

F.1 Bylaws for 1733, 1735, 1737 Fairfield Road: Rezoning Application No. 00821 and Official Community Plan Amendment; 1964 Fairfield and 507 Foul Bay Road: Rezoning Application No. 00845, Official Community Plan Amendment and Heritage Designation Amendment

Moved and Seconded:

1. That the following bylaws **be given first and second readings**:
 - a. Official Community Plan Bylaw, 2012, Amendment Bylaw (No. 54), No. 24-055
 - b. Official Community Plan Bylaw, 2012, Amendment Bylaw (No. 55), No. 24-057
 - c. Zoning Regulation Bylaw, Amendment Bylaw (No. 1343), No. 24-056
 - d. Zoning Regulation Bylaw, Amendment Bylaw (No. 1344), No. 24-058
 - e. Heritage Designation Bylaw, Amendment Bylaw (No. 2), No. 24-051

OPPOSED (2): Councillor Gardiner, and Councillor Hammond

CARRIED (7 to 2)

Moved and Seconded:

2. That subject to approval in principle at the Public Hearing, the applicant prepare and execute the following legal agreements, with form satisfactory to the City Solicitor prior to adoption of the bylaws:
 - a. provision of a 0.86m wide statutory right-of-way for 1733, 1735 and 1737 Fairfield Road, with terms to the satisfaction of the Director of Engineering and Public Works
 - b. provision of no less than nine two-bedroom units, six two-bedroom units with a den and one three-bedroom unit for 1733, 1735 and 1737 Fairfield Road, to the satisfaction of the Director of Sustainable Planning and Community Development
 - c. securing continued public access to the property at 1964 Fairfield Road, consistent with existing public access hours and locations and permitting temporary closures for private events, to the satisfaction of the Director of Sustainable Planning and Community Development
 - d. provision of transportation demand management measures for 1733, 1735 and 1737 Fairfield Road, to the satisfaction of the Director of Engineering and Public Works, including:
 - i. car share memberships and usage credits for all residential units;
 - ii. on-street electric car share vehicle
 - iii. on-street (Beechwood Avenue), level 2, dual head electrical charger, and all associated infrastructure and connections
 - iv. bicycle parking to accommodate oversized bicycles (10% of required long-term spaces)
 - v. bicycle parking with access to an electrical outlet (50% of required long-term spaces)
 - vi. bicycle repair and maintenance station
 - e. securing that the proposed building will be designed to achieve Step Code 4
 - f. provision of required conduits for future solar photovoltaic installation.

3. That the above Recommendations be adopted on the condition that they create no legal rights for the applicant or any other person, or obligation on the part of the City or its officials, and any expenditure of funds is at the risk of the person making the expenditure.

OPPOSED (2): Councillor Gardiner, and Councillor Hammond

CARRIED (7 to 2)

DRAFT



Council Report

For the Meeting of August 1, 2024

To: Council **Date:** July 18, 2024

From: Karen Hoesé, Director, Planning and Development

Subject: **Rezoning Application No. 00821, associated Official Community Plan Amendment, and Development Permit with Variances Application No. 00204 for 1733, 1735 and 1737 Fairfield Road**

Rezoning Application No. 00845 and associated Official Community Plan and Heritage Designation Amendments for 1964 Fairfield Road and 507 Foul Bay Road

RECOMMENDATION

1. That this report be received for information and that the following bylaws be given first and second readings:
 - A. Official Community Plan Bylaw, 2012, Amendment Bylaw (No. 54) - No. 24-055
 - B. Official Community Plan Bylaw, 2012, Amendment Bylaw (No. 55) - No. 24-057
 - C. Zoning Regulation Bylaw, Amendment Bylaw (No. 1343) - No. 24-056
 - D. Zoning Regulation Bylaw, Amendment Bylaw (No. 1344) - No. 24-058
 - E. Heritage Designation Bylaw, Amendment Bylaw (No. 2) - No. 24-051
2. That subject to approval in principle at the Public Hearing, the applicant prepare and execute the following legal agreements, with form satisfactory to the City Solicitor prior to adoption of the bylaws:
 - a. provision of a 0.86m wide statutory right-of-way for 1733, 1735 and 1737 Fairfield Road, with terms to the satisfaction of the Director of Engineering and Public Works
 - b. provision of no less than nine two-bedroom units, six two-bedroom units with a den and one three-bedroom unit for 1733, 1735 and 1737 Fairfield Road, to the satisfaction of the Director of Sustainable Planning and Community Development
 - c. securing continued public access to the property at 1964 Fairfield Road, consistent with existing public access hours and locations and permitting temporary closures for private events, to the satisfaction of the Director of Sustainable Planning and Community Development

- d. provision of transportation demand management measures for 1733, 1735 and 1737 Fairfield Road, to the satisfaction of the Director of Engineering and Public Works, including:
 - i. car share memberships and usage credits for all residential units;
 - ii. on-street electric car share vehicle
 - iii. on-street (Beechwood Avenue), level 2, dual head electrical charger, and all associated infrastructure and connections
 - iv. bicycle parking to accommodate oversized bicycles (10% of required long-term spaces)
 - v. bicycle parking with access to an electrical outlet (50% of required long-term spaces)
 - vi. bicycle repair and maintenance station
 - e. **securing that the proposed building will be designed to achieve Step Code 4**
 - f. **provision of required conduits for future solar photovoltaic installation.**
3. That the above Recommendations be adopted on the condition that they create no legal rights for the applicant or any other person, or obligation on the part of the City or its officials, and any expenditure of funds is at the risk of the person making the expenditure.

EXECUTIVE SUMMARY

The purpose of this report is to provide an update for Council on the concurrent OCP amendments and Rezoning Applications for 1733, 1735 and 1737 Fairfield Road as well as for 1964 Fairfield Road and 507 Foul Bay Road. Since the application was presented to Committee of the Whole on December 7, 2023, the applicant has worked with staff to address the conditions set by Council and has resubmitted revised plans for consideration. In addition, this report provides an update on the correspondence received from the opportunity for consultation on the OCP amendments, which is required pursuant to section 475 of the *Local Government Act*.

BACKGROUND

This application proposes two concurrent proposals:

- a Rezoning, OCP Amendment and Development Permit with Variances application for 1733, 1735 and 1737 Fairfield Road to permit a four-storey multiple dwelling containing 31 units.
- a Rezoning, OCP Amendment and Heritage Designation Amendment application for 1964 Fairfield Road and 507 Foul Bay Road to limit permitted uses and reduce the density to the existing density and uses, which include a garden, restaurant, and gift shop.

The proposal for 1964 Fairfield Road and 507 Foul Bay Road has been submitted to support the increase in density for the proposed development at 1733, 1735 and 1737 Fairfield Road as the development exceeds the density envisioned for Traditional Residential Urban Place Designation.

The application to expand heritage designation to include the maintenance and support area for Abkhazi Garden (located at 507 Foul Bay Road) and the commitment to secure continued public access to Abkhazi Garden will provide heritage protection to the entire site and provide certainty that public access will remain in perpetuity.

The applications for concurrent consideration came before Council on December 14, 2023, where the following resolution was approved:

OCP Amendments with Rezoning

1. *That Council consider who is affected by the proposed changes to the Official Community Plan (OCP) and determine that those within a 200m radius of the subject properties will be affected.*
2. *That Council provide an opportunity for consultation pursuant to section 475 of the Local Government Act, and direct the Director of Sustainable Planning and Community Development to:*
 - a. *mail a notice of the proposed OCP Amendments to the persons within a 200m radius of the subject property*
 - b. *post a notice on the City's website inviting affected persons, organizations and authorities to ask questions of staff and provide written or verbal comments to Council for their consideration.*
3. *That Council consider that no consultation is necessary with the Capital Regional District Board; Councils of Oak Bay, Esquimalt and Saanich; the Songhees and Esquimalt First Nations; the School District Board; or the provincial or federal governments or their agencies because the proposed OCP Amendments do not affect them.*
4. *That Council instruct staff to prepare the necessary Official Community Plan Amendment Bylaws in accordance with Section 475 of the Local Government Act and the necessary Zoning Regulation Bylaw Amendments that would authorize the proposed development outlined in Rezoning Application No. 00821 for 1733, 1735 and 1737 Fairfield Road and proposed zoning changes outlined in Rezoning Application No. 00845 for 1964 Fairfield Road and 507 Foul Bay Road, that first and second reading of the Zoning Regulation Bylaw Amendments be considered by Council and a public hearing date be set once the following conditions are met:*
 - a. *the following revisions to the plans for 1733, 1735 and 1737 Fairfield Road to the satisfaction of the Director of Sustainable Planning and Community Development:*
 - i. *add an additional two-bedroom unit and convert a one-bedroom unit to a two-bedroom unit, as outlined in the applicant's letter, dated September 12, 2023*
 - ii. *improve the transition to lower density buildings (northeast building elevation)*
 - iii. *increase outdoor amenity space, which may include providing parking underground, reducing parking or adding a rooftop amenity area.*
 - b. *the following revisions to the plans for 1733, 1735 and 1737 Fairfield Road, to the satisfaction of the Director of Engineering and Public Works:*
 - i. *increase permeable paved area to improve onsite stormwater management*
 - ii. *to provide 50% of required long term bicycle parking stalls as standard ground mounted stalls.*
 - c. *plan revision for 1733, 1735 and 1737 Fairfield Road to accommodate more replacement trees required under the Tree Protection Bylaw, to the satisfaction of the Director of Parks, Recreation and Facilities.*

5. That subject to approval in principle at the Public Hearing, the applicant prepare and execute the following legal agreements, with form satisfactory to the City Solicitor prior to adoption of the bylaws:
- a. provision of a 0.86m wide statutory right-of-way for 1733, 1735 and 1737 Fairfield Road, with terms to the satisfaction of the Director of Engineering and Public Works
 - b. provision of no less than nine two-bedroom units, six two-bedroom units with a den and one three-bedroom unit for 1733, 1735 and 1737 Fairfield Road, to the satisfaction of the Director of Sustainable Planning and Community Development
 - c. securing continued public access to the property at 1964 Fairfield Road, consistent with existing public access hours and locations and permitting temporary closures for private events, to the satisfaction of the Director of Sustainable Planning and Community Development
 - d. provision of transportation demand management measures for 1733, 1735 and 1737 Fairfield Road, to the satisfaction of the Director of Engineering and Public Works, including:
 - i. car share memberships and usage credits for all residential units
 - ii. on-street electric car share vehicle
 - iii. on-street (Beechwood Avenue), level 2, dual head electrical charger, and all associated infrastructure and connections
 - iv. bicycle parking to accommodate oversized bicycles (10% of required long-term spaces)
 - v. bicycle parking with access to an electrical outlet (50% of required long-term spaces)
 - vi. bicycle repair and maintenance station.
6. That the above Recommendations be adopted on the condition that they create no legal rights for the applicant or any other person, or obligation on the part of the City or its officials, and any expenditure of funds is at the risk of the person making the expenditure.

Development Permit with Variances Application (1733, 1735 and 1737 Fairfield Road)

That Council, after giving notice and allowing an opportunity for public comment at a meeting of Council, and after the public hearing for Rezoning Application No. 00821 and Rezoning Application No. 00845, if they are approved, consider the following motion:

- “1. That subject to the adoption of the necessary Zoning Regulation Bylaw Amendment, Council authorize the issuance of Development Permit with Variances No. 00204 for 1733, 1735 and 1737 Fairfield Road, in accordance with plans submitted to the Planning department and date stamped by Planning on August 22, 2023, subject to:
- a. the proposed development meeting all City zoning bylaw requirements, except for the following variances:
 - i. reducing the minimum front yard setback (Beechwood Avenue) from 4.00m to 2.70m and increasing stairs projection from 1.80m to 2.73m
 - ii. reducing the minimum rear yard setback from 10.00m to 2.60m
 - iii. reducing the minimum side yard setback (Fairfield Road) from 4.00m to 2.30m
 - iv. reducing the minimum side yard setback (southeast) from 6.00m to 0.50m
 - v. increasing the maximum site coverage from 40% to 65%
 - vi. decreasing the open site space from 50% to 23%

- vii. *reducing vehicle parking from 40 spaces to 23 spaces*
 - viii. *permitting long-term bicycle parking to be provided in a stacked format.*
 - b. *The property being consolidated into one lot.*
- 2. *That the Development Permit with Variances, if issued, lapses two years from the date of this resolution.”*

Heritage Designation Amendment Application (1964 Fairfield Road and 507 Foul Bay Road)

That Council instruct the Director of Sustainable Planning and Community Development to:

- 1. prepare a heritage designation bylaw to amend Heritage Designation (1964 Fairfield Road) Bylaw (No. 530) No. 05-75 to add the property at 507 Foul Bay Road, that first and second reading of the bylaw be considered by Council and that a joint public hearing date be set with Rezoning Application No. 00821 and Rezoning Application No. 00845*
- 2. add the Statement of Significance for 1964 Fairfield Road and 507 Foul Bay Road, attached as Attachment G to this report, recognizing the building exterior and natural landscape elements as the historic features of the property, to the above noted heritage designation bylaw.*

The applicant has now responded to the conditions contained in the December 14, 2023 motion to allow a public hearing date to be set. In addition, the applicant is now offering to secure a commitment to design the building to Step 4 This report also provides an update on the opportunity for consultation for proposed changes to the Official Community Plan (OCP).

UPDATE

The following sections provide a summary of the OCP referral and an update on the plan revisions set out in the December 14, 2023 Council motion.

Official Community Plan Referral

1733, 1735 and 1737 Fairfield Road

Notification of the proposed Official Community Plan Amendment was mailed to properties within a 200m radius of the subject properties on June 6, 2024, and a notice was posted on the City's website, inviting affected persons, organizations and authorities to ask questions of staff and provide written or verbal comments to Council for consideration. The comment period ended on July 6, 2024 and out of 350 notifications mailed out, 23 responses were received, which is a 6.6% response rate. A general summary of the commentary received is provided below.

The responses identified the following areas of support:

- provision of needed housing
- aligns well with the Provincial government direction for increasing housing
- does not propose use of natural gas
- support for more density along Fairfield, hope to see similar closer to/in the Plaza.

The responses identified the following areas of concern:

- impact of density on existing capacity of surrounding services and infrastructure
- impact on aesthetic appeal, quality of life and character of the neighbourhood
- potential to exacerbate traffic, parking shortages
- four storey height will impact the neighbourhood look, feel and sightlines
- history of developer to not delivery affordability
- concern for lack of setbacks, useable outdoor space and landscaping
- lack of community amenities proposed to merit the proposed density
- not providing family-focused, attainable housing or rentals
- concern for construction phase impacts
- shade impacts for abutting neighbour at 1745 Fairfield Road
- developer proposed a different building height in early consultations
- densification does not, by default, meet the objectives of the missing middle
- Newer townhomes can cost as much or more than an existing single family and have no suite to reduce mortgage costs
- Support housing but missed opportunity for a mixed used buildings for access to daily needs
- Support the housing but disappointed to see surface parking
- Potential to decrease property value.

1964 Fairfield Road and 507 Foul Bay Road

Notification of the proposed Official Community Plan Amendment was mailed to properties within a 200m radius of the subject properties on June 6, 2024, and a notice was posted on the City's website, inviting affected persons, organizations and authorities to ask questions of staff and provide written or verbal comments to Council for consideration. The comment period ended on July 6, 2024 and out of 255 notifications mailed out, six responses were received, which is a 2.4% response rate.

All responses received were in support of the proposal, which included the following:

- importance of protecting of the garden which is a historic and iconic site
- the garden benefits the community's residents and attracts tourists
- the importance of maintaining public access
- reduction in density and change in permitted uses is appropriate

Revised Proposal for 1733, 1735 and 1737 Fairfield Road

The applicant has submitted a revised application package in response to the Council motion. The following section provides a summary of the revisions made in response to the Council motion as well as other revisions proposed by the applicant.

Height Transition

The motion included a condition to improve the proposed building's transition to lower density buildings with a specific focus on the northeast elevation design. In response, the applicant has revised the proposal to step back a portion of the fourth storey by 2.1m to pull some building mass away from the northeast neighbouring property. The step back does not extend the full length of the northeast building face and will not be visible from Fairfield Road; therefore, the revision will

reduce the potential for overlook and privacy impacts but will not alter the appearance from the public realm.

Outdoor Amenity Space

The motion included a condition to increase outdoor amenity space, which would be best addressed by providing parking underground to increase open site space, further reductions in vehicle parking or by adding a rooftop amenity area to better utilize the flat roof being proposed. In response, the applicant provided the following as a rationale for not proposing underground parking:

Underground parking should be avoided where possible as it disturbs significant volumes of soil, introduces a large ramp to the street presence, and limits soil depths for planting. We therefore believe that underground parking is not the correct solution for this site. While we appreciate staff's emphasis on minimizing parking to enhance green spaces, we believe the proposal provides a suitable balance for future homeowners and the neighbourhood at large.

The applicant has proposed more minor revisions to modify ground floor unit entry areas to provide usable space for outdoor seating and more effectively contribute to street activity. As a result of the revisions, all units now include a usable private outdoor space.

Parking Area

The motion also included a condition that the permeable paved area be expanded to improve onsite stormwater management. In response, the applicant has fulfilled this condition by providing permeable area for the portions of the surface parking that are uncovered by the building. The motion also included a condition to provide at least 50% of required long term bicycle parking stalls as standard ground mounted stalls, which has been provided in the revised proposal.

Required Replacement Trees

The motion included a condition to revise the site plan to accommodate additional replacement trees, which was not addressed in the revised proposal. The following was provided in the updated letter to Mayor and Council:

Faced with competing priorities for space and the need to balance design and parking requirements, we are unable to reduce the number of impacted trees or provide more replacement trees on the property. We have however committed to a cash-in-lieu agreement with the Parks department where we can provide \$4,000 for each replacement tree, which is double the financial commitment in the current Tree Protection Bylaw.

Unit Size

Prior to consideration at Committee of the Whole, the applicant submitted a letter that outlined a proposed revision to add an additional two-bedroom unit and convert a one-bedroom unit to a two-bedroom unit by removing a common outdoor amenity space on the third floor. This revision has now been included in the revised plans.

Additional Revisions

The applicant has also submitted revisions that were not in response to the Council motion. Below are the proposed revisions put forward for Council consideration.

Energy Efficiency

As noted in the applicant's letter, the proposed multiple dwelling will be designed to achieve Step Code 4 requirements and will be providing conduit for future solar photovoltaic installation, which exceeds current requirements for residential buildings between four and six storeys. The recommendation includes the necessary language to secure this commitment with a legal agreement prior to final adoption of the Bylaw amendments.

Materiality

The applicant has also revised the building design, specifically replacing the proposed brick with a coated fiber cement cladding. The applicant has cited a need to reduce the cost to deliver the project, and notes additional costs anticipated to achieve Step Code 4 requirements noted above. Although the previously proposed light brick was encouraged as a very high quality and appropriate material choice, the revision is considered supportable because the alternative is high quality, durable and capable of weathering gracefully.

CONCLUSIONS

The Official Community Plan Amendment referral period has concluded, and correspondence received has been attached for Council's consideration. The applicant has also revised the proposal in accordance with Council's previous direction; therefore, the recommendation provided for Council's consideration contains the appropriate language to advance this application to a Public Hearing.

Respectfully submitted,

Patrick Carroll
Senior Planner
Development Services

Karen Hoese, Director
Planning and Development Department

Report accepted and recommended by the City Manager.

List of Attachments

- Attachment A – Letter to Mayor and Council dated July 4, 2024
- Attachment B – Revised plans received July 4, 2024
- Attachment C – COTW Report from the meeting held December 7, 2023
- Attachment D – OCP Amendment Correspondence
- Attachment E – OCP Bylaw No. 24-055 for 1733-1737 Fairfield Road
- Attachment F – Rezoning Bylaw No. 24-056 for 1733-1737 Fairfield Road
- Attachment G – OCP Bylaw No. 24-057 for 1964 Fairfield Road and 507 Foul Bay Road
- Attachment H – Rezoning Bylaw No. 24-058 for 1964 Fairfield Road and 507 Foul Bay Road
- Attachment I – Heritage Designation Bylaw No. 24-051 for 1964 Fairfield Road and 507 Foul Bay Road

Update Council Report

Rezoning Application No. 00821, associated Official Community Plan Amendment, and Development Permit with Variances Application No. 00204 for 1733, 1735 and 1737 Fairfield Road & Rezoning Application No. 00845 and associated Official Community Plan and Heritage Designation Amendments for 1964 Fairfield Road and 507 Foul Bay Road

1733-37 Fairfield Road
Letter to Mayor & Council

1733-37 Fairfield Road

Letter to Mayor and Council

ARYZE

May 2024

City of Victoria
1 Centennial Square
Victoria, British Columbia V8W 1P6

RE: 1733-37 Fairfield Road

Dear Mayor Alto, Council and Staff:

We are pleased to present this updated letter which outlines some recent improvements to the proposal at 1733-37 Fairfield Road in the Gonzales neighbourhood. These changes have been as a result of conversations and feedback from City staff, local residents and further technical analysis of the proposal. We strongly believe that these improvements align with feedback from the Advisory Design Panel and the Committee of the Whole, and further reinforce Aryze's commitment to increasing housing supply in a manner that respects the existing neighbourhood while introducing innovative solutions to the current challenges the industry is facing. Taken together, we believe that the updates respond to the conditions in the staff report and meet the test in order to advance to Public Hearing.

