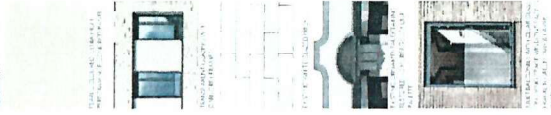


SECTION LOOKING EAST THROUGH NEW 4 STOREY VOLUME



SECTION LOOKING EAST THROUGH NEW 4 STOREY VOLUME & EXISTING BUILDING

NOV 22 2019
 Michael Green Architecture
 1000 Bayview Ave. Suite 1000
 Scarborough, Ontario M1S 1B5



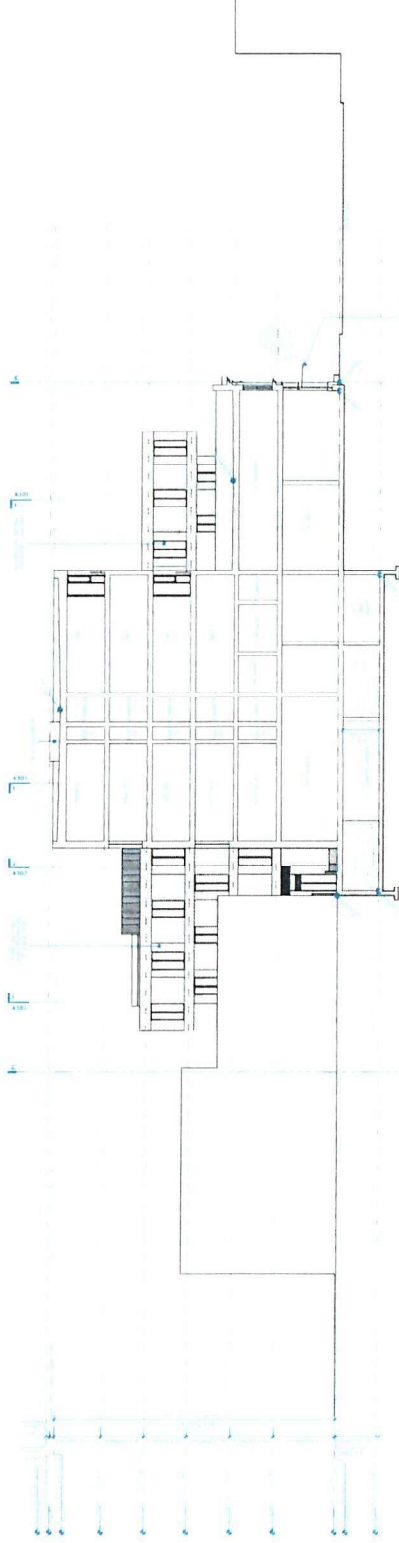
MICHAEL GREEN ARCHITECTURE
1001 PINEGROVE - BIRCHWOOD STREET
ACTON 94 NC
2018/01

2018/01/01 REVISION: REVISIONS
2018/01/01 REVISION: REVISIONS
2018/01/01 REVISION: REVISIONS
DATE: 2018/01/01
REVISION: REVISIONS

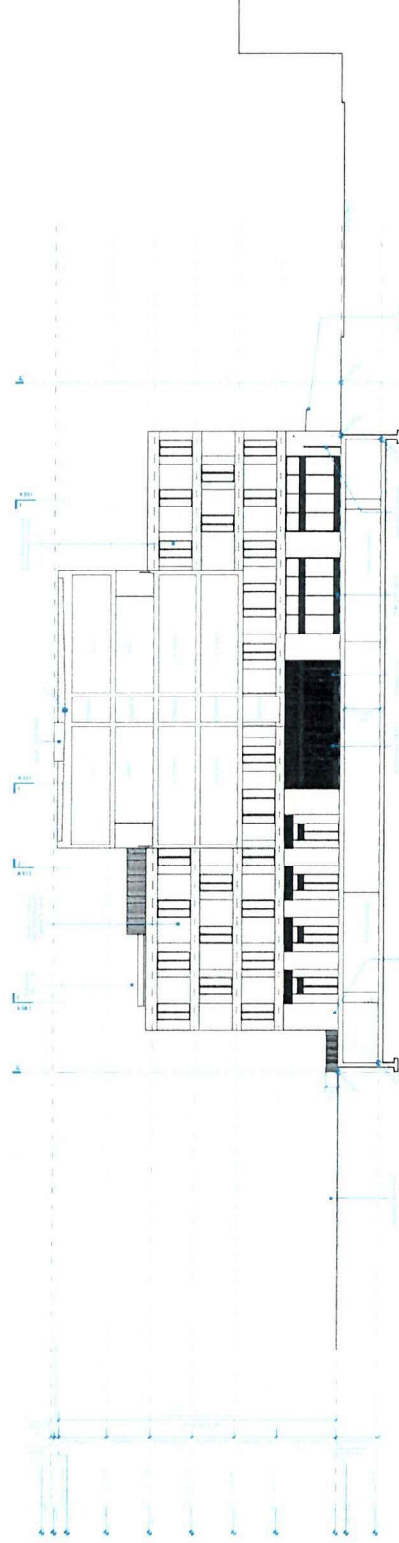
PARKWAY

1001 PINEGROVE - BIRCHWOOD STREET
ACTON 94 NC
2018/01

A303
SECTIONS



SECTION LOOKING NORTH THROUGH EXISTING BUILDING & NEW 6 STOREY VOLUME



SECTION LOOKING NORTH THROUGH NEW 6 STOREY VOLUME





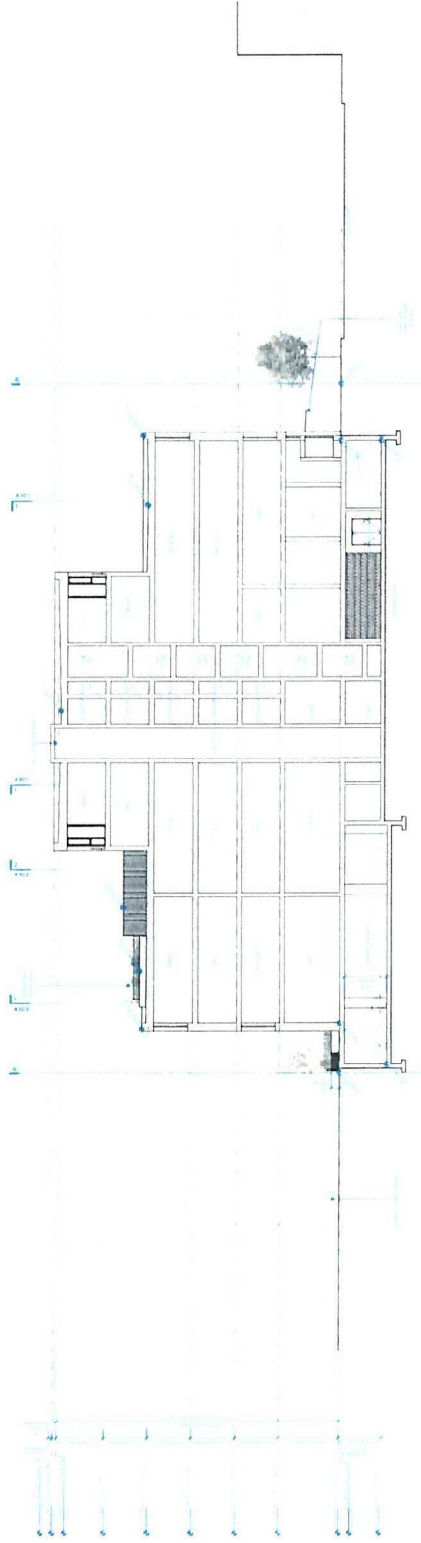
MICHAEL GREEN ARCHITECTURE
1000 10th Avenue, Suite 100
Vancouver, BC V6Z 1B1

2016-03-01 REVISIONS
2016-03-01 REVISIONS
2016-03-01 REVISIONS
DATE REVISION DESCRIPTION

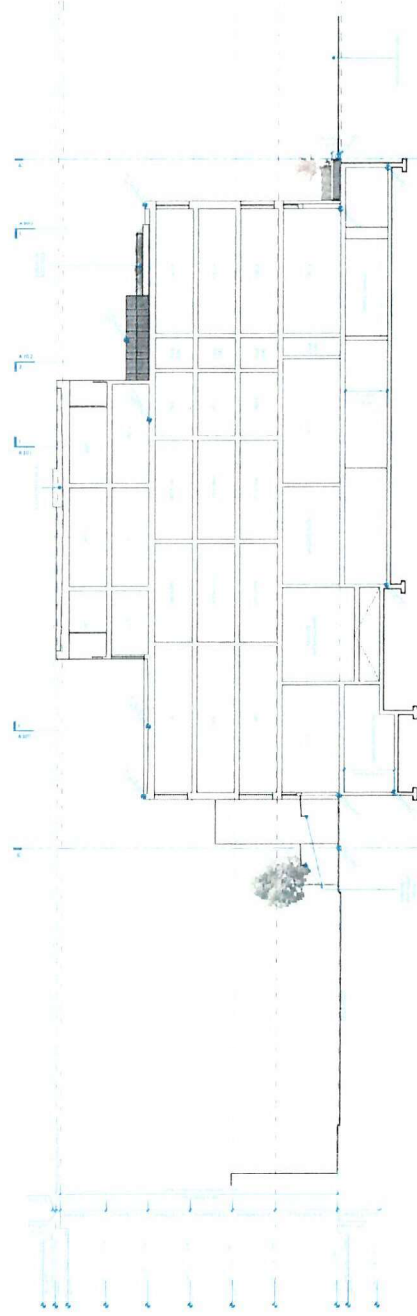
PARKWAY

1000 10th Avenue, Suite 100
Vancouver, BC V6Z 1B1
2016-03-01

A304
SECTIONS



SECTION LOOKING NORTH THROUGH NEW 4 & 5 STOREY VOLUME



SECTION LOOKING SOUTH THROUGH NEW 4 & 5 STOREY VOLUME

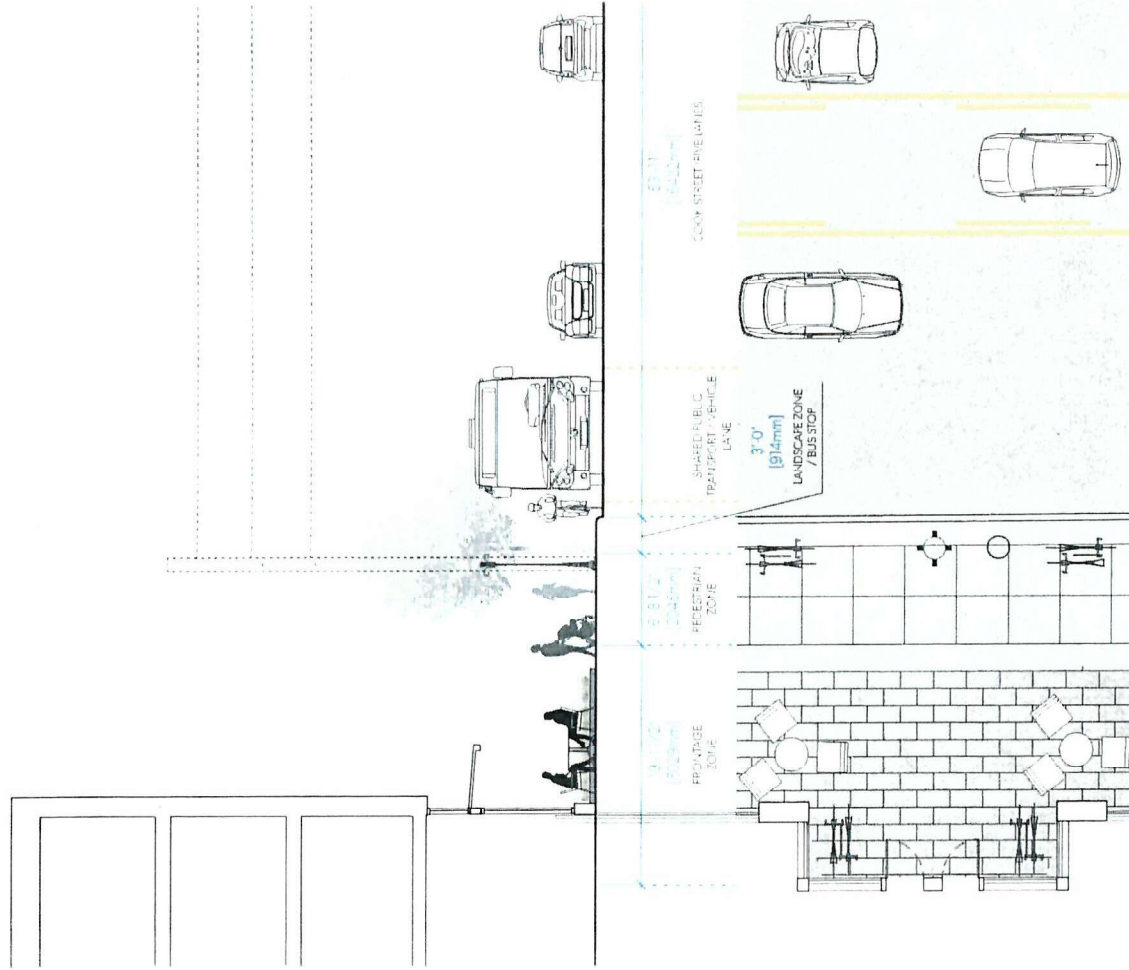




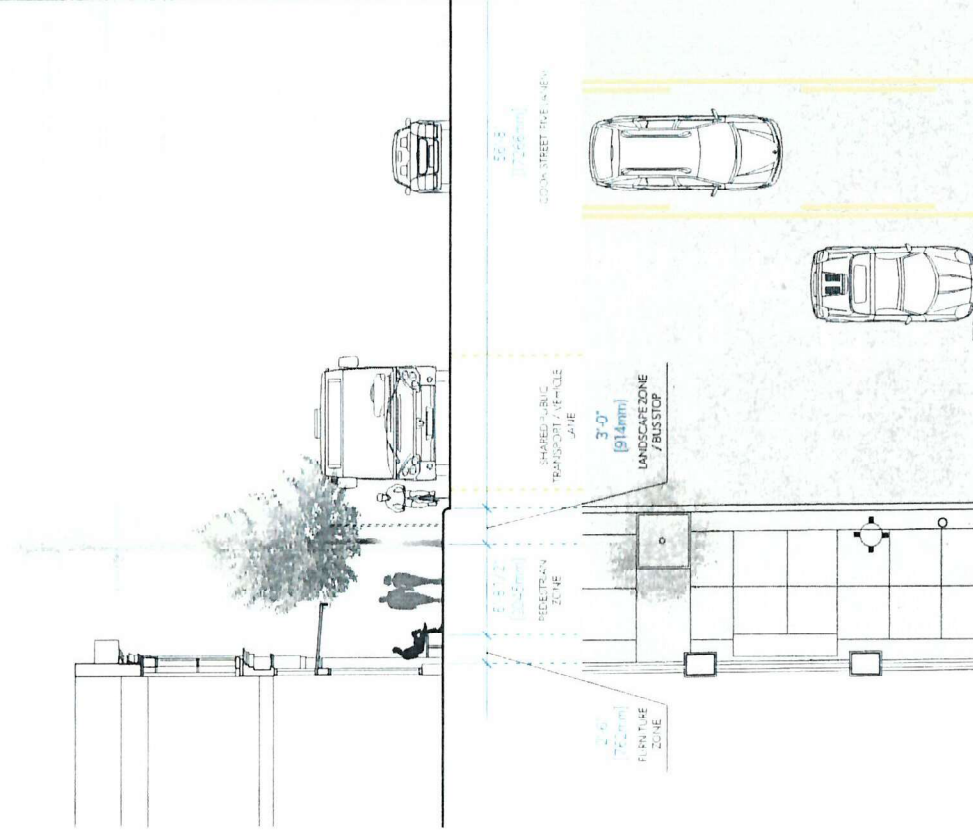
PARKWAY

A305



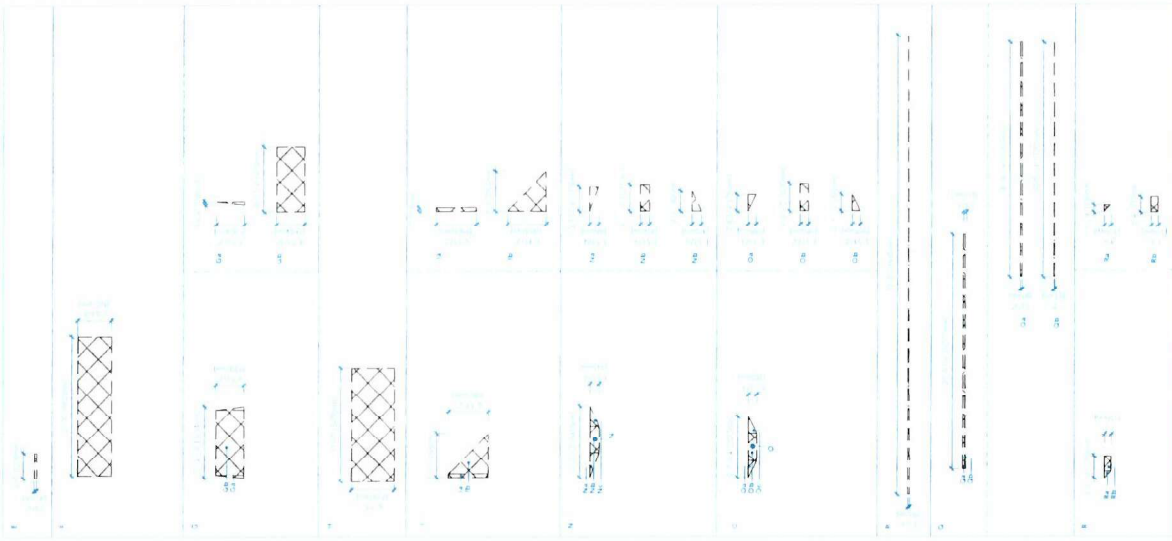
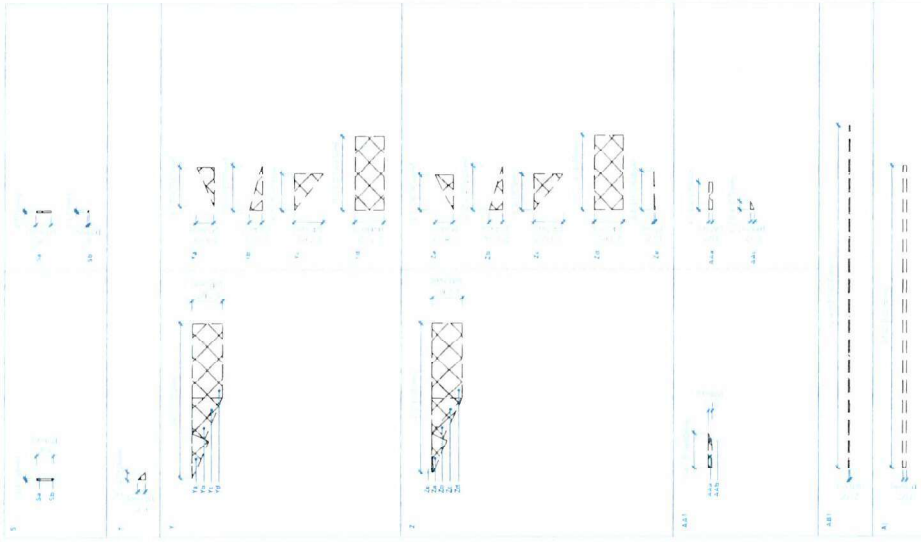
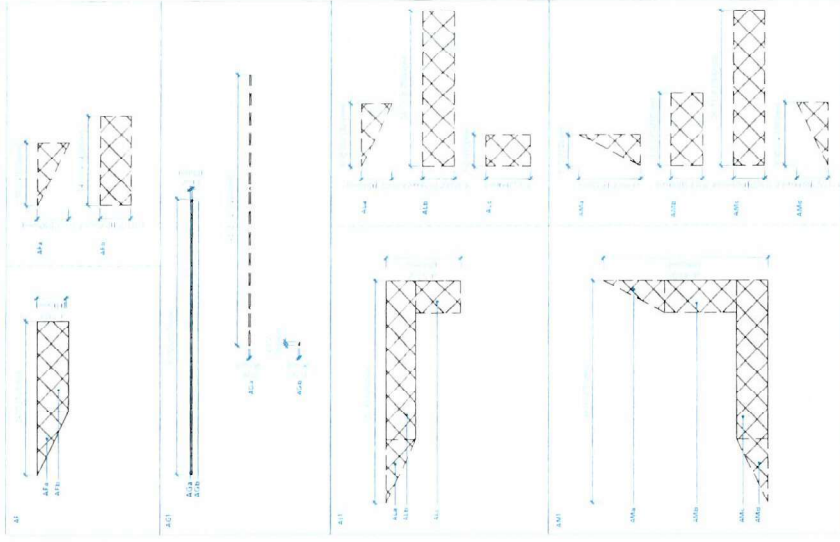


STREETSCAPE SECTION & PLAN THRU COMMERCIAL



2. STREETSCAPE SECTION & PLANT THRU CAFE

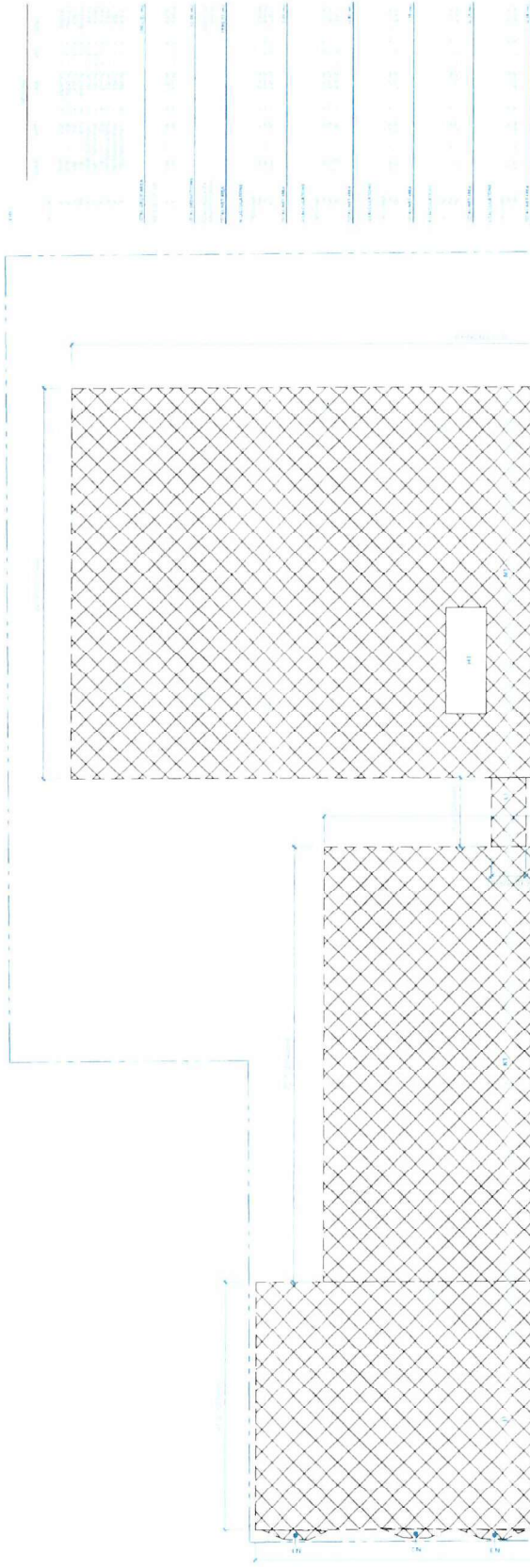
1. CALCULATIONS LEGEND



NOV 22 2019
Planning & Development Department
Building & Planning Services Unit



FSR LEVEL 1



MICHAEL GREEN ARCHITECTURE
 1005 PANDORA AVENUE, SUITE 100
 VANCOUVER, BC V6B 1R8

2019-10-10 REVISION FOR REVISION
 2019-10-13 REVISION FOR REVISION
 2019-10-15 STUDY FOR REVISION
 DATE REVISION DESCRIPTION

PARKWAY

1005 PANDORA AVENUE, SUITE 100
 VANCOUVER, BC V6B 1R8
 2019-10-10

A802
 FSR LEVEL 2

FSR LEVEL 2
 100

1005 PANDORA AVENUE, SUITE 100
 VANCOUVER, BC V6B 1R8
 2019-10-10

FSR LEVEL 3

DATE	2018.05.03	REVISION	1	DESCRIPTION	ISSUED FOR RECORDING
DATE	2018.05.13	REVISION	2	DESCRIPTION	REVISED FOR RECORDING
DATE	2018.05.15	REVISION	3	DESCRIPTION	REVISED FOR RECORDING
DATE	2018.05.15	REVISION	4	DESCRIPTION	REVISED FOR RECORDING
DATE	2018.05.15	REVISION	5	DESCRIPTION	REVISED FOR RECORDING
DATE	2018.05.15	REVISION	6	DESCRIPTION	REVISED FOR RECORDING
DATE	2018.05.15	REVISION	7	DESCRIPTION	REVISED FOR RECORDING
DATE	2018.05.15	REVISION	8	DESCRIPTION	REVISED FOR RECORDING
DATE	2018.05.15	REVISION	9	DESCRIPTION	REVISED FOR RECORDING
DATE	2018.05.15	REVISION	10	DESCRIPTION	REVISED FOR RECORDING

MICHAEL GREEN ARCHITECTURE
1000 WEST 1ST AVENUE
VANCOUVER, BC
CANADA V6J 1P8

2018.05.03 ISSUED FOR RECORDING
2018.05.13 REVISED FOR RECORDING
2018.05.15 REVISED FOR RECORDING
2018.05.15 REVISED FOR RECORDING

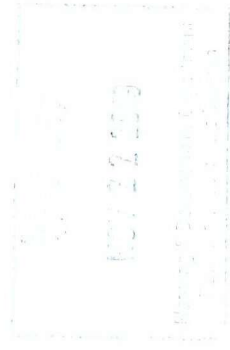
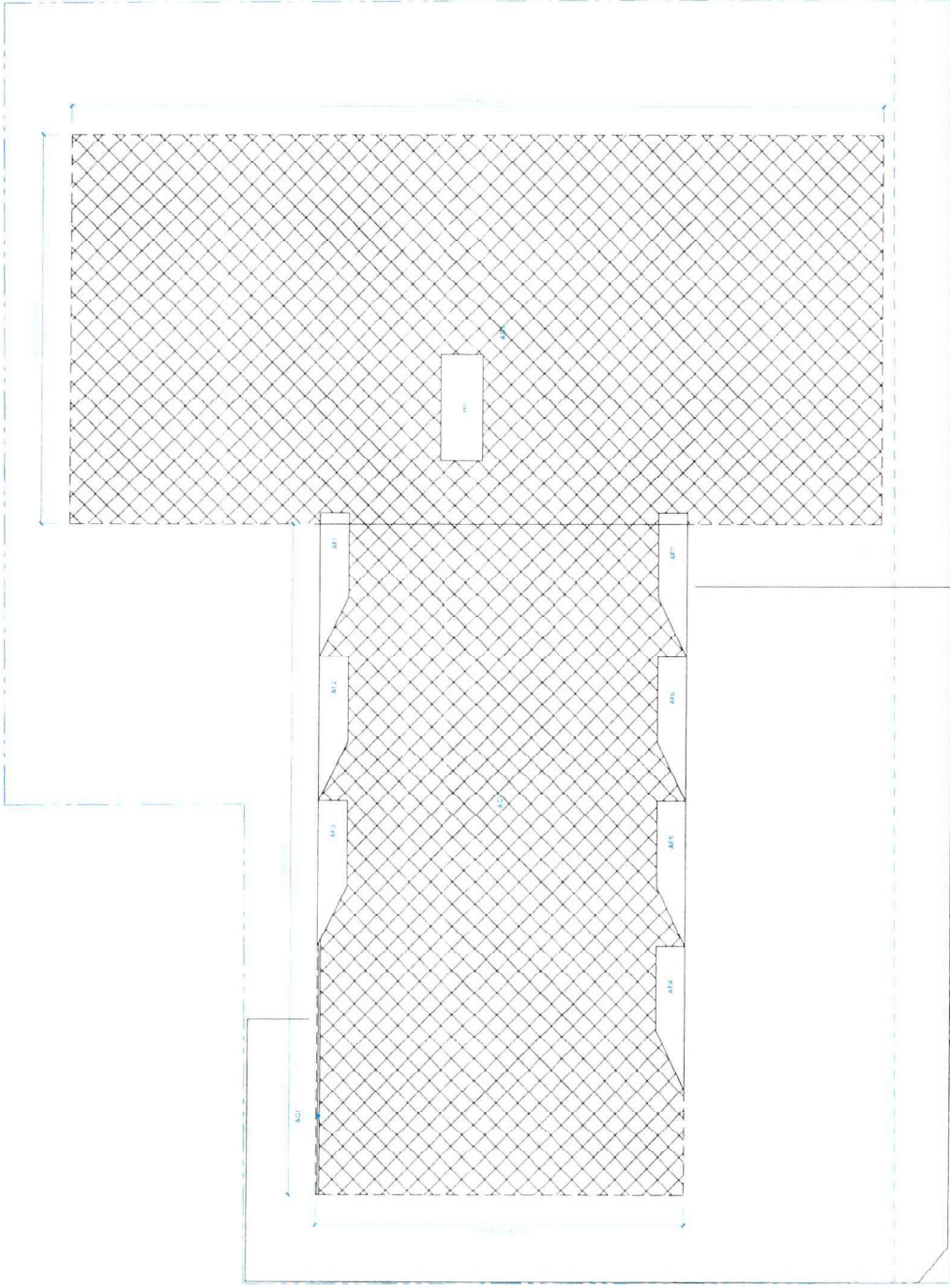
DATE REVISION DESCRIPTION

PARKWAY

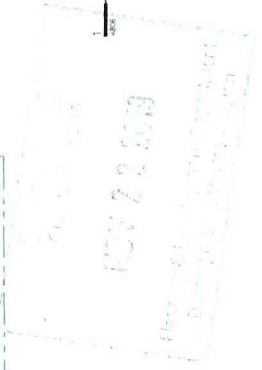
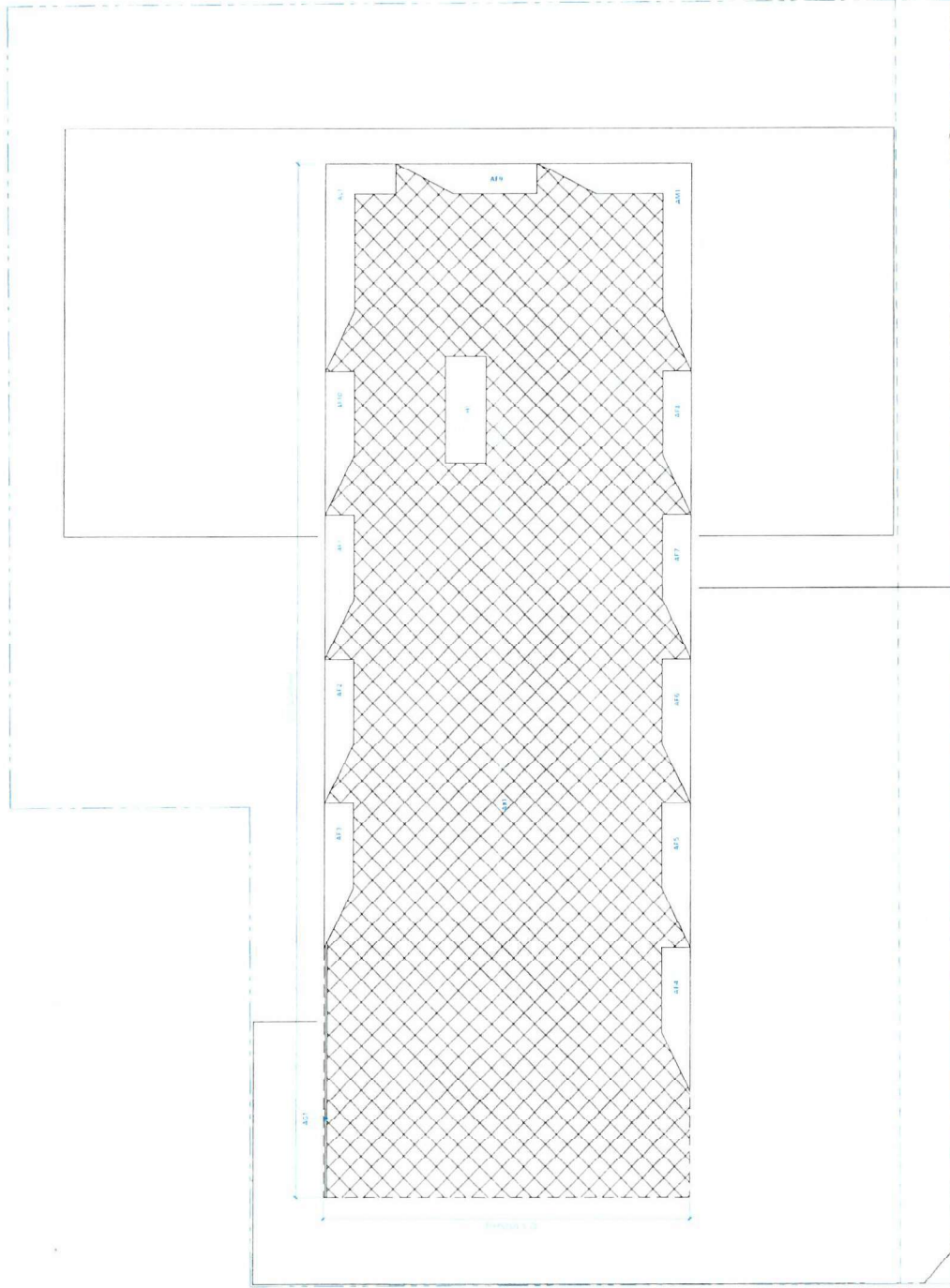
1000 PARKWAY AVE • 18000A STREET
VANCOUVER, BC
2018.05.15

A804
FSR LEVEL 4

FSR LEVEL 4
1:100



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2	REVISION	2019-09-19	REVISION
3	REVISION	2019-10-30	REVISION





MICHAEL GREEN ARCHITECTURE
100 HANCOCK AVENUE, SUITE 200
VICTORIA, BC V8P 1X1

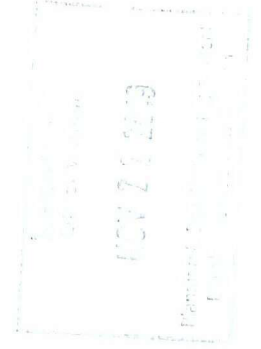
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2014-03-03 REVIEWED BY MICHAEL GREEN
2014-03-03 REVIEWED BY MICHAEL GREEN
DATE REVIEWED BY MICHAEL GREEN

PARKWAY

100 HANCOCK AVENUE, SUITE 200
VICTORIA, BC V8P 1X1

A811
3D RENDERS

VIEW NORTHWEST FROM CORNER OF PANDORA & COOK





MICHAEL GREEN ARCHITECTURE
1155 W. 3RD AVE. SUITE 200
VANCOUVER, BC V6C 3E8

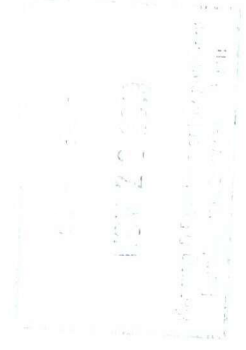
2015-1512 REBID FOR RENOVATION
2015-09-13 REBID FOR RENOVATION
2015-03-14 REBID FOR RENOVATION
DATE REVISED: 04/07/15