Climate Change and Energy

Responding to the threat of climate change is increasingly a high priority for the City of Victoria and we take our commitment to supporting this goal very seriously. While the current BC Building Code has made some improvements in building for climate resiliency, we would like this project to exceed base requirements and achieve Step Code 4. Early in the project we committed to full electrification of the homes by removing natural gas. This was an important step for greenhouse gas reduction and now we would like to go further by delivering a highly energy efficient building.

We have completed energy model tests in order to determine the different pathways to Step Code 4 and through the Building Permit process we will provide further detail on the energy demand and intensity of the building. At this stage we are incredibly excited to be able to commit to delivering Step Code 4 before it becomes a requirement later in 2024 in Victoria.

We have also conducted a Solar Assessment for the project to understand racking layout, cost and payback period, and structural implications of adding solar panels to the roof, and will be providing conduit for future Solar Photovoltaic installation.

This will be important both as a case study for other buildings throughout Victoria, but also as a way to increase the knowledge and experience of both developers and builders ahead of these important policy changes.

Building Transition

The *Design Guidelines for Multi-Unit Residential, Commercial and Industrial Development* state that a building should provide a transition in form and massing to surrounding buildings. Throughout the design of this project we have been sensitive to neighbouring properties to the side and rear of the site. On the north-east side of the proposed building we have relocated mechanical louvers away from this area and increased planting in order to create an improved interface with 1745 Fairfield Road.

Following further analysis we believe that the building form can be further amended to provide a transition that aligns with the recommendations in the *Design Guidelines*. We propose to step back part of the fourth floor to pull some building mass away from this side of the site. This will reduce overlook from the bedroom windows and provide some benefits to the shadowing impact. Fairfield Road is a major road connector in the City and we maintain that a 4-storey building form is appropriate in this location. This change will deliver both a 4-storey form on the front of the site and a more respectful building step to the rear to respond to the existing neighbouring condition.

Finally, it is important to recognise that the recent Missing Middle regulations would support a height of 12m and a side yard setback of 1.5m on the neighbouring property. Our proposal is only slightly above the maximum building height under Missing Middle policies (0.9m above the max.) and provides a greater setback. We therefore believe the proposal responds to current regulations and how the neighbourhood may develop in the future.

Through conversations with staff and the immediate neighbour to the north-east, we have made a number of design moves that, taken together, improve the relationship between the proposed multi-family building and the existing neighbouring home. These include:

- Removal of mechanical louvers on the north-east elevation and relocation to elsewhere on the site, thus removing any concerns with noise and aesthetics;
- Changes to the fence design on the property line and amendments to the street tree planting locations in order to ensure a safe travel path and increased visibility for cars in and out of the neighbouring driveway;
- Careful location of windows in order to minimize overlook and be respectful of privacy; and
- Stepping of the building at level 4 to respond to the Design Guidelines and provide a respectful transition.

Housing Mix

The Victoria Housing Strategy outlines the goal of increasing housing choice (Goal 3) by providing a mix of housing and unit types. Recognizing the evolving housing needs of our community—in particular the shortfall in larger unit sizes—we are proposing to increase the number of 2-bedroom homes by eliminating the smaller studio units. The project now includes a total of 29 homes of which 19 or 66% are providing a minimum of 2 bedrooms. Our commitment to providing diverse and inclusive housing options

remains as a cornerstone of this project.

Open Space & Parking

Through the evolution of the design we have aimed to provide a transportation strategy that balances the desire to reduce car dependency, while also providing sufficient vehicular and bicycle parking for future homeowners. This has also been balanced with delivering a landscape design that introduces new plantings to the site.

Underground parking should be avoided where possible as it disturbs significant volumes of soil, introduces a large ramp to the street presence, and limits soil depths for planting. We therefore believe that underground parking is not the correct solution for this site. While we appreciate staff's emphasis on minimizing parking to enhance green spaces, we believe the proposal provides a suitable balance for future homeowners and the neighbourhood at large.

Open space demands can be met through the proposed selective planting articulated in the landscape plan and the site's proximity to Hollywood Park. With the change in unit mix (removing the studios), private outdoor amenity space has now been provided to every unit. Responding to the conditions in the staff report, the ground floor Townhouse entry areas have been reconfigured to provide space for a table and chairs to the side of the suite entry door, creating more of a 'porch' feel for residents. This will be key for both the homeowners but also activating this part of the street.

Faced with competing priorities for space and the need to balance design and parking requirements, we are unable to reduce the number of impacted trees or provide more replacement trees on the property. We have however committed to a cash-in-lieu agreement with the Parks department where we can provide \$4,000 for each replacement tree, which is double the financial commitment in the current Tree Protection Bylaw.

Permeable Pavers

The project is committed to improving the on-site stormwater management and we have therefore increased the total area of permeable pavers in the rear parking area. All surfaces that are not under the cover of the proposed building will be constructed with permeable pavers that reduce the negative impacts of stormwater runoff. The landscape plans have been updated to reflect this change.

Building Materiality

In our ongoing efforts to refine and optimize the project, we are proposing a change to the material on parts of the building exterior. We are proposing to replace the brick with Ceraclad, a high performing fiber cement cladding product. This choice is meticulously considered, ensuring compliance with the *Design Guidelines* which advocate for the use of high-quality, durable materials to preserve the condition of facades. The coated cement panel (such as the Ceraclad product) will complement the metal panel and still maintain the design approach of how the townhouse units and overall 'base' of the building have

a more substantial and solid expression. One feature also worth noting with the Ceraclad product is that the triple coated membrane will block UV light and rain to reduce deterioration over time. This aligns with the *Guidelines* request for durable materials.

The project has been in design for over two years and this—combined with the construction cost pressures of achieving Step Code 4—have forced us to reconsider the use of brick. We believe this switch will still allow us to maintain the aesthetic integrity while also delivering upon other project benefits.

Summary

The proposed development at 1733-37 Fairfield Road will meet a number of policy goals that have been outlined in the OCP, Victoria Housing Strategy and related transportation and climate action plans. We believe that this location—on a major transit and mobility corridor close to a range of day-to-day services and popular destinations—is an ideal candidate for a four-storey residential building. The recent improvements outlined above strengthen the overall proposal and respond to the conditions in the staff report that was supported at the Committee of the Whole. The delivery of a Step Code 4 multi-family building will be an important milestone for the City and we are excited to bring forward this level of innovation in the housing sector.

Project at a Glance



Height
4 storeys



North Setback
2.33m



Number of Homes
29 Homes



Short-term Bike Stalls
6 Stalls

Site Area
18,751 sqft

East Setback
2.62m

Townhomes
6 Townhomes

Long-term Bike Stalls
51 Stalls

Floor Area
33,506 sqft

South Setback
4.78m-11.90m

1 Bedroom
10 Homes

Parking Stalls
23 Stalls

FSR
1.79 FSR

West Setback
2.73m

2 Bedroom
6 Homes

Residential Tenure
Strata Ownership

Site Coverage
65%

2 Bedroom + Den
7 Homes

Unique Features

- High performance & sustainable building strategies
- Built to meet BC Step Code 4 energy efficiency Standards wherever possible
- Oversized bike room and storage lockers for residents

- Zero-carbon / 100% Electric HVAC and hot water delivery system.
- The building will provide one on street Modo car for the use of the broader community

In conclusion, we acknowledge staff recommendations and commit to align with them as previously described, while continually striving to go above and beyond where possible.

We believe these changes not only align with the evolving needs of our community but also contribute positively to our City vision for sustainable, family-friendly and aesthetically pleasing urban development.

Thank you for your time and consideration. We look forward to any feedback or further discussion on these proposed changes.

Sincerely,

A handwritten signature in black ink, appearing to read "Mike Wagar". The signature is fluid and cursive, with the first name "Mike" and last name "Wagar" clearly distinguishable.

Mike Wagar

Development Manager
Aryze Developments
Aryze.ca



PROJECT LOCATION PLAN (NTS)



PROJECT SITE (NTS)

CONTACTS

OWNER Aryze Developments Inc. 1839 Fairfield Road Victoria, BC V8S 1G9	Mike Wagar mwagar@aryze.ca 250.940.3568 ext. 375
ARCHITECT Cascadia Architects 101 - 804 Broughton Street Victoria, BC V8W 1E4	Gregory Damant, Architect AIBC greg@cascadiaarchitects.ca 250.590.3223
LANDSCAPE DESIGNER Biophilia 1608 Camosun Street Victoria, BC V8T 3E6	Bianca Bodley bianca@biophilialcollective.ca 250.590.1156
CIVIL ENGINEER McElhanney 500 - 3960 Quadra Street Victoria, BC V8X 4A3	Nathan Dunlop ndunlop@mcelhanney.com 778.746.7417

LIST OF DRAWINGS

ARCHITECTURAL	
A-000	COVER
A-009	PERSPECTIVES
A-010	PERSPECTIVE & WINDOW OVERLAY
A-011	CONTEXT ELEVATIONS & PERSPECTIVES
A-012	SHADOW STUDIES
A-050	SITE PLAN & PROJECT DATA
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ARYZE
1733-1737 Fairfield Rd
JUNE 25, 2024

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		17	



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1 BEECHWOOD ELEVATION PERSPECTIVE
SCALE = 1 : 100



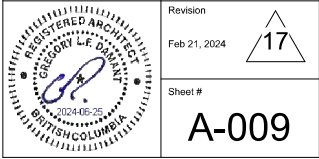
2 FAIRFIELD PEDESTRIAN VIEW
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17	DPRZ Rev5	Feb 21, 2024
13	DPRZ Rev4	Jan 18, 2024
4	DPRZ Rev2	Jan 26, 2023
3	DPRZ Rev1	July 20, 2022
2	DPRZ Submission	April 29, 2022
1	Development Tracker Application	March 7, 2022
NO.	DESCRIPTION	DATE

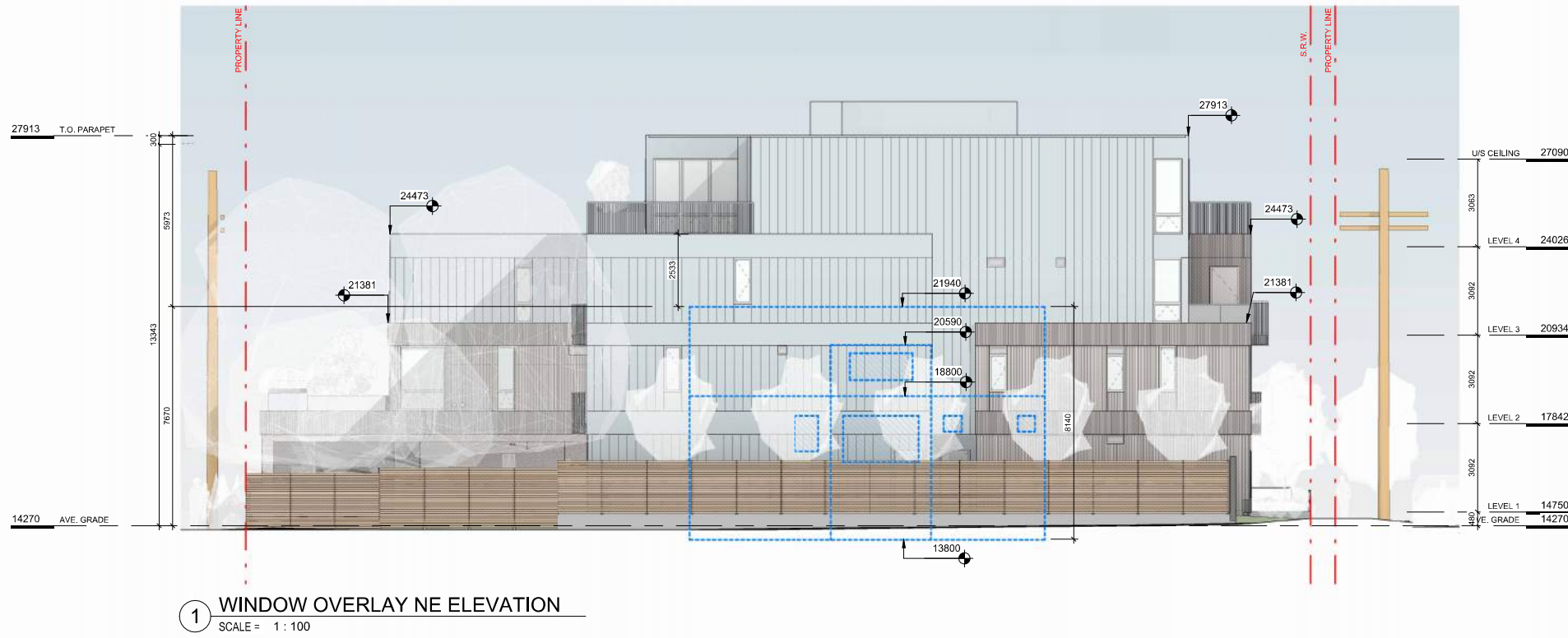


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Project		1733-1737 Fairfield Rd
		ARYZE
Sheet Name		PERSPECTIVES
Date		JUNE 25, 2024
Scale	1 : 100	Project # 2123
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17	DPR2 Rev5	Feb 21, 2024
13	DPR2 Rev4	Jan 15, 2024
4	DPR2 Rev2	Jan 26, 2023
3	DPR2 Rev1	July 20, 2022



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Project 1733-1737 Fairfield Rd		
ARYZE		
Sheet Name PERSPECTIVE & WINDOW OVERLAY		
Date JUNE 25, 2024		
Scale 1 : 100	Project # 2123	Revision Feb 21, 2024 17
Sheet # A-010		

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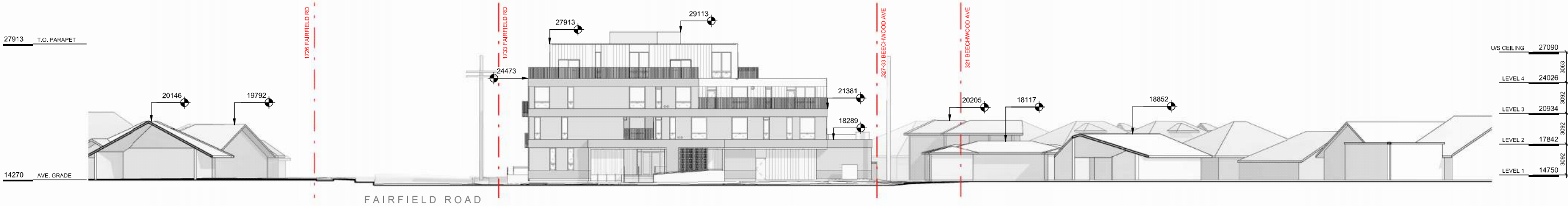
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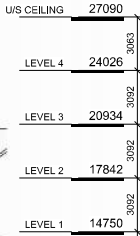
1 CONTEXT PERSPECTIVE FAIRFIELD ROAD
SCALE = 1 : 100



2 CONTEXT PERSPECTIVE BEECHWOOD AVE
SCALE = 1 : 100



3 CONTEXT ELEVATION - BEECHWOOD AVENUE
SCALE = 1 : 200



4 CONTEXT ELEVATION - FAIRFIELD ROAD
SCALE = 1 : 200

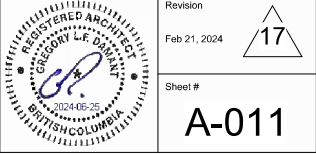
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13	DPIR2 Rev4	Jan 18, 2024
4	DPIR2 Rev2	Jan 28, 2023
2	DPIR2 Submission	April 29, 2022
1	Development Tracker Application	March 7, 2022
NO.	DESCRIPTION	DATE



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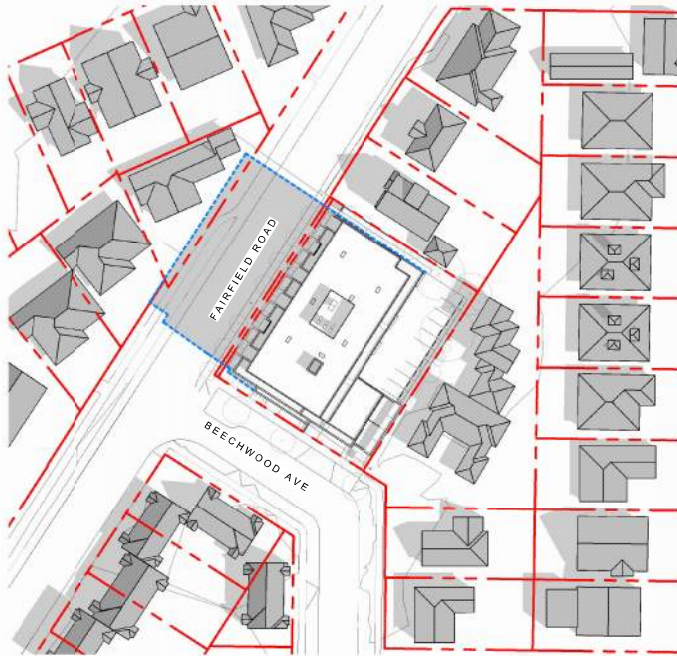
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	ARYZE	
Sheet Name	CONTEXT ELEVATIONS & PERSPECTIVES	
Date	JUNE 25, 2024	
Scale	As indicated	Project # 2123
	Revision	Feb 21, 2024 17
	Sheet #	A-011



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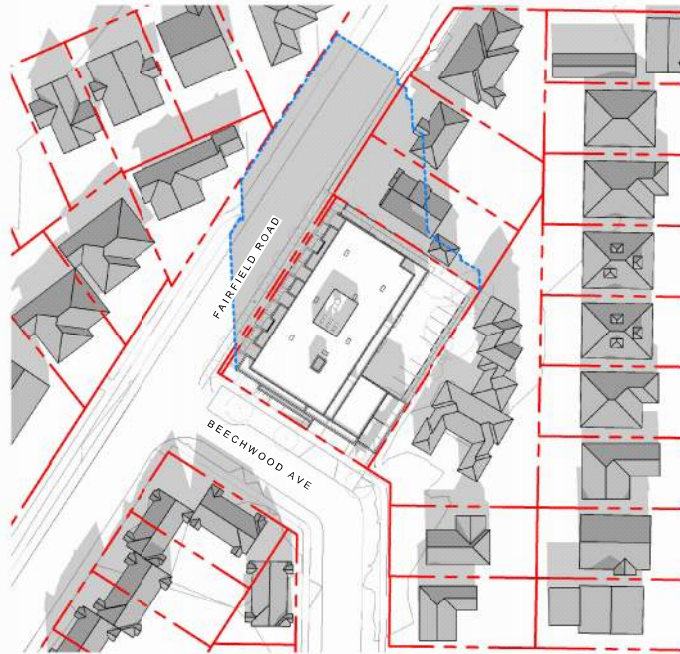
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SUMMER SOLSTICE 12:00 PM



EQUINOX 12:00 PM



WINTER SOLSTICE 12:00 PM



SUMMER SOLSTICE 4:00 PM



EQUINOX 4:00 PM



WINTER SOLSTICE 4:00 PM

4	DP/RZ Rev2	Jan 26, 2023
2	DP/RZ Submission	April 29, 2022
1	Development Tracker Application	March 7, 2022
NO.	DESCRIPTION	DATE

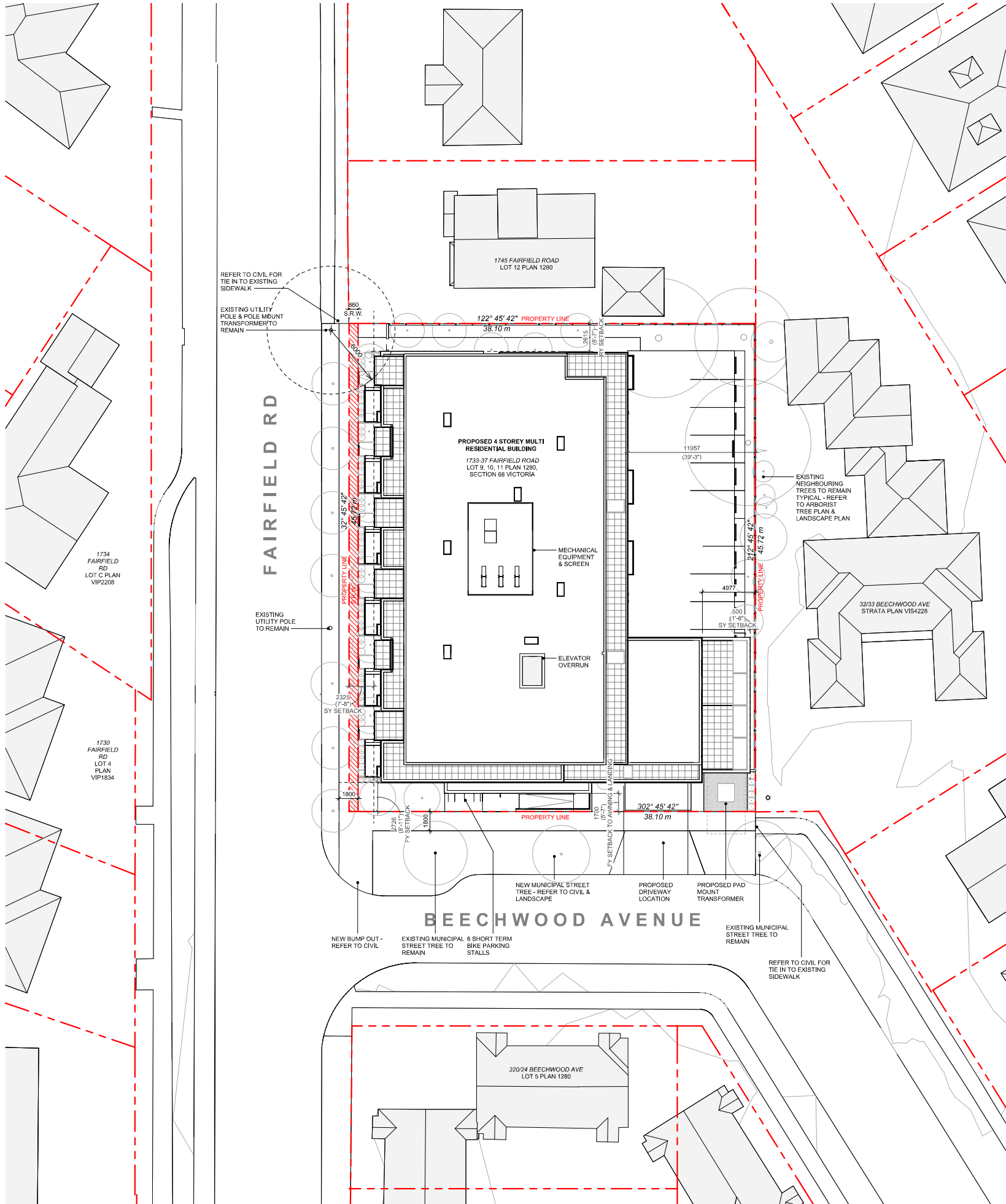


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Project	1733-1737 Fairfield Rd	
	ARYZE	
Sheet Name	SHADOW STUDIES	
Date	JUNE 25, 2024	
Scale	1 : 750	Project # 2123
Revision	Jan 26, 2023	4
Sheet #	A-012	

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2 PROPOSED SITE PLAN
SCALE = 1 : 200

DATA

MUNICIPALITY	CITY OF VICTORIA
MUNICIPAL ADDRESS	1733-37 FAIRFIELD ROAD, VICTORIA BC
LEGAL DESCRIPTION	LOT 9, 10, 11, PLAN 1280, SECTION 68 VICTORIA
PROJECT DESCRIPTION	4 STOREY MULTI UNIT RESIDENTIAL BUILDING

PROJECT INFORMATION

ZONE (EXISTING)	R1-G	
PROPOSED ZONE	UNKNOWN	
SITE AREA (m ²)	1,741.93m ²	
TOTAL FLOOR AREA (m ²)	3113.67m ²	
COMMERCIAL FLOOR AREA (m ²)	0	
FLOOR SPACE RATIO	1.79:1	
SITE COVERAGE (%)	85%	
OPEN SITE SPACE (%)	23%	
HEIGHT (m)	13.39m	
NUMBER OF STOREYS	4	
	REQUIRED	PROVIDED
PARKING STALLS (#) TOTAL ON SITE		23 (+1sm)
PARKING STALLS - VISITOR (#) ON SITE	3	2
BICYCLE PARKING (#) SHORT TERM	6	6
BICYCLE PARKING (#) LONG TERM	37	51

BUILDING SETBACKS (m)

	PROPOSED
FRONT YARD (BEECHWOOD AVE)	2.73m
FRONT YARD (TO AWNING/LANDING)	1.70m
INTERNAL SIDE YARD (TO BALCONY)	0.50m
INTERNAL SIDE YARD (TO BUILDING FACE)	4.78m
SIDE YARD (FAIRFIELD RD)	2.33m
REAR YARD	2.62m
COMBINED SIDE YARDS	2.83m

RESIDENTIAL USE DETAILS

TOTAL NUMBER OF UNITS	29
UNIT TYPE	TOWNHOMES (6), 1 BEDS (10), 2 BEDS (13).
GROUND-ORIENTATED UNITS	6
MINIMUM UNIT FLOOR AREA (m ²)	51m ²

17	DPRZ Rev5	Feb 21, 2024
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5	DPRZ Rev3	Aug 10, 2023
4	DPRZ Rev2	Jan 26, 2023
3	DPRZ Rev1	July 20, 2022
2	DPRZ Submission	April 29, 2022
1	Development Tracker Application	March 7, 2022
NO.	DESCRIPTION	DATE

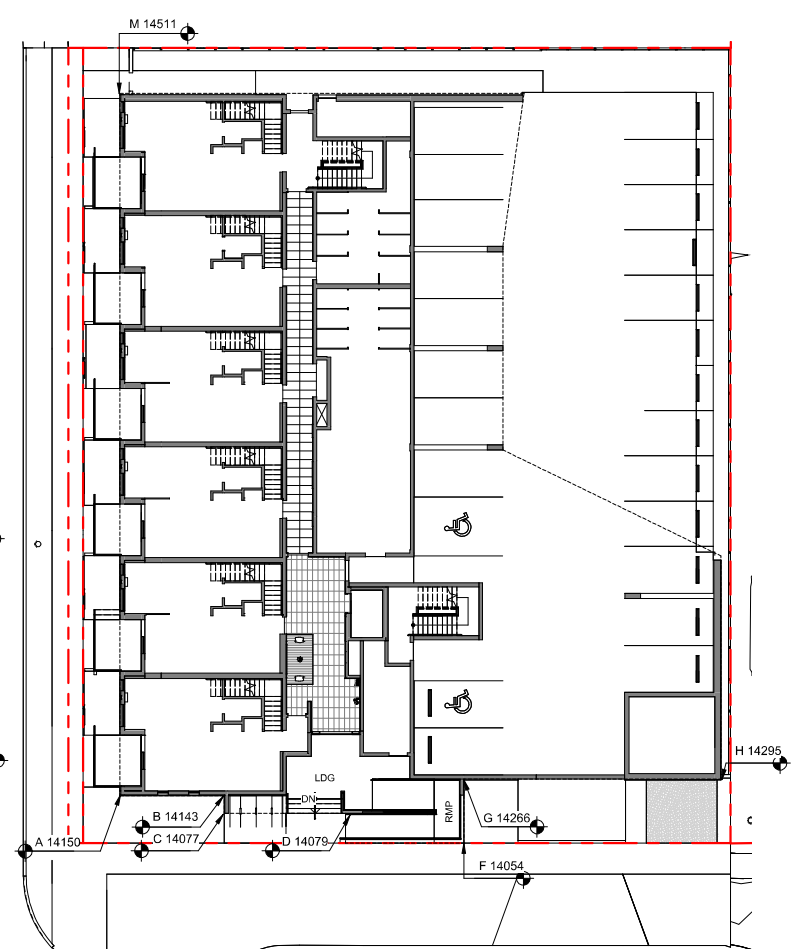
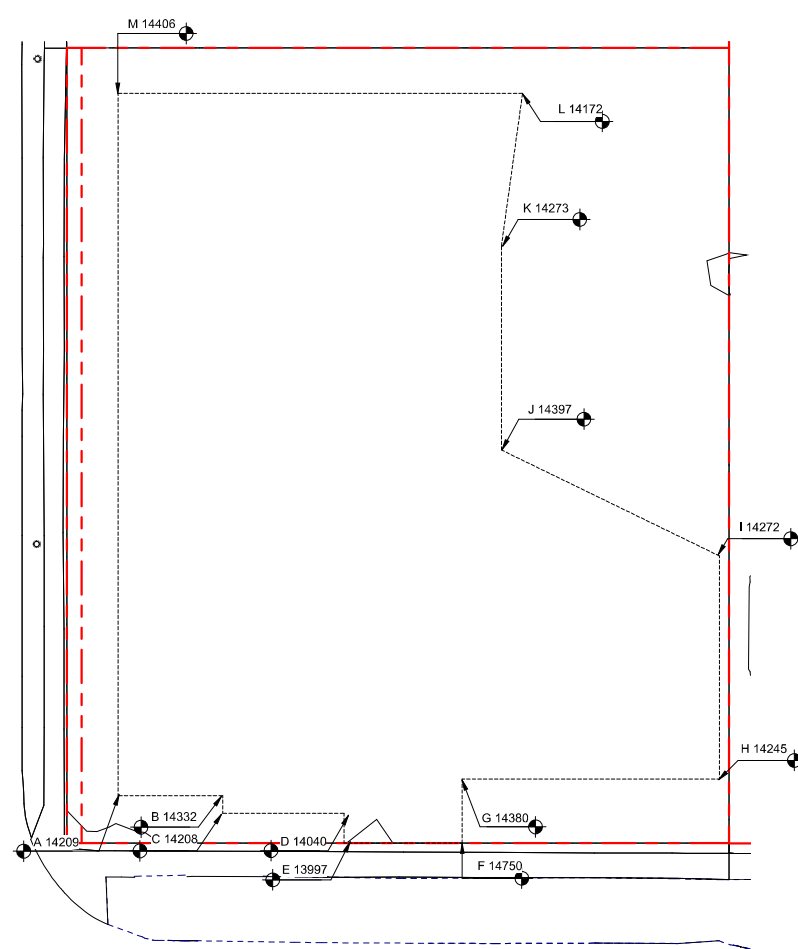
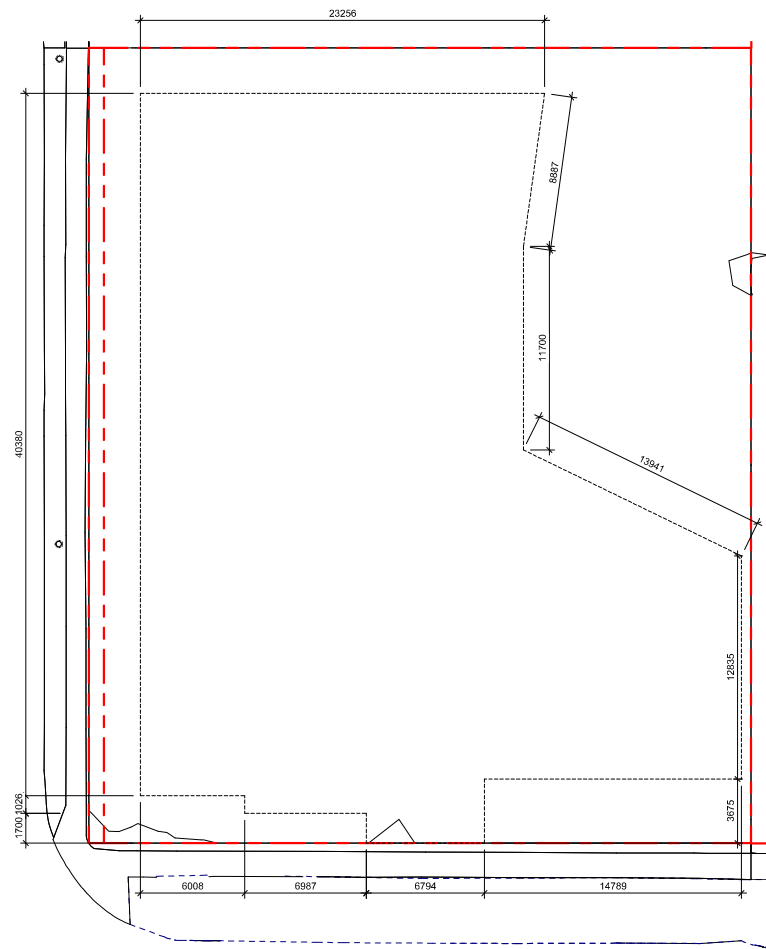


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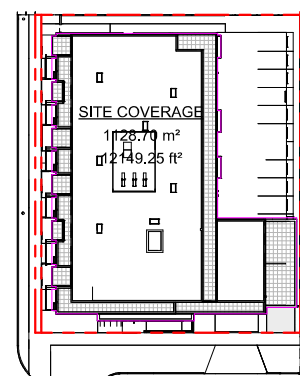
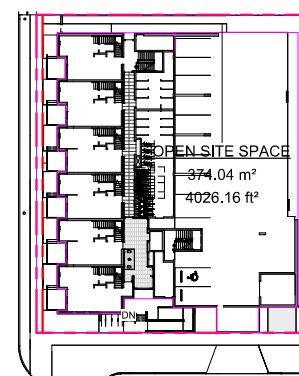
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		ARYZE
Sheet Name		SITE PLAN & PROJECT DATA
Date		JUNE 25, 2024
Scale	As indicated	Project # 2123
	Revision	17
	Feb 21, 2024	
	Sheet #	A-050

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POINT NAME	NATURAL	PROPOSED	DISTANCE BETWEEN POINTS		AVE. OF POINTS
A	14.209	14.42	Point A & B	6.008	85.20
B	14.332	14.15	Point B & C	1.026	14.50
C	14.208	14.12	Point C & D	7.381	103.92
D	14.04	14.11	Point D & E	1.7	23.83
E	13.997	14.10	Point E & F	6.4	89.96
F	14.75	14.11	Point F & G	3.63	51.72
G	14.38	14.44	Point G & H	13.35	191.33
H	14.346	14.28	Point H & I	12.88	183.90
I	14.273	14.41	Point I & J	12.663	181.52
J	14.397	14.69	Point J & K	11.7	167.72
K	14.273	14.64	Point K & L	8.887	126.40
L	14.172	14.62	Point L & M	23.256	332.30
M	14.406	14.64	Point M & N	40.38	577.74
			Total	149.26	2130.03
			Average Grade	14.27	

*NOTE: Lowest grade point governs per Schedule A definition



SUPPORTING CALCULATIONS

ZONING REGULATION BYLAW FLOOR AREAS	
LEVEL 1	543.81 m ²
LEVEL 1	21.29 m ²
LEVEL 2	955.88 m ²
LEVEL 3	899.87 m ²
LEVEL 4	692.82 m ²
Grand total: 5	3113.67 m ²

FLOOR SPACE RATIO
 $3113.67\text{m}^2 / 1741.93\text{m}^2 = 1.787$
 FSR - **1.79 : 1**

SITE COVERAGE
1128.70m² / 1741.93m² = 0.65 (65%)

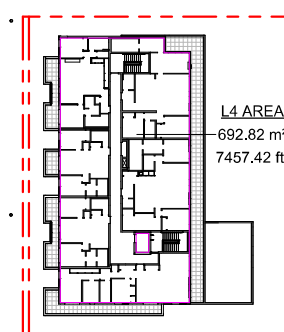
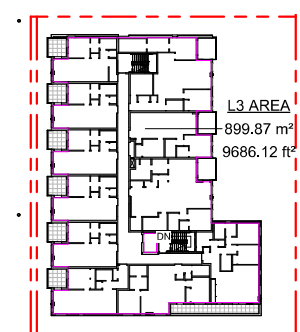
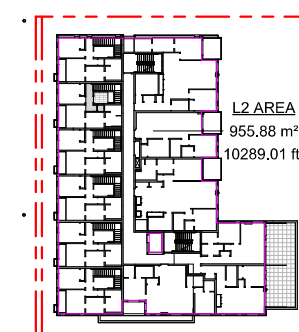
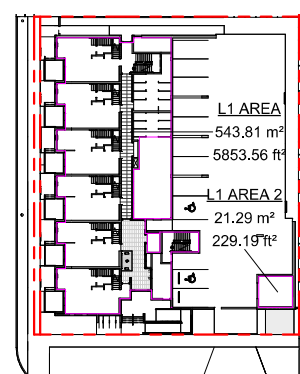
OPEN SITE SPACE
395.01m² / 1741.93m² = 0.23 (23%)

HEIGHT OF PATIOS, STAIRS, & RAMP TO
EXISTING & PROPOSED GRADE
REFER TO GRADE KEY PLANS

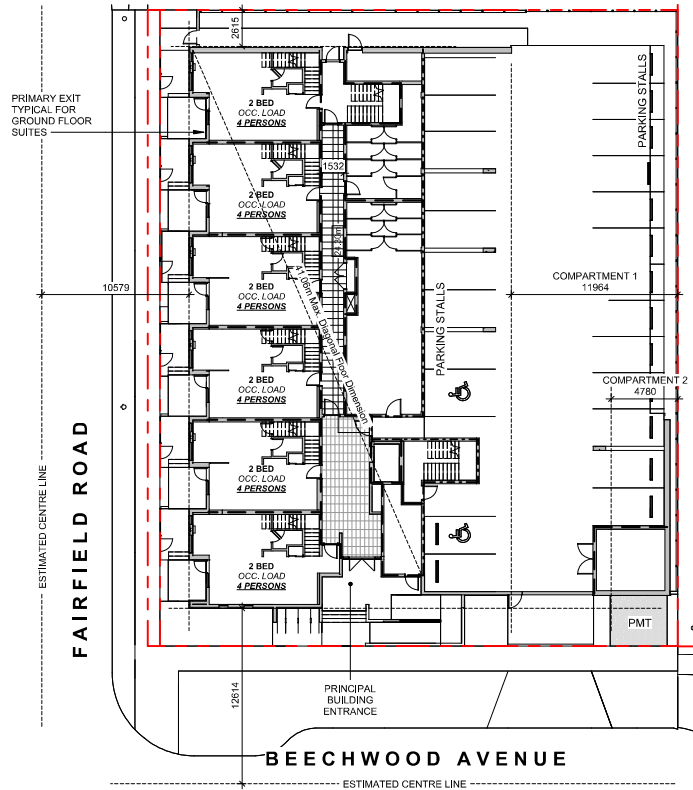
1TH PATIO
(WORST CASE GRADING ALONG
FAIRFIELD RD - THEREFORE ALL <600mm)
14750 - 14200 (PROPOSED) = 550mm
14750 - 14171 (EXISTING) = 579mm

LDG (LANDING AT PRIMARY ENTRANCE)
14750 - 14114 (PROPOSED) = 636mm
14750 - 14040 (EXISTING) = 710mm

RMP (INTERMEDIARY RAMP LANDING)
14507 - 14114 (PROPOSED) = 393mm
14507 - 14280 (EXISTING) = 227mm



C:\Users\jacob\Documents\2024\1733 Fairfield Road\1733 Fairfield Road.dwg 26/03/25