PARKWAY

1055 AND 800 W. 11TH ST. WEST
VICTORIA, BC
2018-201

A812
3D RENDERS

VIEW WEST THROUGH RESIDENTIAL MEWS





MICHAEL GREEN ARCHITECTURE
1001 HANCOCKWAY #1000 • VICTORIA, BC
V8N 2K1

2016-2020
2016-2020
2016-2020
2016-2020
DATE: REVISION: 02/20/2020

PARKWAY

1001 HANCOCKWAY #1000 • VICTORIA, BC
V8N 2K1

A813
3D RENDERS



VIEW SOUTHWEST FROM COOK STREET

1001 HANCOCKWAY #1000
VICTORIA, BC
V8N 2K1



MICHAEL GREEN ARCHITECTURE
1000 10TH AVENUE, SUITE 1000
DENVER, CO 80202

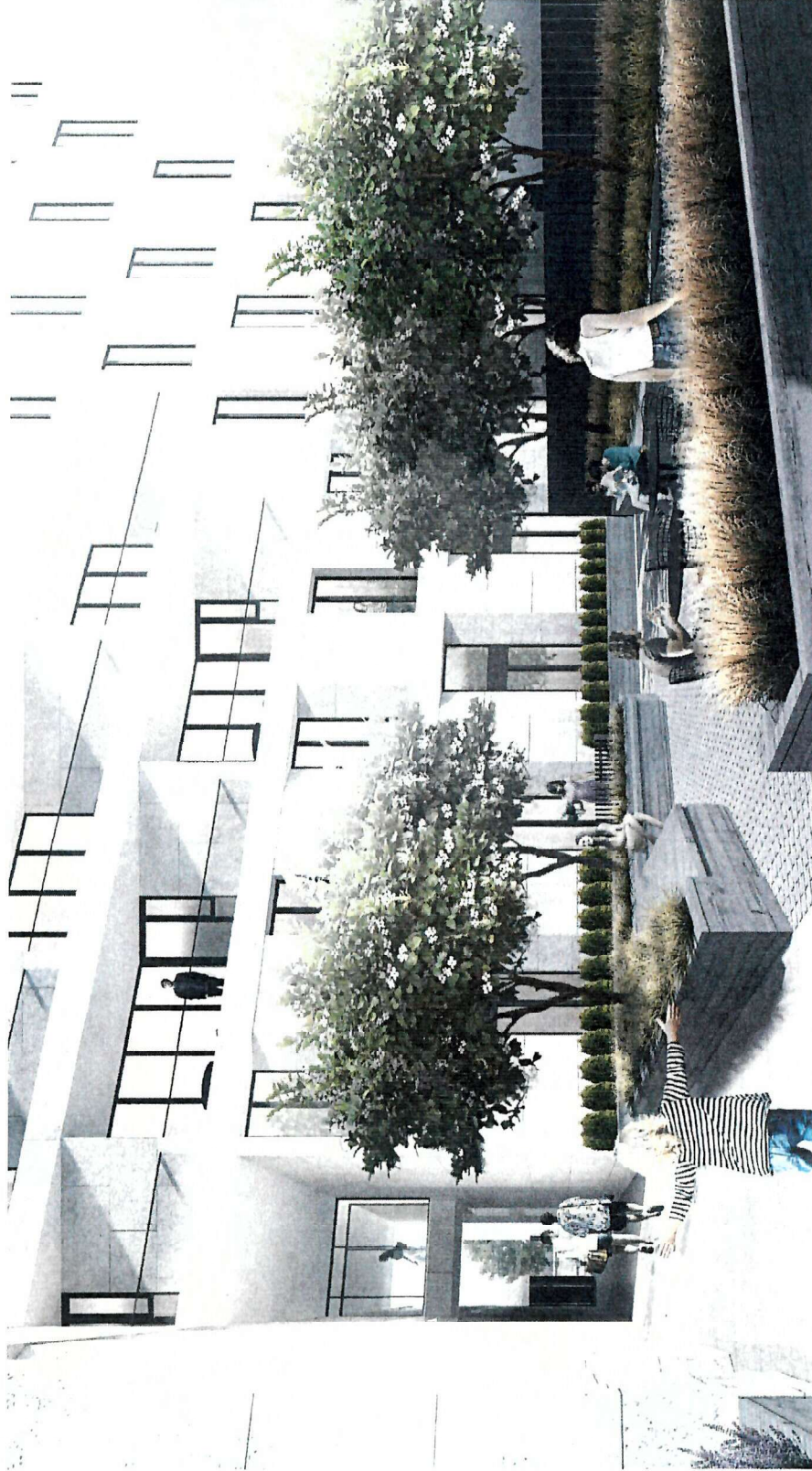
2019.03.31 REVISIONS
2019.03.31 REVISIONS
2019.03.31 REVISIONS
DATE REVISIONS DESCRIPTION

PARKWAY

1000 10TH AVENUE, SUITE 1000
DENVER, CO 80202

A814
3D RENDERS

VIEW EAST TO COURTYARD





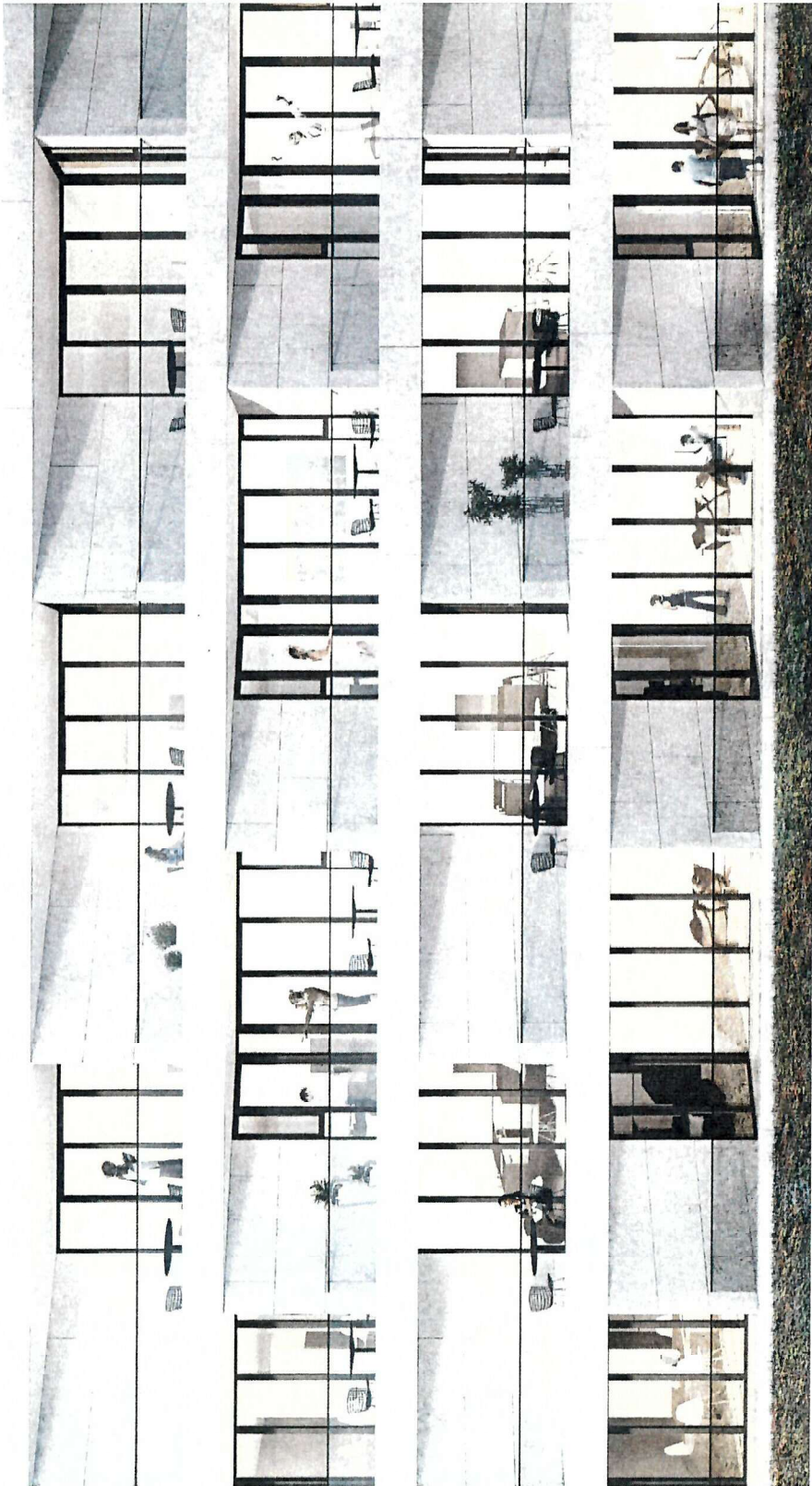
MICHAEL GREEN ARCHITECTURE
1111 BROADWAY, SUITE 200
SAN FRANCISCO, CA 94103

2016.10.10
2016.10.10
2016.10.10
DATE
REVISION
REVISION

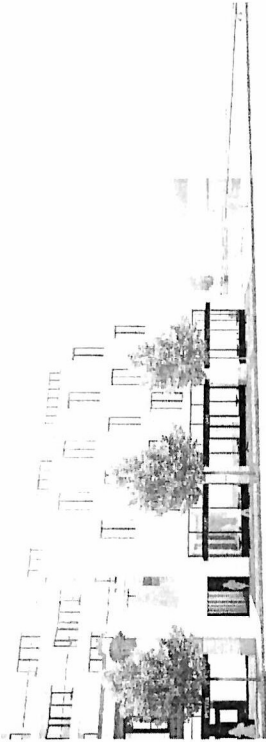
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SAN FRANCISCO, CA 94103

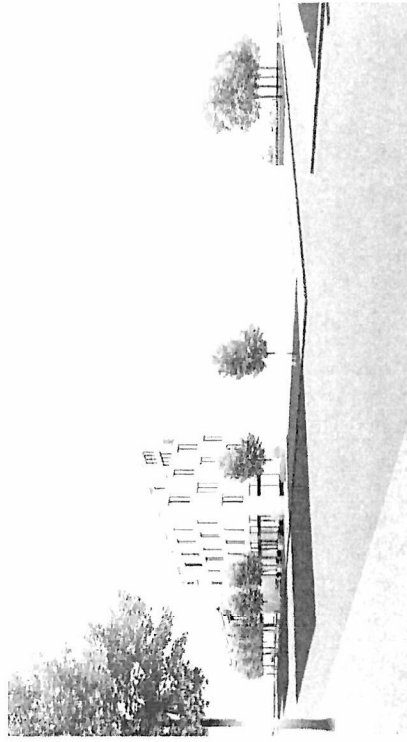
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3D RENDERS



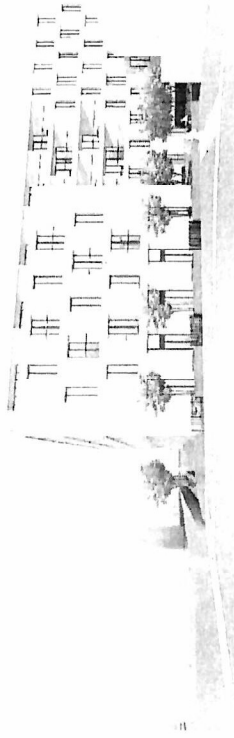
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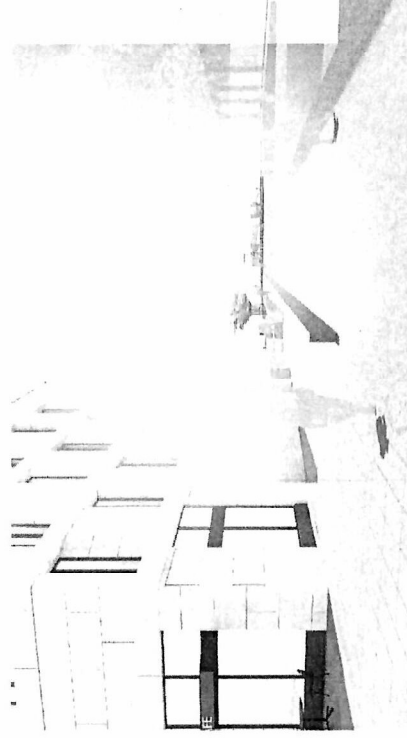
1 ADJACENT PROPERTY STUDY | VIEW WEST ACROSS COOK STREET



2 ADJACENT PROPERTY STUDY | VIEW SOUTHWEST DOWN COOK STREET



3 ADJACENT PROPERTY STUDY | VIEW EAST ACROSS FRANKLIN GREEN PARK



4 ADJACENT PROPERTY STUDY | VIEW WEST ALONG NORTH WALKWAY



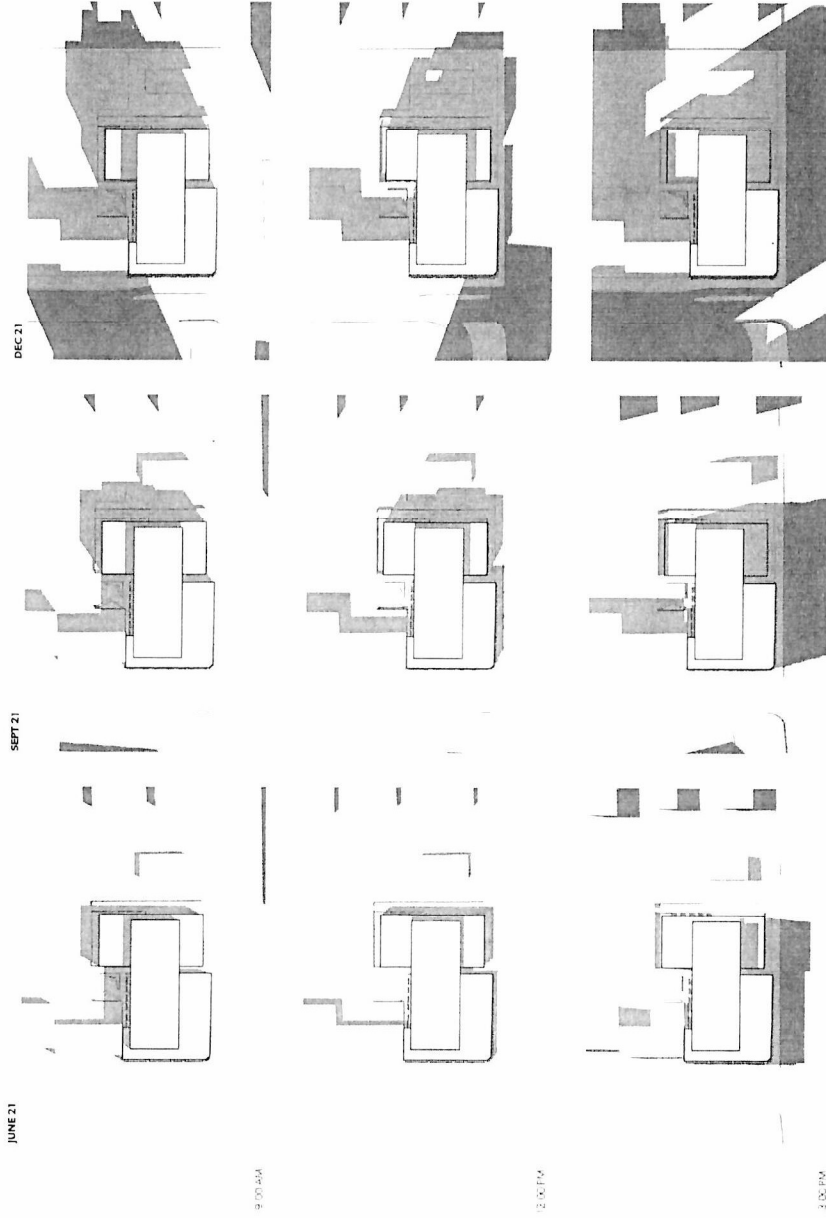
MICHAEL GREEN ARCHITECTURE
554 DOWNSIDE AVENUE
CANADA V6T 1R8

2019-10-30 REVISION FOR REZONING
2019-09-13 REVISION FOR REZONING
2019-05-15 REVISION FOR REZONING
DATE REVISION DESCRIPTION

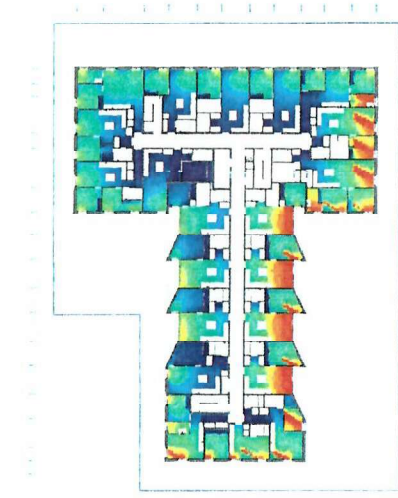
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1050 HANCOCK AVENUE • 1318 COOK STREET
V6T 1C6A BC
2018-01-01

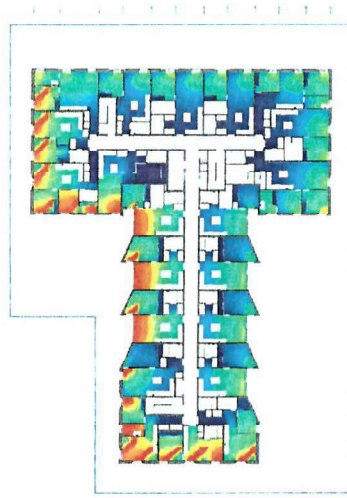
A817
ADJACENT PROPERTY
STUDY



Planning & Development
BCV 22209
SOLAR IMPACT ANALYSIS



LEVEL 4, 9:00 AM, 34.83 (LUX)



LEVEL 4, 3:00 PM, 34.83 (LUX)



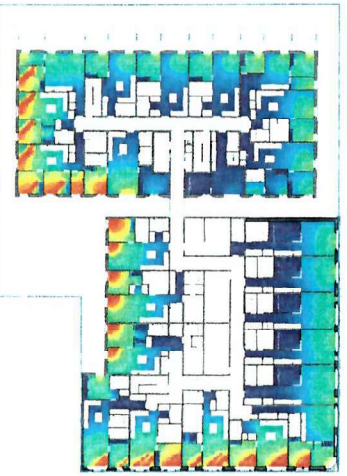
LEVEL 3, 9:00 AM, 24.67 (LUX)



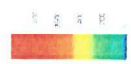
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LEVEL 2, 9:00 AM, 14.50 (LUX)



LEVEL 2, 3:00 PM, 14.50 (LUX)



MICHAEL GREEN ARCHITECTURE
1335 N. 3RD AVE. #100, CHICAGO, IL 60610
312.467.1000

PROJECT: A822
DATE: 08/07/14
DESIGNER: MGA

PARKWAY
1335 N. 3RD AVE. #100, CHICAGO, IL 60610
312.467.1000

A822

Illuminance Analysis

456 A.L.



REV	REV DATE	REVISION DESCRIPTION	DR
0	10-MAY-21	REV PER CITY COMMENTS	BA
7	10-OCT-23	REV LANDING FLOOR PLAN	DO
8	10-OCT-23	REVISION	DO
9	10-OCT-21	REV SITE PLANNING REQUEST	DO
10	10-OCT-21	REV CD SET	BA
11	10-OCT-27	REV PER CITY COMMENTS	BA
12	10-SEP-27	REV PER CITY COMMENTS	BA
13	21-AUG-19	REV PER CITY COMMENTS	BA
14	10-AUG-19	REV PER CITY COMMENTS	BA

PROJECT

**PARKWAY
MIXED USE DEVELOPMENT
11050 PANDORA AVENUE
VICTORIA, BC**

DRAWING THE LINE

LANDSCAPE
PLAN[illegible]

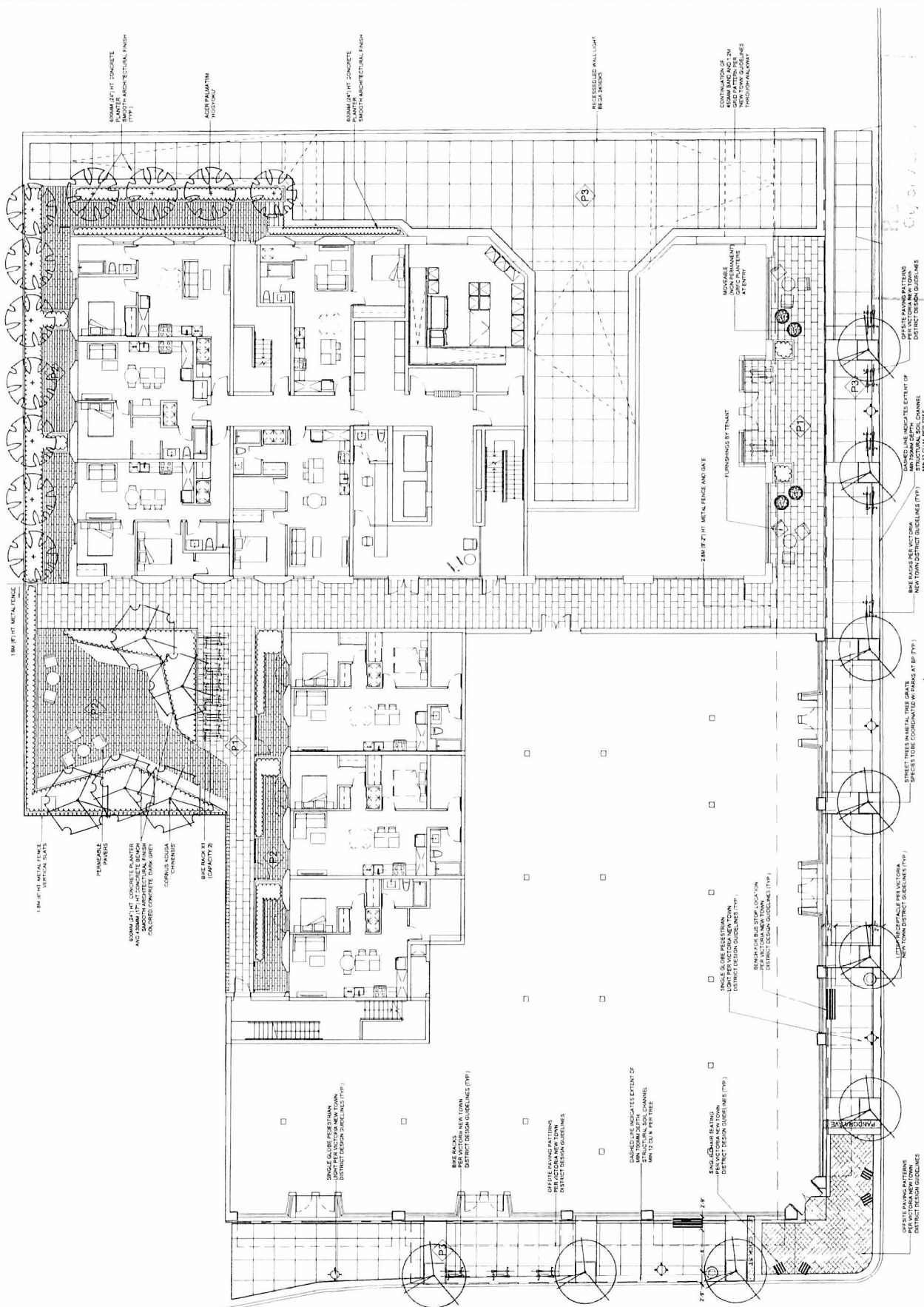
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930

18240

PMG PROJECT NUMBER:

18240



2017

NO	DATE	REVISION DESCRIPTION	DR
1	15-MAY-21	REV. PER CITY COMMENTS	BA
2	18-OCT-21	NEW GRADING FLOOR PLANS	DO
3	18-OCT-21	PER PLAN	DO
4	18-OCT-21	NEW SITE PLACEMENT RE-USE 1ST	DO
5	18-OCT-21	MAIN EX-ENT	BA
6	25-MAY-21	RE-ZONING	BA
7	21-AUG-20	REV. PER CITY/CLERK COMMENTS	BA
8	18-MAY-20	3RD EX SUBMISSION	BA

PROJECT

PARKWAY
MIXED USE DEVELOPMENT
1050 PANDORA AVENUE
VICTORIA, BC

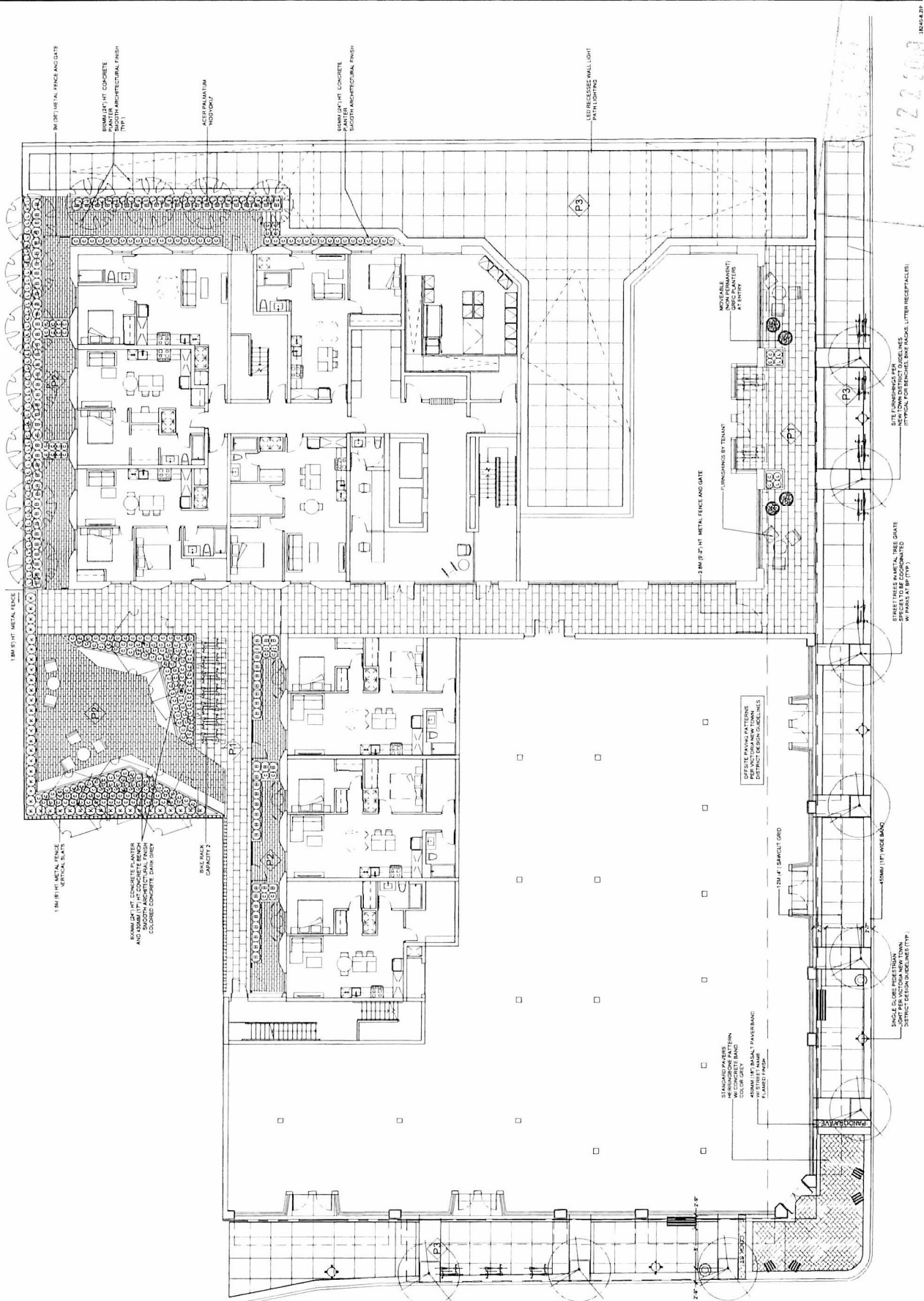
DRAWING TITLE
SHRUB
PLAN

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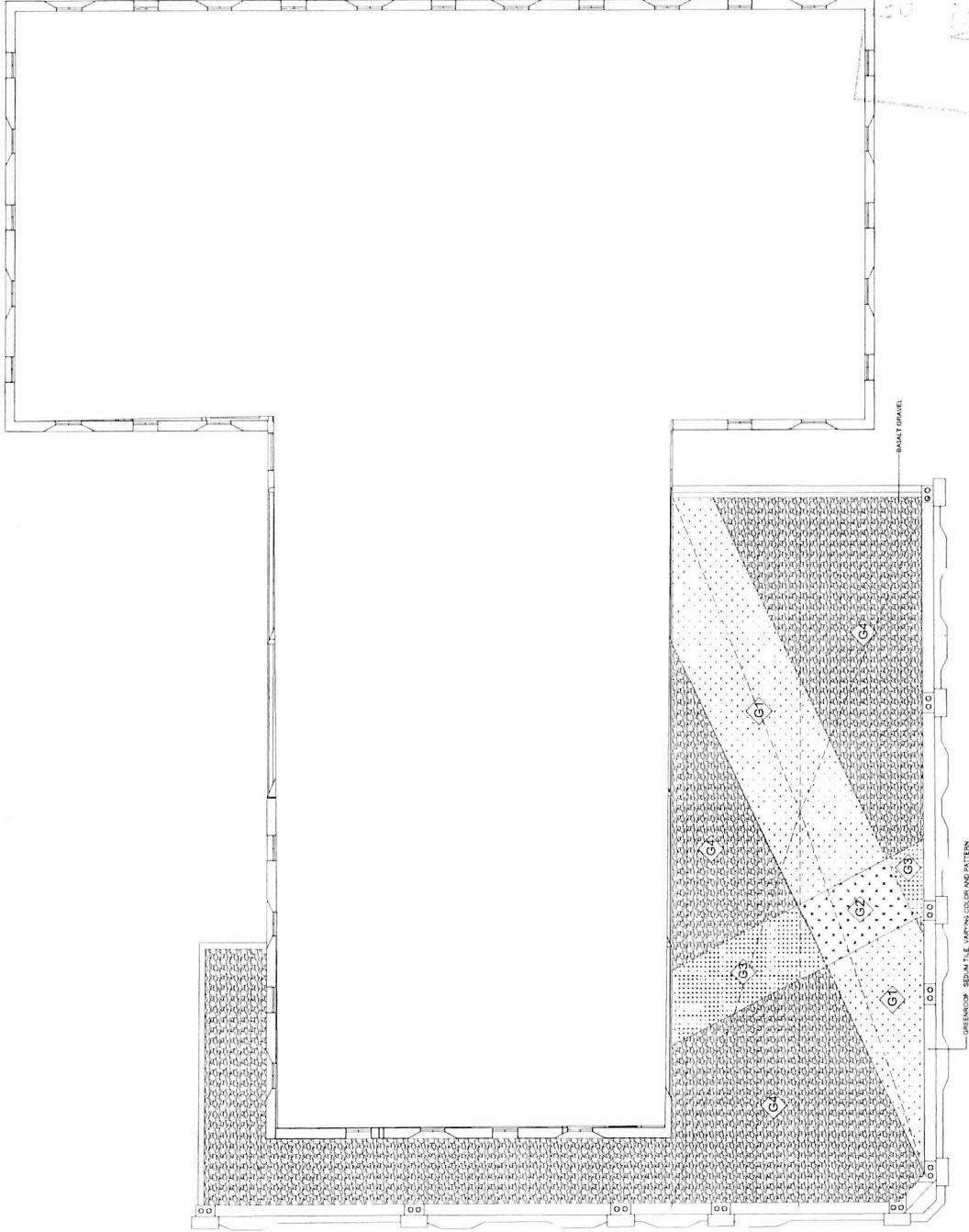
PMG PROJECT NUMBER: 18240



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pmg
LANDSCAPE
ARCHITECTS
SUNSHINE COAST
Suite C100 - 4185 Dill Drive
Bundaberg, QLD 4670
P: (07) 2460711 F: (07) 2460722

SCALE



NO	DATE	REVISION/DESCRIPTION	BY
1	19 JUL 19	REV. PRELIMINARY	SA
2	19 JUL 19	REV. PRELIMINARY	SA
3	19 JUL 19	REV. PRELIMINARY	SA
4	19 JUL 19	REV. PRELIMINARY	SA
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14	19 JUL 19	REV. PRELIMINARY	SA
15	19 JUL 19	REV. PRELIMINARY	SA
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CLIENT

PROJECT

PARKWAY
MIXED USE DEVELOPMENT
1050 PANDORA AVENUE
VICTORIA, 3C

DRAWING TITLE

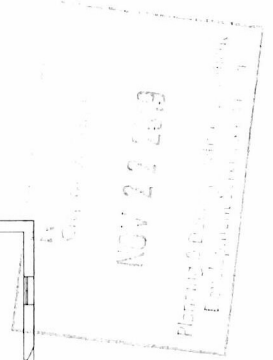
LEVEL 3
ROOF LANDSCAPE

DATE 19 JUL 19
SCALE 1:100
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CHECKED
PMG PROJECT NUMBER 18240

L4

OF 5

18240



NO	DATE	REVISION DESCRIPTION	DR
1	10-MAY-11	REV. PER CITY COMMENTS	SA
2	15-OCT-13	REV. BASED ON FLOOR PLAN	DR
3	15-OCT-12	REV. PLAN	DR
4	15-OCT-11	REV. PER PLANNING REQUEST	DR
5	15-OCT-03	ISSUED SET	SA
6	10-MAY-17	REWORKING	SA
7	21-JAN-19	REV. PER CITY/CLERK COMMENTS	SA
8	20-NOV-20	ISSUE FOR SUBMISSION	SA

PROJECT

PARKWAY
MIXED USE DEVELOPMENT
1050 PANDORA AVENUE
VICTORIA, BC

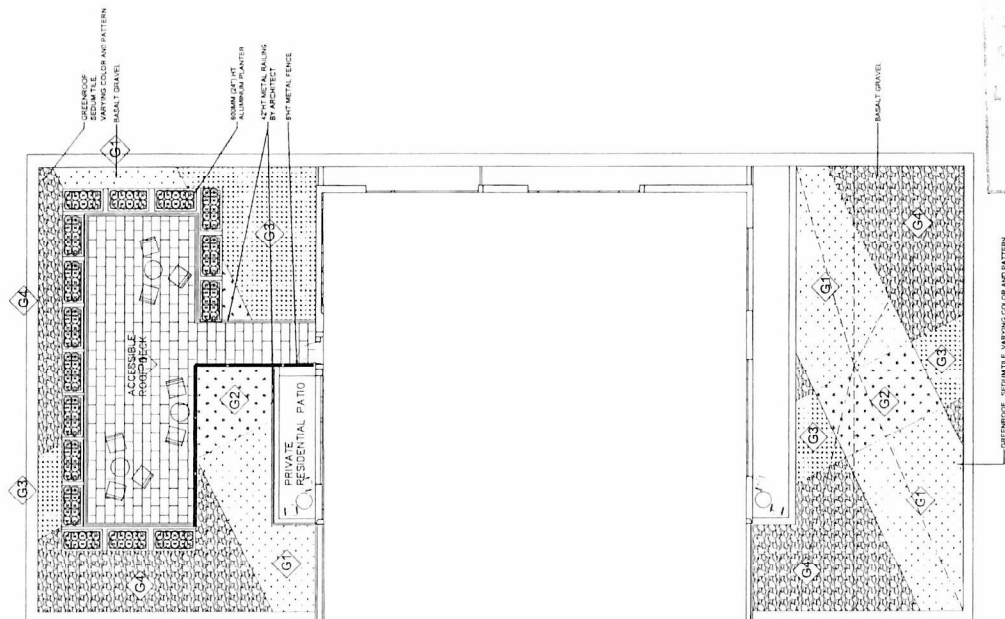
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LEVEL 5
ROOF LANDSCAPE

DATE 19 JUL 10
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DRAWING NUMBER 18240

PROJECT NUMBER

18240



REFERENCE: KETIMATS E. VARYING CO₂ AND SATURD

City of Victoria

2017

974087P

124042P

PLANT SCHEDULE

[illegible][illegible]

1. *Explain the term "T-shaped" and provide a brief definition of its components.*
2. *Identify the two main components of a T-shaped person and describe their importance.*
3. *Discuss the role of a T-shaped person in a team and how they contribute to the team's success.*
4. *Explain the importance of a T-shaped person in a company and how they can help the company achieve its goals.*
5. *Describe the benefits of being a T-shaped person and how they can be achieved.*
6. *Identify the challenges of being a T-shaped person and how they can be overcome.*
7. *Explain the importance of a T-shaped person in a company and how they can help the company achieve its goals.*
8. *Describe the benefits of being a T-shaped person and how they can be achieved.*
9. *Identify the challenges of being a T-shaped person and how they can be overcome.*
10. *Explain the importance of a T-shaped person in a company and how they can help the company achieve its goals.*

1. Why would an investor want to invest in a high-risk asset?
2. Why do the returns on the assets differ? (the average return on a 10-year Treasury note is 6.5%, the average return on a 10-year Treasury note is 10.5%)
3. What is the risk premium?
4. What is the risk premium on a 10-year Treasury note?
5. What is the risk premium on a 10-year Treasury note?
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10. What is the risk premium on a 10-year Treasury note?

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October 30, 2019

Mayor and Council

City of Victoria
c/o Leanne Taylor, Senior Planner
Sustainable Planning and Community Development

1 Centennial Square
Victoria, BC
V8W 1P6

RE: Rezoning Resubmission for Parkway Mixed-Use Development Post HADP & ADP

Dear Mayor Helps, City Council,

We are pleased to present to you our revised application for the revitalization and addition to the Wellburn's Building, the heritage building located at the north west corner of Pandora Avenue & Cook St, in the North Park neighborhood.

After meeting with the Heritage Advisory Design Panel and the Advisory Design Panel, the project received an approved motion with recommendations from both panels. Each recommended change is outlined below with our responses:

Heritage Advisory Design Panel | October 08, 2019

- *Recommendation 1a:* *That the setback from Pandora be increased to a minimum of 4m so as to be clearly distinguishable from Pandora Avenue and conserve that portion of the original structure not covered by the addition.*

Response: Per the recommendation, we have revised the design to clearly distinguish between the existing heritage building & the proposed development. The six storey volume has now been set back to provide the requested 4m setback from the south property line. The proposed parkade on Level 0 is located directly underneath this 4m setback along the south property line to provide parking stalls for the building. As a result, this portion of the existing heritage building, behind the façade, cannot be retained within this setback.

- *Recommendation 1b:* *That the applicant retains an original residential unit interior in situ, if feasible, or be relocated within the development.*

Response: Through the duration of the design phases, the team has explored opportunities to retain original units exactly as located. However, given the vast amount of rehabilitation required, including extensive removal of hazardous materials, as well as reconstruction to retain the heritage portion, it has been determined that this exact retention is not possible. Instead, where practical heritage components that are safe and salvageable will be repurposed throughout the development, notably in key common areas such as lobbies and corridors. As we get into further detail with our Interior Design team we will continue to investigate the feasibility of incorporating any of the existing interior features into some residential units.

Advisory Design Panel | October 09, 2019

- *Recommendation 2a: Increase the building setback facing Pandora Avenue to 4m from the street.*
Response: In response to this and the similar recommendation (1a) from the Heritage Advisory Design Panel, we have revised the design to clearly distinguish between the existing heritage building & the proposed development, setting the six storey volume back to provide a 4m setback from the south property line.
- *Recommendation 2b: Revise the 2nd floor unit that face the mews to improve livability.*
Response: We recognize that these units are atypical such that they have windows that face the mews, and will therefore have less direct light than the other units. We have already studied these units in earlier revisions to the design, in response to comments from the planning department. We responded to these concerns by performing a daylight analysis and in turn reorienting windows where possible to allow for more daylight penetration. With the optimized daylight with oversize windows, as well as unique presence and overlook of the dynamic mews, we anticipate that residents who select these units will appreciate the exceptionally distinct character and unique livability.
- *Recommendation 2c: Reconsider the windowless interior dens and bedrooms*
Response: As priority was given to providing a significant 13m setback between the east-facing, existing heritage and proposed elevations, this resulted in a deep floor plan on Level 2. Along with two storage rooms, the residential suites on this level are provided with larger than typical dens to be used as an additional storage/office/family/flex space within their suites.
- *Recommendation 2d: Resolve the proposed materials*
Response: We confirm that the cladding material will be a pearl coloured Ceraclad panel, with a contemporary smooth finish.
- *Recommendation 2e: Consider adding usable roof spaces*
Response: In response to this recommendation, the west-facing roof deck on level 5 has been revised to provide an accessible roof patio for the residential users of the building, with direct views onto Franklin Green Park.
- *Recommendation 2f: Consider revising the articulation of the four storey building's façade facing Franklin Green Park and consider the addition of balconies or Julietts.*
Response: As a solution to this recommendation, the windows along the west elevation of the four storey volume have been revised to include Juliet balconies with direct visual access from the living spaces onto Franklin Green Park.
- *Recommendation 2f: Reconsider the width of the alleyway (mews) to increase accessibility.*
Response: To provide a clear accessible path along the mews, linking Cook St to Franklin Green Park, all planters have been removed in this area and the four short-term bike stalls have been relocated to the residential courtyard.

Thank you for reviewing our submission. We appreciate the opportunity to respond to the recommendations provided by the HADP & ADP and are pleased to present a design that addresses the recommendations comprehensively. We look forward to further communication with you as the **project continues its path**.

Sincerely,

MGA | MICHAEL GREEN ARCHITECTURE INC

A handwritten signature in black ink, appearing to read 'M Green', written in a cursive, fluid style.

Michael Green
M.AIBC FRAIC AIA

NORTH PARK NEIGHBOURHOOD ASSOCIATION**COMMUNITY ASSOCIATION LAND USE COMMITTEE (CALUC) MEETING****Re: 1050 Pandora Ave/1518 Cook St. Redevelopment proposal****MINUTES**

[N.B. The figures quoted for the number of retail parking stalls (p.2, third question under "Discussion") and setbacks of new construction (p.4, mid-page) were unable to be confirmed by NPNA at the time that these minutes were submitted.]

PROPOSER:

Brandon Crema, Executive Vice-President & COO, District Group

ARCHITECT:

Marie-Claire Bligh, Associate Architect, Michael Green Architecture

ATTENDANCE:

Twenty-one people signed the attendance register.

Attendees were primarily North Park and Fernwood residents and business owners.

Also present were Victoria Councillors Alto and Thornton-Joe, and representatives of Victoria Planning Department, Downtown Residents Association, Victoria Heritage Advisory Panel.

OPENING REMARKS:

1. North Park Neighbourhood Association (NPNA) — Jenny Farkas, President
 - This is the second CALUC meeting for this property, hereafter referred to as the "Wellburn's site". Because more than six months have passed since the initial CALUC meeting for the Wellburn's site, it was necessary to call a second CALUC meeting for presentation of a revised proposal.
 - NPNA requested that the developer and the architect introduce themselves and give a brief summary of their companies' history and approach.
2. District Group — Brandon Crema, Executive Vice-President & COO
 - District is a development company with a 15-year history on Vancouver Island.
 - District prefers working on projects that are moderate in size. They are committed to long-term rental projects.
3. Michael Green Architecture (MGA) — Marie-Claire Bligh, Associate Architect
 - MGA is a Vancouver company.
 - MGA's main focus is wood construction.
 - MGA wants its projects to be good neighbours to the surrounding buildings and their occupants.

PRESENTATION:

- The Wellburn's site is on the edge of downtown and is transitional between the Harris Green (Downtown) and North Park neighbourhoods. It is the northern gateway to the downtown core.
- At this transitional area, there is a building height change from taller structures in Harris Green to lower buildings in North Park.
- The allowed maximum density is 5.5:1, which represents 5X the site's area. [Either height or footprint can affect density.]
 - Proposed density for this project is 3.3:1, which would serve as a bridging factor from Harris Green to North Park.
- Pandora Ave. and Cook St. are the two main streets. The plan is for the building to face both streets, as well as Franklin Green.

- The intention is to restore and retain the historic Wellburn's façade, and to apply for heritage designation.
- The Wellburn's building needs a structural upgrade. Restructuring of the interior would include seismic protection and full underground parking. Reports from a structural engineer are pending.
- Use of the building would be kept similar to the current use, with commercial tenants on the ground level and residential tenants above.
 - Café space is planned for one corner (northeast corner, facing Cook St.)
 - A new residential block would be built above the current Wellburn's space. New construction at the north side (current parking lot) would be mixed use. Heights step down from six, to four, to two storeys.
 - The Wellburn's façade would be allowed to speak for itself without too much contrast.
 - A mews entry space would be built for tenants' use. There are generous balconies on the upper floors
 - As much greenery as possible would be provided.
- Priorities:
 - a. Retain historic façade.
 - b. Look at proximity of new construction to existing structures
 - Entrance mews
 - Maintain street wall
 - New volume to be subordinate to existing volume
 - 13.5 meter setback from Cook St.; further setback to six-story residential block
 - Avoid top-heavy proportions
 - Façade articulation of new rental units would have diagonal recesses
 - c. Surrounding context
 - Maintain northern gateway to downtown in context
 - Building height in context
 - Residential access through mews, off Cook St., with gate closed after working hours.
 - Residential-only courtyard and garden area
 - Multiple residential entrances and patios open onto public walkway from Cook St. to Franklin Green. Low fencing, soft landscaping would form delineation, not barrier, between residential and public space.
 - Boulevard tree planting would be part of the project.
 - Ground floor retail space could be large (e.g. grocery) or subdivided
 - Three meter setback from neighbouring Cook St. property (cinder block building at Cook and Mason Streets). This neighbouring property also must have a three meter setback.

Drawings available at <https://districtgroup.ca/portfolio-item/parkway/>.

DISCUSSION:

Q: Is the parking entry off Cook St.?

A: Yes; according to bylaw, Cook St. is classified as the lesser-used street. In addition to this, the bike lane is on Pandora.

Q: How close is the parking entry to Mason St.? What would the parking entry's impact be on Mason St.?

A: The developer always looks at traffic impact. A traffic engineer currently is looking at this.

Q: Does the underground parking include parking for the retail portion of the building?

A: Yes, there are 17 retail stalls.

Q. What is the market rent for a 550 sq. ft. one-bedroom unit?

A: At \$2.90 - \$3.20/sq. ft., a one-bedroom would rent for approximately \$1,700.

Comments (speaker lives and works on Mason St.):

- The current Wellburn's was built in approximately 1912. The proposed development would mean losing a piece of heritage. In addition, it would mean loss of residential space (currently 11 units) with affordable rents, and a grocery store with lower prices.
- The speaker referred back to the neighbourhood's ideas about having more open and shared space around Franklin Green, which had been formed prior to the nearby development at the corner of Pandora Ave. and Vancouver St.
- The speaker is opposed to "façadism", which will be expensive to achieve. It would be better to retain the current Wellburn's and build on the parking lot instead.
- The height of new construction should be dropped down and/or it should step back from Franklin Green, to save Mason St. from further shading.

Q: Is there storage for tenants outside of the residential units?

A: There will be approximately 140 bike parking stalls, which can be multi-purpose.

Q: How long would the new building remain as rental?

A: The developer plans to keep it as long-term rental. A rental agreement has not yet been determined with the City. There is a possibility that it could be rental in perpetuity.

Comment: The neighbourhood would be interested in learning the terms of the rental agreement once it is confirmed.

Comment: It is a benefit to the community that Franklin Green is shared space, both visually and in terms of activities in the park.

Q: The current residential units are quite reasonable in price. Would the developer apply to governments for grants to allow affordable rents according to CMHC guidelines?

A: Yes, the developer is looking at this. It is a long process and they still are investigating possibilities. In addition, District Group is looking at re-location strategies in the area for current tenants, in other buildings owned by District with business partners.

Comments (speaker is a current resident in Wellburn's building):

- The current building is aging and in need of a change.
- Recent developments such as the Atrium are good, as they foster engagement with the community. Other developments are unfriendly, as their commercial tenants are appointment-only businesses. The speaker is concerned that this proposed development provide a welcoming atmosphere (e.g. coffee shop that includes outdoor seating).
 - A: Commercial tenants probably would be allowed to do their own planning.
- Loss of light on Franklin Green shouldn't be a problem. The park already is shaded by trees.

Comments:

- The speaker would like NPNA to urge that perpetuity rental be put in place.
- The proposed units provide small rental space, but these days, "the living room is the street". Furniture is smaller now and storage space is built in. We have to change the way we look at things.

A: Storage space is at a premium. Space needs to be looked at in terms of whether it can be used in multi-purpose ways.

Q (the speaker uses Wellburn's and Franklin Green):

- The current open interface with the park is good. The proposal's effort to interface with the neighbourhood is appreciated. The property gains more from the park than the other way around. Would the developer add landscaping or some other arrangement?
- Is the underground parking the full width of the Wellburn's building?

A: Yes, the parking is full width of the building.

Q: It is hard to find an affordable unit. Can the proposed development be made more dense? Increased density would make it more affordable.

A: Increased density can only happen if the commercial space is included. It would not make the residential rents more affordable.

Q/Comments:

- Comment: Shelter Aid for Elderly Renters (SAFER) is a program that low to moderate income seniors can use to make rents more affordable.
- How wide is the Cook St. sidewalk?
 - A: Curb to building is 16 ft.
- How will deliveries be made to retail tenants?
 - A: There will be a loading area to the left of the residential/public walkway. Delivery times will be managed.
- Q: Are we keeping Wellburn's grocery store?
 - A: Discussions are continuing. Garbage, loading, etc. need to be considered. District Group needs to learn Wellburn's needs and plans. Commercial tenants need to be amenable to the community.
- Comments:
 - The lower walls along the residential/public walkway are good. The speaker has seen a similar arrangement working well in other locations.
 - The barber shop next to Wellburn's is a great asset to the community — it has a lot of engagement with the neighbourhood.

Q: What does tenant re-location entail?

A: The city has a process.

- District Group wants to find homes for everyone currently living in the Wellburn's building. The property manager needs to sit down with everyone to learn their needs and plans.
- Some re-locations could be temporary, then the tenant would move back into the new building.

Q (speaker is from Downtown Residents Association):

- What is different in this design in terms of setbacks and massing? Setback from the heritage façade and the new construction looks insignificant.
 - A: The setback is seven feet. The standard is six meters. Increasing the setback is being considered.
- What is the material of the exterior surface of the new part?
 - A: Ceramic tile, of similar pattern to the current exterior.

Comment: It would add continuity if glazed brick could be carried over to the new part.

Q: Comments from Heritage Advisory Panel?

A (member of panel):

- One or more façades should be retained.
- Setback has been increased; maybe it could be more. Heritage always looks for a greater setback.
- Any new development should be compatible, with similar type or quality of materials. There are various ways of making this possible.
- There are no prescribed guidelines for setback.

Q: Previously there was an understanding that Wellburn's grocery store was leaving and there would be smaller commercial spaces. Local businesses and local proprietors are appreciated.

A: Options are being kept open for now. There are two additional recessed entries if small businesses go in; therefore there are various opportunities.

Q/Comments (speaker is owner of barber shop in current Wellburn's building):

- Nobody from District Group came to the barber shop yet about re-locating.
 - A: District Group is working on strategy and getting a more confident timeline first.
- Barber shop might be interested in returning to the wellburn's site.
- The six- and four-storey setbacks are good; they allow space for interaction.

Q/Comments:

- Comment: The heritage building is being used to "buy space" now.
 - A: The standard setback from the sidewalk is 4.19 meters. The developer is talking to the City about the current Wellburn's to allow a bridging effect to the new part.

- Comment (member of Victoria Planning Dept.): Setbacks depend on context.
- Q: What will happen where the current Wellburn's roof is?
 - A: It's not definite how it would be.
- Comment: If the proposal were more dynamic (e.g. more condos) that would offset affordable rental. More diversification might be more beneficial to the community.

Q/Comments (member of Heritage Advisory Panel):

- Comment: There is a Downtown Core Development Plan. North Park has a deficit of heritage buildings. There are many policies that come into play. In development presentations it would be helpful to talk more about that. It would be helpful to talk more about comments from the Heritage Advisory Panel.
- Q: How many heritage buildings has this architect worked on?
 - A: This is the first.

Q: Would retention of the façade be eligible for tax exemption?

A: No, but the parapet might be.

Comment: Thank you to everyone for the open discussion.

Q: How can a developer control the nature of local businesses that wish to be tenants?

A: It is in the interest of the developer to retain valued, interactive, dynamic businesses.

CLOSING REMARKS — Jenny Farkas

In relation to land use, it is important for communities to ask, "What are the policies?"

"What do we gain or lose as a community?" We need to make informed decisions.

Thank you to all for attending and participating in this meeting.

ADJOURNMENT

The meeting adjourned at 9:25 p.m.

Recorder: Penny Bond, NPNA

MEMORANDUM

To: Jessica Gibson, Development Manager - District Group
From: Tim Shah, RPP, MCIP
Our File #: 2569.B01
Date: October 29, 2019
RE: Parking Variance Review: The Parkway Development

Watt Consulting Group was retained by District Group to review a proposed parking variance for the Parkway Development in the City of Victoria. The purpose of this memorandum is to [a] review and document the City of Victoria's minimum parking supply requirements (Schedule C), as well as related Official Community Plan policies that support increased density and sustainable transportation, [b] comment on transportation demand management (i.e., transit passes, carsharing, electric bike parking, etc.) and their impact on parking demand that may be suitable for the site to justify a parking variance, and [c] identify parking management approaches to inform the parking allocation at the site.

1.0 SITE LOCATION + CONTEXT

The proposed development is located at 1050 Pandora Avenue and 1518 Cook Street in the City of Victoria. The existing Wellburns Market Building at 1050 Pandora Avenue is a heritage registered building in the City of Victoria. The following describes the context within which the site is located and which partially defines the transportation options available:

- Planning Context | The site is located within the core area of the City. The OCP identifies the site as "core residential", which supports multi-unit residential buildings from three storeys up to 20. The OCP also directs 50% of future population growth in to Urban Core, suggesting that this area may see significant redevelopment in future.
- Walking | Being located within the Urban Core, the site benefits from proximity to a number of commercial destinations including grocery stores, cafés, pharmacies, and more. The site has a WalkScore of 97 – "Walker's Paradise: Daily errands do not require a car".¹
- Cycling | The site is directly located on the Pandora Avenue Two-Way Cycle Track, which will offer future residents, employees, and customers access to an all ages and abilities bike route. Access to this separated bike lane can result in improved safety, convenience, and comfort for future residents of the site who cycle and can help make

¹ More information about the site's Walk Score is available online at:
<https://www.walkscore.com/score/1058-pandora-ave-victoria-bc-canada>

cycling a viable commuting option which thereby reduces the need to own a vehicle. In addition, Vancouver Street is proposed as one of the north-south connections in the City's All Ages and Abilities (AAA) cycling network, which will enhance cycling opportunities for future residents / customers at the site.

- Transit | Bus stops are located directly in front of the building (on Pandora and Cook Streets) and is served by five bus routes (2, 24, 25, 27, 28). The site is also located within a 2-minute walk (300m) of Yates Street, which is served by seven bus routes that provide frequent transit service during peak hours. The Victoria Transit Future Plan identifies Pandora Avenue, Johnson Street, and Yates Street as frequent transit corridors, which will greatly enhance the site's access to frequent transit service.

2.0 PROPOSED DEVELOPMENT

2.1 Land Use

The proposal for the Parkway development is to include 103 units comprising a mix of studio, one-, two-, and three-bedroom units. The proposal also include approximately 1,054m² of commercial space comprising 888m² of retail / grocery store and 166m² of restaurant use.

2.2 Parking Supply

Due to construction constraints with the existing heritage building—and the challenges with constructing underground parking due to the heritage structure—the proposal is to provide 44 parking spaces, which will comprise 33 residential, 5 commercial, 4 visitor spaces, and two MODO carshare spaces. A detailed breakdown of the parking supply is available in the applicant's site plans and parking calculations.

The proposal also includes 172 bicycle parking spaces comprising 154 long-term spaces and 18 short-term spaces. Of the 154 long term spaces, 40 (26%) will be electric and cargo bike spaces.

3.0 OFF-STREET PARKING REQUIREMENT

The City of Victoria adopted its new Off-Street Parking Regulations ("Schedule C") in July 2018.² Using the Schedule C requirements, the two properties are required to provide 93 parking spaces comprising 66 resident spaces, 10 visitor spaces, and 17 commercial spaces. Therefore, with the proposal to provide 44 parking spaces, the building would be short 49 parking spaces.

² Schedule C is available online at:
<https://www.victoria.ca/assets/Departments/Planning~Development/Development~Services/Zoning/Bylaws/Schedule%20C.pdf>

4.0 TRANSPORTATION DEMAND MANAGEMENT

Transportation demand management (TDM) refers to policies, programs and services that influence whether, why, when, where and how people travel.³ TDM initiatives typically aim to reduce single-occupant vehicle ("SOV") trips and parking demand, and encourage alternative travel options such as walking, cycling, public transit and shared rides.

The City of Victoria supports TDM and parking management strategies as outlined in sections 7.11 and 7.12 of the OCP.⁴ Specifically, Section 7.12 indicates that reductions in the parking requirements should be considered where:

7.12.1 Geographic location, residential and employment density, housing type, land use mix, transit accessibility, walkability, and other factors support non-auto mode choice or lower parking demand

7.12.2 Activities and circumstances of land uses, structures or buildings include the provision of a comprehensive suite of permanent on-site alternative travel supports and active transportation infrastructure, including such things as short-term and long-term bicycle parking facilities including shower and locker facilities, ridesharing, car-share co-ops, payroll transit passes and other automobile trip reduction measures.

The applicant could consider the following TDM programs to support its parking variance, and more importantly, reduce the need for resident parking. These measures also align with policy direction from the OCP.

4.1 Transit Passes

As discussed above, the site has excellent transit access and as the Transit Future Plan becomes implemented, transit service is anticipated to improve significantly, which will make transit more appealing to future residents.

Consideration may be given to providing a subsidized transit pass program for residents. BC Transit offers monthly transit passes for regular customers. Residents of each residential unit would be provided with monthly transit passes upon move-in for a defined time period (i.e., one to three years). The developer contribution could be a full subsidy or a fund set aside for 50-50 matching (the latter helps ensure that contributions are used to subsidize transit among only those that use it).

BC Transit currently offers the EcoPASS Program for New Developments, which is a program that provides Capital Regional District developers with a potential transit-oriented solution for

³ Definition based on Transport Canada, TDM for Canadian Communities, March 2011

⁴ City of Victoria. (2017). Official Community Plan, Section 7: Transportation and Mobility. Available online at: https://www.victoria.ca/assets/Departments/Planning-Development/Community-Planning/OCP/Replaced/OCP_Sec7_Jul2017_web.pdf

parking variance requests. Under the EcoPASS Program, the occupants of a new residential, commercial or mixed-use development receive annual bus passes for a pre-determined number of years that are valid for use throughout the Victoria Regional Transit System. Each annual pass has a cost to the developer of \$1,000. The size and value of the TDM program is established by the municipal government, with a minimum required program value of \$5,000.

The applicant could consider approaching the City and BC Transit in the future to learn more about this program and whether it may be feasible during long-term operations of the building,

Other jurisdictions have implemented similar transit pass programs such as the ORCA Multifamily Development Passport in King County, Washington. The ORCA Multifamily Development Passport is an annual transportation pass that property managers can offer to residents where the costs are either covered in full by the property manager or through a 50% subsidy. This pass gives residents comprehensive access to transit services in the Puget Sound Region, including local and express bus service, Link light rail, and Sounder commuter rail, among others.⁵

Developers / property managers such as Sustainable Kirkland LLC are actively participating in the program. One of their properties offers the passport to all 290 residential units at a cost of \$24,000 USD (~\$83 per unit). Since the program was introduced at the property, transit ridership has increased by 150% among residents.⁶ It was reported that the ORCA program is generally successful once initially adopted by a property manager; however, where the program has had challenges is the inability for property managers to secure ongoing funding to continue the program for its tenants.⁷

If the applicant is able to secure and administer a transit pass program, a 10% reduction in resident parking demand would be supported. This would effectively reduce demand by 7 spaces at the site.

⁵ King County Metro. (2018). ORCA Multifamily Development Passport. Available online at: <https://kingcounty.gov/depts/transportation/metro/fares-orca/orca-cards/multifamily-passport.aspx>

⁶ Email correspondence with King County Senior Transportation Planner on November 26, 2018.

⁷ Ibid.

4.2 On-Site Carsharing + Memberships

The Modo Car Cooperative ("Modo") is the most popular carsharing service in Greater Victoria. In 2015, there were 23 cars and 800 members; as of June 2019, there were 82 Modo vehicles and 2,849 members across the Greater Victoria region, suggesting that Modo is growing in popularity.⁸ There are currently four Modo vehicles located within a 500 meter walk of the development:

- Two Modo vehicles are located on Johnson St & Chambers St (250 meters from the site)
- One Modo vehicle is located on Yukon St & Chambers St (350 meters from the site)
- One Modo vehicle is located on North Park St & Vancouver St (500 meters from the site)

The applicant has committed to the provision of two Modo vehicles and parking spaces directly on-site (see letter from Modo dated October 28, 2019 for more details). This agreement commits the applicant to make a one-time financial contribution of \$59,000 for the purchase of two vehicles. Modo will provide the applicant with a 'Partnership Membership' valued at \$59,000, which allows for a maximum of 118 residents to have membership privileges and not have to pay the \$500 membership fee. The membership would be valid for the lifetime of the development and be tied to each unit. Access to a Modo membership provides a strong incentive to residents to explore carsharing.

Research has shown that carsharing programs have a significant impact on reducing vehicle ownership and thereby lowering parking demand. Below is a summary of key findings:

- One of the most comprehensive North American studies to date surveyed 6,281 households in carsharing organizations across the continent. The study found a statistically significant decrease in average vehicle ownership from 0.47 to 0.24 vehicles per household among households that joined carshare services, an approximately 50% reduction in vehicle ownership.⁹
- A 2009 study of carshare programs in the City of Toronto found that vehicle ownership rates at condominium sites without carshare vehicles was 1.07 vehicles per unit, whereas buildings with one or more carshare vehicles had significantly lower rates at 0.53 vehicles per unit, which represents a 50% reduction in vehicle ownership rates.¹⁰
- A 2013 study from the City of Toronto looked at the relationship between the presence of carsharing in a residential building and its impact on vehicle ownership. This was one of

⁸ Email correspondence with Modo Director of Business Development on June 24, 2019.

⁹ Martin & Shaheen. (2011). The Impact of Carsharing on Household Vehicle Ownership. Access Magazine, Spring 2011. Available online at: http://sfpark.org/wp-content/uploads/carshare/access38_carsharing_ownership.pdf

¹⁰ City of Toronto. (2009). Parking Standards Review: Examination of Potential Options and Impacts of Car Share Programs on Parking Standards. Available online at: https://www1.toronto.ca/city_of_toronto/city_planning/zoning_environment/files/pdf/car_share_2009-04-02.pdf

the first studies to examine this relationship at the building level as previous research explored impacts at the neighbourhood or city level. The study surveyed residents of buildings with and without dedicated carshare vehicles. According to the author's regression model, the presence of dedicated carshare vehicles had a statistically significant impact on reduced vehicle ownership and parking demand.¹¹

- Two studies from Metro Vancouver explored the impact of carsharing on vehicle ownership. Over 3,400 carshare households participated in the study. The key findings are as follows:
 - On average, up to 3 private personal vehicles were shed per carshare vehicle.
 - A regression analysis found that those living in rental housing and in a smaller household size are statistically more likely to give up vehicle ownership compared to the reference case.¹²
 - The number of carshare vehicles within walking distance has a small but statistically significant relationship with apartment household vehicle holdings.¹³

Regulations in New Westminster, Coquitlam, and Richmond allow for a 5-15% reduction where carshare vehicles are accessible. Correspondence with the Victoria Carshare Cooperative (now Modo)¹⁴ supported a 5-10% reduction in parking demand where memberships are provided and where a vehicle is easily accessible. A similar reduction of 5-10% is recommended in *Parking Management Best Practices*.¹⁵

Overall, the research cited above confirms that access to a carsharing vehicle and the provision of memberships is associated with reduced vehicle ownership and parking demand. With the applicant committing to two carshare vehicles directly on-site, and memberships for each unit, a 30% reduction in resident parking demand is supported, which would lower demand by 20 spaces at the site.

4.3 Electric Bike Parking

Electric Bikes (E-Bikes) are an emerging transportation phenomenon that are gaining popularity worldwide. With supportive cycling infrastructure in place, E-Bikes have the potential to substitute for, or completely replace, almost all trips taken by a gasoline powered car, which could address congestion issues and mitigate parking challenges within urban areas.

¹¹ Engel-Yan, D., & D. Passmore. (2013). Carsharing and Car Ownership at the Building Scale. *Journal of the American Planning Association*, 79(1), 82-91.

¹² Ibid, pg. 54.

¹³ Metro Vancouver. (2014). The Metro Vancouver Car Share Study: Technical Report. Available online at: <http://www.metrovancouver.org/services/regional-planning/PlanningPublications/MetroVancouverCarShareStudyTechnicalReport.pdf>

¹⁴ Correspondence from Victoria Carshare Cooperative (now Modo), received August 2009.

¹⁵ Litman, T. (2007). *Parking Management Best Practices*, American Planning Association.

Research has reported that one of the main barriers facing prospective E-Bike users is the lack of secure parking available, which is critical for helping minimize theft of the electric bike.¹⁶ As part of a larger strategy to discourage vehicle ownership the applicant has designed the long-term bicycle parking spaces to accommodate 40 cargo and electric bikes, which will allow E-Bike users to park in the bicycle parking room and avoid having to bring their heavy bicycle to their unit. Electric bikes are typically longer than regular bicycles because they are capable of carrying cargo and/or multiple passengers with the assistance of the battery. Electric cargo bikes can be as long as 2.5m.

In addition to designing larger long-term bicycle parking spaces, the applicant should consider the provision of additional security features such as video surveillance and self-contained bicycle lockers as well as access to an 110V wall outlet for each E-Bike parking space. Specifically, the applicant should provide each electric / cargo bike parking space with direct access to an 110V wall outlet (approx. 26% of all the long-term bike parking spaces). This would help facilitate charging opportunities for future E-Bike users at the site, especially for those who may have an E-Bike that has a more typical bicycle length and can park in a regular long-term bicycle parking rack.

As electric bikes are an emerging form of mobility, there is limited research that has quantified the impact of these bikes on vehicle ownership / parking demand. A recent study presented results of a North American survey of electric bike owners. The study reported that E-Bikes have the capacity to replace various modes of transportation commonly used for utilitarian and recreational trips including motor vehicles, public transit, and regular bicycles. Specifically, the study reported that 62% of E-Bike trips replaced trips that otherwise would have been taken by car. Of these trips previously taken by car, 45.8% were commute trips to work or school, 44.7% were other utilitarian trips (entertainment, personal errands, visiting friends and family, or other), and 9.4% were recreation or exercise trips. The average length of these previous car trips was 15 kilometres.¹⁷

Given that E-Bikes have the potential to replace private motor vehicles, and with the applicant committing to provide 40 long-term cargo E-Bike parking spaces, a 20% reduction in resident parking demand is supported, which would lower vehicle parking demand by 13 spaces. Access to an 110V wall outlet should accompany all of the cargo and E-Bike parking spaces to facilitate E-Bike charging with consideration to add more outlets to support opportunities for future E-Bike users.

¹⁶ WATT Consulting Group. (2018). Capital Region Local Government Electric Vehicle + Electric Bike Infrastructure Backgrounder. Available online at: https://www.crd.bc.ca/docs/default-source/climate-action-pdf/reports/electric-vehicle-and-e-bike-infrastructure-backgrounder-sept-2018.pdf?sfvrsn=a067c5ca_2

¹⁷ MacArthur, J., Harpool, M., & D. Schepke. (2018). A North American Survey of Electric Bicycle Owners. National Institute for Transportation and Communities, NITC-RR-1041.

4.4 TDM Summary

Table 1 presents a summary of the impact of the TDM measures in reducing resident parking demand. The reduction may be as high as 40 spaces, which would reduce the required resident parking supply from 66 to 26 spaces.

TABLE 1. SUMMARY OF TDM PROGRAMS + PARKING DEMAND REDUCTIONS

TDM Option	Parking Reduction	
	Quantity	Approx. Total Reduction (resident parking spaces)
Transit Passes	10%	- 7
Carsharing (Two On-site Vehicles + Memberships)	30%	- 20
Electric Bike Parking	20%	- 13
TOTAL		- 40

5.0 PARKING MANAGEMENT

5.1 Shared Parking, Visitor & Commercial

The peak parking demand for residential visitor parking occurs in the evening whereas the peak parking demand for commercial uses such as cafés and retail is typically during the lunch period. The peak demand for these uses occur at different times of the day, which may allow for some of the parking to be shared. According to the Urban Land Institute, visitor parking demand typically peaks after 7:00PM. Therefore, from 6:00AM to 6:00PM, few visitor vehicles are expected to visit the subject site. Commercial demand typically peaks at lunchtime (between 12:00PM and 1:00PM) with the exception of restaurant uses, where demand is close to 100% in the evening.

Given the above discussion, the applicant could consider sharing the commercial parking spaces between commercial users and visitors, which would eliminate the need to provide designated visitor spaces. Further, given the availability of on-street parking in the vicinity of the building, visitor vehicles would likely use on-street parking and take advantage of free evening parking when they are more likely to visit the site.

With shared parking between visitor and commercial, the applicant could consider reallocating the 4 visitor parking spaces to residential spaces, which would increase the residential supply to 37 spaces. **Table 2** presents the recommended parking allocation for the site with two core assumptions: (1) all TDM strategies are adopted by the applicant and (2), commercial and visitor parking spaces are shared.

While all of the TDM strategies are recommended to the applicant, the only one directly outside of their control is the transit pass program, which would require coordination and cooperation

from BC Transit. In the event that a transit pass program cannot be secured, then the resident parking demand—adjusted for TDM—would be 33 spaces, which is still within the recommended parking allocation in **Table 2**.

TABLE 2. RECOMMENDED PARKING ALLOCATION

Land Use	Number of Parking Spaces
Multi-Family Residential	37
Commercial / Visitor (Shared)	5
Modo Carshare	2
TOTAL	44

6.0 SUMMARY

The proposed parking supply for the Parkway development is 44 parking spaces, which is 49 spaces short of what is required from the City under Schedule C (93 spaces). The site benefits from access to frequent transit service, high walkability, and a protected bicycle lane, which helps reduce the need to own a vehicle. A number of TDM measures were also identified including transit passes for each unit, the provision of two carshare vehicles on-site and memberships for each unit, and the provision of long-term bicycle parking spaces to accommodate electric bikes.

Committing to the recommended TDM measures could reduce resident parking demand by 40 parking spaces at the Parkway site. This would effectively lower the required amount of parking to 53 parking spaces at the site, which is still 9 more than proposed. The applicant could also consider sharing the commercial and visitor parking spaces, which would allow the applicant to reallocate the 4 visitor spaces to residential.

Sincerely,

Watt Consulting Group



Tim Shah, RPP, MCIP
Transportation Planner



October 28, 2019

District Developments Corp.
c/o Pandora Cook Development Corp.
200-8809 Heather Street
Vancouver, B.C.
Canada V6P 3T1

Attention: Jessica Gibson

Dear Jessica,

Re: Carshare arrangements at 1050 Pandora Avenue & 1518 Cook Street in Victoria, British Columbia

This letter will confirm that Modo sees the location of the proposed mixed-use development at 1050 Pandora Avenue & 1518 Cook Street in Victoria, British Columbia as having good potential for carsharing. Under the following arrangements, Modo is willing to enter into an agreement with Pandora Cook Development Corp. (the "Owner") to provide carsharing services:

1. Prior to the issuance of a development permit by the City of Victoria for the proposed development, the Owner and Modo will enter into a legally binding agreement for the provision of carsharing services at the location of the proposed development in compliance with the City of Victoria requirements;
2. The Owner will provide, at no cost to Modo, two (2) designated parking spaces at location of the proposed development to be accessible to all Modo members on a 24 hours a day, 7 days a week basis;
3. When final parking drawings become available, Modo will review them to ensure that the parking spaces to be provided will be suitable for carsharing and will comply with Modo construction standards for shared vehicle parking space;
4. The Owner will provide to Modo a one-time financial contribution of \$59,000.00 inclusive of taxes and fees (the "Project Fee") for the purchase of two (2) shared vehicles, to be located at the proposed development, in the parking spaces designated for carsharing;
5. Modo will provide the Owner with a Partnership Membership in Modo with a public value of \$59,000.00, valid for the lifetime of the proposed development and allowing a maximum of 118 residents of the proposed development to simultaneously benefit from Modo membership privileges without the need to themselves pay a \$500 membership fee;
6. Modo will provide a promotional incentive worth \$100 of driving credits to each resident of the proposed development joining Modo; and

7. Modo will share data with the City of Victoria on the utilization of the vehicles provided in connection with the proposed development, including the ratio of hours booked by residents of the development vs non-residents.