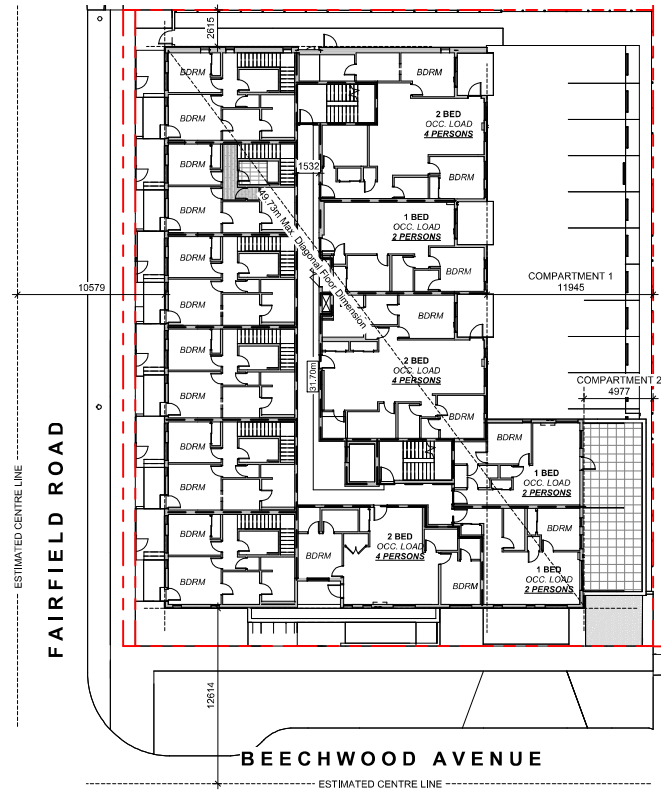


OCCUPANT LOAD:
Occupancy Group C
12 Bedrooms x 2 persons/bedroom
= 24 persons

MINIMUM EXIT WIDTH:
Ramps, Corridors, Passageways
the greater of 6.1mm x 24 = 146.4mm
or 1100mm (minimum 1500mm for
accessible path of travel)

Stairs
the greater of 8mm x 24 = 192.0mm
or 1100mm

1 L1 CODE REVIEW KEY PLAN
SCALE = 1 : 250

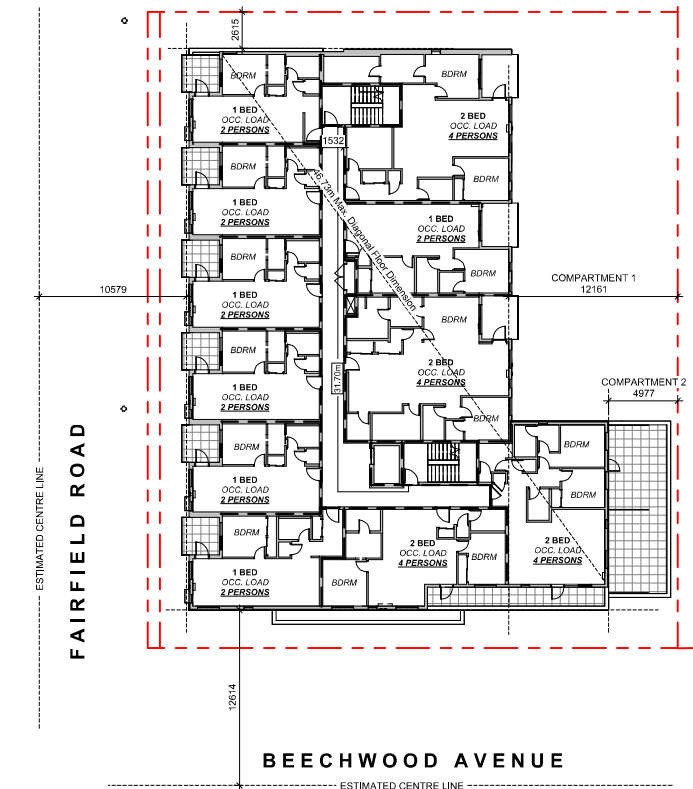


OCCUPANT LOAD:
Occupancy Group C
9 Bedrooms x 2 persons/bedroom
= 18 persons

MINIMUM EXIT WIDTH:
Ramps, Corridors, Passageways
the greater of 5.1mm x 18 = 105.8mm
or 1100mm (minimum 1500mm for
accessible path of travel)

Stairs
the greater of 8mm x 18 = 144.0mm
or 1100mm

2 L2 CODE REVIEW KEY PLAN
SCALE = 1 : 250

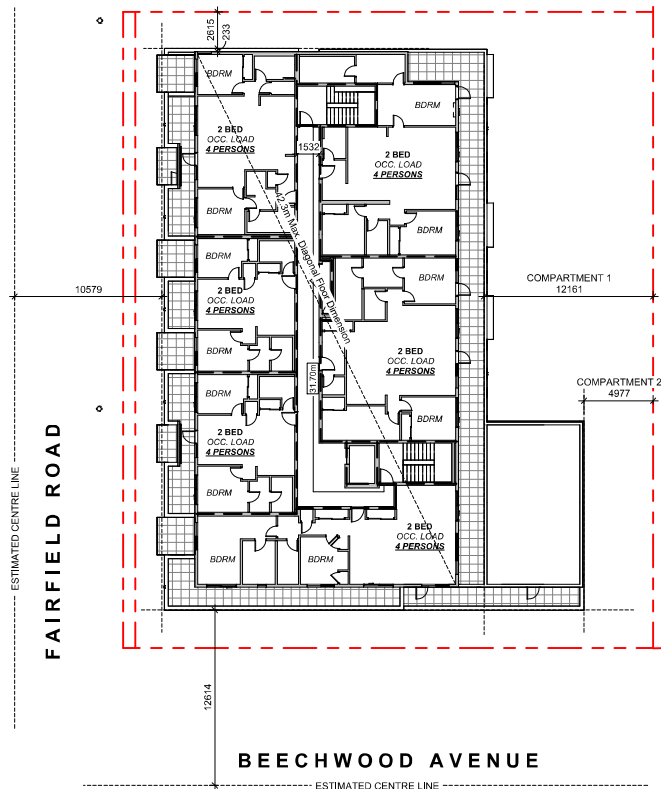


OCCUPANT LOAD:
Occupancy Group C
15 Bedrooms x 2 persons/bedroom
= 30 persons

MINIMUM EXIT WIDTH:
Ramps, Corridors, Passageways
the greater of 6.1mm x 30 = 183mm
or 1100mm (minimum 1500mm for
accessible path of travel)

Stairs
the greater of 8mm x 30 = 240mm
or 1100mm

3 L3 CODE REVIEW KEY PLAN
SCALE = 1 : 250



OCCUPANT LOAD:
Occupancy Group C
12 Bedrooms x 2 persons/bedroom
= 24 persons

MINIMUM EXIT WIDTH:
Ramps, Corridors, Passageways
the greater of 6.1mm x 24 = 146.4mm
or 1100mm (minimum 1500mm for
accessible path of travel)

Stairs
the greater of 8mm x 24 = 192.0mm
or 1100mm

4 L4 CODE REVIEW KEY PLAN
SCALE = 1 : 250

BUILDING CODE ANALYSIS

PROJECT TYPE	<input checked="" type="checkbox"/> NEW CONSTRUCTION <input type="checkbox"/> RENOVATION <input type="checkbox"/> ADDITION	
GOVERNING BUILDING CODE	2018 BC BUILDING CODE <input checked="" type="checkbox"/> PART 3 <input type="checkbox"/> PART 9	
MAJOR OCCUPANCY	A1 A2 A3 A4 B1 B2 B3 C D E F1 F2 F3	3.1.2.1
BUILDING AREA	1,004 m ² (approx)	1.4.1.2
GRADE	18.75 m geodetic	1.4.1.2
BUILDING HEIGHT (STOREYS)	4 STOREYS ABOVE GRADE 0 STOREYS BELOW GRADE	1.4.1.2

BUILDING FIRE SAFETY & CONSTRUCTION CLASSIFICATION

CLASSIFICATION	GROUP C, UP TO 4 STOREYS, SPRINKLERED	3.2.2.51
MAXIMUM BUILDING AREA	1800 m ²	3.2.2.51
No. OF STREETS FACING	1 (BEECHWOOD AVE UNDER 9M MINIMUM STREET WIDTH)	3.2.2.10
CONSTRUCTION TYPES PERMITTED	<input checked="" type="checkbox"/> COMBUSTIBLE <input type="checkbox"/> NON-COMBUSTIBLE	3.2.2.51
FIRE ALARM SYSTEM	YES NO	3.2.4.1
STANDPIPE SYSTEM	YES NO	3.2.5.8
HIGH BUILDING	YES NO	3.2.6.1
INTERCONNECTED FLOOR SPACE	YES NO EXCEPT WITHIN AT GRADE RESIDENTIAL UNITS	3.2.8

FIRE RESISTANCE RATINGS

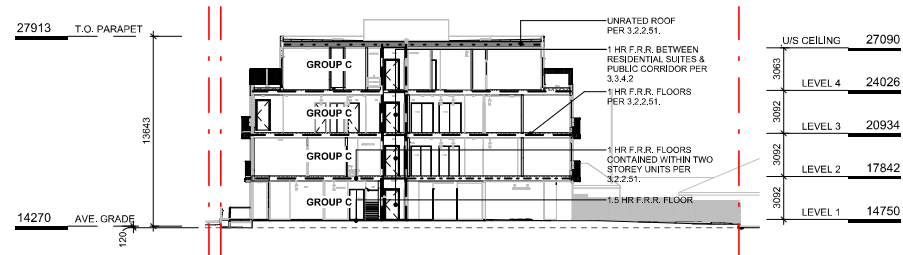
HORIZONTAL SEPARATIONS	1 hr FLOORS 1 hr MEZZANINE Unrated ROOF	3.2.2.51
LOADBEARING WALLS, COLUMNS & ARCHES	1 hr (NOT LESS THAN THAT REQUIRED FOR SUPPORTED STRUCTURE)	3.2.2.51
EXITS	1 hr	3.4.4.1

EXITS FROM FLOOR AREAS

NUMBER OF EXITS REQUIRED	2	3.4.2.1
SEPARATION OF EXITS (MIN.)	1/2 MAX. DIAGONAL FLOOR AREA, BUT NEED NOT BE > 9 m	3.4.2.3
MAX. TRAVEL DISTANCE ALLOWED	45 m, GROUP C	3.4.2.5
MEZZANINE	YES NO	3.2.8

FIRE RESISTANCE RATING - KEY

--- UNRATED FIRE SEPARATIONS
--- 45 MIN
--- 1 HOUR
--- 1.5 HOUR
--- 2 HOUR



5 CODE REVIEW KEY SECTION
SCALE = 1 : 250

4	DPR2 Rev2	Jan 26, 2023
2	DPR2 Submission	April 29, 2022



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Project	1733-1737 Fairfield Rd
	ARYZE

Sheet Name	CODE REVIEW
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Date	JUNE 25, 2024
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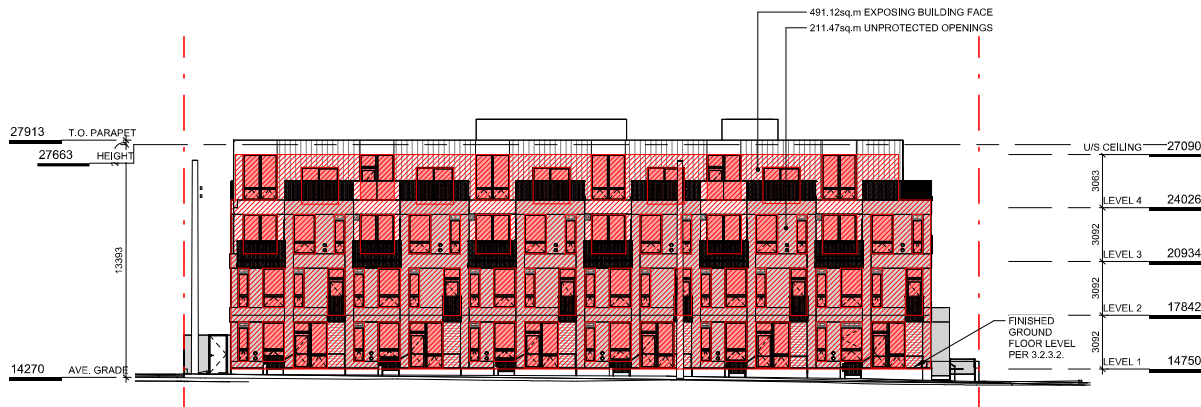
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Revision	4
Jan 26, 2023	

Sheet #	A-052
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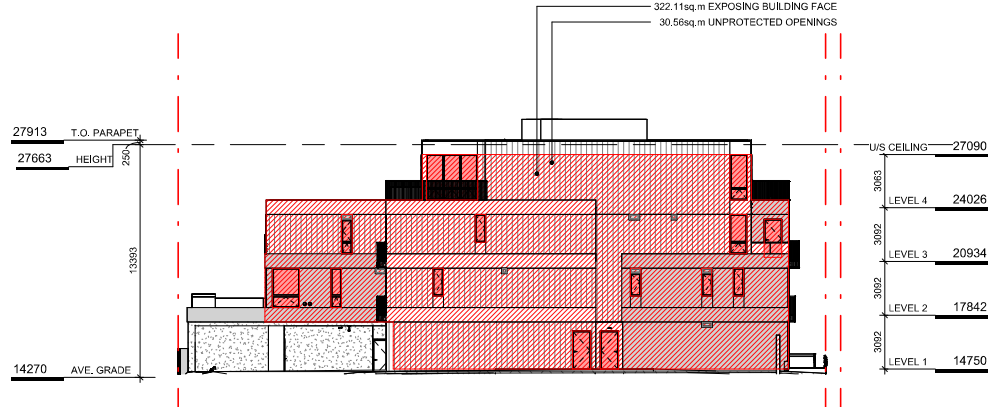


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NW ELEVATION

TABLE 3.2.3.1 - D	LIMITING DISTANCE (m)	ALLOWABLE OPENINGS (%)	AREA OF EXPOSING BUILDING FACE (sq.m)	PROPOSED OPENINGS (sq.m)	PROPOSED OPENINGS (%)
	10.60m (>9m)	100	491.12	211.47	43.1
TABLE 3.2.3.7	LIMITING DISTANCE (m)	ALLOWABLE OPENINGS (%)	REQUIRED FRR	REQUIRED TYPE OF CONSTRUCTION	REQUIRED TYPE OF CLADDING
	10.12m (>9m)	100	45 minutes	Any	Any

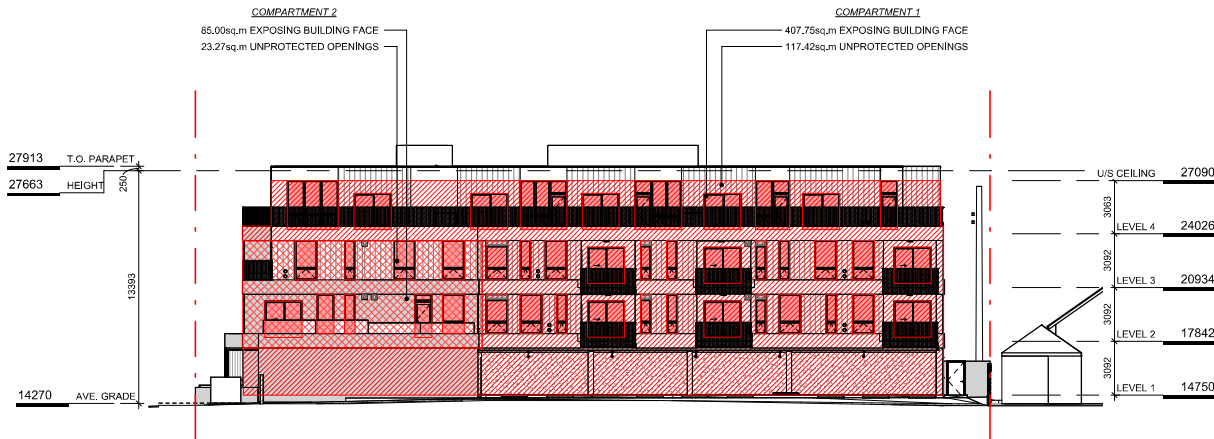


NE ELEVATION

TABLE 3.2.3.1 - D	LIMITING DISTANCE (m)	ALLOWABLE OPENINGS (%)	AREA OF EXPOSING BUILDING FACE (sq.m)	PROPOSED OPENINGS (sq.m)	PROPOSED OPENINGS (%)
	2.62m (>2m)	16	322.11	30.56	9.4
TABLE 3.2.3.7	OCCUPANCY CLASSIFICATION	ALLOWABLE OPENINGS (%)	REQUIRED FRR	REQUIRED TYPE OF CONSTRUCTION	REQUIRED TYPE OF CLADDING
	GROUP C	10 - 25	1 hr	Any	Noncombustible

1 NW ELEVATION - LD KEY
SCALE = 1 : 200

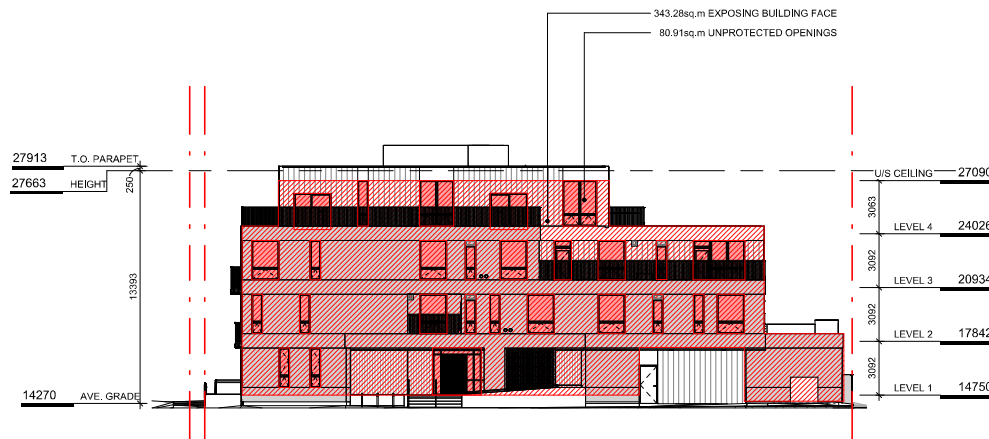
2 NE ELEVATION - LD KEY
SCALE = 1 : 200



SE ELEVATION

TABLE 3.2.3.1 - D COMPARTMENT 1	LIMITING DISTANCE (m)	ALLOWABLE OPENINGS (%)	AREA OF EXPOSING BUILDING FACE (sq.m)	PROPOSED OPENINGS (sq.m)	PROPOSED OPENINGS (%)
	11.95m (>9m)	100	407.75	117.42	28.8
TABLE 3.2.3.7 COMPARTMENT 1	OCCUPANCY CLASSIFICATION	ALLOWABLE OPENINGS (%)	REQUIRED FRR	REQUIRED TYPE OF CONSTRUCTION	REQUIRED TYPE OF CLADDING
	GROUP C	50 - 100	45 minutes	Any	Any
TABLE 3.2.3.1 - D COMPARTMENT 2	LIMITING DISTANCE (m)	ALLOWABLE OPENINGS (%)	AREA OF EXPOSING BUILDING FACE (sq.m)	PROPOSED OPENINGS (sq.m)	PROPOSED OPENINGS (%)
	4.98m (>4m)	56	85.00	23.27	27.4
TABLE 3.2.3.7 COMPARTMENT 2	OCCUPANCY CLASSIFICATION	ALLOWABLE OPENINGS (%)	REQUIRED FRR	REQUIRED TYPE OF CONSTRUCTION	REQUIRED TYPE OF CLADDING
	GROUP C	50 - 100	45 minutes	Any	Any

4 SE ELEVATION - LD KEY
SCALE = 1 : 200



SW ELEVATION

TABLE 3.2.3.1 - D	LIMITING DISTANCE (m)	ALLOWABLE OPENINGS (%)	AREA OF EXPOSING BUILDING FACE (sq.m)	PROPOSED OPENINGS (sq.m)	PROPOSED OPENINGS (%)
	12.61m (>9m)	100	343.28	80.91	23.6
TABLE 3.2.3.7	OCCUPANCY CLASSIFICATION	ALLOWABLE OPENINGS (%)	REQUIRED FRR	REQUIRED TYPE OF CONSTRUCTION	REQUIRED TYPE OF CLADDING
	GROUP C	100	45 minutes	Any	Any

3 SW ELEVATION - LD KEY
SCALE = 1 : 200

5	DP/RZ Rev3	Aug 10, 2023
4	DP/RZ Rev2	Jun 26, 2023
3	DP/RZ Rev1	July 20, 2022
2	DP/RZ Submission	April 29, 2022



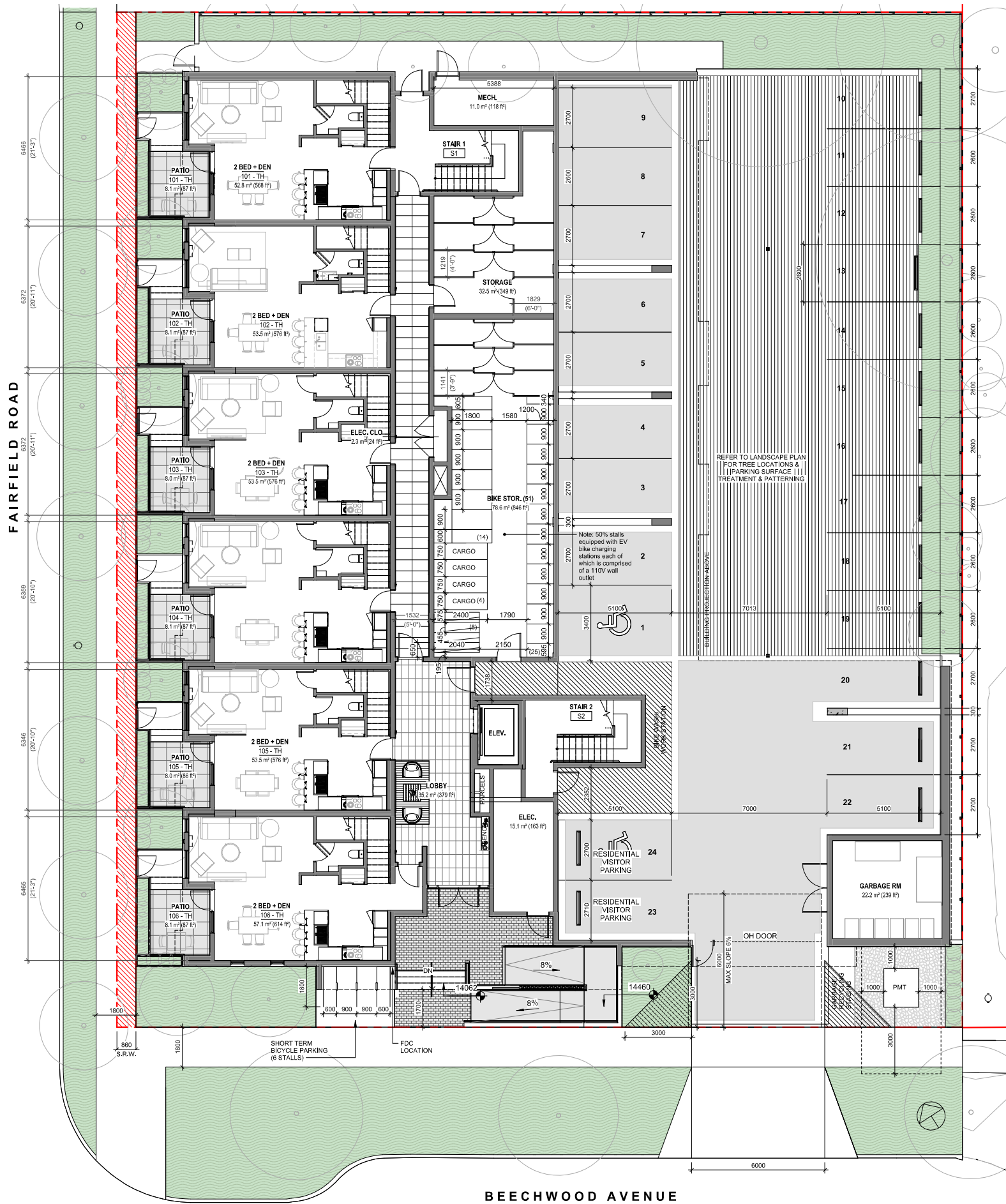
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Project		1733-1737 Fairfield Rd	
		ARYZE	
Sheet Name SPATIAL SEPARATION			
Date JUNE 25, 2024			
Scale 1 : 200	Project # 2123	Revision Aug 10, 2023	
5		Sheet # A-053	



2024-06-25 11:37:02 AM



1 L1 PROPOSED PLAN
SCALE = 1 : 100

19	DPRZ Rev6	May 27, 2024
17	DPRZ Rev5	Feb 21, 2024
13	DPRZ Rev4	Jan 18, 2024
5	DPRZ Rev3	Aug 10, 2023
4	DPRZ Rev2	Jan 26, 2023
3	DPRZ Rev1	July 20, 2022
2	DPRZ Submission	April 29, 2022
1	Development Tracker Application	March 7, 2022
NO.	DESCRIPTION	DATE

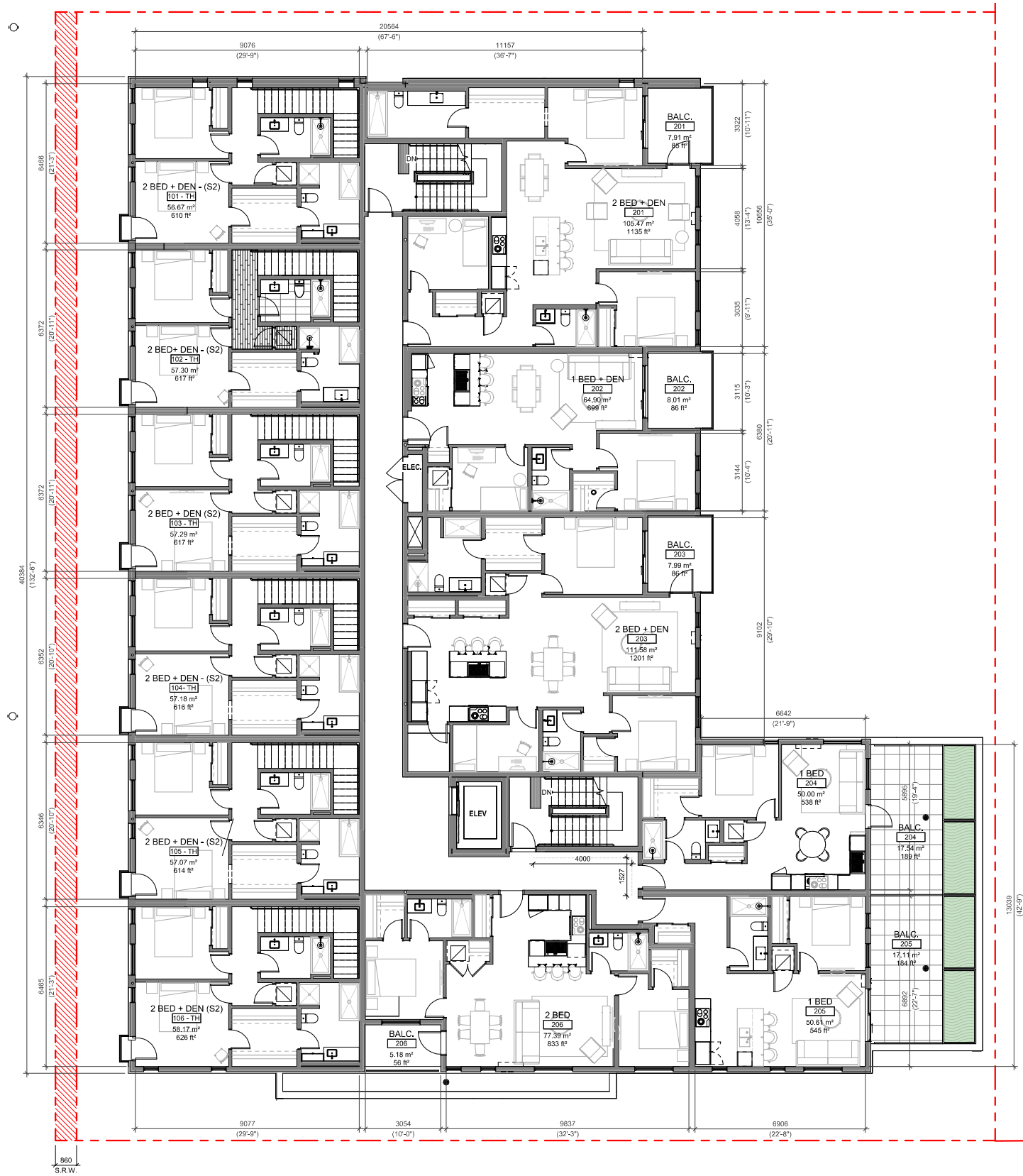


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Project	1733-1737 Fairfield Rd	ARYZE
Sheet Name	LEVEL 1 PROPOSED PLAN	
Date	JUNE 25, 2024	
Scale	1 : 100	Project # 2123
Revision	May 27, 2024	19
Sheet #	A-101	





1 L2 PROPOSED PLAN
SCALE = 1 : 100

19	DPRZ Rev6	May 27, 2024
13	DPRZ Rev4	Jan 18, 2024
5	DPRZ Rev3	Aug 10, 2023
4	DPRZ Rev2	Jan 26, 2023
3	DPRZ Rev1	July 20, 2022
2	DPRZ Submission	April 29, 2022
1	Development Tracker Application	March 7, 2022
NO.	DESCRIPTION	DATE

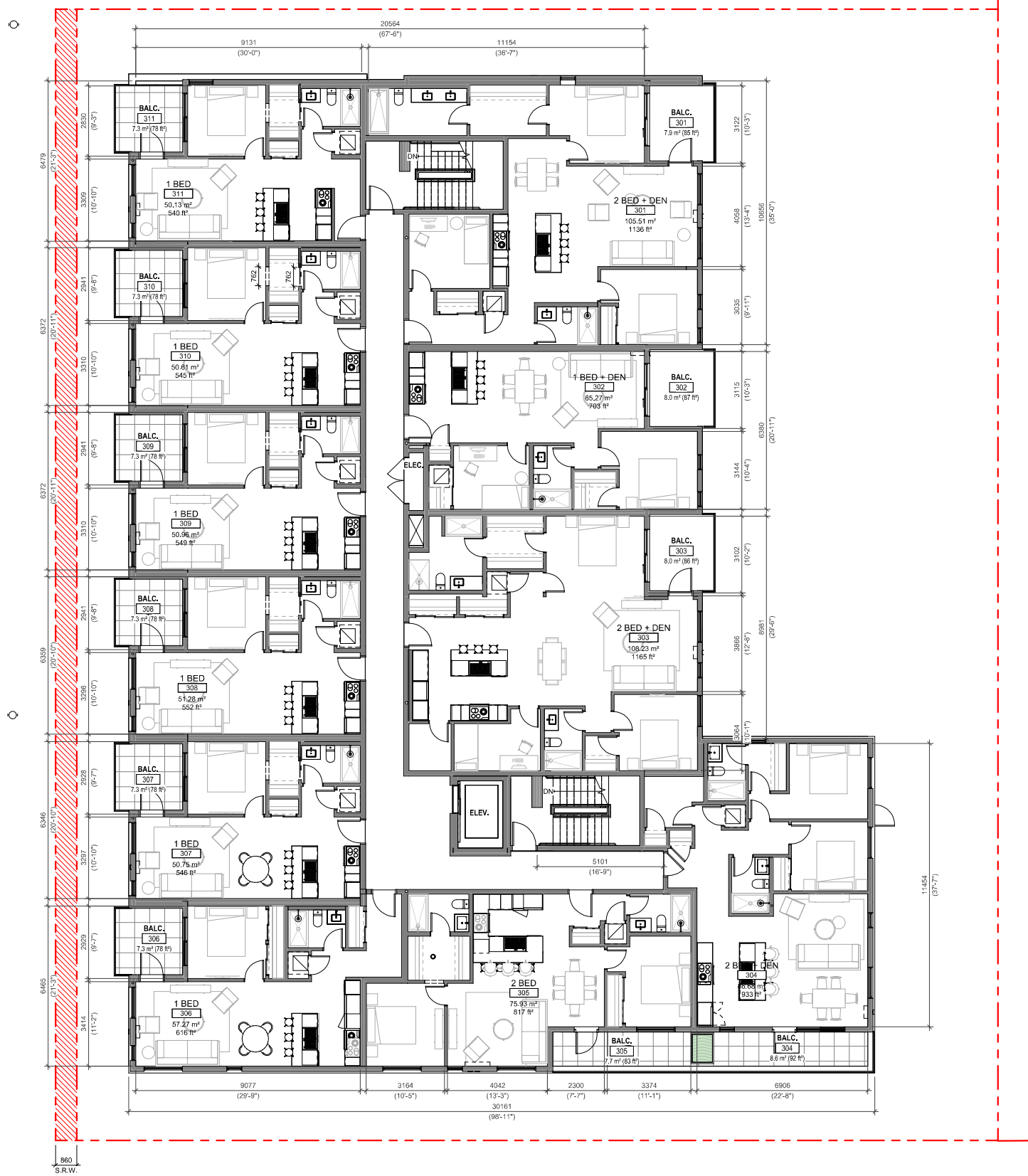


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Project	1733-1737 Fairfield Rd	
	ARYZE	
Sheet Name	LEVEL 2 PROPOSED PLAN	
Date	JUNE 25, 2024	
Scale	1 : 100	Project # 2123
	Revision	19
	May 27, 2024	
Sheet #	A-102	

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19	DPRZ Rev6	May 27, 2024
13	DPRZ Rev4	Jan 18, 2024
4	DPRZ Rev2	Jan 26, 2023
3	DPRZ Rev1	July 20, 2022
2	DPRZ Submission	April 29, 2022
1	Development Tracker Application	March 7, 2022
NO.	DESCRIPTION	DATE



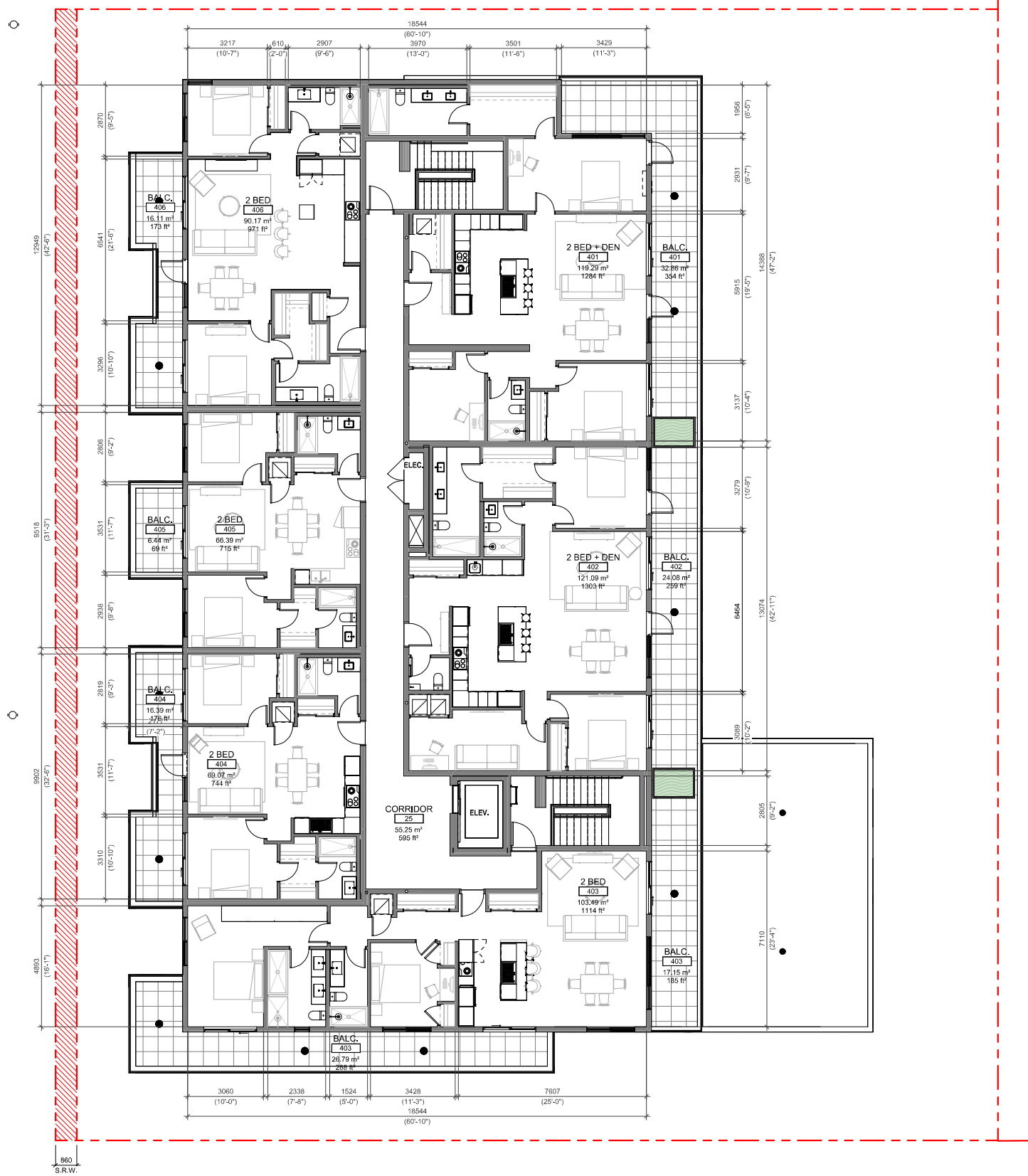
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Project	1733-1737 Fairfield Rd	
	ARYZE	
Sheet Name	LEVEL 3 PROPOSED PLAN	
Date	JUNE 25, 2024	
Scale	1 : 100	Project # 2123
	Revision	May 27, 2024 19
	Sheet #	A-103



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1 L4 PROPOSED PLAN
SCALE = 1 : 100

19	DPRZ Rev6	May 27, 2024
13	DPRZ Rev4	Jan 18, 2024
4	DPRZ Rev2	Jan 26, 2023
3	DPRZ Rev1	July 23, 2022
2	DPRZ Submission	April 29, 2022
1	Development Tracker Application	March 7, 2022
NO.	DESCRIPTION	DATE



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Project	1733-1737 Fairfield Rd	
	ARYZE	
Sheet Name	LEVEL 4 PROPOSED PLAN	
Date	JUNE 25, 2024	
Scale	1 : 100	Project # 2123
	Revision	19
	May 27, 2024	
Sheet #	A-104	

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1 NW ELEVATION
SCALE = 1 : 100



2 SW ELEVATION
SCALE = 1 : 100

FINISH SCHEDULE

01. 'CERACLAD' CLADDING
LIGHT FINISH
02. PREFINISHED METAL
FLAT PANEL
03. PREFINISHED METAL
STANDING SEAM
04. METAL PICKET GUARD
05. GLAZING
06. WOOD GRAIN FINISH
07. HORIZONTAL WOOD FENCE

19	DPRZ Rev6	May 27, 2024
17	DPRZ Rev5	Feb 21, 2024
13	DPRZ Rev4	Jan 18, 2024
5	DPRZ Rev3	Aug 10, 2023
4	DPRZ Rev2	Jan 26, 2023
3	DPRZ Rev1	July 20, 2022
2	DPRZ Submission	April 29, 2022
1	Development Tracker Application	March 7, 2022
NO.	DESCRIPTION	DATE



CASCADIA ARCHITECTS INC

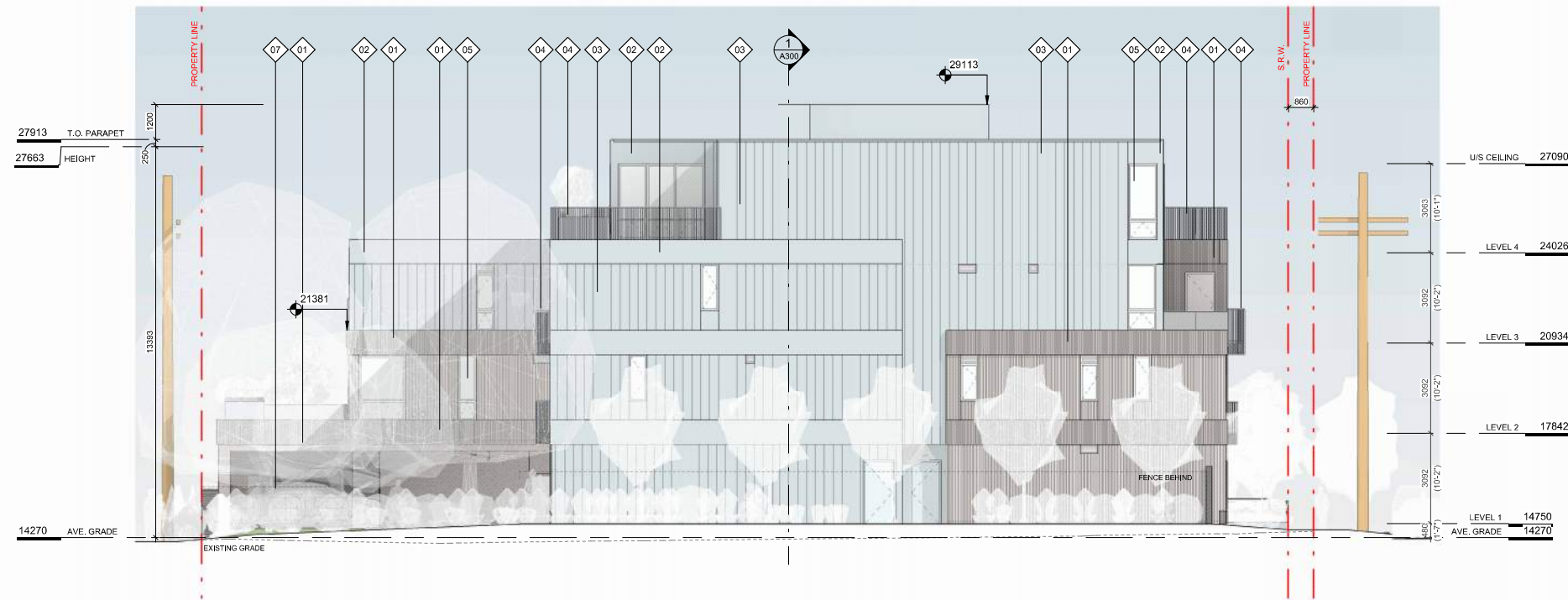
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Project		1733-1737 Fairfield Rd
		ARYZE
Sheet Name		EXTERIOR ELEVATIONS
Date		JUNE 25, 2024
Scale	As indicated	Project # 2123
		Revision May 27, 2024 19
		Sheet # A-200