Modo is interested in working with District Developments Corp. and Pandora Cook Development Corp. and be part of the proposed development at 1050 Pandora Avenue & 1518 Cook Street in Victoria whose residents and nearby neighbours may no longer need to own a car of their own for their personal and business needs.

Thank you for your support of carsharing in the City of Victoria.

Regards,

A handwritten signature in black ink, appearing to be 'Sylvain Cellaire', with a stylized, cursive script.

Sylvain Cellaire
Director of Business Development

Enclosed:

- Modo Carshare Agreement - District Group - 1050 Pandora Avenue & 1518 Cook Street, Victoria - October 28, 2019

October 30, 2019

City of Victoria
1 Centennial Square
Victoria, BC V8W 1P6
Attn: Leanne Taylor, Senior Planner

Re: 1050 Pandora Avenue & 1518 Cook Street – Outline of TDM Commitments

District Development Corp on behalf of Pandora Cook Development Corp. would like to confirm that the project located at 1050 Pandora Avenue and 1518 Cook Street will be providing, in agreement with Modo Carshare, two designated parking spaces at the proposed location and a one-time financial contribution of \$59,000 for the purchase of two shared vehicles. Please find attached a letter of support from Modo, an agreement has also been drafted and will be fully executed shortly.

As well, District Development Corp has communications with BC Transit in regards to their Eco Pass program and have received confirmation from the Sales and Revenue Manager that the program is available and ready for use on the proposed development. BC Transit will confirm implementation directly with the City of Victoria closer to the project completion date. District Development Corp. on behalf of Pandora Cook Development Corp. will be contributing \$100,000 towards this program which is the equivalent of 100 transit passes.

Lastly, the proposed development will be providing a total of 40 cargo and electric bike stalls on site as can be seen in the revised parkade design drawing submitted with the Heritage Alteration Permit.

We hope this provides City staff with the comfort that but the developer and owner are fully committed to providing numerous transportation alternatives to support our parking variance request.

Regards,

District Development Corp.



Per: Jessica Gibson, Development Manager

Tenant Assistance Plan

This form must be submitted with your rezoning or development application. For contact, please send questions to your development services planner.

SUMMARY: Instructions and steps for Developers and Property Owners

STEP 1	BACKGROUND: Understand your rights and responsibilities as a landlord. Please review the documents in the background section pertaining to relocating tenants and the City's rental replacement policies.
STEP 2	POLICY APPLICATION: Complete tenant impact assessment to determine the requirements of your application.
STEP 3	Complete application requirement, including: a. Current Site Information b. Tenant Assistance Plan c. Tenant Communication Plan d. Appendix A - Current Occupant Information and Rent Rolls (For office use only) e. Appendix B - Correspondence with Tenants Communication (For office use only)
STEP 4	SUBMIT: Complete form and submit to: a. Email digital copy of plan to housing@victoria.ca (include appendices)
STEP 5	REVISE: Applicant to update and return application requirements with staff input.
STEP 6	FINALIZE: City staff to finalize the review and signs off application requirements and used as attachment for the Committee of the Whole report.

BACKGROUND: Rights and Responsibilities of Landlords and Tenants

The rights and responsibilities of landlords and tenants are regulated by the Province and is set out in the [Residential Tenancy Act](#).

Please refer to the City of Victoria's [website](#) for more information regarding the City of Victoria's rental housing policies. Supporting documents include:

- Tenant Assistance Instructions and Checklist
- Tenant Assistance Policy
- Frequently Asked Questions
- Sample Letter to Tenants
- Request for Tenant Assistance Form and Privacy Guidelines
- Final Tenant Assistance Report

POLICY APPLICATION: Tenant Impact Assessment to Determine the Requirements of your Application

Answer the questions below to determine whether a plan is required with your application:

Tenant Impact	Indicate:		Application Requirement
Are you redeveloping or demolishing a building that will result in loss of existing residential units?	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	If yes, complete the next question.
Does your work require the permanent relocation of tenant(s) out of the building?	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	If yes, complete and submit a tenant assistance plan.
Do you have tenant(s) who have been residing in the building for more than one year?	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	If yes, tenants are eligible under the tenant assistance plan

If any are selected no, then a tenant assistance plan is not required as part of your application.

TENANT ASSISTANCE PLAN

A. Current Site Information

Site Address:	1050 Pandora St. Victoria BC
Owner Name:	Andrew Rennison
Company Name:	Pandora Cook Development Corp.
Tenant Relocation Coordinator (Name, Position, Organization):	Jessica Gibson, District Properties Group; DJ MAC Consulting

EXISTING RENTAL UNITS

Unit Type	# of Units	Average Rents (\$/Mo.)
Bachelor	3	
1 BR	6	
2 BR	2	
3 BR		
3 BR+		
Total	11	

B. Tenant Assistance Plan

For any renovation or redevelopment that requires relocation of existing tenants, the property owner must create a Tenant Assistance Plan that addresses the following issues:

- Early communication with the tenants
- Appropriate compensation
- Relocation assistance
- Moving costs and assistance
- Right of first refusal

The City has developed a Tenant Assistance Plan template that is available for applicant use. The template includes the required FOIPPA section 27(2) privacy notification which should be identified for tenants.

Please refer to the Tenant Assistance Policy with Tenant Assistance Plan guidelines for Market Rental and Non-Market Rental Housing Development.

Required under the Residential Tenancy Act

Notice to End Tenancies

A landlord may issue a Notice to End Tenancy only after all necessary permits have been issued by the City. In addition, landlords must give four months' notice to end tenancies for renovation, demolition, and conversions. Tenants have 30 days to dispute the notice.

For more information, please refer to the [Landlord Notice to End Tenancy](#).

Renovations and Repairs

Renovations and repairs must be so extensive that they require the unit to be empty in order for them to take place, and the only way to achieve the necessary emptiness or vacancy is by terminating a tenancy. The RTA and associated guidelines provide specific guidance pertaining to whether a landlord may end a tenancy in order to undertake renovations or repairs to a rental unit.

For more information, please refer to [Ending a Tenancy for Landlord's use of Property](#).

Right of First Refusal

In instances of renovations or repairs requiring vacancy, the RTA requires tenants be offered the right of first refusal to enter into a new tenancy agreement at a rent determined by the landlord. This right of first refusal applies only to a rental unit in a residential property containing 5 or more units, and there are financial penalties for non-compliance.

For more information, please refer to [Tenant Notice: Exercising Right of First Refusal](#).

For full details, please check the Government of British Columbia [website](#).

Tenant Assistance Plan Components	APPLICANT		CITY STAFF
	Tenant Assistance Plan		Did the Applicant meet policy?
	Date:	October 18 2019	
Compensation Please indicate how you will be compensating the tenant(s).	All tenants will be compensated accordingly by their tenant length. We confirm that existing tenants whose rental rates are below CMHC average rents will be based on both length of tenancy and at rates that meet the CMHC average rents. Each tenant also will be provided with additional compensation, in the form of a lump sum payment totaling two months rent. This additional compensation exceeds policy expectations.		Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>
Moving Expenses Please indicate how the tenant(s) will receive moving expenses and assistance.	Fixed rates for moving will be provided to all tenants		Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>
Relocation Assistance Please indicate how the tenant(s) will receive relocation assistance.	All tenants will be offered tenant relocation assistance. Tenant Relocation Specialists Don MacPherson and Lee Murphy of DJ Mac Consulting have been contracted. We are committed to providing three options comparable in size, location and rent amount (min. of one option in same neighbourhood)		Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>
Right of First Refusal Please indicate whether the applicant is offering right of first refusal to the tenant(s). Please indicate your reasoning.	Right of first refusal will be offered to all tenants when the new building is complete with a 10% discount.		Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>
Tenants Requiring Additional Assistance Please indicate whether there are tenants requiring additional assistance. If so, please indicate how the applicant plans to provide additional support.	One tenant currently on social assistance, tenant relocation will be done in coordination with staff from Pacifica Housing.		Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>
Other Comments			

Tenant Communication Plan Components	APPLICANT	
	Tenant Communication Plan	
	Date:	
How and when did you inform tenants of the rezoning or development application?	In person meetings were held on April 24-25, 2019 with District Group and Pacific Cove. Communication by DJ Mac Consulting began September 25-26, 2019 and has continued through October/November.	
How will you be communicating to tenants throughout the rezoning or development application (including decisions made by Council)?	<p>Notices will be posted in the building as the City process progresses.</p> <p>All tenants have been provided with District Group contact info if any further questions arise about the development process in between postings.</p> <p>Jessica Gibson is the District Group contact gibson@districtgroup.ca / 604-322-5762</p>	
What kind of resources will you be communicating to your tenants and how will you facilitate tenants in accessing these resources? (Please see the City's website for a list of resources)	Will be communicating to tenants the City Tenant Assistance Policy and the Residential Tenancy Act via email or paper copy.	
Have tenant(s) confirmed with you whether they request assistance? If so, please indicate the staff responsible or whether a third-party service is requested.	<p>Yes, during the in person meetings held on April 24-25, 2019 tenants were asked to identify any criteria that would be of importance when looking for apartments, which locations they prefer and any additional notes they wanted us to be aware of.</p> <p>Tenants were happy with the compensation being offered and were comforted that they will have a significant lead time.</p> <p>Tenants also understand that these were high level preliminary conversations and they will be able to get into more detail with the tenant relocation specialist.</p>	
Other communications notes:	DJ Mac Consulting will ensure all communications will be in written form.	

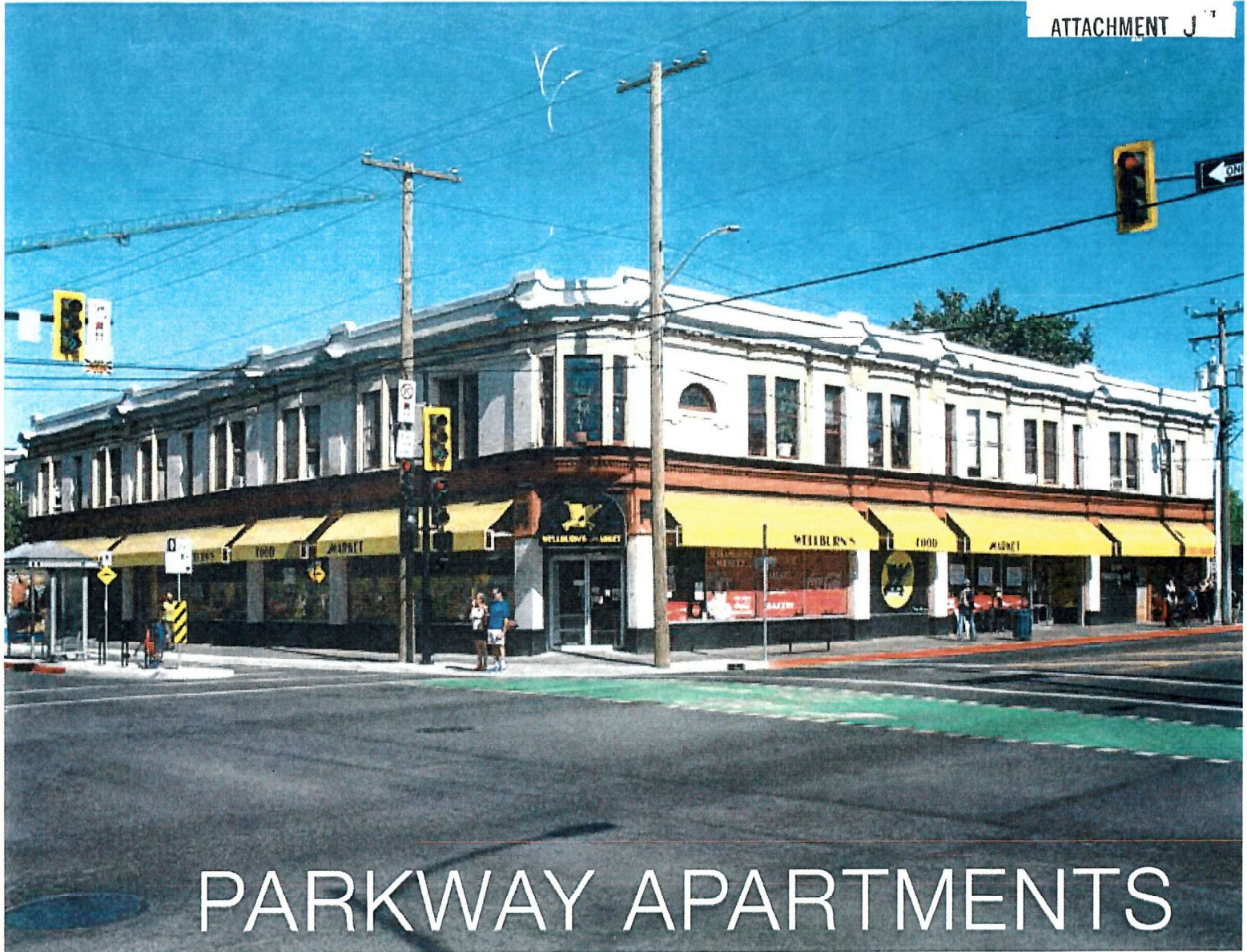
FINAL TAP Review - [For City Staff to complete]

Application received by Hollie McKeil  (City Staff) on November 2 2019  (Date)

Did the applicant meet TAP policy? Yes ☒ No ☐

Staff Comments on
final plan:

The applicant has met policy expectations, and has provided tenants with additional monetary compensation that exceeds policy expectations. Written correspondence with tenants that confirms the details of the Tenant Assistance Plan and Relocation Assistance provided by DJ MAC Consulting has been provided to staff.



PARKWAY APARTMENTS

1050 PANDORA AVENUE, VICTORIA, BC

CONSERVATION PLAN

JULY 2019

DONALD LUXTON
AND ASSOCIATES INC



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Above: Wellburn's Market's Pandora Avenue and Cook Street façades, 1960, City of Victoria Archives M01462

1.0 INTRODUCTION

HISTORIC NAME: Parkway Apartments
CIVIC ADDRESS: 1050 Pandora Avenue, BC
ORIGINAL OWNER: David R. Ker
DATE OF CONSTRUCTION: 1911

Parkway Apartments, located at 1050 Pandora Avenue in Victoria's North Park neighbourhood, is a two-storey masonry building with commercial space on the ground floor and apartments on the second floor. The Edwardian-era building is characterised by its: glazed white brick exterior; full-height commercial storefronts on the ground floor separated by brick pilasters that extend to the parapet level; substantial storefront and roof level metal cornices; chamfered entry on the ground floor; and regular arrangement of second floor fenestration including single and double assembly hung wood windows and projecting oriole wood windows.

The building was constructed in 1911 for David R. Ker and is reflective of the nature of Edwardian-era development in the city. Constructed during the economic boom that preceded the First World War, Parkway Apartments represents the north and eastward push of Victoria's commercial core. The building's combination of residential and commercial spaces reflected the growing need for multi-family housing in the city as well as the commercial vitality of Pandora Avenue, a major thoroughfare in the city.

An overall redevelopment scheme for this property has been prepared by Michael Green Architecture. The nature of the redevelopment of the site involves integrating retained portions of the historic resource within the new construction.

The major proposed interventions of the overall project to the Parkway Apartments building includes:

- Retention of 49% of the original two-storey L-shaped building including: the historic façades of Pandora Avenue, Cook Street, section of the north façade back to the external face of the new six storey volume; structure (columns, beams, joists) of the existing two-storey building from the retained façades to the new six storey volume; retention of interior wood finishes of second floor, where possible.
- Preservation of all character-defining elements that are intact, and completion of in-kind repairs where required. Overall rehabilitation project with restoration of missing character-defining elements.
- Rehabilitation the site through the construction of a new structure behind the retained elements that extends north beyond the heritage resource's current footprint to the adjacent.

This Conservation Plan is based on Parks Canada's *Standards & Guidelines for the Conservation of Historic Places in Canada*. It outlines the preservation, restoration, and rehabilitation that will occur as part of the proposed overall site redevelopment.

2.0 HISTORIC CONTEXT

Parkway Apartments at 1050 Pandora Avenue represents the Edwardian era history of the North Park neighbourhood of Victoria. The building is a tangible reminder of the booming economy of the early twentieth century, which saw the construction of some of Victoria's most prominent buildings. The block's original design consisted of a ground floor with multiple storefronts and upper floor broken into residential suits. The mixed-use design of Parkway Apartments is reflective of the period's increased need for commercial and residential spaces and illustrates the growing presence of locally-based amenities in residential neighbourhoods. The block's architecture and materiality evoke feelings of prosperity and potential reflective of the city's economic position prior to the pre-First World War.

2.1 NORTH PARK NEIGHBOURHOOD HISTORY

North Park is one of Victoria's oldest residential neighbourhoods, and maintains its historical

character of a diverse mixed-use community, bounded by Bay Street, Blanshard Street, Cook Street and Pandora Street. This is Victoria's second smallest neighbourhood, after Harris Green, at one square kilometre, or about eighteen blocks. It is primarily a residential community, grounded by businesses, recreational facilities, and religious landmarks.

The southern portion of the neighbourhood, including the area around Parkway Apartments, developed early and eventually became the business core of North Park. The lot subdivisions of the area were created as part of the Hudson's Bay Company's early town plans and were not initially intended to be used for residential development. However, the close proximity to downtown Victoria contributed to further subdivision and the early residential growth of North Park. The neighbourhood was fairly developed by the 1880s and the 1890 arrival of the electric streetcar line further accelerated growth.



Above: 1050 Pandora Avenue, note 'Groceries' sign to the left of Canadian Imperial Bank of Commerce signs, as well as the streetcar lines overhead, circa 1914, Royal BC Museum D-09079

2.0 HISTORIC CONTEXT



Above: Harris Green with 1050 Pandora Avenue on the left mid-photo, circa 1925, City of Victoria Archives (CVA) M00854

The turn of the twentieth century brought additional waves of prosperity to Victoria, as part of an overall economic expansion through the Edwardian era. It was during this time the Parkway Apartments building was constructed on Pandora Avenue. Some of Victoria's oldest and most beloved apartment blocks, including the Parkway, were constructed in North Park during the early 1910s.

Owing to its location near the city core of Victoria, North Park was one of the first suburban neighbourhoods where wealthy Chinese businessmen and their families settled, after leaving Chinatown before the First World War. The neighbourhood continued to add density through the early twentieth century, as new houses were constructed, larger homes were converted into multiple dwelling units, and additional businesses moved to the area. Growth increased dramatically during the postwar era, as multi-family residential buildings were constructed throughout North Park and the neighbourhood continues to be a popular community within Victoria today.

2.2 HISTORY OF LONG-STANDING TENANT: WELLBURN'S MARKET

The Parkway Apartments building was constructed in 1911 as shops and an apartment complex for businessman David R. Ker, a resident of Rockland. One of the very first occupants was a dry goods merchant, George Alfred Richardson, who also resided in Rockland. The upper floor residential space was originally named Parkway Apartments. Eight tenants were listed there in 1917 and there were eleven in 1921.

Matthew Wellburn was a grocer for his entire working life; he helped in the family grocery store and attended church at St. John the Devine until just three months before his death. Wellburn was quoted as saying that he came to Vancouver Island for a visit in 1910 "and never got over it." In 1911, he returned to England to bring his wife Geraldine and five children to Victoria. Their first home was on Grant Street and the children attended the new Victoria High School. Matthew first worked for another grocer, but around 1912 he set up his own shop at the corner of Camosun Street and Pandora

2.0 HISTORIC CONTEXT



Above: Wellburn's Market before it had taken over nearly the entire ground floor, 1960, CVA M01462

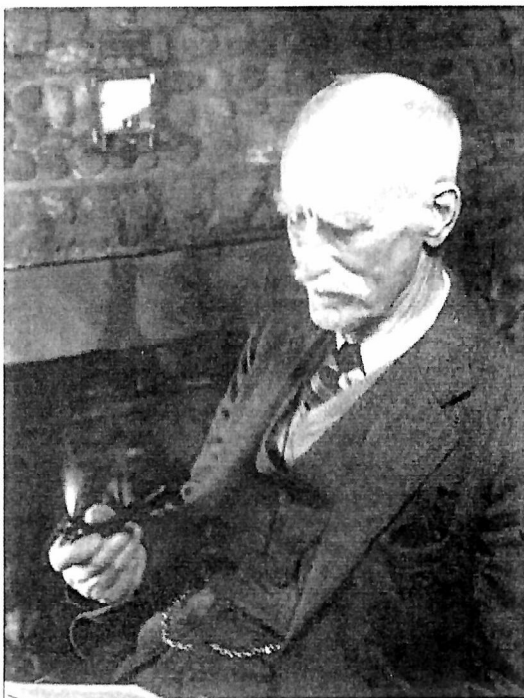
Avenue (the building remains standing). Two years later, he moved the shop into the building at 1050 Pandora Avenue, where two previous grocers had failed. Originally named Wellburn's Cash Grocery Store, the shop grew steadily, taking over a bakery, a bank, a pharmacy and other businesses until it finally became a supermarket. It was managed by Matthew's son George by the end of the 1960s. Several of the Wellburn children were amongst Victoria's top swimmers in the 1920s, most notably son Tom, who established a new Canadian record racing against Johnny Weismuller at the newly opened Crystal Garden pool in 1925. Eldest Wellburn son Gerry went on to establish the BC Forest Museum north of Duncan in 1965. Original Architect: William Ridgway-Wilson. Matthew Wellburn passed away in 1969 at age 98. Wellburn's Market, as well as the upper storey rental apartments, continued to operate well into the second decade of the twenty-first century.

2.3 ORIGINAL ARCHITECT: WILLIAM RIDGWAY-WILSON

Colonel Ridgway-Wilson was a prolific and surprisingly versatile architect. He designed many impressive residences still standing today, and was also responsible for numerous commercial buildings and for several grand military and institutional landmarks. Wilson was born in Hong Kong, China on July 24, 1862, and his family moved to England soon after. He began serving articles in the office of Bromiton Cheers, a Liverpool architect, as early as age thirteen, and later moved to London to work as assistant in the offices of architects, Searles & Hayes, and also with the "legendarily fat" Sir Horace Jones (1819-1887), London City Architect. During his time in London he passed examinations at the South Kensington Science and Art school, which allowed him to lecture on building construction, and to pursue studies at the Royal Academy.

At the end of 1887, Wilson arrived in Victoria and set up his practice. In May 1888 he entered into a

2.0 HISTORIC CONTEXT



W. Ridgway Wilson

short-lived but productive partnership with the experienced and well-travelled frontier architect, Elmer H. Fisher, and they completed buildings in Victoria and Vancouver between 1888-89 before Fisher left for Seattle. In 1889, Wilson was hired by John Mahrer to design the Nanaimo Opera House, a three-storey, brick-faced Italianate structure that seated up to 600 persons. The same year he designed the Queen Anne-style residence for banker Alexander A. Green, named Gyppeswyk, the Saxon name for Green's birthplace, Ipswich. Gyppeswyk survives today as part of the Victoria Art Gallery. In 1890-91, Wilson had a short-lived partnership with T.C. Sorby, and they provided the designs for the Begbie Block in New Westminster, 1890-91, and a commercial block for the five daughters of Sir James Douglas, which

became known as the Five Sisters Block.

In 1889, Wilson had married Flora Alexandra Jenns, daughter of Reverend Jenns of St. John the Divine Church. This was the old iron church that was replaced by the present Hudson's Bay Company department store. When the church sold their property to the HBC, Wilson designed a new Gothic Revival St. John the Divine in 1912, of dark brick with contrasting light stonework. His funeral would be held in this church forty-five years later. From 1892, Wilson operated as a sole practitioner, and his career took off with numerous grand houses, commercial blocks, and large institutional projects to his credit. His early institutional projects included Victoria's South Park School, 1894, inspired by the contemporary school architecture of London. The passage of the revised Public Schools Act in 1891 transferred control of educational funds to local school boards, and rapid population growth put a great deal of pressure the boards, to provide more classroom space. On May 11, 1893 the Victoria School Board announced a competition, open to Victoria architects, to design two graded elementary schools, in the North and South Wards. The rules specified brick construction, with stone foundations and slate roofs, with each school containing eight classrooms. A total of twelve entries were received, and the Board, unable to make a decision, asked Vancouver architect, R. Mackay Frupp, to act



Right: Gyppeswyk, now part of the Art Gallery of Greater Victoria

2.0 HISTORIC CONTEXT

as judge. Fripp awarded the highest honours to Messrs. Soule & Day's entry, which was assigned to the North Ward site. The rest of the entries were considered "fairly level in merit," and S.M. Goddard was awarded a qualified second place, and Wilson placed third. The School Board, however, preferred Wilson's interior layout, and chose his design for the other school, which survives today as South Park School. Wilson also undertook the isolation hospital at Royal Jubilee, 1893, and alterations and additions to the Provincial Asylum in New Westminster in 1897-98. Victoria West School, 1907-08, and additions to Lampson Street School, Esquimalt, 1913, followed later in Wilson's career.

Commercial projects of interest included his Italianate designs for Chinese clients in Victoria's "Chinatown," such as the Loo Tai Cho Building, 1893. Wilson, in addition to being the architect for the B.C. Land & Investment Agency from 1894 on, designed many buildings like the two-storey Porter Block, 1897, and Mahon Block, 1907, in Victoria. The W. & J. Wilson Building, 1912, is a remarkable exercise in restrained Classicism, with two ornate pilasters serving as bookends to a wall of glass display windows, the entire structure capped by a simple cornice. Examples of his diverse residential work in Victoria include: Schuuum, a large Tudor residence, built for Hewitt Bostock, 1894; the Charles Spratt Residence on the Gorge, 1894, later the home of Premier McBride who named it Glenelg; a two-storey brick house for Dr. Charles F. Newcombe on Dallas Road, 1907-08; a grand turreted house on Rockland Avenue for Dr. Jones, 1908; and Lotbinière, also for Charles Spratt, on Lotbinière Avenue, 1909. In 1899, Wilson built his own Arts and Crafts-influenced home on the Gorge, with distinctive jerkin-headed roofs and elaborate coursed shingle siding.

Wilson joined the militia in 1899, and two of his last major commissions were related to his military contacts. He provided plans for both the Victoria Drill Hall, now the Bay Street Armoury, Victoria, 1913-15, and the Colquitz Jail, on Wilkinson Road in Saanich, 1914. These two large structures maintain the Victorian aesthetic prevalent through Wilson's career, their crenellated tops and towers

giving a picturesque Gothic interpretation to these institutions of discipline. Wilson achieved the rank of Colonel, and during the First World War was in charge of the internment of enemy aliens on the west coast. At first, the aliens were kept in the Saanich prison that Wilson himself designed, and later they were moved to an Internment Camp in Vernon, of which Wilson became the commander.

After the outbreak of war in 1914, Wilson went into partnership with Alexander Robert Hennell, an association that lasted until 1918. Hennell carried on the architectural business while Wilson went into the army full-time. By the 1920s Wilson was approaching retirement age and took on smaller projects, although he did not officially retire until 1940.

Ridgway and Flora had five children: Basil, Guy, Percy, Hebden, and Daisy. His grandchildren, who all called him "Pop," were expected to stand when he entered the room. They remember his military bearing and declare that he marched everywhere, never merely walked. He had a bad habit of jay-walking, as he believed it his right to step off the curb wherever and whenever it suited him. Asked to give away his granddaughter Barbara, at her wedding, he was willing but declared, "I have to get to a cricket match that day." She arranged a morning service and wedding breakfast to accommodate his plans, and afterwards the groom drove him to the match which he was attending not as a player, but merely as a devoted spectator.

Despite these stiff characteristics of an English-born military officer, the present writer remembers his friendly piercing eyes and his smart, straight bearing. In 1927, the Wilsons moved in with their son Basil; Flora died in 1939. Wilson continued to be seen daily climbing the stairs from the fifth floor of the Royal Trust (Union) Building to the roof-top quarters of the British Empire Club of which he was a member. He died on February 21, 1957 at the age of ninety-four, and was interred in the family plot in Colwood Burial Park.

3.0 STATEMENT OF SIGNIFICANCE

Address: 1050 Pandora Avenue, Victoria, British Columbia

Name: Parkway Apartments

Original Owner: David R. Ker

Original Architect: William Ridgway Wilson

Construction Date: 1911

Description of the Historic Place

The Parkway Apartments building is a two-storey commercial building situated on the north side of Pandora Avenue in the North Park neighbourhood of Victoria. The historic building is distinguishable by its chamfered corner, prominent cornices, and alternating bays along the second storey.

Heritage Value of Historic Place

Parkway Apartments is significant for its association with the Edwardian-era development of Victoria and its long-standing grocery store tenancy on the ground floor. The building is valued additionally for its commercial architectural style, as designed by William Ridgway-Wilson.

Constructed during the upswing of the pre-World War One real estate boom, Parkway Apartments is valued as part of the surge of development that characterized Victoria's gateway economy during the Edwardian-era period. Built 1911, the building has been used continuously for commercial purposes on the ground floor, with residential space on the upper floor. Originally constructed for Rockland resident David R. Ker, this two-storey structure represents the north and eastward expansion of Victoria's commercial core. The variety of commercial uses attest to the adaptability of this structure and the commercial vitality of Pandora Avenue, one of the major thoroughfares to the eastern part of the City and the adjacent municipality of Oak Bay.

The building is additionally significant for its vernacular Edwardian era architecture as designed by William Ridgway-Wilson. Ridgway-Wilson designed many architecturally important projects that continue to define the character of Victoria, such as Gyppeswyk, now part of the Art Gallery of Greater Victoria (1889), St. John the Divine Anglican Church (1912), and the Victoria Drill Hall (now the Bay Street Armoury, 1913-15).

Character-Defining Elements

The key elements that define the heritage character of Parkway Apartments include, but are not limited to its:

- location on north side of Pandora Avenue at Cook Street in the North Park neighbourhood of Victoria;
- siting on the property lines, with no setbacks;
- mixed commercial and residential use;
- commercial form, scale and massing as expressed by its two-storey height, L-shaped plan, low-slope shed roof, full retail storefront on ground level of Pandora Avenue and Cook Street façades;
- masonry construction of red brick with grey mortar laid in common bond; face brick of white glazed brick laid in running bond with white mortar on the façades of Pandora Avenue and Cook Street; parging on the base of brick pilasters and at storefront transom level on the street façades; concrete lintels; internal red brick chimney;
- Edwardian-era architectural features including its: commercial and residential design with full-height storefront on the ground floor; brick piers extending from grade to parapet delineating storefront bays; decorative pressed metal cornices at the storefront and roof levels; regular arrangement of fenestration on the upper floor; flat parapet punctuated by raised portions which align with brick piers on Cook Street and Pandora Avenue façades; and
- original fenestration including: multi-light wood storefront transoms spanning each storefront bay of Pandora Avenue and Cook Street façades; single assembly hung wood frame and sash windows with parged brick sills on rear façade; single and double assembly hung wooden-sash windows, oriole windows with hung wood frame and sash windows on either side of a fixed multi-light wood frame and sash windows on the second storey of Pandora Avenue and Cook Street façades; half-moon leaded glass window with wood frame on Cook Street façade; three panel wood doors with wood transom windows on rear façade.

4.0 CONSERVATION GUIDELINES

4.1 STANDARDS AND GUIDELINES

Parkway Apartments is a registered property included on the City of Victoria's Register of Heritage Properties and is a significant historical resource in the City of Victoria and its North Park neighbourhood. Parks Canada's *Standards & Guidelines for the Conservation of Historic Places in Canada* (2010) is the source used to assess the appropriate level of conservation and intervention to heritage resources. Under the *Standards & Guidelines*, the work proposed for Parkway Apartments will include aspects of preservation, rehabilitation, and restoration.

Preservation: *the action or process of protecting, maintaining, and/or stabilizing the existing materials, form, and integrity of a historic place or of an individual component, while protecting its heritage value.*

Restoration: *the action or process of accurately revealing, recovering or representing the state of a historic place or of an individual component, as it appeared at a particular period in its history, while protecting its heritage value.*

Rehabilitation: *the action or process of making possible a continuing or compatible contemporary use of a historic place or an individual component, through repair, alterations, and/or additions, while protecting its heritage value.*

Interventions to Parkway Apartments should be carried out based upon the Standards outlined in the *Standards & Guidelines*, which are conservation principles of best practice. The following **General Standards** should be followed when carrying out any work to an historic property.

STANDARDS

Standards relating to all Conservation Projects

1. Conserve the heritage value of a historic place.

Do not remove, replace, or substantially alter its intact or repairable character-defining elements. Do not move a part of a historic place if its current location is a character-defining element.

2. Conserve changes to a historic place, which over time, have become character-defining elements in their own right.
3. Conserve heritage value by adopting an approach calling for minimal intervention.
4. Recognize each historic place as a physical record of its time, place and use. Do not create a false sense of historical development by adding elements from other historic places or other properties or by combining features of the same property that never coexisted.
5. Find a use for a historic place that requires minimal or no change to its character defining elements.
6. Protect and, if necessary, stabilize a historic place until any subsequent intervention is undertaken. Protect and preserve archaeological resources in place. Where there is potential for disturbance of archaeological resources, take mitigation measures to limit damage and loss of information.
7. Evaluate the existing condition of character-defining elements to determine the appropriate intervention needed. Use the gentlest means possible for any intervention. Respect heritage value when undertaking an intervention.
8. Maintain character-defining elements on an ongoing basis. Repair character-defining elements by reinforcing the materials using recognized conservation methods. Replace in kind any extensively deteriorated or missing parts of character-defining elements, where there are surviving prototypes.
9. Make any intervention needed to preserve character-defining elements physically and visually compatible with the historic place and identifiable upon close inspection. Document any intervention for future reference.

Additional Standards relating to Rehabilitation

10. Repair rather than replace character-defining elements. Where character-defining elements are too severely deteriorated to repair, and

4.0 CONSERVATION GUIDELINES

where sufficient physical evidence exists, replace them with new elements that match the forms, materials and detailing of sound versions of the same elements. Where there is insufficient physical evidence, make the form, material and detailing of the new elements compatible with the character of the historic place.

11. Conserve the heritage value and character-defining elements when creating any new additions to a historic place and any related new construction. Make the new work physically and visually compatible with, subordinate to and distinguishable from the historic place.
12. Create any new additions or related new construction so that the essential form and integrity of a historic place will not be impaired if the new work is removed in the future.

Additional Standards relating to Restoration

13. Repair rather than replace character-defining elements from the restoration period. Where character-defining elements are too severely deteriorated to repair and where sufficient physical evidence exists, replace them with new elements that match the forms, materials and detailing of sound versions of the same elements.
14. Replace missing features from the restoration period with new features whose forms, materials and detailing are based on sufficient physical, documentary and/or oral evidence.

4.2 CONSERVATION REFERENCES

The following conservation resources should be referred to when considering any interventions to the Parkway Apartments building:

Standards and Guidelines for the Conservation of Historic Places in Canada, Parks Canada, 2010.
<http://www.historicplaces.ca/en/pages/standards-normes/document.aspx>

National Park Service, Technical Preservation Services. Preservation Briefs:

Preservation Brief 1: Assessing Cleaning and Water-Repellent Treatments for Historic Masonry Buildings.

<http://www.nps.gov/tps/how-to-preserve/briefs/1-cleaning-water-repellent.htm>

Preservation Brief 2: Repointing Mortar Joints in Historic Masonry Buildings.

<http://www.nps.gov/tps/how-to-preserve/briefs/2-repoint-mortar-joints.htm>

Preservation Brief 3: Improving Energy Efficiency in Historic Buildings.

<http://www.nps.gov/tps/how-to-preserve/briefs/3-improve-energy-efficiency.htm>

Preservation Brief 4: Roofing for Historic Buildings.

<http://www.nps.gov/tps/how-to-preserve/briefs/4-roofing.htm>

Preservation Brief 6: Dangers of Abrasive Cleaning to Historic Buildings.

<http://www.nps.gov/tps/how-to-preserve/briefs/6-dangers-abrasive-cleaning.htm>

Preservation Brief 9: The Repair of Historic Wooden Windows.

<http://www.nps.gov/tps/how-to-preserve/briefs/9-wooden-windows.htm>

Preservation Brief 10: Exterior Paint Problems on Historic Woodwork.

<http://www.nps.gov/tps/how-to-preserve/briefs/10-paint-problems.htm>

Preservation Brief 11: Rehabilitating Historic Storefronts.