2024-06-25 11:39:08 AM



1 SE ELEVATION
SCALE = 1 : 100



2 NE ELEVATION
SCALE = 1 : 100

FINISH SCHEDULE

01. 'CERACLAD' CLADDING
LIGHT FINISH
02. PREFINISHED METAL
FLAT PANEL
03. PREFINISHED METAL
STANDING SEAM
04. METAL PICKET GUARD
05. GLAZING
06. WOOD GRAIN FINISH
07. HORIZONTAL WOOD FENCE

NO.	DESCRIPTION	DATE
19	DP/RZ Rev6	May 27, 2024
17	DP/RZ Rev5	Feb 21, 2024
13	DP/RZ Rev4	Jan 18, 2024
5	DP/RZ Rev3	Aug 10, 2023
4	DP/RZ Rev2	Jan 26, 2023
3	DP/RZ Rev1	July 20, 2022
2	DP/RZ Submission	April 29, 2022
1	Development Tracker Application	March 7, 2022




CASCADIA ARCHITECTS INC

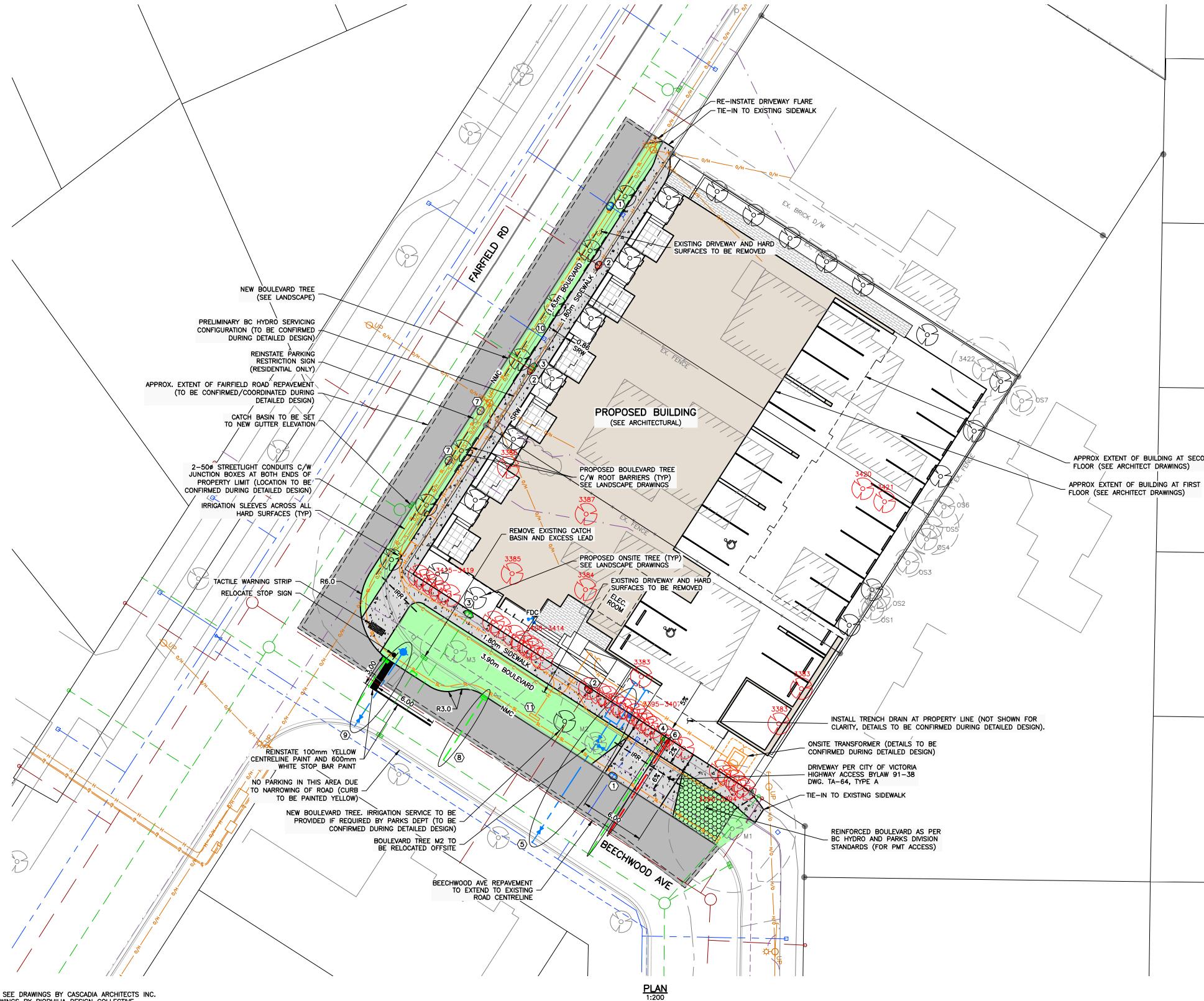
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Project	1733-1737 Fairfield Rd
	ARYZE
Sheet Name	EXTERIOR ELEVATIONS
Date	JUNE 25, 2024
Scale	As indicated
Project #	2123
Revision	May 27, 2024
Sheet #	A-201

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Project 1733-1737 Fairfield Rd		ARYZE	
Sheet Name BUILDING SECTIONS			
Date JUNE 25, 2024			
Scale 1 : 100	Project # 2123		
Revision Jan 18, 2024			
Sheet # A-301			



SHEET NOTES	
No.	DESCRIPTION
1	EXISTING HYDRO SERVICE TO BE CAPPED AND ABANDONED BY CoV FORCES AT DEVELOPER'S EXPENSE. HYDRO POLE TO BE SUPPORTED DURING CONSTRUCTION.
2	EXISTING SANITARY SEWER SERVICE TO BE CAPPED AND ABANDONED AT PROPERTY LINE.
3	EXISTING STORM SEWER SERVICE TO BE CAPPED AND ABANDONED AT PROPERTY LINE.
4	NEW 150# STORM SERVICE CONNECTION c/w INSPECTION CHAMBER TO PROPERTY LINE BY CoV FORCES AT DEVELOPER'S EXPENSE.
5	NEW 150# FIRE SERVICE c/w METER CHAMBER AS PER CoV STD. DWG. SD W26 AND 50# DOMESTIC WATER SERVICE BY CoV FORCES AT DEVELOPER'S EXPENSE.
6	NEW 150# SANITARY SERVICE c/w INSPECTION CHAMBER TO PROPERTY LINE BY CoV FORCES AT DEVELOPER'S EXPENSE.
7	EXISTING GAS SERVICE TO BE CAPPED AND ABANDONED BY FORTISBC FORCES.
8	NEW CATCH BASIN PER CoV STD. DWG. SD S11a BY DEVELOPER'S CONTRACTOR. ENSURE OUTLET ELEVATION IS SET TO ACHIEVE MIN. 2% GRADE TO MAIN. 150# PVC CATCH BASIN LEAD AND CONNECTION BY CoV FORCES AT DEVELOPER'S EXPENSE.
9	NEW FIRE HYDRANT BY CoV FORCES AT DEVELOPER'S EXPENSE. LOCATION TO BE CONFIRMED DURING DETAILED DESIGN.
10	EXISTING WATER SERVICE TO BE RE-USED FOR IRRIGATION. BACKFLOW PREVENTOR TO BE INSTALLED.
11	NEW DUAL HEAD ELECTRIC VEHICLE CHARGER (LOCATION TO BE CONFIRMED DURING DETAILED DESIGN).
	EXISTING TREE TO BE REMOVED.
	EXISTING TREE TO REMAIN.

JULY 3, 2024
**ISSUED FOR
REZONING**

PERMIT TO PRACTICE
McElhanney Ltd.
PERMIT NUMBER: 1003299
Engineers and Geoscientists of BC

SEE ARCHITECTURAL AND LANDSCAPE
DRAWINGS FOR ADDITIONAL INFORMATION

LEGAL PLAN & TOPOGRAPHIC
SURVEY PROVIDED BY J.E.
ANDERSON & ASSOCIATES



- NOTES:**
- FOR BUILDING INFORMATION, SEE DRAWINGS BY CASCADIA ARCHITECTS INC.
 - FOR LANDSCAPING, SEE DRAWINGS BY BIOPHILIA DESIGN COLLECTIVE.
 - FOR LEGAL INFORMATION, SEE DRAWINGS BY J.E. ANDERSON & ASSOCIATES.
 - UTILITY SIZES AND LOCATIONS TO BE CONFIRMED DURING DETAILED DESIGN.
 - ALL EXISTING ON-SITE BUILDINGS, DRIVEWAYS, FENCES, HEDGES, RETAINING WALLS, ETC. TO BE REMOVED AND DISPOSED OFF-SITE (NOT SHOWN FOR CLARITY).
 - BASE INFORMATION SHOWN IS PRODUCED FROM A COMBINATION OF FIELD SURVEY AND GIS INFORMATION PROVIDED BY THE CITY OF VICTORIA (VICTORIA). ALL INFORMATION TO BE CONFIRMED IN THE FIELD PRIOR TO CONSTRUCTION (INCLUDING COMPLETING A BCONECALL).
 - ONSITE BIKE PARKING, ETC. NOT SHOWN FOR CLARITY (SEE ARCHITECT AND LANDSCAPE DRAWINGS).

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NO.	DATE	BY	ISSUED	NO.	DATE	BY	REVISIONS
4	2024-07-03	ND	ISSUED FOR REZONING				
3	2023-08-21	ND	ISSUED FOR REZONING				
2	2023-01-25	ND	ISSUED FOR REZONING				
1	2022-04-27	ND	ISSUED FOR DEVELOPMENT PERMIT				


**McElhanney**

500 - 3960 QUADRA STREET
VICTORIA, BC V8X 4A3

PH (250) 370-9221



PROJECT: 1733 FAIRFIELD ROAD, VICTORIA, BC	SCALE HORIZ: 1:200 PROJECT NO. 21-187	VER:1 ISSUED/REVISION 4
TITLE: PRELIMINARY CIVIL PLAN	APPROVING AUTHORITY FILE NO.	DRAWING NO. 21-187-CSP



Revisions

Received Date:

July 22, 2024

Deemed Date:

July 4, 2024

1733 FAIRFIELD

1733 FAIRFIELD ROAD, VICTORIA, BC

SHEET LIST

L0.1 TREE MANAGEMENT & REMOVAL PLAN

L1.1 MATERIALS PLAN - LEVEL 1

L1.2 MATERIALS PLAN - UPPER LEVELS

L2.1 LAYOUT PLAN

L3.1 GRADING PLAN

L4.1 SOIL VOLUME PLAN

L4.2 TREE PLANTING PLAN

L4.3 PLANTING PLAN - LEVEL 1

L4.4 PLANTING PLAN - UPPER LEVELS

L5.1 LIGHTING PLAN

L6.1 IRRIGATION PLAN - LEVEL 1

L6.2 IRRIGATION PLAN - UPPER LEVELS

L7.1 PAVING DETAILS

L7.2 SITE FURNISHINGS DETAILS

L7.3 PLANTING DETAILS

GENERAL LANDSCAPE NOTES

- GENERAL:
- DO NOT PROCEED IN UNCERTAINTY.
 - DO NOT SCALE DRAWINGS.
 - DRAWINGS AND SPECIFICATIONS ARE TO BE READ IN CONJUNCTION WITH ALL OTHER DRAWINGS/SPECIFICATIONS IN THIS PROJECT SET. ANY DISCREPANCIES AMONG DRAWINGS, SPECIFICATIONS AND INDUSTRY BEST PRACTICES TO BE REPORTED TO THE PROJECT / CONSTRUCTION MANAGER AND THE LANDSCAPE CONSULTANT FOR DIRECTION.
 - ALL LANDSCAPE SPECIFICATION SECTIONS AND DRAWINGS ARE AFFECTED BY REQUIREMENTS OF DIVISION 01 SECTIONS (PROVIDED IN THE PROJECT MANUAL).
 - CONTRACTOR TO FAMILIARIZE THEMSELVES WITH ALL SITE CONDITIONS, INCLUDING THE LIMITS OF WORK AND EXISTING FEATURES TO BE PROTECTED, PRIOR TO SUBMITTING BIDS/QUOTES.
 - CONTRACTOR TO CONFIRM LOCATION AND ELEVATION OF ALL UNDERGROUND UTILITIES/FEATURES PRIOR TO COMMENCING WORK.
 - CONTRACTOR TO TAKE NECESSARY PRECAUTIONS TO PROTECT SITE FEATURES / CONDITIONS, WITHIN AND BEYOND THE LIMITS OF WORK EXISTING AT THE TIME OF CONSTRUCTION. ALL DISTURBED SURFACES, AREAS, STRUCTURES, VEGETATION, HABITAT ETC. ON PUBLIC / PRIVATE PROPERTY TO PROMPTLY BE RESTORED TO EQUAL OR BETTER CONDITION THAN EXISTING AND TO THE SATISFACTION OF THE MUNICIPALITY HAVING JURISDICTION / PROPERTY OWNER.
 - CONTRACTOR TO MAINTAIN THE SITE IN A SAFE AND TIDY CONDITION AT ALL TIMES. DO NOT OBSTRUCT PEDESTRIAN OR VEHICULAR CIRCULATION. DO NOT LEAVE UNPROTECTED HOLES / PITS / OPENINGS OVERNIGHT. ALL EXCESS MATERIALS AND REFUSE TO BE REMOVED FROM THE SITE DAILY UNLESS OTHERWISE DIRECTED BY THE CONSTRUCTION MANAGER.

- STANDARDS:
- ALL WORK ON MUNICIPAL PROPERTY TO CONFORM TO THE CITY OF VICTORIA DEVELOPMENT STANDARDS (AUTHORITY HAVING JURISDICTION) INCLUDING BUT NOT LIMITED TO:
 - CITY OF VICTORIA SUPPLEMENTARY SPECIFICATIONS SCHEDULE 'B' TO THE SUBDIVISION AND DEVELOPMENT SERVICING BYLAW NO. 12-042
 - CITY OF VICTORIA SUPPLEMENTAL DRAWINGS
 - CITY OF VICTORIA SUPPLEMENTARY SPECIFICATIONS FOR STREET TREES AND IRRIGATION, SCHEDULE C ALL LANDSCAPE WORK ON THE DEVELOPMENT SITE TO CONFORM TO THE CURRENT EDITION OF THE *Canadian Landscape Standard* (CLS), UNLESS SPECIFICALLY STATED OTHERWISE IN WRITTEN SPECIFICATIONS AND ON DRAWINGS.
 - ALL HARD SURFACE (INCLUDING BUT NOT LIMITED TO PAVING, CONCRETE RETAINING WALLS AND CONCRETE PLANTERS) TO CONFORM TO THE *Master Municipal Construction Documents Association PLATINUM EDITION* (MMCD) AND MMCD SUPPLEMENTAL DRAWINGS AND SPECIFICATIONS, UNLESS SPECIFICALLY STATED OTHERWISE IN WRITTEN SPECIFICATIONS AND ON DRAWINGS.
 - IN CASES OF CONFLICT BETWEEN THE CLS AND THE MMCD, THE MORE STRINGENT REQUIREMENT WILL TAKE PRECEDENCE.
 - LANDSCAPE CONTRACTOR TO BE FAMILIAR WITH MUNICIPAL DEVELOPMENT STANDARDS AND BE IN POSSESSION OF THE CLS AND MMCD MANUALS AND SUPPLEMENTAL DRAWINGS AND SPECIFICATIONS.
 - GROWING MEDIUM AND GROWING MEDIUM TESTING TO CLS Section 6.
 - IRRIGATION DESIGN AND INSTALLATION TO IAABC STANDARDS AND CLS Section 5.

- COORDINATION:
- CONTRACTOR TO COORDINATE INSTALLATION OF IRRIGATION AND ELECTRICAL SLEEVES WITH ARCHITECTURAL, MECHANICAL, ELECTRICAL AND CIVIL WORKS.

- SUBMITTALS:
- CONTRACTOR TO PROVIDE SAMPLES, TEST RESULTS AND SHOP DRAWINGS TO LANDSCAPE CONSULTANT FOR REVIEW AND APPROVAL 45 DAYS PRIOR TO INSTALLATION.
 - SEE ALSO SUBMITTALS TABLE BELOW FOR ADDITIONAL INFORMATION.

- GROWING MEDIUM TESTING:
- GROWING MEDIUM TEST RESULTS ARE MANDATORY.
 - TEST RESULTS TO INCLUDE ANALYSIS OF ALL GROWING MEDIUM NUTRIENTS NOTED IN CLS 6.2.7 AND ARE TO BE REPORTED IN THE SAME UNITS AS NOTED IN THAT SECTION.
 - TEST RESULTS TO INCLUDE ANALYSIS OF SOIL TEXTURE, ORGANIC CONTENT AND ACIDITY AS PER CLS 6.3.5 AND BE REPORTED IN THE SAME UNITS AS NOTED IN THAT SECTION.
 - TEST RESULTS TO INCLUDE RECOMMENDATIONS FOR AMENDMENTS TO MEET THE REQUIREMENTS FOR EACH GROWING MEDIUM TYPE.
 - RECOMMENDED GROWING MEDIUM TESTING FACILITY: Pacific Soil Analysis Inc, 11720 Voyageur Way, Richmond, BC V6X 3G9.

- COMPACTION TESTING:
- COMPACTION TESTS FOR HARD SURFACE SUBGRADE, GRANULAR SUB-BASE, AND GRANULAR BASE ARE MANDATORY.

- INSPECTIONS:
- CONTRACTOR TO ALERT THE LANDSCAPE CONSULTANT A MINIMUM OF 3 WORKING DAYS PRIOR TO REQUIRED LANDSCAPE INSPECTIONS.

- SUBSTITUTIONS:
- REQUESTS FOR SUBSTITUTIONS TO CONFORM TO THE DIVISION 01 SECTION AND BE SUBMITTED TO THE LANDSCAPE CONSULTANT, THROUGH THE PROJECT ADMINISTRATOR, A MINIMUM OF 45 DAYS PRIOR TO SCHEDULED WORK.
 - PLEASE NOTE THAT SOME SUBSTITUTIONS MAY REQUIRE MUNICIPAL APPROVAL.

- WARRANTY:
- CONTRACTOR SHALL WARRANTY ALL WORKMANSHIP AND MATERIALS FOR 1 FULL YEAR FOLLOWING THE DATE OF TOTAL PERFORMANCE AS PER MMCD UNLESS SPECIFICALLY NOTED OTHERWISE. FAULTY MATERIALS AND WORKMANSHIP SHALL BE PROMPTLY REPAIRED / REPLACED TO THE SATISFACTION OF THE LANDSCAPE CONSULTANT.

- ENVIRONMENTAL PROTECTION:
- CONTRACTOR TO INSTALL AND MAINTAIN SEDIMENTATION FILTRATION MEASURES AS REQUIRED FOR LANDSCAPE WORKS TO PREVENT MATERIALS FROM LEAVING THE SITE AND / OR ENTERING STORM DRAINS; STOCKPILED LANDSCAPE MATERIALS ARE TO BE KEPT TARPED.