<http://www.nps.gov/tps/how-to-preserve/briefs/11-storefronts.htm>

Preservation Brief 14: New Exterior Additions to Historic Buildings: Preservation Concerns.

<http://www.nps.gov/tps/how-to-preserve/briefs/14-exterior-additions.htm>

4.0 CONSERVATION GUIDELINES

Preservation Brief 16: The Use of Substitute Materials on Historic Buildings.

<http://www.nps.gov/tps/how-to-preserve/briefs/16-substitute-materials.htm>

Preservation Brief 17: Architectural Character – Identifying the Visual Aspects of Historic Buildings as an Aid to Preserving their Character.

<http://www.nps.gov/tps/how-to-preserve/briefs/17-architectural-character.htm>

Preservation Brief 32: Making Historic Properties Accessible.

<http://www.nps.gov/tps/how-to-preserve/briefs/32-accessibility.htm>

Preservation Brief 33: The Preservation and Repair of Historic Stained and Leaded Glass.

<http://www.nps.gov/tps/how-to-preserve/briefs/33-stained-leaded-glass.htm>

Preservation Brief 35: Understanding Old Buildings: The Process of Architectural Investigation.

<http://www.nps.gov/tps/how-to-preserve/briefs/35-architectural-investigation.htm>

Preservation Brief 37: Appropriate Methods of Reducing Lead-Paint Hazards in Historic Housing.

<http://www.nps.gov/tps/how-to-preserve/briefs/37-lead-paint-hazards.htm>

Preservation Brief 39: Holding the Line: Controlling Unwanted Moisture in Historic Buildings.

<http://www.nps.gov/tps/how-to-preserve/briefs/39-control-unwanted-moisture.htm>

Preservation Brief 41: The Seismic Retrofit of Historic Buildings: Keeping Preservation in the Forefront.

<http://www.nps.gov/tps/how-to-preserve/briefs/41-seismic-retrofit.htm>

Preservation Brief 44: The Use of Awnings on Historic Buildings.

<http://www.nps.gov/tps/how-to-preserve/briefs/44-awnings.htm>

Preservation Brief 47: Maintaining the Exterior of Small and Medium Size Historic Buildings.

<http://www.nps.gov/tps/how-to-preserve/briefs/47-maintaining-exterior.htm>

4.3 GENERAL CONSERVATION STRATEGY

The primary intent is to preserve components of the existing structure while undertaking a rehabilitation that will upgrade its structure and services to increase its functionality for use. As part of the scope of work, intact character-defining elements will be preserved, damage or missing elements will be repaired in-kind using existing examples, historic precedents, and archival documents as guides in their rehabilitation. An overall redevelopment scheme for the site has been prepared by Michael Green Architecture, and includes the heritage resource situated at 1050 Pandora Avenue as well as the parking lot to the north of the building.

The major proposed interventions of the overall project include the:

- Retention of 49% of the original two-storey L-shaped building including: the historic façades of Pandora Avenue, Cook Street, section of the north façade back to the external face of the new six storey volume; structure (columns, beams, joists) of the existing two-storey building from the retained façades to the new six storey volume; retention of interior wood finishes of second floor, where possible.
- Preservation of all character-defining elements that are intact, and completion of in-kind repairs where required. Overall rehabilitation project with restoration of missing character-defining elements.
- Rehabilitation the site through the construction of a new structure behind the retained elements that extends north beyond the heritage resource's current footprint to the adjacent.

Due to the proposed addition to the historic building, all new visible construction will be considered a

4.0 CONSERVATION GUIDELINES

modern addition to the historic structure. *Standards & Guidelines* list recommendations for new additions to historic places. The proposed design scheme should follow these principles:

- Design a rehabilitation of the exterior of the existing buildings that will be sympathetic to heritage character-defining elements.
- Design additions in a manner that draws a clear distinction between what is historic and what is new.
- Design for the new work should be contemporary, but should be compatible in terms of mass, materials, relationship of solids to voids, and colour, yet be distinguishable from the historic place.
- The new additions should be physically and visually compatible with, subordinate to and distinguishable from the preserved historic façades.

Additions or new construction should be visually compatible with, yet distinguishable from, the historic place. To accomplish this, an appropriate balance must be struck between mere imitation of the existing form and pointed contrast, thus complementing the historic place in a manner that respects its heritage value.

4.4 SUSTAINABILITY STRATEGY

Heritage conservation and sustainable development can go hand in hand with the mutual effort of all stakeholders. In a practical context, the conservation and re-use of historic and existing structures contributes to environmental sustainability by reducing solid waste disposal, saving embodied energy, and conserving historic materials that are often less consumptive of energy than many new replacement materials.

In 2016, the Federal Provincial Territorial Ministers of Culture & Heritage in Canada (FPTMCHC) published a document entitled, *Building Resilience: Practical Guidelines for the Retrofit and Rehabilitation of Buildings in Canada* that is “intended to establish a common pan-Canadian ‘how-to’ approach for practitioners, professionals, building owners, and

operators alike.”

The following is an excerpt from the introduction of the document:

*[Building Resilience] is intended to serve as a “sustainable building toolkit” that will enhance understanding of the environmental benefits of heritage conservation and of the strong interrelationship between natural and built heritage conservation. Intended as a useful set of best practices, the guidelines in **Building Resilience** can be applied to existing and traditionally constructed buildings as well as formally recognized heritage places.*

These guidelines are primarily aimed at assisting designers, owners, and builders in providing existing buildings with increased levels of sustainability while protecting character-defining elements and, thus, their heritage value. The guidelines are also intended for a broader audience of architects, building developers, owners, custodians and managers, contractors, crafts and trades people, energy advisers and sustainability specialists, engineers, heritage professionals, and officials responsible for built heritage and the existing built environment at all jurisdictional levels.

***Building Resilience** is not meant to provide case-specific advice. It is intended to provide guidance with some measure of flexibility, acknowledging the difficulty of evaluating the impact of every scenario and the realities of projects where buildings may contain inherently sustainable elements but limited or no heritage value. All interventions must be evaluated based on their unique context, on a case-by-case basis, by experts equipped with the necessary knowledge and experience to ensure a balanced consideration of heritage value and*

4.0 CONSERVATION GUIDELINES

sustainable rehabilitation measures.

***Building Resilience** can be read as a stand-alone document, but it may also further illustrate and build on the sustainability considerations in the *Standards and Guidelines for the Conservation of Historic Places in Canada*.*

4.5 ALTERNATE COMPLIANCE

Parkway Apartments is a registered heritage building listed on the City of Victoria's Register of Heritage Properties. Parkway Apartments is eligible for designation and therefore may be eligible for heritage variances that will enable a higher degree of heritage conservation and retention of original material, including considerations available under the following municipal legislation.

4.5.1 BRITISH COLUMBIA BUILDING CODE

Building Code upgrading ensures life safety and long-term protection for historic resources. It is important to consider heritage buildings on a case-by-case basis, as the blanket application of Code requirements do not recognize the individual requirements and inherent strengths of each building. Over the past few years, a number of equivalencies have been developed and adopted in the British Columbia Building Code that enable more sensitive and appropriate heritage building upgrades. For example, the use of sprinklers in a heritage structure helps to satisfy fire separation and exiting requirements. Table A-1.1.1.1., found in Appendix A of the Code, outlines the "Alternative Compliance Methods for Heritage Buildings."

Given that Code compliance is such a significant factor in the conservation of heritage buildings, the most important consideration is to provide viable economic methods of achieving building upgrades. In addition to the equivalencies offered under the current Code, the City can also accept the report of a Building Code Engineer as to acceptable levels of code performance.

4.5.2 ENERGY EFFICIENCY ACT

The provincial Energy Efficiency Act (Energy Efficiency Standards Regulation) was amended in 2009 to exempt buildings protected through heritage designation or listed on a community heritage register from compliance with the regulations. Energy Efficiency standards therefore do not apply to windows, glazing products, door slabs or products installed in heritage buildings. This means that exemptions can be allowed to energy upgrading measures that would destroy heritage character-defining elements such as original windows and doors.

These provisions do not preclude that heritage buildings must be made more energy efficient, but they do allow a more sensitive approach of alternate compliance to individual situations and a higher degree of retained integrity. Increased energy performance can be provided through non-intrusive methods of alternate compliance, such as improved insulation and mechanical systems. Please refer to the *Standards & Guidelines for the Conservation of Historic Places in Canada* for further detail about "Energy Efficiency Considerations."

4.6 SITE PROTECTION & STABILIZATION (FOR SHORT PERIOD OF TIME)

It is the responsibility of the owner to ensure the heritage resource is protected from damage at all times. At any time that the building is left vacant, it should be secured against unauthorized access or damage through the use of appropriate fencing and security measures.

Additional measures to be taken include:

- Are smoke and fire detectors in working order?
- Are wall openings boarded up and exterior doors securely fastened once the building is vacant?
- Have the following been removed from the interior: trash, hazardous materials such as inflammable liquids, poisons, and paints and

4.0 CONSERVATION GUIDELINES

canned goods that could freeze and burst?

The retained elements of the building should be protected from movement and other damage at all times during demolition, excavation and construction work. Install monitoring devices to document and assess cracks and possible settlement of the masonry façade.

Parkway Apartments is currently occupied by multiple commercial businesses on the ground floor. The upper floor consists of multiple apartments, which at the time of the site visit appear to all be occupied by tenants. If during the course of the project the building is vacated by its tenants, the structure should be made safe and closed up to protect it from the weather, unauthorized access, and vandalism.

The following checklist will ensure that work items for the protection during the temporary mothballing of the historic structure are not inadvertently omitted and the listed heritage resource secured:

Moisture

- ☐ Is the roof watertight?
- ☐ Is exterior cladding in good condition to keep water out?
- ☐ Is the site of the temporary location properly graded for water run-off?

Ventilation

- ☐ Have steps been taken to ensure proper ventilation of the building?
- ☐ Have interior doors been left open for ventilation purposes?
- ☐ Has the secured building been checked within the last 3 months for interior dampness or excessive humidity?

Pests

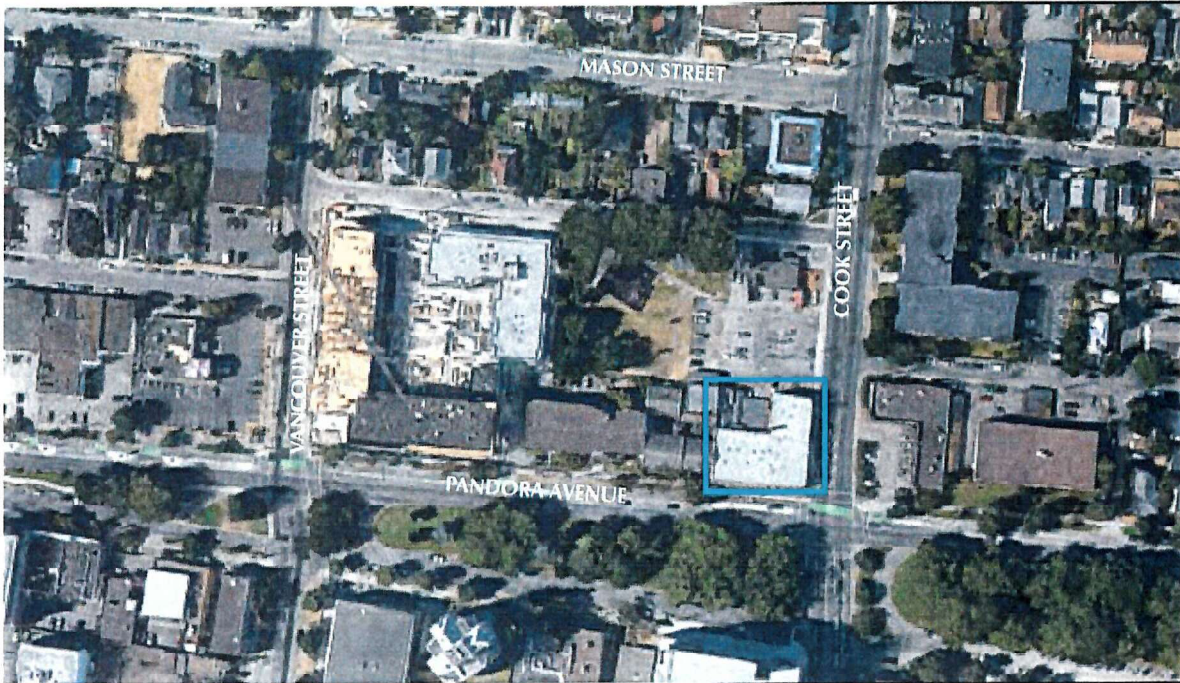
- ☐ Have nests/pests been removed from the building's interior and eaves?
- ☐ Are adequate screens in place to guard against pests?
- ☐ Has the building been inspected and treated for termites, carpenter ants, rodents, etc.?

Security

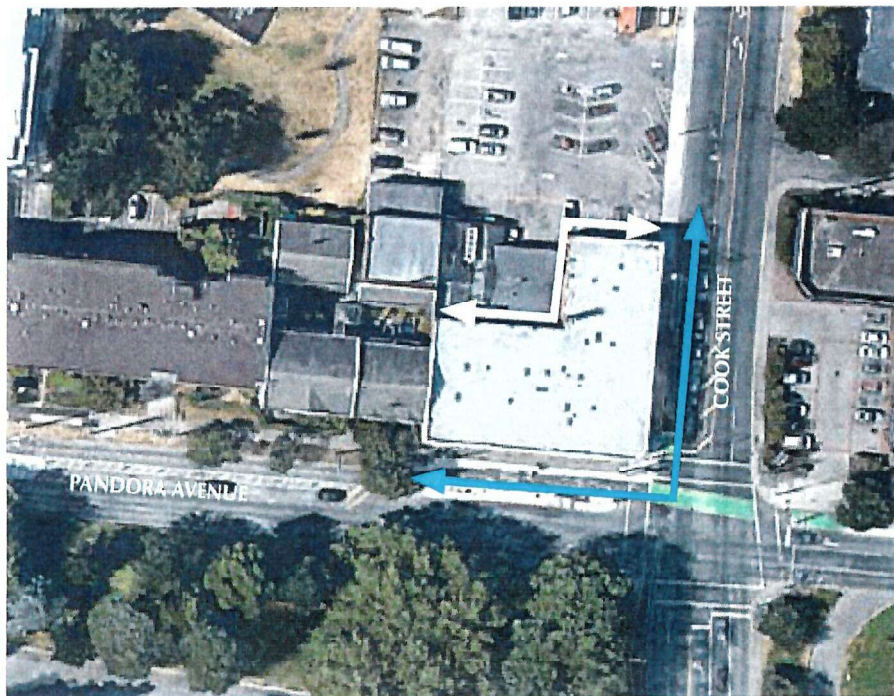
- ☐ Are smoke and fire detectors in working order?
- ☐ Are wall openings boarded up and exterior doors securely fastened?
- ☐ Are plans in place to monitor the building on a regular basis?
- ☐ Are the keys to the building in a secure but accessible location?
- ☐ Are the grounds being kept from becoming overgrown?
- ☐ Have the following been removed from the interior: trash, hazardous materials such as inflammable liquids, poisons, and paints and canned goods that could freeze and burst?
- ☐ Is the site securely fenced and regularly patrolled?
- ☐ Is the building signed identifying it as a protected heritage building with a phone number for citizens to call with questions or concerns or report vandals?

The aforementioned items will assist in protecting the listed heritage resource that is currently unoccupied during the planning process until actual site work commences.

5.0 CONSERVATION RECOMMENDATIONS



Above: Parkway Apartments (within blue box) and adjacent streets in Victoria.



Above: For reference clarification "street" façades are those noted in blue arrows and "non-street" façades are those in white arrow.

5.0 CONSERVATION RECOMMENDATIONS

A condition review of the Parkway Apartments was carried out during a site visit in July 2018. The condition assessment was limited to a visual review of the condition of the exterior of the building. The interior of the building was not assessed and no intrusive or destructive testing was carried out as part of the condition assessment. The recommendations for the conservation of the historic resource are based on the site review and available archival documents that provide valuable information about the original design and construction of the historic building. Further investigation may be required to determine the structural integrity of the original materials, in addition to retrieval of samples (e.g. additional paint samples from inaccessible areas) from the exterior of the building for further review.

The following describes the materials, their condition and recommended conservation strategy for Parkway Apartments based on Parks Canada's *Standards & Guidelines for the Conservation of Historic Places in Canada*.

5.1 SITE

The historic Parkway Apartments is situated in its original location at the northwest corner of the intersection of Pandora Avenue and Cook Street in Victoria's North Park neighbourhood. The building sits on a large lot, built to the property line with the primary façades orientated to Pandora Avenue and Cook Street. To the north of the extant heritage resource is a large lot currently being used for parking. A small park, Franklin Green, is located northwest of the building and a residential development meets the west façade of Parkway Apartments. The rear of the building possesses later one-storey additions.

The intended redevelopment plan for the site is for the façades of Pandora Avenue and Cook Street, and the north façade to the external face of the proposed new six storey volume, as well as portion of the existing two-storey structure parallel to Cook Street back to the new six storey volume to be retained. The redevelopment of the site encompasses the construction of new volume behind and connected

to the retained elements. The new construction will also encompass the lot to the north of the extant building currently being used for parking.

Conservation Strategy: Preservation and Rehabilitation

- Preserve the original setting of the building. All rehabilitation work should occur within the property lines.
- Retain the main frontages of Pandora Avenue and Cook Street. Preserve as well as the north façade and portion of the two-storey structure parallel to Cook Street both to the external face of the new six storey volume. New construction to be constructed in a manner that is "physically and visually compatible with, subordinate to, and distinguishable from the historic place" as recommended in **Standard 11**.



Top: Insurance Plan of Victoria, British Columbia 1903, revised 1909, showing northwest corner of Pandora Avenue and Cook Street.

Bottom: Insurance Plan of Victoria, British Columbia Volume 1, 1911 revised 1913 showing northwest corner of Pandora Avenue and Cook Street with Parkway Apartments constructed.

5.0 CONSERVATION RECOMMENDATIONS

5.2 FORM, SCALE, & MASSING

Parkway Apartments is a two-storey masonry building reflective of the commercial/residential buildings constructed during Victoria's pre-First World War boom period. The two-storey portion of the building possesses a L-shaped plan with its "street" façades fronting Pandora Avenue and Cook Street. Single storey additions are present on the rear, north, façade of the building. The building's form, scale, and massing of: L-shaped plan; two-

storey height; low-pitch roof are character-defining elements which should be preserved. Parkway Apartments' original form, scale, and massing are largely intact; however, there have been alterations made to the rear of the building through multiple one-storey additions.

The current understanding of the redevelopment of the site is for the façades of Pandora Avenue and Cook Street, and the north façade to the external face of the proposed new six storey volume, as



Above and Below: Street façades of Pandora Avenue (above) and Cook Street (below). The building is characterised by its two-storey height, masonry construction, storefront bays, second floor fenestration patterning, storefront and rooftop cornices, parapet, chamfered entry are shown.



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Above: "Non-street" (north) façade of Parkway Apartments showing later added additions to the rear of the building and parking lot located adjacent and to the north of the building.



Above: "Non-street" west façade and adjacent residential development.

well as portion of the existing two-storey structure parallel to Cook Street extending to the external face of the new six storey volume to be preserved. Any new construction above the original two-storey height of the building should be sensitive to the historic scale and massing of the building as well as sympathetic to its materiality. This may included such approaches as stepping back the new construction from the historic façades, utilizing materials that are sympathetic but do not detract from the historic appearance of the retained façades.

Conservation Strategy: Preservation and Rehabilitation

- The form and scale of the Pandora Avenue Cook Street, and the north façade to the external face of the proposed new six storey volume will be retained. A portion of the existing two-storey structure parallel to Cook Street extending back to the external face new six storey volume shall also be retained.
- All modern additions should be sensitive to the scale and massing of the retained façades of the building, and should read as contemporary additions.

5.0 CONSERVATION RECOMMENDATIONS

5.3 FOUNDATIONS

The building's foundation was not accessed for review. Based on the current understanding of the redevelopment of the site and the retention of the street façades, portion of the north façade as well as portion of the structure parallel to Cook Street, the latter two will be retained back to the external face of the new volume, it is assumed as part of this work, the existing foundation will be preserved to achieve this level of retention. Careful attention should be taken during stabilization, demolition, repair, and new construction to ensure the exterior masonry walls above grade and retained structure are not damaged during any phase of the project. Care should also be taken to ensure that the decorative elements of the "street" façades such as the cornices and fenestration are not damaged during the course of the project.

Conservation Strategy: Preservation and Rehabilitation

- Existing foundations should be preserved, if possible.
- Foundations should be reviewed by a Structural Engineer. Once condition is assessed, conservation recommendations can be finalized.
- To ensure the prolonged preservation of the all foundations, all landscaping should be separated from the foundations at grade by a course of gravel or decorative stones, which help prevent splash back and assist drainage. New vegetation may assist in concealing the newly exposed foundations, if desired.

5.4 EXTERIOR MASONRY WALLS

5.4.1 "STREET" FAÇADES - PANDORA AVENUE (SOUTH) AND COOK STREET (EAST)

Parkway Apartments features façades load-bearing brick walls. The façades of Pandora Avenue and Cook Street are faced with a wythe of glazed white brick with white mortar laid in running bond. This brick is present on the pilasters separating the storefront bays, the second floor, and the parapet.

Materials and finishes of higher value were often placed on street façades to project a sense of status and prosperity. When the building was completed, the white brick contrasted the darker finish of the fenestration, storefronts, and cornices, as shown in historic photographs. The glazed brick contributes significantly to the heritage character of Parkway Apartments and is to be preserved.

Visual inspection of the brick from the ground was conducted. Overall, the glazed brick of the Pandora Avenue and Cook Street façades is in good condition. Deterioration that is present appears in the form of chips to the glazing (typically at the arises of bricks and outside corners of the building), mortar loss, localized areas of staining of mortar, organic deposits in some areas, and over painting on to the brick. Redundant fixtures and anchors are also present on both "street" façades which should be removed and any resulting holes repaired.



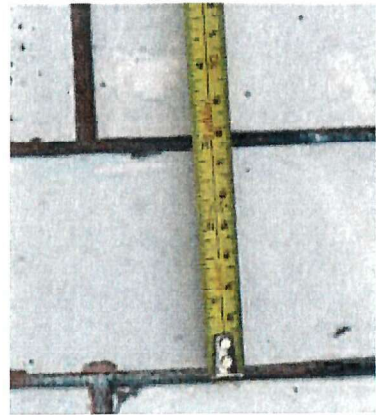
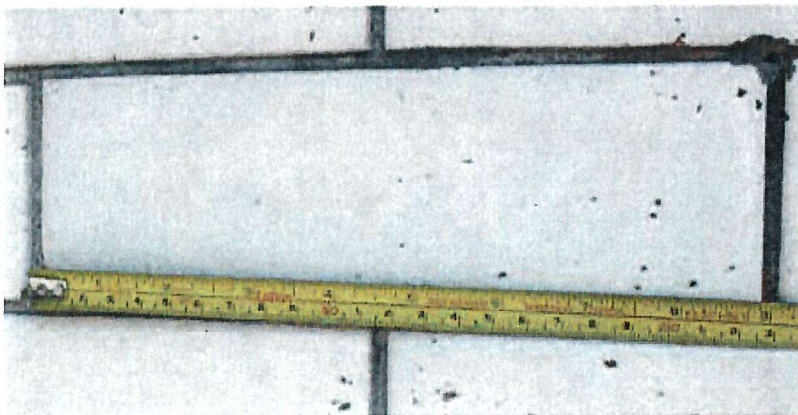
Above: Typical brick damage of chips and paint on brick face from painting adjacent wood elements, present on both "street" façades.

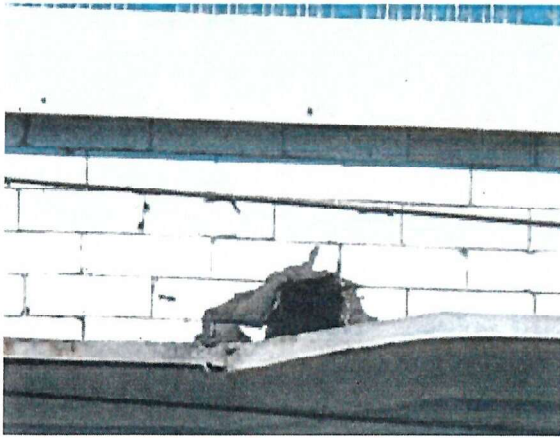
5.0 CONSERVATION RECOMMENDATIONS



Left: Exterior of “street” façades with brick laid in running bond with white mortar. Brick present on pilasters separating storefront bays, second floor walls, and parapet. Overall in good condition with local areas of deterioration.

Below Left and Right: Existing brick of “street” façades showing mortar joints





Above: Later installed scupper above roof cornice on Cook Street façade. Weather proofing material applied onto the face of the parapet.

Alterations to the “street” façades include the addition of a thru-wall scupper installed above the rooftop cornice on the Cook Street façade. The waterproofing of the scupper has been spread onto the adjacent brick. Consideration should be given to ensuring the roof drains in a manner that permits the deletion of this later added scupper. Additional alterations include: the base of the glazed brick pilasters being parged; increase height and parging of the original glazed brick storefront bulkhead; removal of the pairs of cylindrical ‘chimney-like’ elements positioned at the top of the pilasters at the parapet level.

Conservation Strategy: Preservation

- Overall, preserve the brick of retained façades and replace in-kind brickwork that is too deteriorated to retain.
- Undertake complete condition survey of all brick surfaces.
- Retain sound exterior masonry or deteriorated exterior masonry that can be repaired.
- All masonry cleaning, repair, and repointing specifications to be reviewed by Heritage Consultant.
- All redundant metal inserts/anchors should be removed and, where possible, services mounted on the exterior walls removed or reconfigured.
- Any holes in the brick should be filled, if possible, or replace damaged brick with new

to match existing.

- Overall cleaning of the brick façades should be carried out. Do not use any abrasive methods that may damage the fireskin surfaces. Use a soft natural bristle brush and mild water rinse. Only approved chemical restoration cleaners may be used. Sandblasting or any other abrasive cleaning method of any kind is not permitted.
- Determine whether or not it is feasible to remove paint present on glazed brick and on retained portion of north and west façades without damaging underlying material. Undertake test samples of paint removal in an inconspicuous area using only approved restoration products. If paint removal is determined to be feasible, prepare removal specification. If not, leave paint *in situ* and ensure brick is adequately protected during any future painting of adjacent assemblies.
- Repoint brickwork by raking out loose mortar material to a uniform depth. Take care that the rises of the brick are not damaged. Work should only be undertaken by skilled masons. Do not use power tools to cut or grind joints. Repoint mortar joints with new mortar that matches existing in consistency, composition, strength, colour and pointing profile; note the finely tooled profile of the original mortar joints.

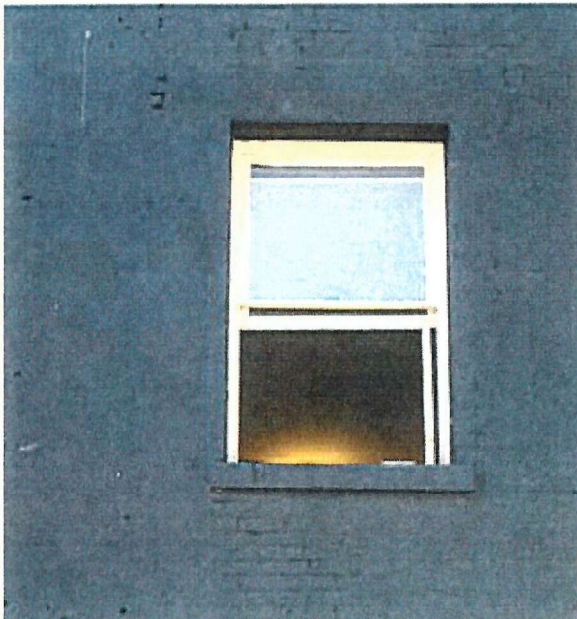
5.4.2 “NON-STREET” (NORTH AND WEST) FAÇADES

The “non-street” façades consist of red brick with cream mortar laid in common bond. Overall, the brick is in fair condition with localized areas of deterioration noted. Portions of the brick walls have been painted. The paint prevents the examination and assessment of the underlying brick. Paint can trap moisture behind the surface in the brick causing it to deteriorate. The paint may also contain lead and necessary precautions must be taken in dealing with such hazardous materials.

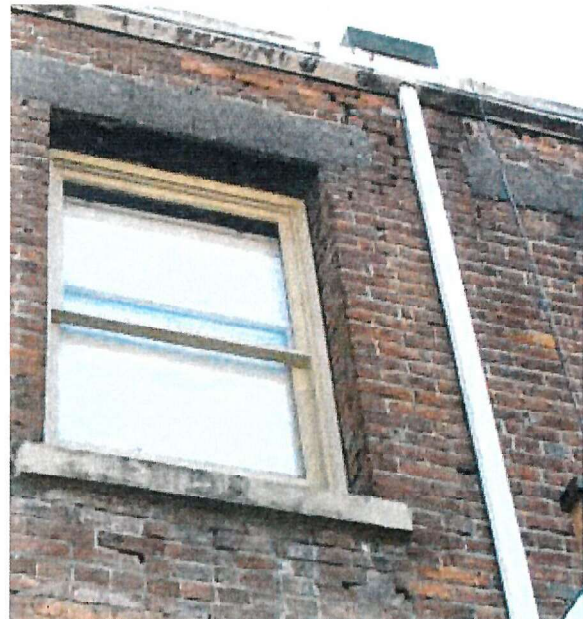
The brick on the “non-street” façades also possesses localized areas of significant staining and organic deposits. These areas of deterioration are especially



Above: Painted brick of the north façade. Paint is a later intervention and limits assessment of underlying brick. Surface damage to some brick and mortar loss present in areas at base of wall at approximate height of car mufflers and bumpers.



Above: Typical example of mortar loss and unpatched past penetrations in brick wall on "non-street" façade.



Above: Mortar loss, distortion of brick, and staining on rear of building likely caused by overflowing gutters and undersized downspout.

5.0 CONSERVATION RECOMMENDATIONS



Above: Typical deterioration of masonry on rear of building including staining, cracked bricks, mortar loss, efflorescence and past unsympathetic repairs.

evident on the brick below second floor window sills and below the roof's gutter. The latter is likely the result of leaking or poorly maintained gutters that have permitted moisture to saturate the brick below the gutter. Mortar loss, past unsympathetic repointing, cracked bricks, eroded bricks, stepped cracks, roofing tar spills, and holes from past penetrations are evident at multiple locations on the "non-public" façades of the building.

The anticipated redevelopment scheme for the site includes the retention of the two street façades. A portion of the north façade and portion of the structure that is parallel to Cook Street will also be retained back to the external face of the new volume. The remainder of the building demolished. There is the opportunity to salvage the materials from the demolished portions of the building to divert them from the landfill.

Conservation Strategy: Preservation and Rehabilitation

- Document portions of building to be demolished prior to work commencing, including areas covered by later additions.

- For those portions of the "non-street" façades' masonry walls being demolished, brick could be considered for salvage.
- Refer to 5.4.1 Conservation Strategy for portions of brick walls on "non-street" façades being retained.

5.5 ARCHITECTURAL METALWORK

Parkway Apartments features a number of architectural metalwork details on its Pandora Avenue and Cook Street façades. These include elements such as the projecting storefront cornice with dentils, prominent roof level cornice with groupings of square modillions, and oriole window surround on the second floor.

Overall the storefront cornice is in fair condition with the visual inspection from ground level identifying areas of deterioration of varying degrees. The storefront cornice appears to be well anchored to the building with no gaps from the face of the building noted. The nature of the anchoring and its adequacy was not determined, and requires further

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Above: Profile of storefront and roof level cornices that are present on Pandora Avenue and Cook Street façades.

investigation. A number of dentils of the storefront cornice are damaged on both the Pandora Avenue and Cook Street sides of the building. The roof level cornice, located above the second floor windows of the Pandora Avenue and Cook Street façades, is more substantial and ornate in design than the storefront cornice. Metalwork components of the oriole windows on the second floor include metalwork detailing as part of the window surround as well as the top of the brick pilasters. This metalwork is in fair condition with localized damage and paint failure.

The exterior surface of both cornices is uneven indicative of multiple layers of paint. There are localized areas of paint failure present on the cornices. The drip edge of the cap flashing is bent and lifted in areas, which provides a means for moisture and pests to access the space behind the cornice. There is also localized corrosion, some extensive in areas, on the underside of the cornice, dentils, and modillions. The top surface of the cornices was not viewed; therefore, it is unknown the condition of the top of the cornices nor the extent and/or condition of any flashings. Further investigation is necessary to determine the structural integrity of the metalwork, anchoring, and condition and detail of flashings to determine extent of repairs, or if larger replacement is necessary.

The placement, design, and materials of the architectural metalwork of the Pandora Avenue and Cook Street façades are original and character-defining, and should be preserved. Any deteriorated components are to be repaired in-kind with localized repairs. Component too deteriorated to be retained are to be replaced. Work is to be carried out in a manner that maintains the original dimensions, design, materials, and finish.

Conservation Strategy: Preservation

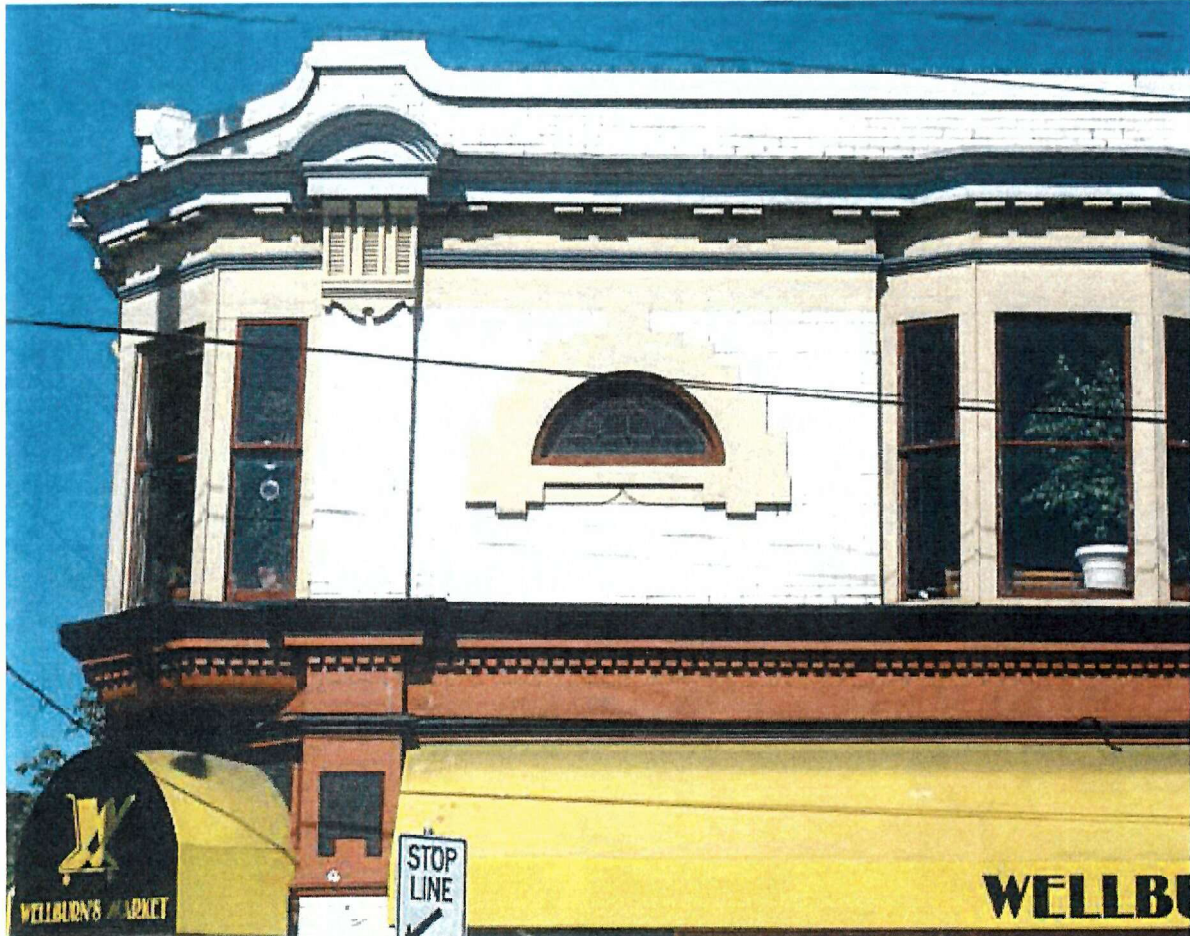
- Evaluate the overall condition of both cornices and window surrounds to determine whether more than protection, maintenance and limited repair or replacement in-kind is required.
- The current attachment/anchoring of all metalwork should be inspected and evaluated

5.0 CONSERVATION RECOMMENDATIONS

to determine adequacy of existing anchors and if additional anchors are required.