ELECTRICAL - DESIGN BUILD

- LANDSCAPE LIGHTING:
- IT IS THE INTENTION OF THE PROJECT THAT THE LANDSCAPE CONTRACTOR FURNISH A FUNCTIONAL LIGHTING SYSTEM AS DESCRIBED ON THE DRAWINGS AND IN THIS SECTION.
 - THE SCOPE OF WORK INCLUDES SUPPLY AND INSTALLATION OF LOW VOLTAGE LIGHTING PRODUCTS, CABLING, TRANSFORMER(S) / POWER SUPPLIES / DRIVERS AND ELECTRICAL SLEEVES.
 - ELECTRICAL CONTRACTOR IS TO INCLUDE POWER (CONDUIT AND CONDUCTORS) TO LOW VOLTAGE TRANSFORMERS/DRIVERS IN SUPPORT OF THE LANDSCAPE LIGHTING PLAN. THE NUMBER OF CONNECTION POINTS IS NOT DETERMINED - THIS DEPENDS ON HOW THE LANDSCAPE CONTRACTOR LAYS OUT THE SYSTEM, BUT WILL NOT EXCEED 5. REFER TO THE LANDSCAPE LIGHTING PLAN FOR GENERAL INFORMATION.
 - 15A 1P CIRCUIT FROM HOUSE PANEL, SWITCHED BY A PROGRAMMABLE TIMER INSTALLED IN THE ELECTRICAL ROOM.
 - TRANSFORMER TO BE ADEQUATELY SIZED FOR THE LOAD PLUS 40% RESERVE CAPACITY. TRANSFORMERS TO BE ARRANGED SUCH THAT NO LOAD REQUIRES WIRING IN EXCESS OF 75'.
 - TIMER FUNCTION ON TRANSFORMER TO BE DISABLED IF PROVIDED.
 - TRANSFORMERS, DRIVERS AND POWER SUPPLIES ARE TO BE INSTALLED IN GROUND JUNCTION BOXES, SUPPLIED AND INSTALLED BY THIS DIVISION.
 - THE LANDSCAPE CONTRACTOR SHALL COORDINATE WITH THE ELECTRICAL CONTRACTOR FOR AC CIRCUIT TO EACH JUNCTION BOX LOCATION AND WITH THE GENERAL CONTRACTOR FOR THE INSTALLATION OF ELECTRICAL SLEEVES.
 - THE USE OF GEL FILLED WIRE NUTS IS REQUIRED.

- IRRIGATION COORDINATION:
- ELECTRICAL CONTRACTOR TO COORDINATE WITH CONSTRUCTION MANAGER AND IRRIGATION DESIGNER/INSTALLER TO PROVIDE ELECTRICAL FOR THE OPERATION OF AN AUTOMATIC IRRIGATION SYSTEM AND CONTROLLER.

MATERIAL SPECIFICATIONS

- ON-SITE GROWING MEDIUM:
- GROWING MEDIUM TESTING, MIXING, HANDLING AND PLACEMENT TO CLS SECTION 6.
 - ON SITE GROWING MEDIUM TYPE TO BE: AS PER DRAWING LEGEND.
 - PLANTER GROWING MEDIUM TO BE:
 - 50% BY VOLUME 1H HIGH TRAFFIC LAWN GROWING MEDIUM
 - 50% BY VOLUME Pro-Mix HP Mychortizae OR APPROVED EQUAL
- OFF-SITE GROWING MEDIUM:
- BOULEVARD GROWING MEDIUM TO BE:
 - MMCD Planting Areas Growing Medium to City of Victoria Supplementary Specifications Schedule 'B', Table 2.
 - AS PER MUNICIPAL STANDARD.
 - IF NO MUNICIPAL STANDARD, GROWING MEDIUM TO CLS SECTION 6 - 2L LOW TRAFFIC LAWN AREAS.

- MULCH:
- MULCH IN ALL OTHER AREAS TO BE: Mitchell Excavating Ltd. Fine Bark Mulch - fir/hemlock OR APPROVED EQUAL.
 - HANDLING AND PLACEMENT TO CLS Section 10 - Mulching/Mulch

- SOD:
- MUNICIPAL SOD TO BE: No. 1 Premium Grade Nursery Turfgrass.
 - HANDLING AND PLACEMENT TO BCLS Section 8 - Turfgrass Sod.
 - SHALL BE SUITED TO THE LOCALITY, SITE CONDITIONS AND INTENDED FUNCTION OF THE PROJECT AREA.

- ROOT BARRIER:
- ROOT BARRIER TO BE: Deeproot 18" Universal Guide OR APPROVED EQUAL.

- GEOTEXTILE:
- FILTER FABRIC TO BE: Soprema Soprafilre non-woven geotextile OR APPROVED EQUAL.
 - INSTALL AS PER MANUFACTURER'S SPECIFICATIONS AND LANDSCAPE DETAILS.

- AGGREGATE:
- PLANTER DRAIN ROCK TO BE: 19 mm MINUS GRANULAR BASE (TO MMCD SPECIFICATION)
 - GREEN ROOF DRAIN ROCK TO BE: 6 mm WASHED DRAIN ROCK

- PLANT MATERIAL AND PLANTING:
- ALL PRODUCTS AND WORKMANSHIP, INCLUDING: PLANT MATERIAL, TRANSPORTATION, HANDLING AND PROTECTION, SCHEDULING AND STORAGE, WATERING AND IRRIGATION, TIME OF PLANTING, PLANTING, CONDITIONS FOR ACCEPTANCE, AND INSECTS PESTS AND DISEASE CONTROL, TO BCLS SECTION 9 - PLANTS AND PLANTING, WRITTEN SPECIFICATIONS AND LANDSCAPE DETAILS.
 - CONDITIONS FOR TOTAL PERFORMANCE TO MMCD SECTION - PLANTING OF TREES, SHRUBS AND GROUNDCOVERS.
 - IN THE EVENT OF A DISCREPANCY BETWEEN QUANTITIES LISTED ON THE PLANTING PLAN AND IN THE PLANT LIST, THE PLANTING PLAN QUANTITIES TO TAKE PRECEDENCE.

- CAST IN PLACE CONCRETE:
- ALL CAST IN PLACE CONCRETE AND ASSOCIATED WORK AND MATERIALS TO CONFORM TO MMCD SPECIFICATIONS; ASSOCIATED WORK INCLUDES GRADING, SUBGRADE COMPACTION, GRANULAR BASE MATERIALS AND COMPACTION.

IRRIGATION - DESIGN BUILD

- IRRIGATION:
- IT IS THE INTENTION OF THE PROJECT THAT THE LANDSCAPE CONTRACTOR FURNISH A FUNCTIONAL AUTOMATIC IRRIGATION SYSTEM TO ALL ON-SITE AND OFF SITE LANDSCAPE AREAS AS DESCRIBED ON THE LANDSCAPE DRAWINGS AND IN THIS SECTION.
 - THE SCOPE OF WORK INCLUDES SUPPLY AND INSTALLATION OF BACKFLOW PREVENTER, CONTROLLER, ALL IRRIGATION COMPONENTS, IRRIGATION SLEEVES, YEAR MAINTENANCE/WARRANTY (SEE WRITTEN SPECIFICATIONS AND REQUIREMENTS BELOW) AND AN OPERATION MANUAL.
 - THE IRRIGATION CONTRACTOR SHALL COORDINATE WITH THE GENERAL CONTRACTOR AND ELECTRICAL CONTRACTOR FOR THE INSTALLATION OF IRRIGATION SLEEVES.

- MAINTENANCE
- UPON COMPLETION OF IRRIGATION INSTALLATION, CONTRACTOR TO SUBMIT AS-CONSTRUCTED DRAWINGS, OPERATION AND MAINTENANCE MANUAL, TOOLS PROVIDED BY THE MANUFACTURER AND BASE IRRIGATION SCHEDULE TO IAABC STANDARDS (SECTION 5 OF BCLS).
 - INSPECT AND CALIBRATE IRRIGATION SYSTEM AS PER SEASONAL REQUIREMENTS.
 - INSPECT GROWING MEDIUM FREQUENTLY, MINIMUM MONTHLY AND WEEKLY DURING PERIODS OF DROUGHT AND HIGH HEAT FOR MOISTURE CONTENT AND ADJUST IRRIGATION SCHEDULE AS OFTEN AS REQUIRED TO MAINTAIN THE HEALTH OF THE PLANT MATERIAL DUE TO: RAPID DRAINAGE IN THE GROWING MEDIUM, ABSENCE OF A WATER TABLE, FINITE AMOUNT OF GROWING MEDIUM, AND EXPOSURE TO CLIMATIC CONDITIONS.
 - CONTRACTOR TO COORDINATE WITH THE OWNER'S MAINTENANCE REPRESENTATIVE AND TOGETHER PERFORM ONE WINTERIZATION AND ONE SPRING START-UP DURING THE ONE YEAR WARRANTY PERIOD.

SUBMITTALS

SUBMITTALS					
SPECIFICATION SECTION	TITLE	SHOP DRAWINGS	MOCK-UP	SUBMITTALS	FOR APPROVAL
MMCD 03 30 20	CONCRETE WALKS, CURBS AND GUTTERS				
MMCD 03 30 53	CAST IN PLACE CONCRETE				
MMCD 31 02 17	AGGREGATES AND GRANULAR MATERIALS				
MMCD 31 22 01	SITE GRADING				
MMCD 31 24 13	ROADWAY EXCAVATION, EMBANKMENT AND COMPACTION				
MMCD 32 11 23	GRANULAR BASE				
MMCD 32 14 01	UNIT PAVING		yes	yes	product cut sheets, mockup 3.0m paved area on site
CLS 4	GRADING AND DRAINAGE				
CLS 5	IRRIGATION	yes			irrigation design for approval
CLS 6	GROWING MEDIUM			yes	supplier name, growing medium test results
CLS 7	LAWNS AND GRASS (HYDRAULIC SEEDING)			yes	supplier name, product information
CLS 8	TURFGRASS SOD			yes	supplier name, product information
CLS 9	PLANTS AND PLANTING		yes	yes	nursery name, tree and shrub layout on site prior to planting
CLS 10	MULCHING			yes	
CLS 11	LANDSCAPE OVER STRUCTURES - GREEN ROOFS				
CLS 12	CONTAINER GROWN PLANTS				



Table 1. Tree Inventory

Previous tag #	Tag or ID #	Surveyed ? (Yes / No)	Location (On / Off Street, City)	Bylaw protected ? (Yes / No / City)	Name		dbh (cm)	Ht (m)	Critical root zone radius (m)	Drip-line radius (m)	Condition		Retention Suitability (on-site trees)	Relative tolerance	General field observations/remarks	Tree retention comments	Retention status		
					Common	Botanical					Health	Structural							
	M1	Yes	City	City	Purple leaf cherry plum	<i>Prunus cerasifera</i>	47	10	5.9	5	Good	Fair/good		Moderate	Multiple stems form at 2m above grade - included bark of main stem union with active response growth, historic pruning wounds with associated surface decay.	Project arborist to supervise all excavation required within the critical root zone. Will be impacted by the installation of the proposed water line.	Retain		
	M2	Yes	City	City	Pacific sunset maple	<i>Acer truncatum</i>	5	6	0.6	1	Good	Good		Moderate	Recently planted - still staked, included bark at stem union. Multiple stems form at 4m above grade - no major weaknesses visible at stem union, historic pruning wounds with associated surface decay.	"Possible for transplant."	Remove*		
	M3	Yes	City	City	Purple leaf cherry plum	<i>Prunus cerasifera</i>	26	6	3.3	4	Good	Fair/good		Moderate	Codominant stems form at 3m above grade - included bark - not active, historic pruning wounds with associated decay.	Project arborist to supervise all excavation required within the critical root zone.	Retain		
313	3383	Yes	On	No	Fig	<i>Ficus sp.</i>	11.8, 6	5	2	2	Good	Fair	Unstable	Good	Growing within close proximity to existing building. Located within the footprint of the proposed building.	Located within the footprint of the proposed building.	Remove		
316	3384	Yes	On	No	Japanese maple	<i>Acer palmatum</i>	3.3, 6	5	1.2	1.5	Good	Fair	Unstable	Moderate	Multiple stems form at 1m above grade - narrow angles of attachment, stems topped and decayed with sloughing bark. Topped historically at 7m above grade for overhead utilities clearance. stem decay with sloughing bark.	Located within the footprint of the proposed building.	Remove		
317	3385	Yes	On	Yes	Pacific dogwood	<i>Cornus nuttallii</i>	28, 23, 34, 32	8	8.8	4	Fair	Fair/poor	Unstable	Moderate	Topped historically at 5m above grade - multiple regrowth leaders form at topping locations.	Located within the footprint of the proposed building.	Remove		
322	3386	Yes	On	Yes	Golden chan tree	<i>Laburnum sp.</i>	21, 28	7	6.3	4	Fair	Poor	Unstable	Poor		Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will also be impacted by excavation for the proposed parking stall.	Remove		
321	3387	Yes	On	Yes	Apple	<i>Malus sp.</i>	14, 23	10	3.9	3	Fair	Poor	Unstable	Moderate	Flowering cherry, multiple stems form at 2m above grade - narrow angles of attachment, main stem topped historically at 4m above grade - large topping wound.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will also be impacted by excavation for the proposed parking stall.	Remove		
334	3388	Yes	On	Yes	Cherry	<i>Prunus sp.</i>	55	5	6.9	4	Fair	Fair/poor	Unstable	Moderate	Flowering cherry, codominant stems form at base - crossing at 5m above grade, large heading cuts.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove		
298	3389	Yes	On	Yes	Cherry	<i>Prunus sp.</i>	19, 25	5	4.6	4	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 5 year trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will also be impacted by excavation for the proposed parking stall.	Remove		
3390	Yes	On	No	English yew	<i>Taxus baccata</i>	10.8, 8.6	5	2.5	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 5 year trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3391	Yes	On	No	English yew	<i>Taxus baccata</i>	5.5, 5	5	1.4	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 5 year trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3392	Yes	On	No	English yew	<i>Taxus baccata</i>	10.8, 9	5	2.5	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 5 year trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3393	Yes	On	No	English yew	<i>Taxus baccata</i>	8.8, 4.3	5	1.9	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 5 year trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3394	Yes	On	No	English yew	<i>Taxus baccata</i>	10.9, 9.4	5	2.6	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 5 year trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3395	Yes	On	No	English yew	<i>Taxus baccata</i>	11, 16	5	2.8	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 13 year trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3396	Yes	On	No	English yew	<i>Taxus baccata</i>	18	5	2.3	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 13 year trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3397	Yes	On	No	English yew	<i>Taxus baccata</i>	12, 8	5	2.1	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 13 year trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3398	Yes	On	No	English yew	<i>Taxus baccata</i>	10, 10, 8.8	5	2.6	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 13 year trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3399	Yes	On	No	English yew	<i>Taxus baccata</i>	14, 8	5	2.4	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 13 year trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3400	Yes	On	No	English yew	<i>Taxus baccata</i>	12, 5	5	1.9	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 13 year trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3401	Yes	On	No	English yew	<i>Taxus baccata</i>	8.8, 5.5	5	2	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 13 year trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3402	Yes	On	No	English yew	<i>Taxus baccata</i>	8.8, 5.4	5	2	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 13 year trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3403	Yes	On	No	English yew	<i>Taxus baccata</i>	8, 6, 5, 4	5	1.8	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 13 year trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3404	Yes	On	No	English yew	<i>Taxus baccata</i>	10.8, 5, 4	5	2.2	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 13 year trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3405	Yes	On	No	English yew	<i>Taxus baccata</i>	14, 10, 10	5	3.3	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 13 year trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3406	Yes	On	No	English yew	<i>Taxus baccata</i>	16	5	2	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 13 year trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3407	Yes	On	No	English yew	<i>Taxus baccata</i>	10, 10, 5, 5, 5	5	2.4	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 13 year trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3408	Yes	On	No	English yew	<i>Taxus baccata</i>	12, 5, 5	5	2.3	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 7 year trees spaced less than 1.25m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3409	Yes	On	No	English yew	<i>Taxus baccata</i>	8, 4	5	1.3	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 7 year trees spaced less than 1.25m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3410	Yes	On	No	English yew	<i>Taxus baccata</i>	5, 5, 4, 4, 4	5	1.3	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 7 year trees spaced less than 1.25m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3411	Yes	On	No	English yew	<i>Taxus baccata</i>	8.8, 8, 5, 5	5	2.2	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 7 year trees spaced less than 1.25m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3412	Yes	On	No	English yew	<i>Taxus baccata</i>	8, 8, 8	5	2.2	1	Fair	Fair/poor	Unstable	Moderate	Growing within a hedge row of 7 year trees spaced less than 1.25m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove			
3413	Yes	On	No										r	Unstable	Moderate	Growing within a hedge row of 7 year trees spaced less than 1.25m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove	
3414	Yes	On	No										r	Unstable	Moderate	Growing within a hedge row of 7 year trees spaced less than 1.25m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove	
3415	Yes	On	Ye										r	Unstable	Moderate	Growing within a hedge row of 5 year trees spaced less than 1m apart. "11 Bylaw size stem within hedge."	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove	
3416	Yes	On	Ye										r	Unstable	Moderate	Growing within a hedge row of 5 year trees spaced less than 1m apart. "11 Bylaw size stem within hedge."	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove	
3417	Yes	On	Ye										r	Unstable	Moderate	Growing within a hedge row of 5 year trees spaced less than 1m apart. "11 Bylaw size stem within hedge."	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove	
3418	Yes	On	Ye										r	Unstable	Moderate	Growing within a hedge row of 5 year trees spaced less than 1m apart. "11 Bylaw size stem within hedge."	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove	
3419	Yes	On	Ye										r	Unstable	Moderate	Growing within a hedge row of 5 year trees spaced less than 1m apart. "11 Bylaw size stem within hedge."	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove	
Hedge 1	Yes	On	No											Unstable	Moderate	Hedge row consisting of approximately 30 individual stems - no bylaw stems. Codominant stems form at 3m above grade - no major weaknesses visible at stem union, ivy covered trunk.	Located within the footprint of the proposed building. Located within the footprint of the proposed parking stall.	Remove	
314	3420	Yes	On	Ye										r	Unstable	Moderate	Topped historically at 5m above grade - multiple regrowth leaders form at topping locations.	Located within the footprint of the proposed building.	Remove
315	3421	Yes	On	No										r	Unstable	Moderate	Juvenile hedge tree growing within narrow planting strip.	Located within the footprint of the proposed building.	Remove
OS1	Yes	Off	No												Moderate	Juvenile hedge tree growing within narrow planting strip.	Project arborist to supervise all excavator required within the CRZ.	Retain*	

BOULEVARD TREE M2 TO BE REMOVED AND RELOCATED BY CITY OF VICTORIA FOR INSTALLATION OF PROPOSED WATER CONNECTION (SEE CIVIL). REMOVAL TO BE PAID FOR BY DEVELOPER.