- If required, repair and stabilize deteriorated architectural metal elements by structural reinforcement or correction of unsafe conditions. Repairs should be physically and visually compatible.
- Remove corrosion that may be discovered upon closer inspection, patch and repair, caulk joints as required.
- If in depth evaluation of the cornices

Right: Heavily corroded under side of roof level cornice and corrosion of modillions. Typical paint failure also shown.



Above: Architectural metalwork of the storefront cornice, brick piers, oriole windows, roof level cornice and parapet cap.

5.0 CONSERVATION RECOMMENDATIONS



Above: Metal work of oriole windows, roof level cornice, top of brick pilasters, and parapet cap flashing. Paint failure evident and corrosion of drip edge on cap of cornice evident. Gap between parapet cap flashing and wall face noted. Bird spikes on parapet later addition.

determines they are too deteriorated to repair, remove and replace those deteriorated elements, or the entire cornice with one that matches the historic in original dimensions, design, materials, and finish.

- The visual appearance of the cornices should not be altered and should match the historic appearance.
- The sheet metal work will be cleaned and prepared for repainting. Apply appropriate primer for galvanized surfaces. Paint in historically appropriate colour, based on colour schedule prepared by Heritage Consultant.

5.5.1 PARAPET CAP FLASHING

The parapet cap flashing on the “street” façades of Pandora Avenue and Cook Street mirror the unique design of the brick parapet. The parapet is flat with

the exception of raised sections of approximately 3 courses of white glazed brick, that vertically align with the brick pilasters. The parapet is capped with a metal cap flashing. Based on historical photos available, the extent of the current cap flashing reflects what was there when the building was completed. The cap flashing when viewed from the ground appears to be in good condition; however, some of the seams have lifted. Alteration to the cap flashing has occurred in the form of bird spikes being installed and the removal of paired chimney flues of that were once position at the raised portions of the parapet. These may have been removed when the bird spikes were installed.

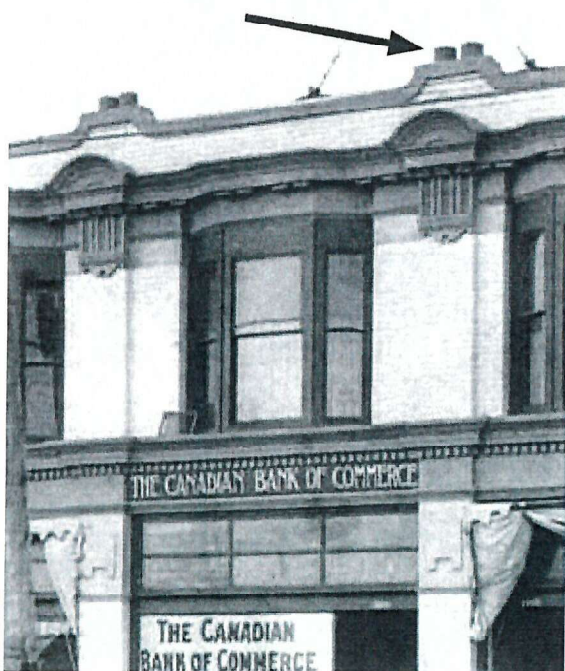
Conservation Strategy: Rehabilitation and Restoration

- Evaluate the overall condition of the parapet cap flashing to determine whether more than protection, maintenance and limited repair or

5.0 CONSERVATION RECOMMENDATIONS



Above and Below: Pairs of cylindrical elements that were once present on top of each raised section of the parapet along the Pandora Avenue and Cook Street façades. These were removed post-1960, perhaps at the time the cap flashing was replaced and/or bird spikes installed.



replacement in kind is required.

- Remove corrosion that may be discovered upon close inspection, patch and repair, caulk joints as required and apply appropriate primer for galvanized surface.
- Repair or replace deteriorated flashing, as required. Repairs should be physically and visually compatible.
- Restoration of missing chimney elements that were associated with the pairs of conical element on raised portions of parapet.
- If new flashings are installed, ensure that the colour is compatible with the overall colour scheme of the historic resource.
- Consider alternative pest (bird) deterrent option that is less visible on the “public” façades.

5.6 FENESTRATION

Windows, doors and storefronts are among the most conspicuous feature of any building. In addition to their function — providing light, views, fresh air and access to the building — their arrangement and design is fundamental to the building's appearance and heritage value. Each element of fenestration is, in itself, a complex assembly whose function and operation must be considered as part of its conservation.

– Standards and Guidelines for the Conservation of Historic Places in Canada.

5.6.1 WINDOWS AND TRIM

5.6.1.1 Pandora Avenue (south) and Cook Street (east) Façades

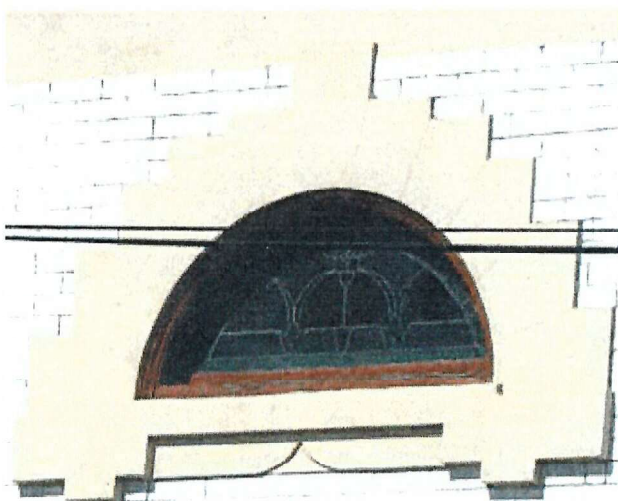
Parkway Apartments' original window assemblies on the “street” façades are largely intact with their configuration and some original materials maintained from the time of the building's construction. The fenestration of the ground floor is addressed in 5.6.2 and 5.6.3. Presently, the second floor windows of the Pandora Avenue and Cook Street façades are characterised by single and double

5.0 CONSERVATION RECOMMENDATIONS



Above Left: Typical second floor wood sash and frame hung window. Typical example of condition of wooden elements and finish of windows on upper floor.

Above Right: Oriole bay window with wood sash and frame hung windows on either side of a fixed wood sash and frame window. Metal window surround present on all oriole windows. Shown condition of wood and metal typical of all oriole windows.



Left: Half-moon wood frame and sash window with leaded glass panel and decorative window surround. Leaded glass panel with rough rolled coloured glass in good condition no missing or broken glass noted. Painted finish on wood elements deteriorate. Cracks in window surround noted.

5.0 CONSERVATION RECOMMENDATIONS

assembly 1-over-1 hung wooden-sash windows, oriole bay windows with 1-over-1 wooden-sash windows on either side of a fixed multi-light wood window, and a single half-moon fixed wooden-sash window with leaded glass on the Cook Street façade. The hung windows are in fair condition. The upper and lower sashes exhibit weathering, putty loss, and significant paint failure. Some of the sashes have been replaced as well as the operation of some windows (hung to fixed) has been changed. The frames also exhibit significant paint loss and localized weathering and deterioration particularly to the lower extent of the side jambs and sill. Single pane glazing is present on the second floor windows on the “public” façades some of which appear to be original.

The half-moon leaded glass window located on the second floor of the Cook Street façade is original and in good condition. The leaded glass panel includes smooth and rough rolled glass in a floral motif. The frame and sash of the window is weathered and possess paint failure. The window surround mirrors

the shape of the window opening and exaggerates the appearance of the window. The surround has been painted, and from street level appears to be in fair condition. There are cracks present in three of the pieces of the surround; however, the extent or nature of these cracks could not be assessed from the street level.

5.6.1.2 “Non-Street” (rear) Façades

The windows on the “non-street” façades retain the majority of their original configuration, sills, lintels, frames, sashes, and brick moulds. These windows are single assembly 1-over-1 wooden hung windows. The sills are parged brick and lintels are concrete, with some lintels spanning more than one window opening. Later additions to the rear of the building obscure the original window assemblies of portions of the first floor.

Based on a visual inspection from the ground the lower sections of the stiles, bottom rail of the lower sashes, and sills show signs of deterioration. This



Above: 1-over-1 hung wood sash and frame windows on rear of building. Most windows appear to still have an operable lower-sash, unknown if upper-sash can be lowered. Concrete lintels span one to two window openings. Deterioration evident on sills lower jambs, bottom rail. Localized areas of paint failure present. Staining on and around brick sills.

5.0 CONSERVATION RECOMMENDATIONS



Above: Parge brick sill of rear façade window showing deteriorated parging.

Below: Window on ground floor of rear façade with missing glass and loose upper sash.



deterioration is most evident on the second floor windows. Some glazing has been replaced. Those first floor windows that are visible possess either broken or missing glass with wood installed in its absence. The painted finish of all windows has deteriorated with peeling paint present.

The concrete lintels are original and in fair condition. The lintels, while possessing surface staining, do not show any significant cracks or failures. The parge sills have localized deterioration in the form of stains, cracks, chips and/or missing pieces of parging. The sills have been painted and the paint is failing on some of the sills.

Based on the proposed redevelopment of the site, the street façades, portion of the north façade as well as portion of the structure parallel to Cook Street will be retained with the latter two elements retained back to the external face of the new volume with the remainder of the building being demolished. Intact original window assemblies of the retained façades are to be preserved and repair in-kind as required. The intact fenestration on those façades that are to be demolished could be salvaged for reuse.

Conservation Strategy: Preservation and Rehabilitation

- Inspect for condition and complete detailed inventory to determine extent of intact original assemblies and the extent of repair or replacement of windows on retained façades that is required.
- Keep intact original existing windows on retained façades; repair as required; install replacement matching sashes where sashes are missing, previously replaced, and/or beyond repair.
- Preserve and repair intact original assemblies as required, using in-kind repair techniques, where feasible.
- Overhaul, tighten/reinforce joints. Repair frame, trim and counterbalances.
- Preserve and repair leaded glass window. Assess condition of cames and repair in-kind if cames have weakened. Retain coloured glass. If glass is broken, replace with glass matching colour and finish of original glass.

5.0 CONSERVATION RECOMMENDATIONS

- Each window should be made weather tight by re-puttying and weather-stripping as necessary.
- Retain historic glass, where possible. Where broken glass exists in historic wood-sash windows, the broken glass should be replaced. When removing broken glass, the exterior putty should be carefully chipped off with a chisel and the glazier's points should be removed. The wood where the new glass will be rested on should be scraped and cleaned well, and given a coat of linseed oil to prevent the wood from absorbing the oil from the new putty. The new glass should be cut 1/16-1/8th smaller than the opening to allow for expansion and irregularities in the opening, to ensure the glazing does not crack due to natural forces. Window repairs should be undertaken by a contractor skilled in heritage restoration.
- Preference that replacement glass be single glazing, and visually and physically compatible with existing.
- Prime and repaint as required in appropriate colour, based on colour schedule devised by Heritage Consultant.

5.6.2 DOORS AND TRIM

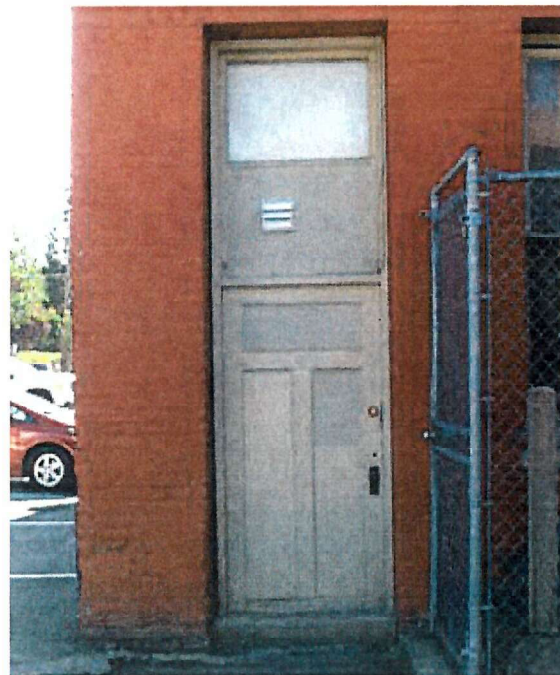
Parkway Apartments retains a number of its original exterior door openings, as well as possible original exterior wood doors. On the south (Pandora Avenue) façade the door opening providing access to the second floor is in its original location. The original door configuration of a single man-door with transom is maintained; however, the existing aluminum door and transom are replacements. A wood door in a commercial unit on Cook Street matches what is shown on the original architectural drawings, however, its original recessed placement has been changed. All other doors on the ground floor of the Pandora Avenue and Cook Street façades have been replaced with modern assemblies.

The rear façade of the building retains original wood doors; one door on the first floor and another door on the second floor of the west wall of the rear façade. The second floor door retains its original



Above: Original door opening on Pandora Avenue façade providing access to second floor. Floor in entry retains penny-tiles mosaic. Door assembly has been replaced with modern materials.

Below: Wood door, frame, and transom on rear façade ground floor intact. Transom glass removed and/or boarded over. Brass hardware of door intact. Lower extent of door and sill shows signs of deterioration.



5.0 CONSERVATION RECOMMENDATIONS



Above: Chamfered entry on ground floor with double doors and transom. Historic assembly has been replaced with aluminum assembly, including transom.

three-panel solid wood door with wood frame and single-light wooden-sash transom. Additionally, the ground floor door retains its brass door knob and plate. Both doors appear to be in fair condition. Deterioration that is present is predominantly located at the bottom rail of the doors and base of side jambs. The glass in the ground floor door's transom has been replaced; however, the second floor transom glass is intact. Later added additions to the rear of the building hide any other intact original door assemblies that may be present.

Based on the redevelopment of the site the two street façades are to be retained. The north façade and structure parallel to Cook Street will also be retained extending to the external face of the new six storey volume. The site will be rehabilitated with a new volume constructed behind and connected to the retained elements. Intact door openings and any original doors of Pandora Avenue and Cook Street should be retained and the door assemblies restored to match original doors. Past unsympathetic door assemblies that have been installed on the retained façades should be restored with wood door assemblies to match the original doors of Parkway Apartments. Reference historic photographs and original architectural drawings (Appendix B) to match design and materials of new wood doors.

Conservation Strategy: Preservation and Restoration

- Retain the door openings in their original locations, and preserve and repair all original door assemblies (frame, trim, door, transom, etc.).
- Restore original entryways to the ground floor of the building.
- New door assemblies should be restored to match originals door assemblies as evident in original architectural drawings.
- Prime and repaint, as required, in appropriate colour, based on colour schedule devised by Heritage Consultant.

5.0 CONSERVATION RECOMMENDATIONS

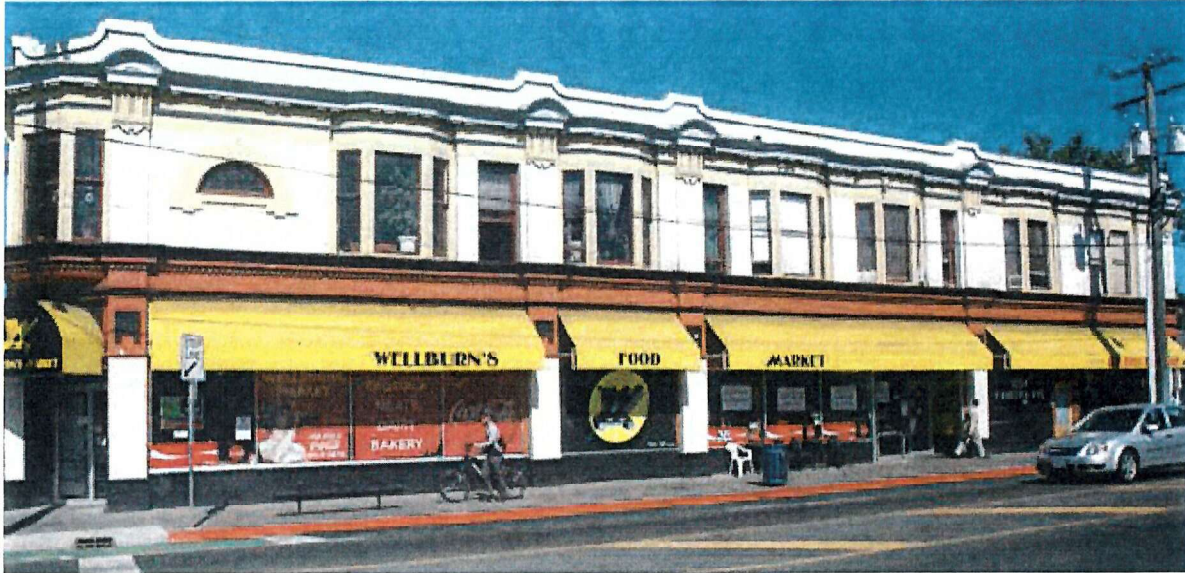
5.6.3 STOREFRONTS

When first built, the ground floor of the Pandora Avenue and Cook Street façades possessed multiple commercial storefronts. Based on historic images and archival drawings, the original storefront design consisted of a low bulkhead of white glazed brick with nearly full-height storefront windows, recessed entries, and multi-light wooden transoms that extended the width of each storefront bay with

reed glass. Each storefront bay, separated by brick pilasters, possessed either one or two commercial units. The exception was the narrow structural bay on the Cook Street façade which originally was a glazed brick wall with a single door with transom and oval window with decorative surround; which was all removed post-1960.

Post-1960, the storefronts began to evolve through the amalgamation of nearly all individual commercial





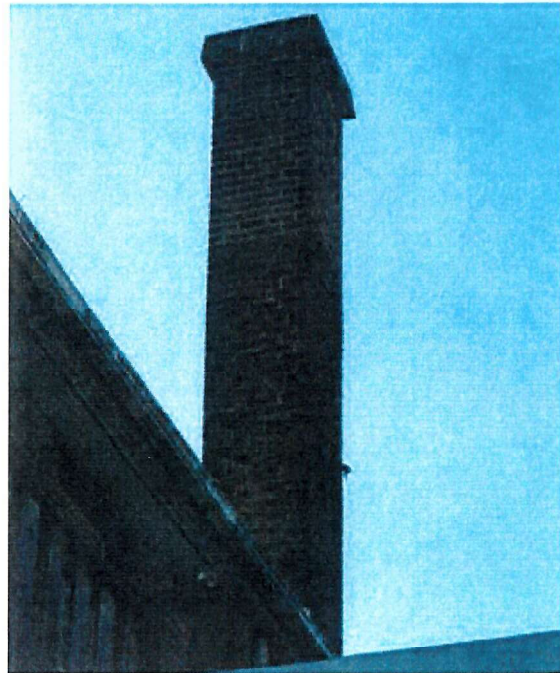
Above: Ground floor storefronts of Cook Street with original storefront design and assemblies altered and/or removed.

Previous Page Top: Pandora Avenue façade circa 1914, showing original design and assemblies of storefronts.

Previous Page Bottom: Current Pandora Avenue façade with altered storefronts with later added awnings.

units, this is reflected through the removal of individual store entries and the installation of new storefront assemblies. The alterations also included increasing the height of the bulkhead with the original brick face being parged over or removed completely. Further changes included boarding over and/or altering or removing of some of the original multi-light wood transoms. These changes limit the determination of extent of intact transoms; however, the western most transom on the Pandora Avenue façade is intact behind later installed boarding. Two storefronts on Cook Street, the northern most storefront bay, allude to the original design of the storefront; however, they too have been altered, but not to the same degree as the others.

The location and configuration of the chamfered corner entry remains intact; however, the transom and double doors have been replaced with aluminum assemblies. New awnings have also been added on both street façades. Further investigation is required to determine the extent, if any, of original materials that remain obscured by later interventions.



Above: Brick chimney with concrete cap. With proposed demolition of building, chimney brick could be salvaged for reuse.

5.0 CONSERVATION RECOMMENDATIONS

The original design of the ground floor of Pandora Avenue and Cook Street façades of multi-unit storefronts with full-height storefront windows, wide multi-light transoms, and recessed entries is indicative of commercial building of the Edwardian period. Those intact storefront assemblies on both “street” façades are to be retained and repaired in-kind. Past unsympathetic changes to the storefronts and its assemblies should be corrected. The storefronts should be rehabilitated to wood assemblies, and their design reflect the original aesthetic and historic precedent of the building. Available archival images and original architectural drawings can be used as reference.

Conservation Strategy: Preservation and Rehabilitation

- Preserve intact original storefront elements such as wood transoms with reed glass glazing, multiple storefront bays on street façades separated by brick pilasters, chamfered corner entry. Repair in-kind those elements too deteriorated to retain.
- Rehabilitate storefront with wood storefront assemblies. Rehabilitation to include bulkhead below glazing, storefront glazing, transoms. Reference the original design as noted in archival images. Restoration of entries and doors to match originals. The design of the storefronts should reflect original historic precedents in design and materials.
- Prime and repaint, as required, in appropriate colour, based on colour schedule devised by Heritage Consultant.

5.7 ROOF & CHIMNEY

The existing roof was not accessed. It is presumed that the roof is a low-pitch sloped roof. Gutter and downspouts are present on the building. The downspouts are undersized for the size of the roof. A thru-wall scupper has been installed above the metal cornice on the Cook Street façade. A tall square red brick internal chimney with concrete cap is present and in good condition. The chimney does not appear to be leaning or significantly deteriorated. There is evidence of past repairs and

repointing.

The proposed redevelopment of the site includes the retention of the two street façades, as well as a portion of the north façade and portion of the structure that is parallel to Cook Street with the latter two retained back to the external face of the new volume. The structure and roof the retained two-storey portion of the building will be rehabilitated. The brick chimney will be removed. The brick from the chimney could be salvaged for reuse, thus diverting materials from the landfill.

Conservation Strategy: Preservation and Rehabilitation

- A portion of the original roof will be retained and rehabilitated including the construction of a new six storey volume that will tie into the retained two-storey structure.
- The roof parapet is to be preserved and repaired in-kind, as required. All new construction should occur behind the retained parapet and set back from it to preserve the façades original scale and ensure new construction aligns with **Standard 11**.
- The chimney will be demolished as part of the redevelopment of the site. The brick could be salvaged, thus diverting it from the landfill.

5.8 SIGNAGE

Commercial signs are an integral feature of historic commercial buildings. Different types of signs were fabricated in traditional materials with painted or three-dimensional letters, including fascia signs, projecting signs and painted window signs. Signs often reflect the ethnic history of a neighborhood and its character, as well as the social and business activities carried within it, and it is important to preserve or commemorate these markers of the building's social and economic history.

When considering new signs on a heritage building, the design should be in accordance with the Parks Canada *Standards & Guidelines for the Conservation of Historic Places in Canada*, which states that “new signage should be compatible with the building in

5.0 CONSERVATION RECOMMENDATIONS

terms of size, scale, material, style and colour. In addition, new signs should not obscure, damage or destroy character-defining elements of the building.”

Conservation Strategy:

- New signs can be inspired by historical signs on the building, signs from an earlier era or contemporary materials that are sympathetic to the building.
- Sign fixings or hangers should be carefully attached to the building in the least intrusive manner possible. On masonry walls, consider attaching into mortar rather than brick or stone.
- Signs were historically illuminated with front lighting.
- Future tenant and building signage will need to conform to current City of Victoria sign bylaws.

Conservation Strategy: Restoration

- Restore the historic finish, hue, and placement of applied colour. Complete all repairs, replacements and remove all dirt before preparing, priming and painting. All surfaces to be painted are to be dry prior to painting. Scrape and sand painted surfaces only as needed to reach a sound base. Do not strip all previous paint except to repair base-material decay.
- Any substitutions or matching of custom colours shall be reviewed by the Heritage Consultant.

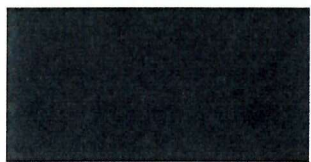





5.9 PRELIMINARY EXTERIOR COLOUR SCHEDULE

Part of the conservation process is to finish the building in historically appropriate paint colours. A restoration colour scheme will be developed in conjunction with the project architect.

Historic precedents and archival images illustrate the areas where paint was applied when the building was first constructed. A preliminary colour scheme has been established based on samples and collected from some of the intact exterior elements. Not all intact painted elements could be safely accessed for the collection of samples. Additional testing will be required to fully develop the building's original paint palette. Those paint samples collected were assessed by microscopic analysis to reveal the original colour scheme of the structure.

Paint draw downs are to be provided and sample application of the identified paint palette on the building to be viewed in natural light should be carried out prior to final paint application.

5.0 CONSERVATION RECOMMENDATIONS

Location	Code	Finish	
Storefront Display Window	Gloss Black VC-35	TBD	
Second floor window sashes, rear windows, door trim	Gloss Black	Gloss	
Sheet Metal in window bays	Vancouver Green VC-20	Matte	
Dentil Courses and Cornices	Dunbar Buff VC-5	Matte	
Pilaster - if painted	Dunbar Buff VC-5	Matte	
Pilaster - if unpainted, match to	Haddington Grey VC-15	NA	

Note - Paint referenced is Benjamin Moore Historical Vancouver True Colours

5.0 CONSERVATION RECOMMENDATIONS

5.10 INTERIOR

“Interior features can include elements such as interior walls, floors and ceilings, mouldings, staircases, fireplace mantels, faucets, sinks, built-in cabinets, light fixtures, hardware, radiators, mail chutes, telephone booths and elevators. Because their heritage value resides not only in their physical characteristics, but also in their location in the historic building, it is important to protect them from removal. This is particularly true of doors, banisters, church pews, fireplace mantels, sinks and light fixtures, which are often replaced instead of being upgraded. Reuse in their original location not only protects their heritage value, but is also a more sustainable approach to conserving these artefacts.” Standards & Guidelines for the Conservation of Historic Places in Canada

Building Code upgrading is one of the most important aspects of heritage building rehabilitation, as it ensures life safety and long-term protection for the resource. However, the interior features of an historic property are often heavily damaged in the process. The British Columbia Building Code offers equivalencies and exemptions to heritage buildings, which enable a higher degree of heritage conservation and retention of original material. The following guidelines pertaining to Health, Safety and Security Considerations from the *Standards & Guidelines* should be followed when faced with the conservation of interior character-defining elements:

- Upgrade interior features to meet health, safety and security requirements, in a manner that preserves the existing feature and minimizes impact on its heritage value.
- Work with code specialists to determine the most appropriate solution to health, safety and security requirements with the least impact on the character-defining elements and overall heritage value of the historic building.
- Explore all options for modifications to existing interior features to meet functional requirements prior to considering removal or

replacement.

- Remove or encapsulate hazardous materials, such as friable asbestos insulation, using the least-invasive abatement methods possible, and only after thorough testing has been conducted.
- Install sensitively designed fire-suppression systems that retain character-defining elements and respect heritage value.

5.10.1 GENERAL

The ground floor interior of Parkway Apartments has been significantly altered from its original design as its use and tenants have evolved over time. These alterations have not only impacted the layout and number of units, but also the original finishes. The upper floor, which contains multiple residential suites, was accessed to assess the extent of intact original elements. In comparison with the ground floor, the upper floor possesses a higher degree of integrity of intact original elements. No intrusive testing or demolition was undertaken as part of the interior evaluation.

When the block was first completed, the ground floor possessed 11 commercial units. As the needs of the commercial tenants of the ground floor changed, commercial units which were once separated have been combined. When Matthew Wellburn moved his grocery into the building at 1050 Pandora Avenue in 1914, he took over where two previous grocers had failed. Originally named Wellburn's Cash Grocery Store, the shop grew steadily, taking over a bakery, a bank, a pharmacy and other businesses until it finally became a supermarket. Through this expansion, a significant portion of the original floor plan of the ground floor and its materials were altered. These alterations, also resulted in extensive modifications to the storefronts of Pandora Avenue and Cook Street.

The upper floor's roughly L-shaped central hallway with suites on both sides is intact. The hallway retains its wood floors, picture rail and baseboard trim, paneled wood doors with single light transom and casings with crown moulding. The interior of the suite accessed was intact and characterised

5.0 CONSERVATION RECOMMENDATIONS

by wood trim and casings, wood floors, wood wainscotting with narrow wood picture shelf, wood fireplace mantel with tile and cast iron insert, and exposed wood beams on the ceiling.

The proposed redevelopment of the site includes the retention of a portion of the north façade and the façades of Pandora Avenue and Cook Street as well as the structure of the building behind these façades to the new six storey volume. As part of the overall rehabilitation of the site, it is recommended retention of intact original interior wood finishes of second floor, where possible. Further investigation is required to confirm the extent and exact finishes to be retained.

Conservation Strategy: Investigation, Documentation, Salvage, Rehabilitation

- Investigate finishes to determine exact finishes to be retained. Document and inventory wood finishes to be retained and salvaged.
- Securely store salvaged materials in environment which will not facilitate their damage or deterioration.
- If possible, reinstate salvaged materials in their origin locations or reuse elsewhere on site such as public areas, hallways, or lobbies.



Above: Second floor hallway with intact wood floors, wood doors with transoms, wood trim including casings, baseboards, and picture rail. Rubber runner has been installed.

Left: Three panel wood door with single light operable wood transom and wood casings and crown moulding at suite entry.

5.0 CONSERVATION RECOMMENDATIONS



Above Left: Suite interior showing intact wood finishes including floor, trim work, wainscoting, and exposed beams.



Above Right: Intact wood fireplace mantle with tile and cast iron insert.

6.0 MAINTENANCE PLAN

6.1 MAINTENANCE GUIDELINES

A maintenance schedule should be formulated that adheres to the *Standards & Guidelines for the Conservation of Historic Places in Canada*. As defined by the *Standards & Guidelines*, maintenance is defined as:

Routine, cyclical, non-destructive actions necessary to slow the deterioration of a historic place. It entails periodic inspection; routine, cyclical, non-destructive cleaning; minor repair and refinishing operations; replacement of damaged or deteriorated materials that are impractical to save.

The assumption that newly renovated buildings become immune to deterioration and require less maintenance is a falsehood. Rather, newly renovated buildings require heightened vigilance to spot errors in construction where previous problems had not occurred, and where deterioration may gain a foothold.

Routine maintenance keeps water out of the building, which is the single most damaging element to a heritage building. Maintenance also prevents damage by sun, wind, snow, frost and all weather; prevents damage by insects and vermin; and aids in protecting all parts of the building against deterioration. The effort and expense expended on an aggressive maintenance will not only lead to a higher degree of preservation, but also over time potentially save large amount of money otherwise required for later repairs.

6.2 PERMITTING

Repair activities, such as simple in-kind repair of materials, or repainting in the same colour, should be exempt from requiring city permits. Other more intensive activities will require the issuance of a Heritage Alteration Permit.

6.3 ROUTINE, CYCLICAL AND NON-DESTRUCTIVE CLEANING

Following the *Standards & Guidelines for the Conservation of Historic Places in Canada*, be mindful of the principle that recommends “using the gentlest means possible”. Any cleaning procedures should be undertaken on a routine basis and should be undertaken with non-destructive methods. Cleaning should be limited to the exterior material such as concrete and stucco wall surfaces and wood elements such as storefront frames. All of these elements are usually easily cleaned, simply with a soft, natural bristle brush, without water, to remove dirt and other material. If a more intensive cleaning is required, this can be accomplished with warm water, mild detergent and a soft bristle brush. High-pressure washing, sandblasting or other abrasive cleaning should not be undertaken under any circumstances.

6.4 REPAIRS AND REPLACEMENT OF DETERIORATED MATERIALS

Interventions such as repairs and replacements must conform to the *Standards & Guidelines for the Conservation of Historic Places in Canada*. The building's character-defining elements – characteristics of the building that contribute to its heritage value (and identified in the Statement of Significance) such as materials, form, configuration, etc. - must be conserved, referencing the following principles to guide interventions:

- An approach of minimal intervention must be adopted - where intervention is carried out it will be by the least intrusive and most gentle means possible.
- Repair rather than replace character-defining elements.
- Repair character-defining elements using recognized conservation methods.
- Replace ‘in kind’ extensively deteriorated or missing parts of character-defining elements.
- Make interventions physically and visually compatible with the historic place.

6.0 MAINTENANCE PLAN

6.5 INSPECTIONS

Inspections are a key element in the maintenance plan, and should be carried out by a qualified person or firm, preferably with experience in the assessment of heritage buildings. These inspections should be conducted on a regular and timely schedule. The inspection should address all aspects of the building including exterior, interior and site conditions. It makes good sense to inspect a building in wet weather, as well as in dry, in order to see how water runs off – or through – a building. From this inspection, an inspection report should be compiled that will include notes, sketches and observations. It is helpful for the inspector to have copies of the building's elevation drawings on which to mark areas of concern such as cracks, staining and rot. These observations can then be included in the report. The report need not be overly complicated or formal, but must be thorough, clear and concise. Issues of concern, taken from the report should then be entered in a log book so that corrective action can be documented and tracked. Major issues of concern should be extracted from the report by the property manager.

An appropriate schedule for regular, periodic inspections would be twice a year, preferably during spring and fall. The spring inspection should be more rigorous since in spring moisture-related deterioration is most visible, and because needed work, such as painting, can be completed during the good weather in summer. The fall inspection should focus on seasonal issues such as weather-sealants, mechanical (heating) systems and drainage issues. Comprehensive inspections should occur at five-year periods, comparing records from previous inspections and the original work, particularly in monitoring structural movement and durability of utilities. Inspections should also occur after major storms.

6.6 INFORMATION FILE

The building should have its own information file where an inspection report can be filed. This file

should also contain the log book that itemizes problems and corrective action. Additionally, this file should contain building plans, building permits, heritage reports, photographs and other relevant documentation so that a complete understanding of the building and its evolution is readily available, which will aid in determining appropriate interventions when needed.

The file should also contain a list outlining the finishes and materials used, and information detailing where they are available (store, supplier). The building owner should keep on hand a stock of spare materials for minor repairs.

6.6.1 LOG BOOK

The maintenance log book is an important maintenance tool that should be kept to record all maintenance activities, recurring problems and building observations and will assist in the overall maintenance planning of the building. Routine maintenance work should be noted in the maintenance log to keep track of past and plan future activities. All items noted on the maintenance log should indicate the date, problem, type of repair, location and all other observations and information pertaining to each specific maintenance activity.

Each log should include the full list of recommended maintenance and inspection areas noted in this Maintenance Plan, to ensure a record of all activities is maintained. A full record of these activities will help in planning future repairs and provide valuable building information for all parties involved in the overall maintenance and operation of the building, and will provide essential information for long term programming and determining of future budgets. It will also serve as a reminder to amend the maintenance and inspection activities should new issues be discovered or previous recommendations prove inaccurate.

The log book will also indicate unexpectedly repeated repairs, which may help in solving more serious problems that may arise in the historic building. The log book is a living document that will

6.0 MAINTENANCE PLAN

require constant adding to, and should be kept in the information file along with other documentation noted in section **6.6 Information File**.

6.7 EXTERIOR MAINTENANCE

Water, in all its forms and sources (rain, snow, frost, rising ground water, leaking pipes, back-splash, etc.) is the single most damaging element to historic buildings.

The most common place for water to enter a building is through the roof. Keeping roofs repaired or renewed is the most cost-effective maintenance option. Evidence of a small interior leak should be viewed as a warning for a much larger and worrisome water damage problem elsewhere and should be fixed immediately.

6.7.1 INSPECTION CHECKLIST

The following high-level checklist considers a wide range of potential problems specific to Parkway Apartments, such as water/moisture penetration, material deterioration and structural deterioration. This does not include interior inspections.

EXTERIOR INSPECTION

Site Inspection:

- ☐ Is the lot well drained? Is there pooling of water?
- ☐ Does water drain away from foundation?

Foundation

- ☐ Does foundation need repair?
- ☐ Paint peeling? Cracking?
- ☐ Moisture: Is rising damp present?
- ☐ Is there back splashing from ground to structure?
- ☐ Is any moisture problem general or local?
- ☐ Is spalling from freezing present? (Flakes or powder?)
- ☐ Is efflorescence present?
- ☐ Is spalling from sub-fluorescence present?
- ☐ Is damp proof course present?
- ☐ Are there shrinkage cracks in the foundation?

- ☐ Are there movement cracks in the foundation?
- ☐ Is crack monitoring required?
- ☐ Is uneven foundation settlement evident?

Masonry

- ☐ Are moisture problems present? (Rising damp, rain penetration, condensation, water run-off from roof, sills, or ledges?)
- ☐ Is spalling from freezing present? Location?
- ☐ Is efflorescence present? Location?
- ☐ Is spalling from sub-fluorescence present? Location?
- ☐ Need for pointing repair? Condition of existing pointing and re-pointing?
- ☐ Is bedding mortar sound?
- ☐ Are weep holes present and open?
- ☐ Are there cracks due to shrinking and expansion?
- ☐ Are there cracks due to structural movement?
- ☐ Are there unexplained cracks?
- ☐ Do cracks require continued monitoring?
- ☐ Are there signs of steel or iron corrosion?
- ☐ Are there stains present? Rust, copper, organic, paints, oils / tars? Cause?
- ☐ Does the surface need cleaning?

Wood Elements

- ☐ Are there moisture problems present? (Rising damp, rain penetration, condensation moisture from plants, water run-off from roof, sills, or ledges?)
- ☐ Is wood in direct contact with the ground?
- ☐ Is there insect attack present? Where and probable source?
- ☐ Is there fungal attack present? Where and probable source?
- ☐ Are there any other forms of biological attack? (Moss, birds, etc.) Where and probable source?
- ☐ Is any wood surface damaged from UV radiation? (bleached surface, loose surface fibres)
- ☐ Is any wood warped, cupped or twisted?
- ☐ Is any wood split? Are there loose knots?
- ☐ Are nails pulling loose or rusted?
- ☐ Is there any staining of wood elements?

6.0 MAINTENANCE PLAN

Source?

Condition of Exterior Painted Materials

- ☐ Paint shows: blistering, sagging or wrinkling, alligatoring, peeling. Cause?
- ☐ Paint has the following stains: rust, bleeding knots, mildew, etc. Cause?
- ☐ Paint cleanliness, especially at air vents?

Windows

- ☐ Is there glass cracked or missing?
- ☐ Are the seals of double glazed units effective?
- ☐ If the glazing is puttied has it gone brittle and cracked? Fallen out? Painted to shed water?
- ☐ If the glass is secured by beading, are the beads in good condition?
- ☐ Is there condensation or water damage to the paint?
- ☐ Are the sashes easy to operate? If hinged, do they swing freely?
- ☐ Is the frame free from distortion?
- ☐ Do sills show weathering or deterioration?
- ☐ Are drip mouldings/flushing above the windows properly shedding water?
- ☐ Is the caulking between the frame and the cladding in good condition?

Doors

- ☐ Do the doors create a good seal when closed?
- ☐ Are the hinges sprung? In need of lubrication?
- ☐ Do locks and latches work freely?
- ☐ If glazed, is the glass in good condition? Does the putty need repair?
- ☐ Are door frames wicking up water? Where? Why?
- ☐ Are door frames caulked at the cladding? Is the caulking in good condition?
- ☐ What is the condition of the sill?

Gutters and Downspouts

- ☐ Are downspouts leaking? Clogged? Are there holes or corrosion? (Water against structure)
- ☐ Are downspouts complete without any missing sections? Are they properly connected?
- ☐ Is the water being effectively carried away from the downspout by a drainage system?

- ☐ Do downspouts drain completely away?

Roof

- ☐ Are there water blockage points?
- ☐ Is the leading edge of the roof wet?
- ☐ Is there evidence of biological attack? (Fungus, moss, birds, insects)
- ☐ Are shingles and/or roofing material wind damaged or severely weathered? Are they cupped or split or lifting?
- ☐ Are the nails sound? Are there loose or missing shingles?
- ☐ Are flashings well seated?
- ☐ Are metal joints and seams sound?
- ☐ If there is a lightning protection system are the cables properly connected and grounded?
- ☐ Does the soffit show any signs of water damage? Insect or bird infestation?
- ☐ Is there rubbish buildup on the roof?
- ☐ Are there blisters or slits in the membrane?
- ☐ Are the drain pipes plugged or standing proud?
- ☐ Is water ponding present?

INTERIOR INSPECTION

Basement

- ☐ Are there signs of moisture damage to the walls? Is masonry cracked, discoloured, spalling?
- ☐ Is wood cracked, peeling rotting? Does it appear wet when surroundings are dry?
- ☐ Are there signs of past flooding, or leaks from the floor above? Is the floor damp?
- ☐ Are walls even or buckling or cracked? Is the floor cracked or heaved?
- ☐ Are there signs of insect or rodent infestation?

Concealed spaces

- ☐ Is light visible through walls, to the outsider or to another space?
- ☐ Are the ventilators for windowless spaces clear and functional?
- ☐ Do pipes or exhausts that pass through concealed spaces leak?
- ☐ Are wooden elements soft, damp, cracked? Is metal material rusted, paint peeling or off altogether?
- ☐ Infestations - are there signs of birds, bats,

6.0 MAINTENANCE PLAN

insects, rodents, past or present?

6.7.2 MAINTENANCE PROGRAMME

INSPECTION CYCLE:

Daily

- Observations noted during cleaning (cracks; damp, dripping pipes; malfunctioning hardware; etc.) to be noted in log book or building file.

Semi-annually

- Semi-annual inspection and report with special focus on seasonal issues.
- Thorough cleaning of drainage system to cope with winter rains and summer storms
- Check condition of weather sealants (Fall).
- Clean the exterior using a soft bristle broom/brush.

Annually (Spring)

- Inspect concrete for cracks, deterioration.
- Inspect metal elements, especially in areas that may trap water.
- Inspect windows for paint and glazing compound failure, corrosion and wood decay and proper operation.
- Complete annual inspection and report.
- Clean out of all perimeter drains and rainwater systems.
- Touch up worn paint on the building's exterior.
- Check for plant, insect or animal infestation.
- Routine cleaning, as required.

Five-Year Cycle

- A full inspection report should be undertaken every five years comparing records from previous inspections and the original work, particularly monitoring structural movement and durability of utilities.
- Repaint windows every five to fifteen years.

Ten-Year Cycle

- Check condition of roof every ten years after last replacement.

Twenty-Year Cycle

- Confirm condition of roof and estimate effective lifespan. Replace when required.

Major Maintenance Work (as required)

- Thorough repainting, downspout and drain replacement, replacement of deteriorated building materials, etc.

APPENDIX A: RESEARCH SUMMARY

CIVIC ADDRESS: 1050 Pandora Avenue, Victoria, British Columbia

NAME: Parkway Apartments

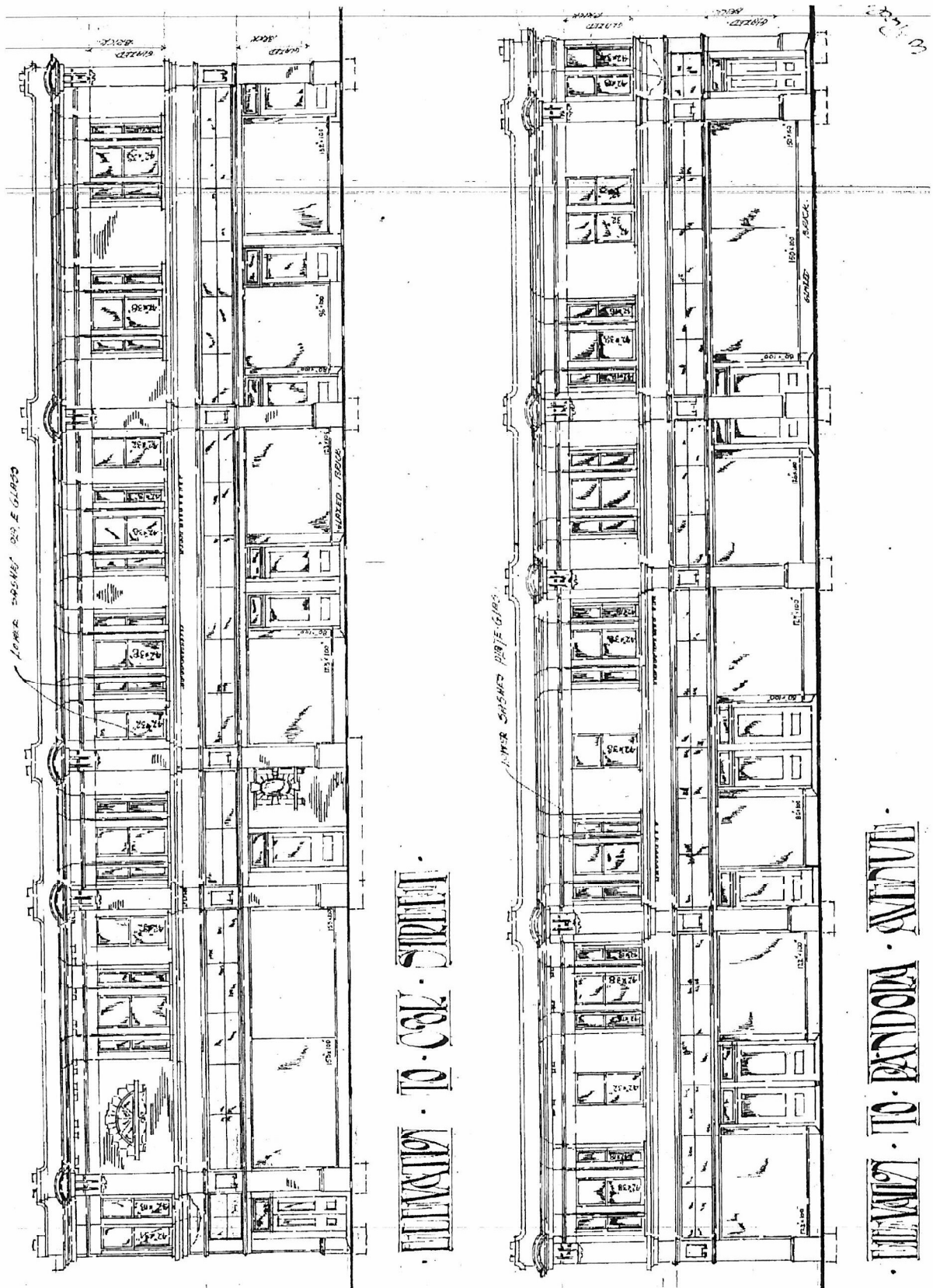
CONSTRUCTION DATE: 1911

ARCHITECT: William Ridgway-Wilson

RESEARCH SOURCES:

- Building the West: The Early Architects of British Columbia
- This Old House: Victoria's Heritage Neighbourhoods

APPENDIX B: ARCHIVAL DRAWING

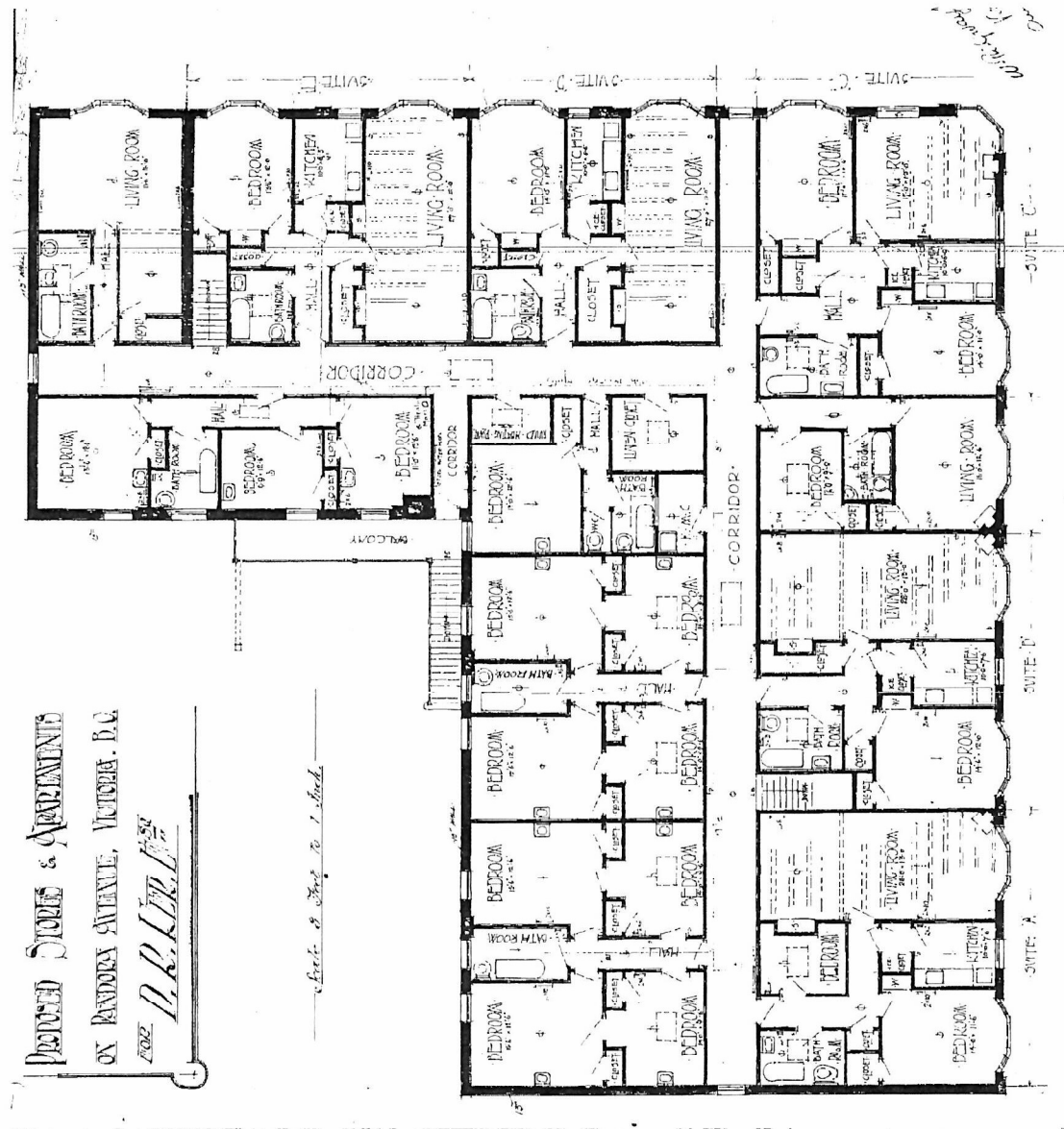


PARKWAY APARTMENTS: 1050 PANDORA AVENUE, VICTORIA, BC
 CONSERVATION PLAN | JULY 2019 | DONALD LUXTON & ASSOCIATES

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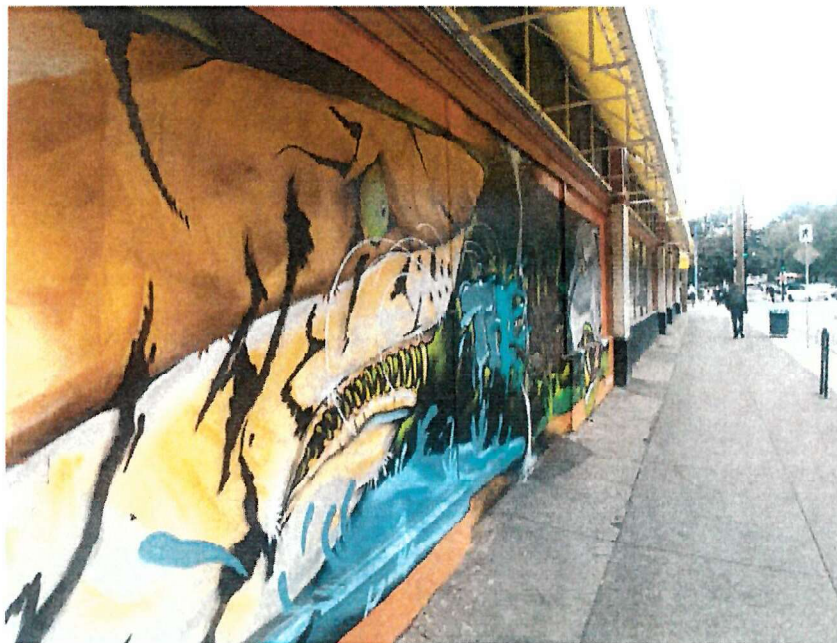
APPENDIX B: ARCHIVAL DRAWING



1050-1058 PANDORA AVENUE & 1508-1518 COOK STREET



View of subject property from corner



Close-up of ground floor condition

1050-1058 PANDORA AVENUE & 1508-1518 COOK STREET



View of north (side) elevation



View of west (rear) and north (rear) interior elevations

1050-1058 PANDORA AVENUE & 1508-1518 COOK STREET



View of rear addition to Parkway Apartments Building

- **November 26, 2019:** The flooding in the basement was discovered and reported to Pacific Cove and Summit Brooke Construction late afternoon;
- **November 27, 2019:** By 8:00am both Summit Brooke Construction and Pacific Cove were on site to review the current conditions. A pump truck was also on site to begin pumping the water, however despite pumping efforts water was continuing to ingress into the basement due to continued water usage from tenants. By the afternoon the water was successfully pumped to the point of being able to expose the broken sewer line showing a drainage line that was badly deteriorated however by the end of day a temporary repair of the pipe was achieved.
- **November 28, 2019:** By 8:00am Summit Brooke returned to site and discovered that the water level had continued to rise throughout the night leading to the immediate understanding that the sewer main had a significant blockage under the building. A technician from Royal Rooter was on site by the afternoon to scope the system from within the basement but due to the water level this was deemed not possible and it was recommended to contact the City of Victoria;
- **November 29, 2019:** A pump out, steam clean and disinfectant of the basement was completed and a temporary pump was installed in an effort to have the site in a stable situation for the weekend and a call placed with a roofer and camera service to arrange further investigation on Monday. Due to the building no longer having running water the commercial tenants were asked to close for health and safety concerns and the residential tenants were relocated to the Sandman Hotel. Residential tenants were given the option of finding alternate accommodation at the same rate, which many of them did. Each tenant was also given a \$200 visa gift card to cover incidentals;
- **December 2, 2019:** Tenants were provided an update notifying them that access to the building would not be restricted so that they may continue to retrieve personal items only, but that staying in the building was prohibited due to the ongoing work, investigation and health risks;
- **December 3, 2019:** Summit Brooke Construction arranged a coordinated effort between the City of Victoria to have the sewer line scoped from the City manhole and additional archival information was requested from the City of Victoria. The developers Civil and Mechanical consultant were also involved to provide additional support;

- **December 4, 2019:** The scoping of the sewer line from the Cook St. manhole was successful, they are able to confirm that the cast iron pipe on the private property has significant deterioration, and deposits of settled gravel are present which provides clear indication that the pipe has disintegrated and the surrounding soils and aggregates has been eroding and collapsing within the pipe space. A list of next step options is compiled by Summit Brooke Construction in coordination with the Civil and Mechanical consultants;
- **December 5, 2019:** Hypothetical timelines were prepared by Summit Brooke for two feasible repair options, both timelines are showing a repair schedule of approximately 181 days or more. Tenants received correspondence and an update on the ongoing work, they were notified that any repair strategy could 6 months or longer and both residential and commercial tenants are encouraged to contact their insurance provider. Residential tenants are encouraged to contact Lee Murphy, the relocation specialist, and are reminded that his services are available to all tenants at no cost;
- **December 9, 2019:** An updated report was provided by Summit Brooke noting that the cast iron sewer main has completely corroded, causing collapse of the surrounding subsoils, completely blocking the main sewer lateral existing the building. Neglect from the previous owner to take action to repair the sewer lateral while it was unrestricted was a contributing factor to the failure of the sanitary sewer service. Now that there is a complete blockage underneath the building, the option of sleeving a pipe through the existing pipe to the city main is no longer a viable option.

The two viable options are to either replace the existing sewer lateral or to provide a new sewer connection. Both options face a number of challenges including the risk of demolition to the portions of the existing building meant to be retained, coordinating cross access with neighbouring sewer lines, the timing of resources and hazmat abatement, all which have added to the schedule of repair putting it at six months or more.

- **December 10, 2019:** Correspondence to be sent to tenants notifying them that it has been determined that the Parkway building will not be habitable again.
- At the time of the flood, there were eight residential units occupied, two already had scheduled move outs for December 1st and one had a scheduled move out for December 6th. An agreement has been reached with a fourth tenant to move out on December 31st, that tenant will be accommodated at the hotel until their move out date. The remaining four tenants continue to be placed in alternate accommodations until December 31st while the work with our relocation specialist who is available 7 days per week. The relocation specialist continues to provide frequent updates on comparable available units.

- The tenant assistance policy compensations discussed with tenants prior to the events of the flood but due to the redevelopment will be honoured, the current offer includes 3-6 months rent depending on the length of their tenancy, moving expense and two months additional compensation in excess of the City of Victoria's requirements.
- Given the cost, timing and technical/environmental challenges to repair the sewer line, the building is going to be secured in a vacant state until redevelopment of the site occurs.

DISTRICT

T 200-8809 Heather Street
Vancouver BC V6P 3T1
districtgroup.ca



Committee of the Whole Report

For the Meeting of December 12, 2019

To: Committee of the Whole **Date:** November 29, 2019

From: Andrea Hudson, Acting Director, Sustainable Planning and Community Development

Subject: Heritage Designation Application No. 000188 and Heritage Alteration Permit Application No. 00016 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street

RECOMMENDATIONS

That Council, after giving notice and allowing an opportunity for public comment at a meeting of Council and after the Public Hearing for Rezoning Application No. 00695, if it is approved, consider the following motions:

1. "That Council authorize the issuance of Heritage Alteration Permit Application No. 00016 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street, in accordance with:
 - a) Plans, date stamped November 22, 2019.
 - b) The Conservation Plan for the Parkway Apartments, date stamped July 2019.
 - c) Final plans to be generally in accordance with the plans identified above to the satisfaction of the Director of Sustainable Planning and Community Development.
 - d) Heritage Alteration Permit lapsing two years from the date of this resolution.
2. That Council approve the designation of the heritage-registered property located at 1050-1058 Pandora Avenue and 1508-1516 Cook Street, pursuant to Section 611 of the *Local Government Act*, as a Municipal Heritage Site, consistent with the plans dated November 22, 2019."

LEGISLATIVE AUTHORITY

In accordance with Sections 617 and 618 of the Local Government Act, Council may issue a Heritage Alteration Permit which may be subject to terms consistent with the purpose of the heritage protection of the property, including: (i) conditions respecting the sequencing and timing of construction, (ii) conditions respecting the character of the alteration or action to be authorized, including landscaping and the siting, form, exterior design and finish of buildings and structures and (iii) security. Council may refuse to issue a Heritage Alteration Permit for an action that, in the opinion of Council, would not be consistent with the purpose of the heritage protection of the property.

In accordance with Section 611 of the *Local Government Act*, Council may designate real property, in whole or in part, as protected property.

EXECUTIVE SUMMARY

The purpose of this report is to provide Council with information, analysis and recommendations for a Heritage Designation Application and a Heritage Alteration Permit Application for the property located at 1050-1058 Pandora Avenue and 1508-1518 Cook Street. The approvals for designation and heritage alterations would apply to the heritage-registered mixed-use building formerly known as the Parkway Apartments and later Wellburn's Market, which is located at the corner of Cook Street and Pandora Avenue. The building was built in 1911 and contributes to the historic character of Victoria's North Park Neighbourhood.

The proposal is for the redevelopment of the site, including the construction of a new four-storey building on the surface parking lot at 1518 Cook Street and construction of a four-storey addition to the Parkway Apartments Building, which will be renovated. The proposed development would contain 103 purpose-built rental units with retail commercial space on the ground floor. The development would result in a major restoration and rehabilitation of the street-facing elevations of the Parkway Apartments Building based on the original architectural plans for the building. Fifty percent of the interior structure of the building would be conserved and seismically upgraded.

The development requires a Rezoning and a Heritage Alteration Permit. The applicant has also proposed to heritage designate the Parkway Apartments Building as part of the application.

The development conserves a substantial amount of the original building, includes significant façade enhancements, and is thoughtfully detailed and positioned relative to the heritage building to achieve a compatible visual relationship. The applications are consistent with the *Official Community Plan* (OCP, 2012) which supports new development that enhances heritage property. The applications are consistent with the *Downtown Core Area Plan* (DCAP, 2011) heritage policies under chapter 7, which supports new development that enhances the form, character and features of heritage property and encourages new development to conserve the façades of heritage property. The applications are consistent with the *Standards and Guidelines for the Conservation of Historic Places in Canada* (2010) which encourages new additions that achieve visual compatibility through a balance between imitation and pointed contrast.

The designation of this building is generally consistent with Section 8: "Placemaking (Urban Design and Heritage)" and Section 21: "Neighbourhood Directions" of the *Official Community Plan* (OCP, 2012), the *North Park Neighbourhood Directions*, the *North Park Neighbourhood Plan* (1996) and the *Victoria Heritage Thematic Framework*.

The Heritage Advisory Panel (HAPL) reviewed the application at its October 8, 2019 meeting. HAPL recommended approval of the designation and the heritage alteration permit, with consideration of changes including an increased upper storey setback of 4 metres from Pandora Avenue, conservation of portions of the building not covered by the new construction and conservation of interior features. The applicant has increased the proposed setback in accordance with HAPL's recommendation and will be salvaging trim and furnishings from existing units. Conservation of a whole unit is not feasible due to the presence of hazardous materials that must be removed as part of this project.

The Advisory Design Panel (ADP) reviewed the Heritage Alteration Permit Application at their meeting on October 9, 2019, and recommended approval with some revisions. The applicant has revised the design in response to the most significant comments.

BACKGROUND

Description of Proposal

The applications propose 103 units of purpose-built rental housing and ground floor commercial uses within a development comprised of the renovated Parkway Apartments Building with a four-storey addition and a new four-storey building at 1518 Cook Street. The applicant has submitted a rezoning application to permit the increased density, height and reduced parking requirements, a Heritage Alteration Permit Application to authorize alterations to the heritage building, and a Heritage Designation Application to ensure the long-term protection of the Parkway Apartments Building.

The proposal includes the following major design components:

- construction of a new four-storey building on 1518 Cook Street with a 2.6 metre wide pedestrian walkway between the south sidewall and the conserved north sidewall of the Parkway Apartments Building
- construction of a four-storey addition to the Parkway Apartments Building set back 13.3 metres from the east elevation facing Cook Street, 4 metres from the south elevation facing Pandora Avenue, and 3.3 metres from the west elevation facing the adjacent property
- comprehensive rehabilitation and restoration of the Parkway Apartments Building ground floor glazing, transom windows and bulkheads based on the original architectural plans for the building
- restoration of the second-storey windows and parapet of the Parkway Apartments Building
- creation of a residential courtyard on the interior of the site accessed by a walkway from Cook Street between the conserved heritage building and the new four-storey building
- provision of 154 long-term bicycle parking spaces and 18 short-term bicycle parking spaces
- provision of 44 underground parking spaces.

The following data table compares the proposal with the existing CA-1 Zone, Pandora Avenue Special Commercial District. An "L" used before a number refers to the level of the building to which the measurement applies. An asterisk is used to identify where the proposal is less stringent than the existing zone. A double asterisk is used to identify where the existing building is legal non-conforming.

Zoning Criteria	Proposal	Zone Standard (CA-1 Zone)
Site area (m ²) - minimum	2879	n/a
Density (Floor Space Ratio) - maximum	2.92:1*	2:1
Total floor area (m ²) - maximum	8412.30*	5758
Height (m) - maximum	20.22*	15.50
Storeys - maximum	6	n/a
Site coverage (%) - maximum	70	n/a
Open site space (%) - minimum	22	n/a
Setbacks (m) - minimum		
Street setback (Pandora Ave. - south)	0* (existing & L1-2) / 3.97 (proposed L3-6)	3
Street setback (Cook St. - east)	0* (existing & L1-2) / 3.08 (proposed L3-4) / 13.13 (proposed L5-6)	3
Side (north)	6.18 (proposed L1-4) / 7.60 (proposed L5-6)	0 or 3
Side (west)	0* (existing & L1-2) / 2.96* (proposed L3-4) / 3.33 (proposed L5-6)	0 or 3
Vehicle parking - minimum		
Residential	33*	66
Commercial	5*	17
Visitor	3*	10
Bicycle parking stalls - minimum		
Residential - long-term	154	127
Residential - short-term	18	10
Commercial - long-term	6	6
Commercial - short-term	8	8

Description of Historic Place

A full description of the historic place is included in the attached Conservation Plan. The Parkway Apartments Building is significant for its association with the Edwardian-era development of Victoria and for its unique commercial architectural style by architect William Ridgway Wilson. The building was constructed during a pre WWI real estate boom and featured ground-floor commercial uses with residential uses above. William Ridgway Wilson designed many significant buildings in the city, including Gyppeswyk, now part of the Art Gallery of Greater Victoria (1889), St. John the Divine Anglican Church (1912), and the Victoria Drill Hall, now the Bay Street Armoury (1913-1915). One of the earliest tenants of the building was the Wellburn's Market on the ground floor, which still occupies the building today. Character-defining elements of the building are paraphrased below:

- location
- siting
- mix of uses
- commercial form, scale and massing and L-shaped plan
- masonry construction details, including red brick in a common bond on the rear and side elevations and the white glazed brick used on the main façades
- Edwardian era design features including its: commercial and residential design with full-height storefront on the ground floor; brick piers extending from grade to parapet delineating storefront bays; decorative pressed metal cornices at the storefront and roof levels; regular arrangement of fenestration on the upper floor; flat parapet punctuated by raised portions which align with brick piers on Cook Street and Pandora Avenue façades
- original windows including: multi light wood storefront transoms spanning each storefront bay of the Pandora Avenue and Cook Street façades; single assembly hung wood frame and sash windows with parged brick sills on rear façade; single and double assembly hung wooden-sash windows, oriel windows with hung wood frame and sash windows on either side of a fixed multi-light wood frame and sash windows on the second storey of the Pandora Avenue and Cook Street façades; half-moon leaded glass window with wood frame on Cook Street façade; three panel wood doors with wood transom windows on rear façade.

Community Consultation

Consistent with the *Community Association Land Use Committee (CALUC) Procedures for Processing Rezoning and Variance Applications*, the applicant has consulted the North Park CALUC at a Community Meeting held on March 20, 2019. The meeting minutes, dated March 20, 2019, are attached to this report.

ANALYSIS

The following sections provide a summary of the application's consistency with the relevant City policies, regulations and policies.

Official Community Plan

The proposed alterations outlined in the application are consistent with the OCP and advance the following policies:

- 8.6 *Conserve and enhance the heritage value, character and special features of areas, districts, streetscapes, cultural landscapes and individual properties throughout the city.*

- 8.11 *Determine the heritage value of areas, districts, streetscapes, cultural landscape and individual properties using the Victoria Heritage Thematic Framework as identified in Figure 12.*
- 8.43 *Encourage high quality architecture, landscape and urban design to enhance the visual identity and appearance of the City*
- 8.49 *Continue to support new additions that conserve and enhance heritage property, as consistent with the National Standards and Guidelines for the Conservation of Historic Places in Canada.*
- 8.50 *Encourage new development to avoid the demolition of heritage property, or one or more of its façades.*
- 8.51 *Continue to give consideration to tools available under legislation to protect or conserve heritage property including, but not limited to: heritage designation bylaws.*

The development preserves and enhances the landmark qualities of the Parkway Apartments Building through significant façade improvements, the strategic positioning of new construction and high quality new architecture that does not compete for attention with the heritage building. The project includes major façade improvements to the Parkway Apartments Building, including the restoration of missing transom windows, re-introduction of ground storey retail glazing across the entire façade and re-animation of the partially vacant ground floor with new commercial uses. Details of the new building and addition respond directly to the heritage building through a matching colour palette, matching horizontal window dimensions and chamfered window edges. The inverted semi-hexagonal shape of the windows on the front elevation of the new building match the proportions of the projecting semi-hexagonal (oriel) windows of the heritage building. The new building is also setback over 3 metres further from the street than the heritage building, which exposes its sidewall and the front wall of the building to pedestrians. Designation of the building furthers the City's objective of ensuring the long-term conservation of heritage property through heritage designation.

Downtown Core Area Plan

The development conserves and restores the significant physical features of the heritage-registered building and the overall design conserves its heritage value as a landmark in the neighbourhood. This reflects the DCAP's vision of a balance between sensitive new development and heritage conservation.

The DCAP includes the following relevant objectives for heritage conservation in the downtown:

1. Retain, protect and improve real property with aesthetic, historic, scientific, cultural, social or spiritual value and heritage character as a benefit to the public.
2. Development and heritage conservation are balanced through sensitive new infill and property additions that respond to the heritage value and character of Downtown Core Area Districts.

The DCAP contains the following relevant policies for the conservation of heritage properties and districts in the downtown:

- 7.3. Conserve heritage values of the Downtown Core Area and its character-defining elements, such as individual buildings, collections of buildings, streetscapes, structures and features.
- 7.18. Support new development that conserves and enhances the form, character and features of heritage property and areas, where controlled and regulated in the Downtown Core Area.
- 7.22. Encourage new development to avoid the demolition of a protected heritage property, or one or more of its facades, where the exterior alterations are assessed to significantly damage the heritage value of the property or an area, and where facade retention is feasible in conjunction with other policies for Downtown Core Area Districts in this Plan.

The development balances new development and heritage conservation through a sensitive property addition set back over 13 metres from the heritage building's Cook Street elevation and 4 metres from the Pandora Avenue elevation. The new building north of the heritage building is strategically sited to preserve views of the heritage building approaching south along Cook Street. The new development conserves the form of the building from many vantage points and the simplified detailing and colour scheme does not compete for attention with it. As discussed above, the development conserves the main façades of the building, a sidewall, a substantial amount of the interior structure and includes significant and meaningful enhancements to the heritage character of the building through the restoration of the main façades.

Advisory Design Guidelines for Buildings, Signs and Awnings

The *Advisory Design Guidelines for Buildings, Signs and Awnings* (2006) states that the overall design of new signage and awnings should enhance the architecture of the building, respond to its significant features, be simple and not busy and include complimentary colours. The applicant has proposed retractable fabric canopies along the ground floor of the building between the storefront and transom, which would reveal the transom window for the first time in decades. Signage could be applied directly to the canopy rather than the building. Historically, the building featured retractable canopies that could expose the transom when not in use. Currently the building features non-retractable, yellow canopies across the entire frontage that cover the transom window, which has been mostly boarded over.

The guidelines advise that new construction should complement any flanking heritage building and consider detailing, a comprehensive design approach and appropriate street relationship. The guidelines suggest that a contemporary solution can be quite acceptable in this context provided it is sympathetic. In staff's opinion, the combination of setbacks, cladding, colour scheme and window pattern and detailing are sympathetic to the Parkway Apartments Building.

Standards and Guidelines for the Conservation of Historic Places in Canada

The proposal is consistent with the *Standards and Guidelines for the Conservation of Historic Places in Canada* (the Standards and Guidelines). The applicant proposes to preserve and retain virtually all intact character-defining elements on the primary elevations, construct a compatible addition to the heritage building and provide a new storefront that replicates the proportions of the original and reinstates missing entrances and glazing. The applicant also proposes to restore the wood transom window across the whole frontage. The conservation

approach is primarily rehabilitation; however, it also includes a significant amount of preservation and restoration. Rehabilitation is defined in the Standards and Guidelines as the sensitive adaptation of an historic place for a continuing or compatible contemporary use while protecting its heritage value.

The proposed new construction meets Standard 11 of the Standards and Guidelines, which applies to rehabilitation projects. Standard 11 states that any new additions should conserve heritage value and character-defining elements, while being physically and visually compatible with, subordinate to and distinguishable from the historic place. The position of the addition over the heritage building with its substantial setback from Cook Street and Pandora Avenue conserve the form, scale and massing of the Parkway Apartments Building, preserving its prominence at the corner of Pandora Avenue and Cook Street from many vantage points. The addition is subordinate to the historic building through its colour palette, design details and significant setbacks. The modern, simplified details of the addition make it clearly distinguishable. The inverted bays on the new building and the matching horizontal widths of windows is a clear reference to the historic building.

Statement of Significance

A Statement of Significance describing the historic place, outlining its heritage value and identifying its character-defining elements can be found on page 7 of the attached Conservation Plan.

Resource Impacts

Heritage designation of 1050-1058 Pandora Avenue and 1508-1516 Cook Street will enable the property owner to apply for heritage grants from the Victoria Civic Heritage Trust and for a tax incentive from City Council to assist with the cost of seismic upgrading.

Heritage Advisory Panel

The application was reviewed by the Heritage Advisory Panel (HAPL) at its October 8, 2019 meeting. The minutes are attached. HAPL provided the following recommendation for Council's consideration:

That the Heritage Advisory Panel recommend that Council approve the designation of the heritage-registered property located at 1050-1058 Pandora Avenue and 1508-1516 Cook Street, pursuant to Section 611 of the *Local Government Act*, as a Municipal Heritage Site.

That the Heritage Advisory Panel recommend that Council approve Heritage Alteration Permit with Variances Application No. 00016 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street with the following changes:

- a) Setback from Pandora Avenue be increased to a minimum of at least 4m so that the separation is clearly visible
- b) Conserve the portion of the old building that will not be covered by the new building
- c) Consider retention of interior features where feasible, preferably in situ or relocated within the development.

The applicant increased the setback of the addition to the Pandora Avenue elevation to 4 metres in response. Conservation of the structure of the building not covered by the addition is not possible due to the location of the underground parking garage. The building does

contain original finishes including wainscoting and built in furniture. The applicant explored conservation of a full unit; however, the hazardous materials removal strategy requires extensive interior renovations. As an alternative, the applicant intends to salvage and reuse safe interior components within the renovated building.

Advisory Design Panel

The Advisory Design Panel (ADP) reviewed the Heritage Alteration Permit Application at their meeting on October 9, 2019. The minutes are attached. They provided the following recommendation for Council's consideration:

That the Advisory Design Panel recommend to Council that Heritage Alteration Permit with Variances Application No. 00016 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street be approved with the following changes:

- increase the building setback facing Pandora Avenue to 4m from the street
- revise the second floor units that face the mews to improve liveability
- reconsider the windowless interior dens and bedrooms
- resolve the proposed materials
- consider adding usable roof spaces
- consider revising the articulation of the four-storey building's façade facing Franklin Green Park, and consider the addition of balconies or Julietts
- reconsider the width of the alleyway (mews) to increase accessibility.

In response to ADP's recommendations above, the applicant made the following changes:

- increased the building setback facing Pandora Avenue to 4m from the street
- re-oriented windows to allow for more daylight penetration
- added a west-facing roof deck on the fifth storey for residents in the building
- added Juliet balconies on the west elevation overlooking Franklin Green Park
- removed planters and short-term bicycle parking stalls from the mews and relocated to the residential courtyard.
- resolved exterior materials

CONCLUSIONS

The development conserves a substantial amount of the original building, includes significant façade enhancements, and is thoughtfully detailed and positioned relative to the heritage building to achieve a compatible visual relationship. The applications are consistent with the *Official Community Plan* and *Downtown Core Area Plan* which both support new development that enhances heritage property. The applications are consistent with the *Standards and Guidelines for the Conservation of Historic Places in Canada* and are particularly successful in meeting the requirements that a new addition be compatible, distinguishable and subordinate. The proposed designation is for a neighbourhood landmark in North Park featuring unique materials and an exceptional design by a notable historic architect. In staff's opinion, the designation and the heritage alteration permit are both worthy of Council's approval.

ALTERNATE MOTION

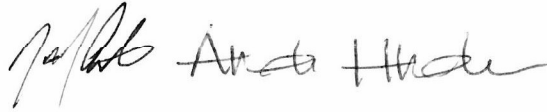
Option 1

That Council decline Heritage Designation Application No. 000188 and Heritage Alteration Permit Application No. 00016 for 1050-1058 Pandora Avenue and 1508-1518 Cook Street

Respectfully submitted,



John O'Reilly, Senior Heritage Planner
Development Services Division



Andrea Hudson, Acting Director
Sustainable Planning and Community
Development Department

Report accepted and recommended by the City Manager:



Date: Dec 4, 2019

List of Attachments

- Attachment A: Subject Map
- Attachment B: Aerial Map
- Attachment C: Plans dated/date stamped November 22, 2019
- Attachment D: Letter from applicant to Mayor and Council dated October 30, 2019
- Attachment E: Community Association Land Use Committee Comments dated March 20, 2019
- Attachment F: Parking Study prepared by WATT Consulting dated October 29, 2019
- Attachment G: Letter from MODO dated October 28, 2019
- Attachment H: Letter from applicant regarding TDM measures dated October 30, 2019
- Attachment I: Tenant Assistance Plan
- Attachment J: Conservation Plan, dated July 2019
- Attachment K: Photographs
- Attachment L: Minutes of the Heritage Advisory Panel, October 8, 2019
- Attachment M: Minutes of the Advisory Design Panel, October 9, 2019.

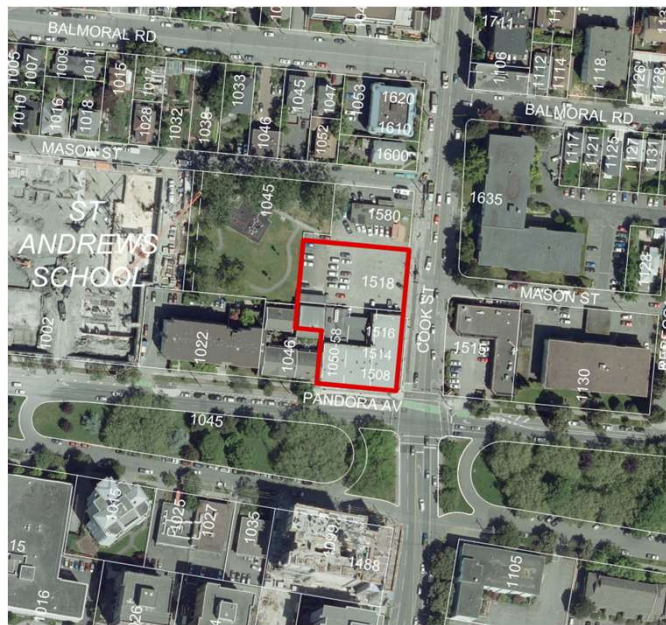
Zoning Bylaw Amendment
Application No. 00695 &
Heritage Alteration Permit
Application No. 00016

for

1050-1058 Pandora Avenue
and 1508-1518 Cook Street
("Wellburn's Site")



1



SITE CONTEXT



2



NORTH-WEST VIEW AT
PANDORA & COOK



3



SOUTH-WEST VIEW ALONG
COOK ST



4



FRANKLIN GREEN PARK



5



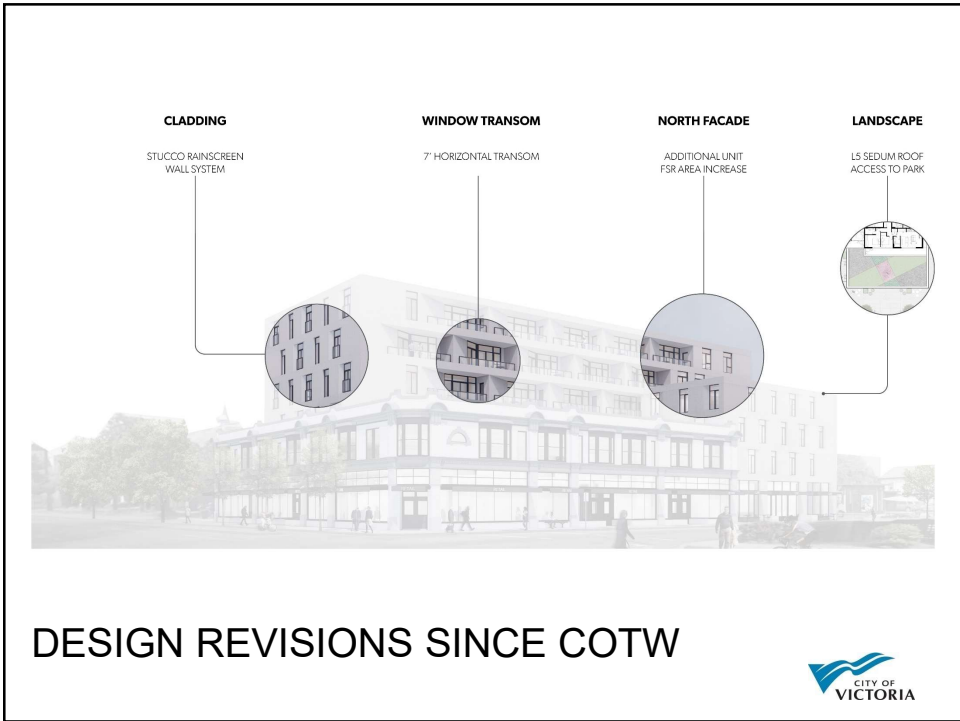
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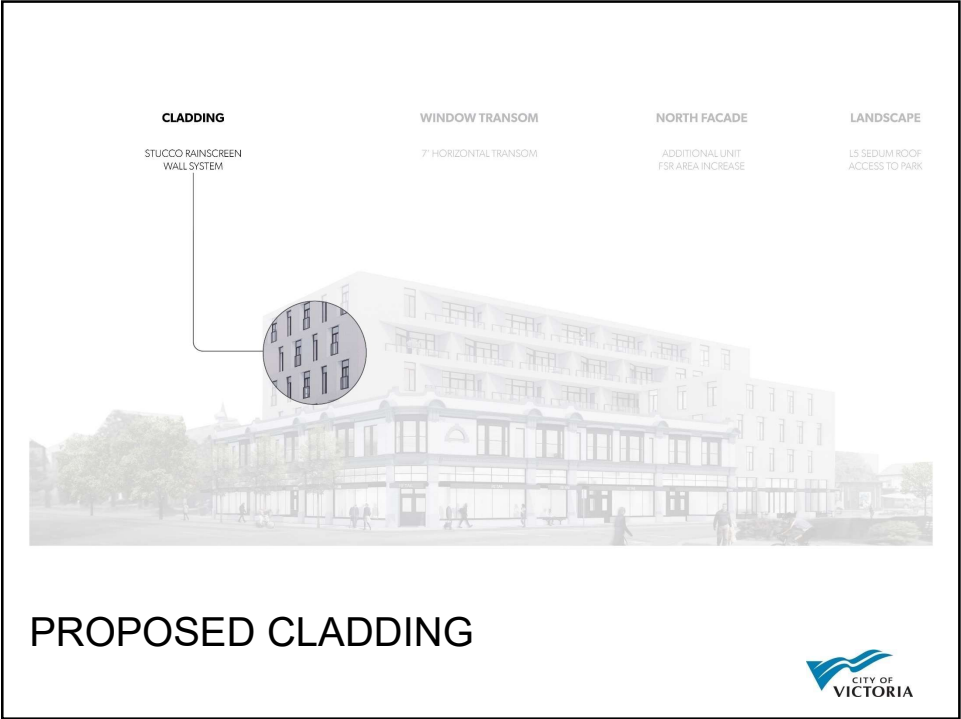
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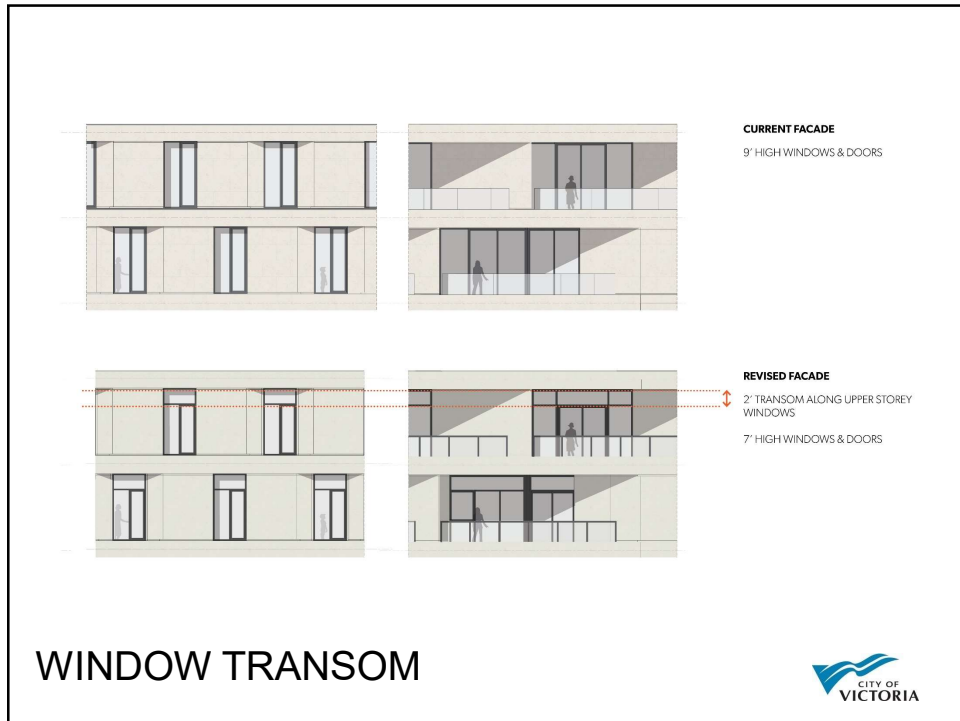
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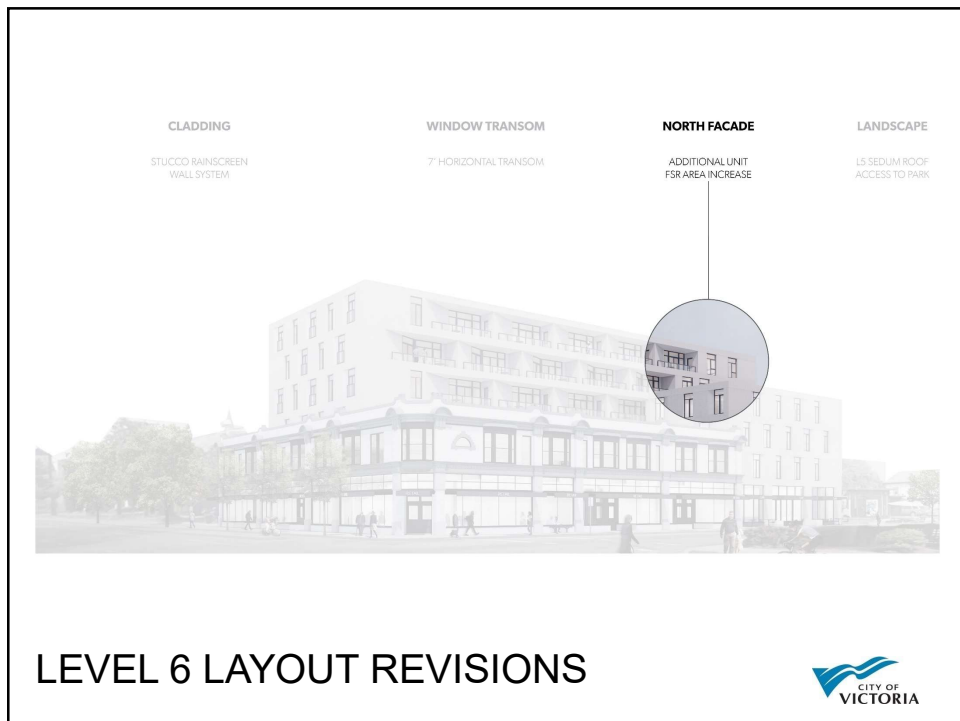
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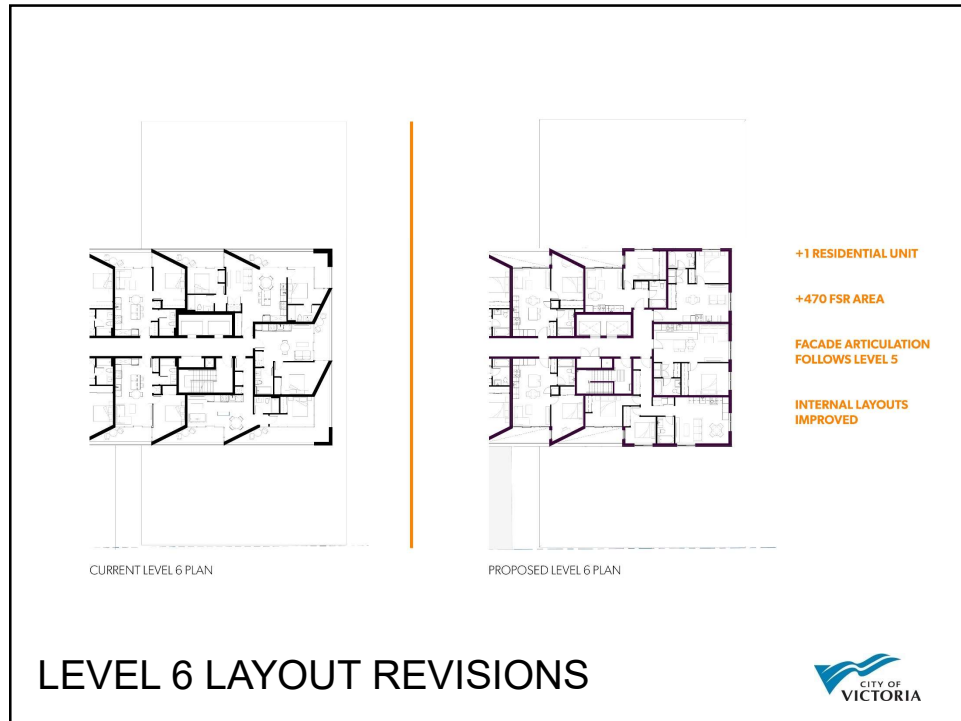
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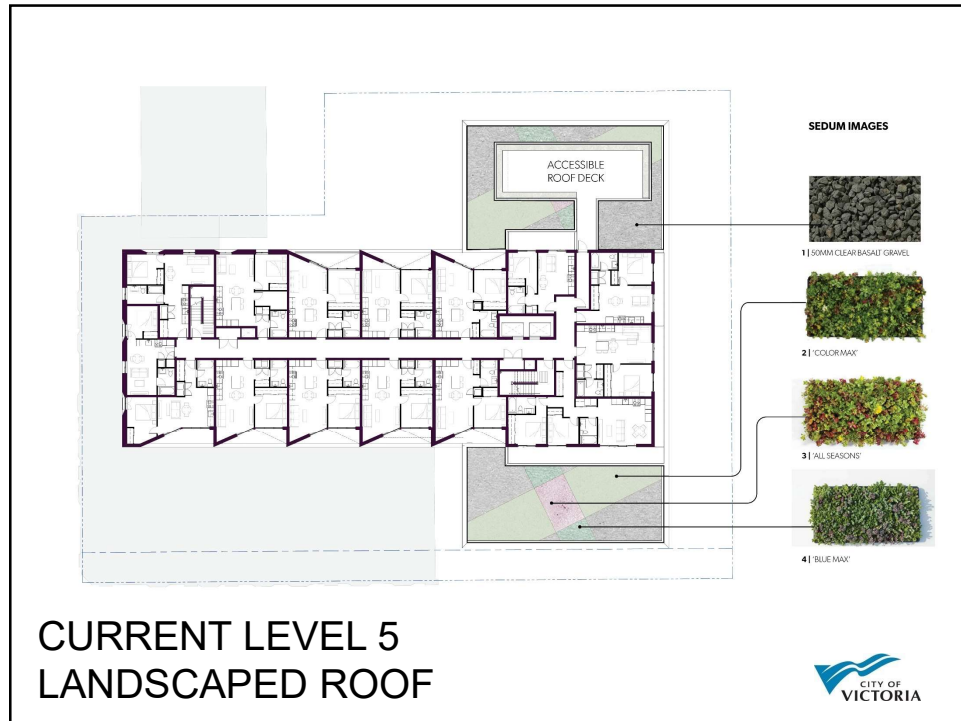
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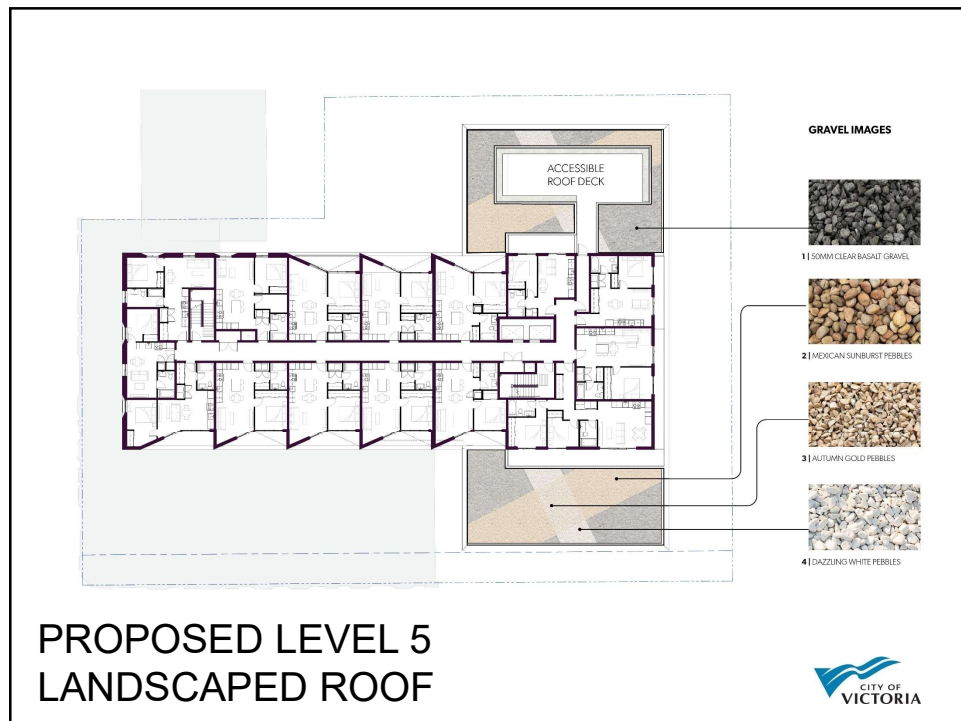
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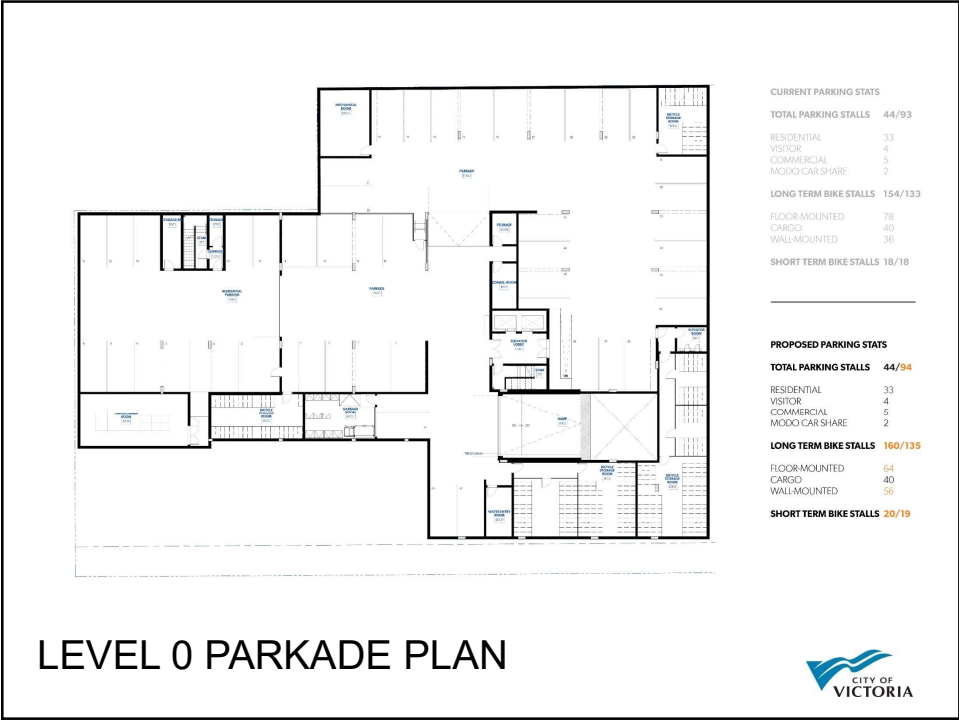
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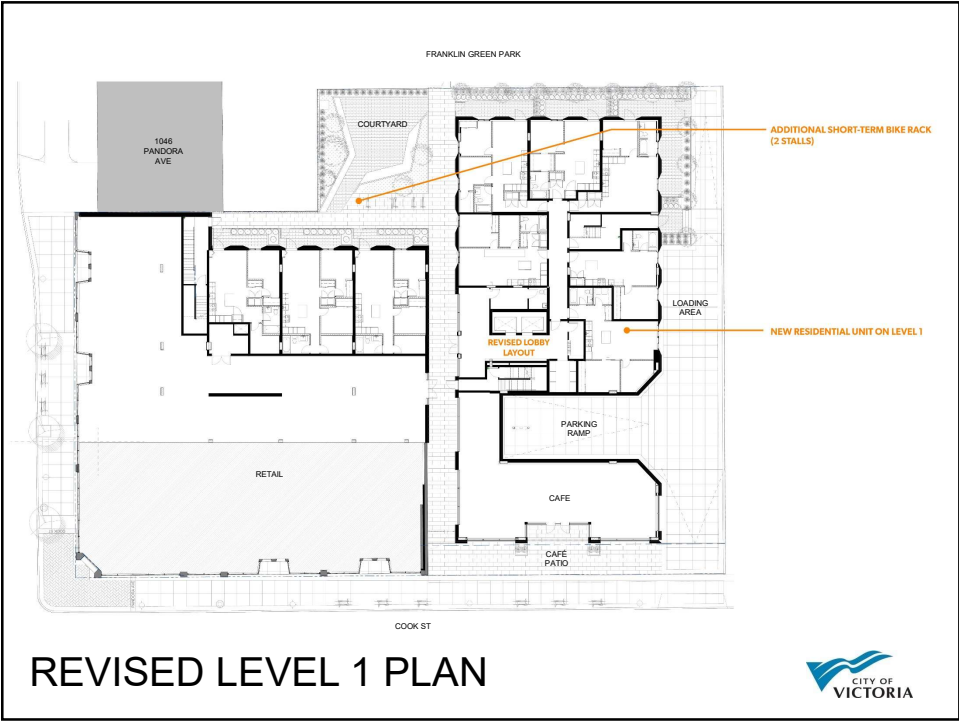
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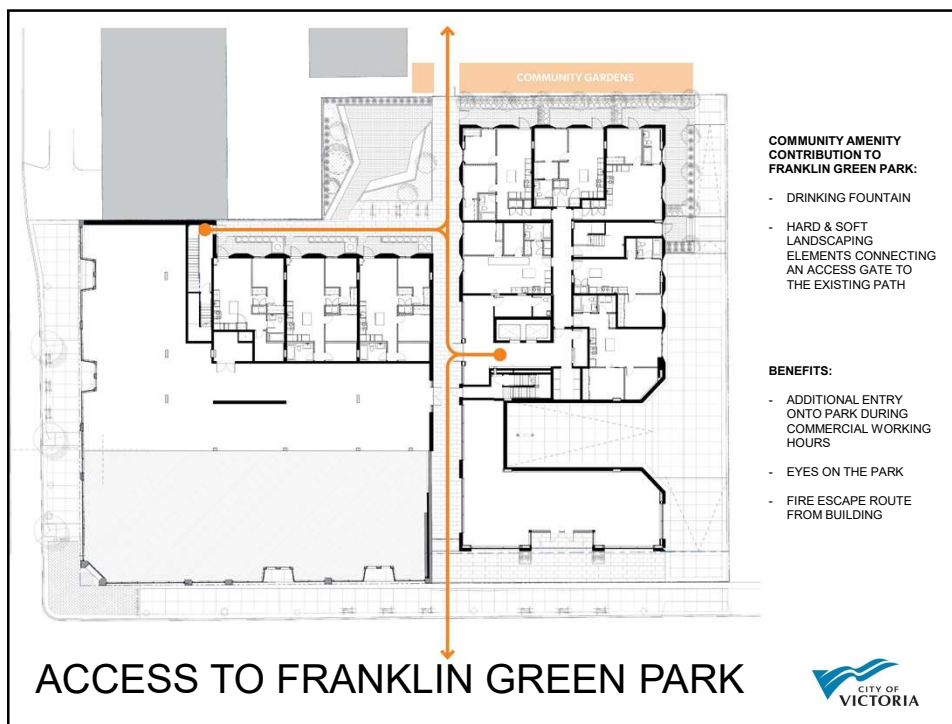
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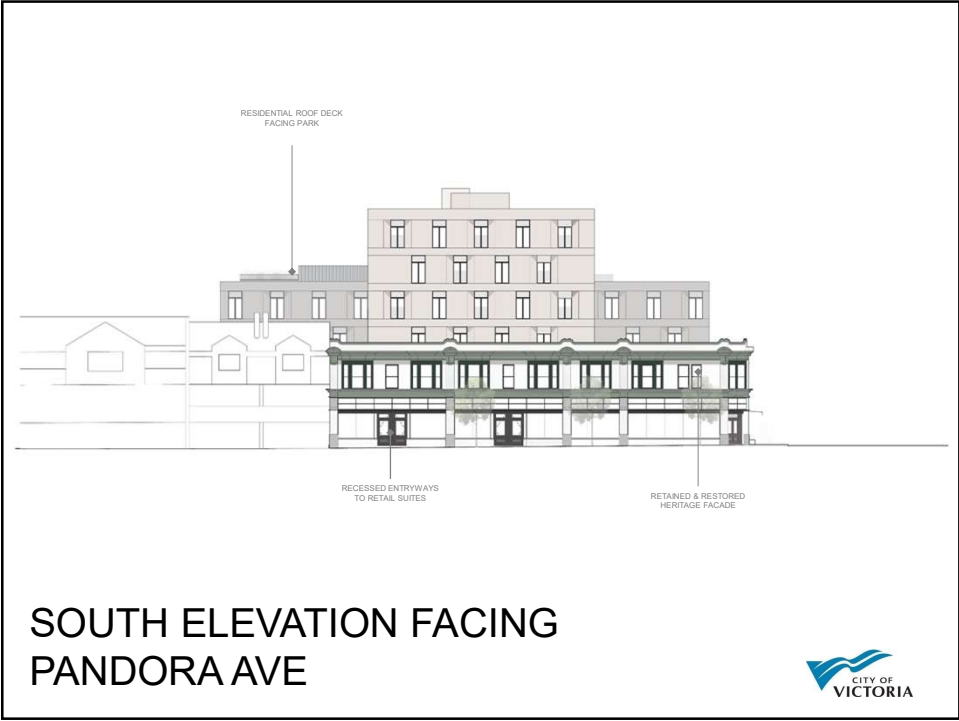
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	FSR AREA	TOTAL RESIDENTIAL UNITS	TOTAL PARKING STALLS	TOTAL LONG-TERM BIKE STALLS	TOTAL SHORT-TERM BIKE STALLS
CURRENT	8412m2	103	44/93	154/133	18/18
PROPOSED	8545m2	105	44/94	160/135	20/19
	+133m2	+2		+6	+2
ALLOWABLE	9330m2				

PROJECT RECAP | REZONING IMPACT

CITY OF VICTORIA

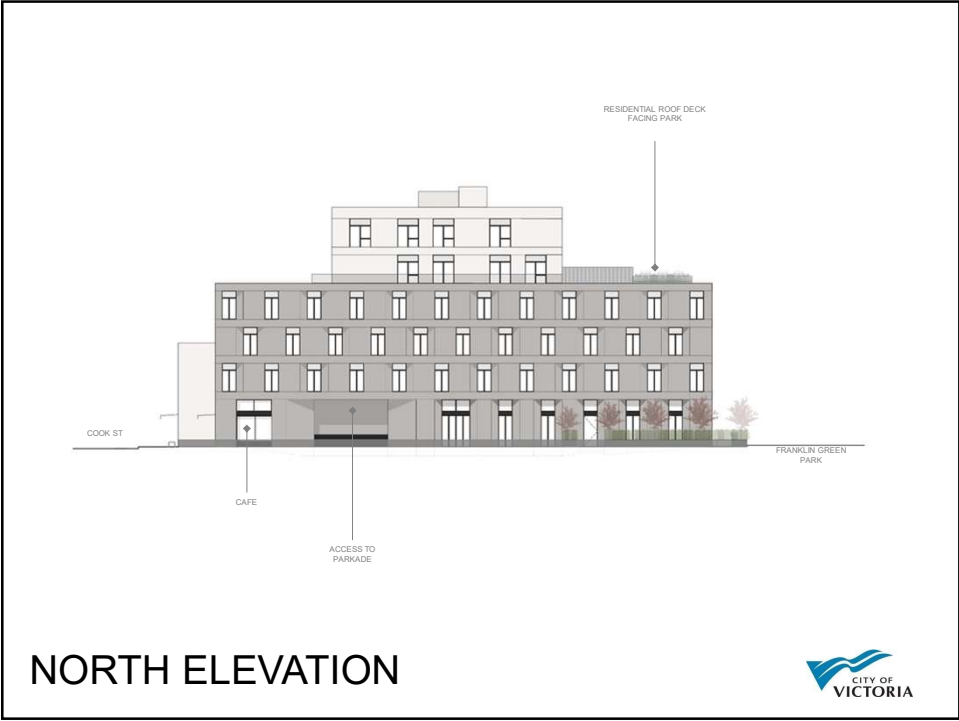
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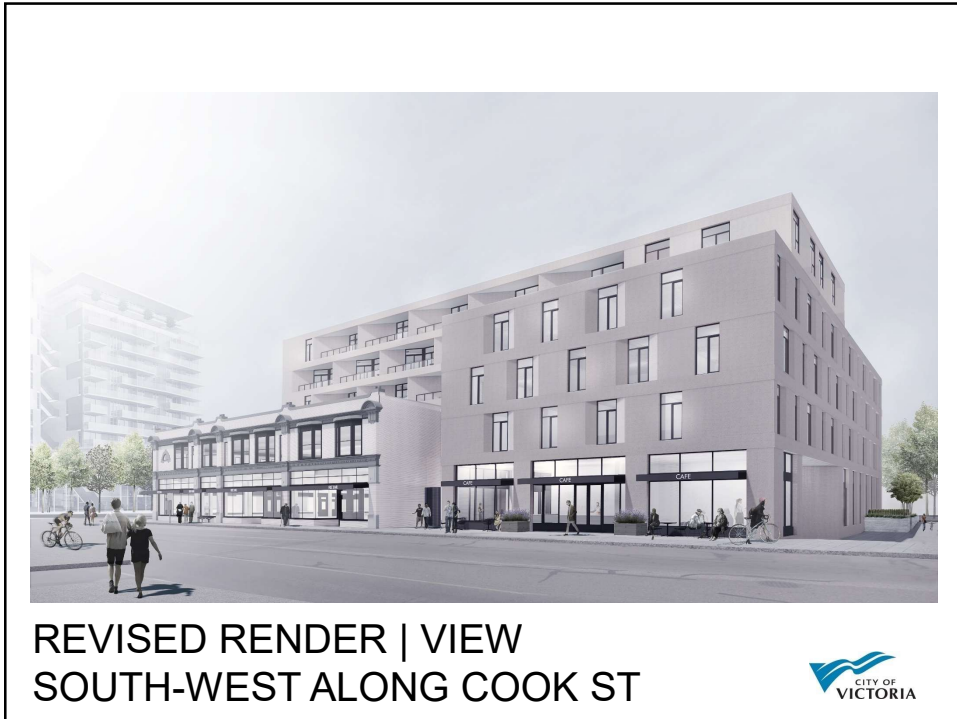
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26



REVISED RENDER | VIEW
SOUTH-WEST ALONG COOK ST

27



REVISED RENDER | VIEW WEST TO
RESIDENTIAL ENTRY MEWS & CAFE

28



REVISED RENDER | VIEW EAST ALONG
RESIDENTIAL MEWS TO COOK ST



29



REVISED RENDER | VIEW NORTH-WEST
ALONG PANDORA AVE & COOK ST



30