Previous tag #	Tag or ID #	Surveyed ? (Yes / No)
	OS2	Yes
	OS3	Yes
	OS4	Yes
	OS5	Yes
	OS6	Yes
	Hedge 2	Yes
	OS7	Yes
	3422	No

NOTE: SEE ORIGINAL TREE INVENTORY AND TABLES IN REPORT PREPARED BY TALBOT & MACKENZIE

WINDMILL PALM REQUESTED TO BE TRANSPLANTED / REMOVED BY LANDSCAPE TO ALLOW FOR PLANTING OF LARGE CANOPY TREE

FAIRFIELD ROAD

BEECHWOOD AVENUE

TREE STATUS LEGEND

- EXISTING TREE TO REMAIN
- EXISTING TREE TO BE REMOVED
- TREE PROTECTION FENCING
 - TO OUTSIDE OF DRIPLINE WHERE POSSIBLE (AS SHOWN)
 - TO BE MAINTAINED UNTIL COMPLETION OF CONSTRUCTION
 - TO MUNICIPAL STANDARD
- PROTECTED TREE AND SHRUB FOR REMOVAL
- MUNICIPAL TREE FOR REMOVAL



1608 Camosun Street, Victoria BC V8T 3E6
Info@biophiliacollective.ca 250 59C 1156

OWNER/CLIENT:
ARYZE DEVELOPMENTS

PROJECT ADDRESS:
1733 FAIRFIELD,
VICTORIA, BC

DESIGNED BY: BIANCA BODLEY
DRAWN BY: TYLER YESTAL

NO.	ISSUE	YY/MM/DD
1	ISSUED FOR DEVELOPMENT TRACKER	22/03/04
2	ISSUED FOR RZ/DP	22/05/02
3	ISSUED FOR COORDINATION	22/07/18
4	ISSUED FOR RZ/ DP	22/07/20
5	ISSUED FOR RZ/ DP RESUBMISSION	23/01/24
6	ISSUED FOR RZ/ DP RESUBMISSION	2023/08/18
7	25% BUILDING PERMIT	2023/10/10
8	ISSUED FOR BP COORDINATION	2023/11/24
9	DP/RZ REV 4	2024/01/12
10	DP/RZ REV 5	2024/02/21
11	ISSUED FOR BUILDING PERMIT	2024/03/01
12	DP/RZ REV 6	2024/07/03

SEAL



NORTH ARROW

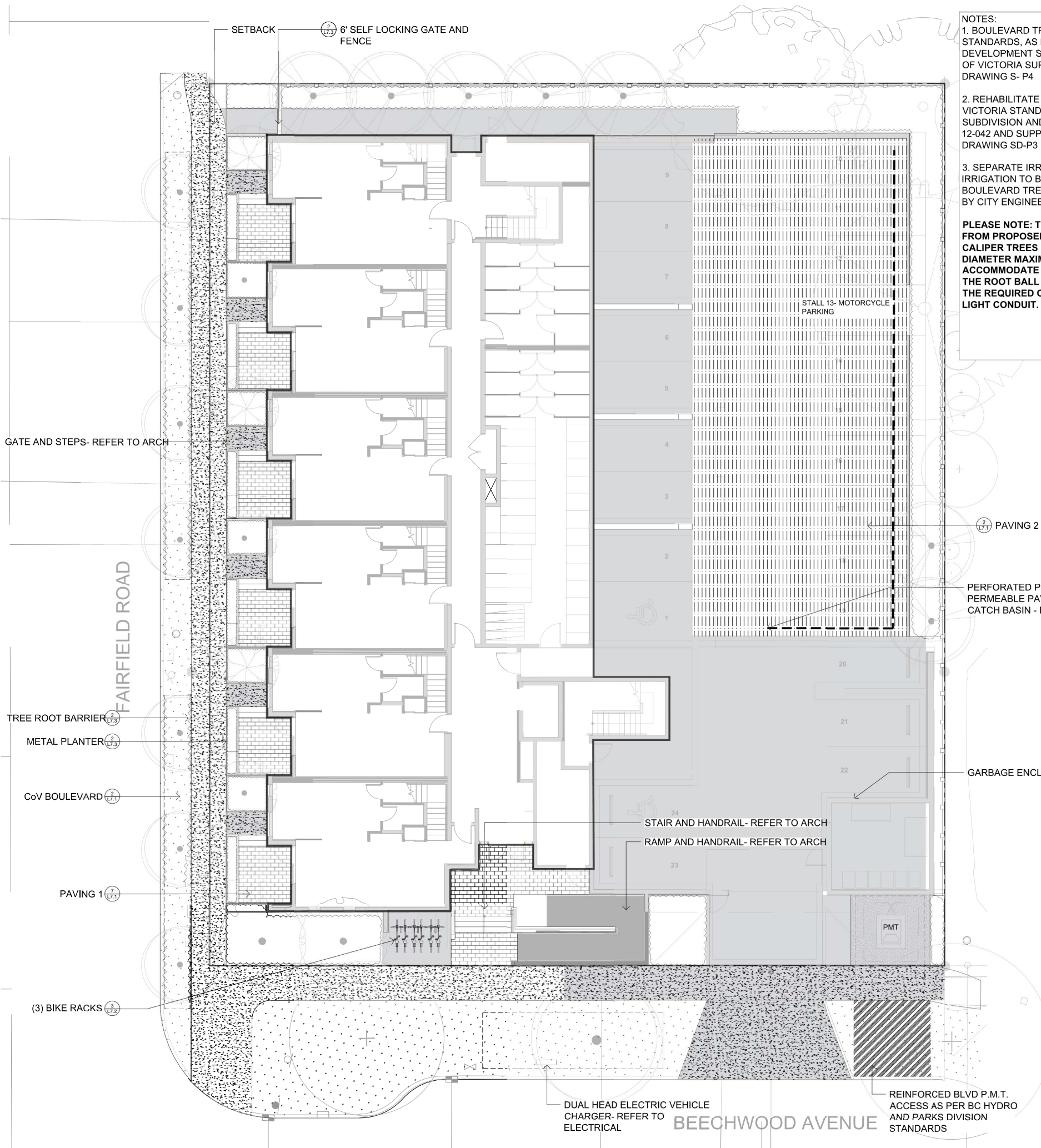


DRAWING TITLE:
TREE REMOVAL &
PROTECTION PLAN

DWG NO:

L0.1

SCALE: 1:150



- NOTES:
1. BOULEVARD TREE PLANTING TO CITY OF VICTORIA STANDARDS, AS PER THE VICTORIA SUBDIVISION AND DEVELOPMENT SERVICES BYLAW NO 12-043, AND CITY OF VICTORIA SUPPLEMENTARY STANDARD DETAIL DRAWING S- P4
 2. REHABILITATE EXISTING BOULEVARD TO CITY OF VICTORIA STANDARDS, AS PER THE VICTORIA SUBDIVISION AND DEVELOPMENT SERVICES BYLAW NO 12-042 AND SUPPLEMENTARY STANDARD DETAIL DRAWING SD-P3
 3. SEPARATE IRRIGATION SERVICE AND AUTOMATIC IRRIGATION TO BE PROVIDED TO MUNICIPAL BOULEVARD TREES IF TREE LOCATIONS ARE APPROVED BY CITY ENGINEERING AND BC HYDRO

PLEASE NOTE: THERE IS INSUFFICIENT SETBACK FROM PROPOSED UTILITIES. IF WE USE SMALL CALIPER TREES (20mm) WITH A ROOT BALL OF 400 mm DIAMETER MAXIMUM, WE BELIEVE WE CAN ACCOMMODATE THE REQUIRED 1.0m OFFSET FROM THE ROOT BALL TO THE BC HYDRO UTILITY BUT NOT THE REQUIRED OFFSET TO THE PROPOSED STREET LIGHT CONDUIT.

SITE PERMEABILITY

TOTAL SITE AREA: 1703 m²
LANDSCAPED AREA: 223 m²
PERMEABLE PAVED AREA: 484 m²
SITE PERMEABILITY RATIO: 41.3%

LEGEND

- PROPERTY LINE
- ROOF OVERHANG
- ON-SITE LANDSCAPE AREA
 - 1000 mm DEPTH GROWING MEDIUM
 - 50 mm DEPTH MULCH
- RAISED PLANTER
 - 450 mm DEPTH PLANTER GROWING MEDIUM
 - 50 mm DEPTH MULCH
- PARKING STALL PAVER
 - NEWSTONE DORADO DRAIN PAVERS
 - COLOR: CHARCOAL
- PATIO AND BALCONY PAVER
 - ARISTOKRAT™ SERIES
 - COLOR: CASCADE GRANITE
 - SIZE: 24" X 48"
 - ON PEDESTALS
- SOD MUNICIPAL BOULEVARD AREA
 - 150 mm DEPTH HIGH TRAFFIC LAWN GROWING MEDIUM
 - SOD
- MUNICIPAL SIDEWALK
 - CIP CONCRETE WITH BROOM FINISH TO CITY OF VICTORIA MUNICIPAL STANDARDS
- CONCRETE PATH AND DRIVEWAY
- TREE PIT WITH 1000mm DEPTH GROWING MEDIUM FOR BOULEVARD TREES
- BIKE RACK
 - SURFACE MOUNT ON CONCRETE PAD
- FENCE
 - 6' HT SOLID CEDAR WOOD FENCE
 - STAIN: HEMLOCK GREY
- EXISTING BOULEVARD TREE TO BE RETAINED
- PROPOSED BOULEVARD TREE TO BE SPECIFIED BY CITY OF VICTORIA PARKS
- PROPOSED TREES



1608 Camosun Street, Victoria BC V8T 3E6
Info@biophilialcollective.ca 250 590 1156

OWNER/CLIENT:
ARYZE DEVELOPMENTS

PROJECT ADDRESS:
1733 FAIRFIELD, VICTORIA, BC

DESIGNED BY: **BIANCA BODLEY**
DRAWN BY: **TYLER YESTAL**

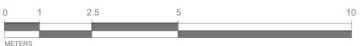
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5	ISSUED FOR RZ/DP RESUBMISSION	23/01/24
6	ISSUED FOR RZ/DP RESUBMISSION	2023/08/18
7	25% BUILDING PERMIT	2023/10/10
8	ISSUED FOR BP COORDINATION	2023/11/24
9	DPIRZ REV 4	2024/01/12
10	DPIRZ REV 5	2024/02/21
11	ISSUED FOR BUILDING PERMIT	2024/03/01
12	DPIRZ REV 6	2024/07/03



DRAWING TITLE:
LANDSCAPE MATERIALS PLAN LEVEL 1

DWG NO:
L1.1

SCALE: 1:100





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11	ISSUED FOR BUILDING PERMIT	2024/03/01
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SEAL



NORTH ARROW

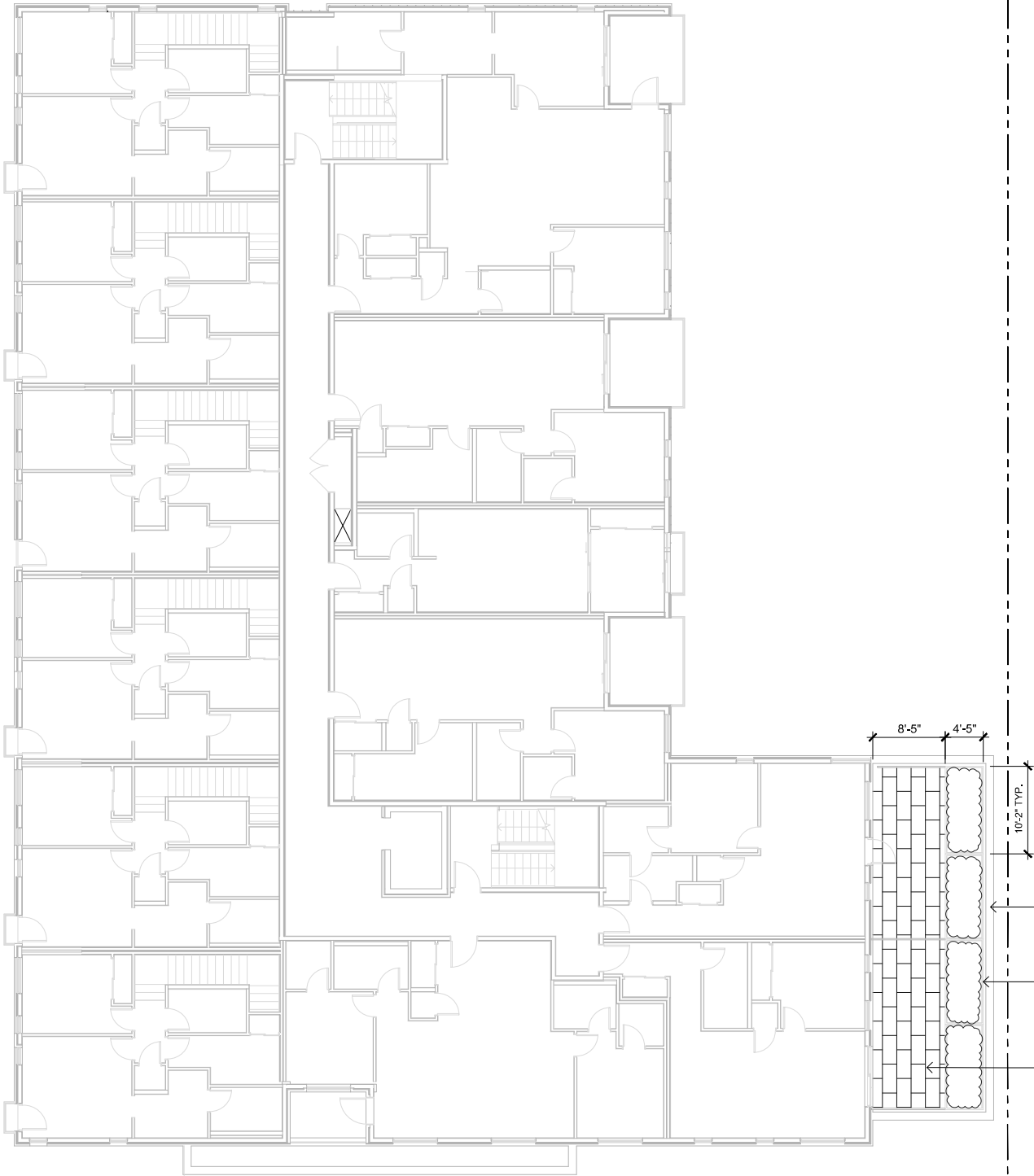


DRAWING TITLE:
LANDSCAPE MATERIALS
AND LAYOUT PLAN
LEVELS 2, 3, & 4

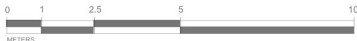
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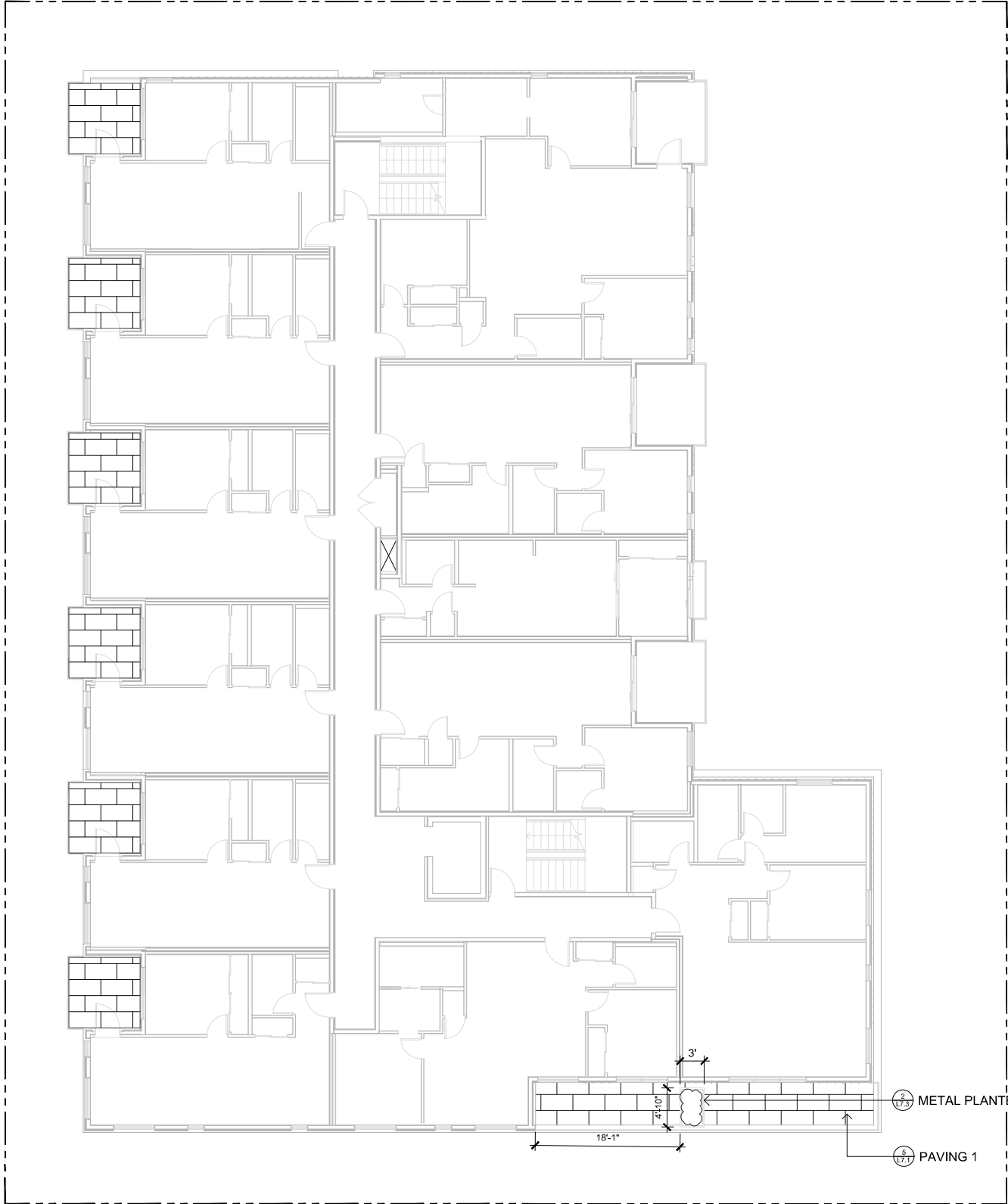
SCALE: AS NOTED

L1.2

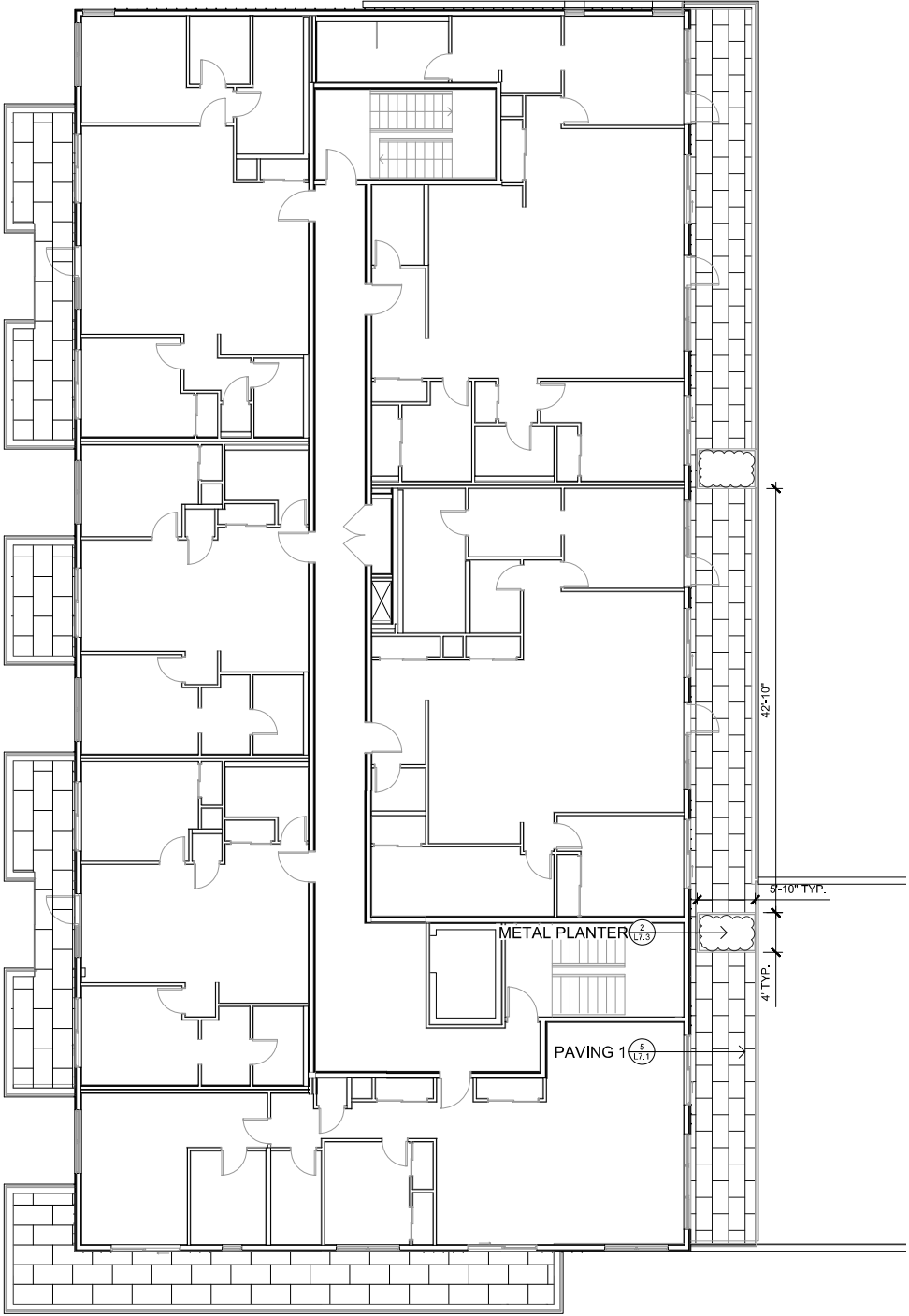


1
L2
LEVEL 2 BALCONY & PLANTER
1:100





1 LEVEL 3 BALCONY & PLANTER
L1.3 1:100



2 LEVEL 4 BALCONY & PLANTER
L1.3 1:100



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SEAL



NORTH ARROW

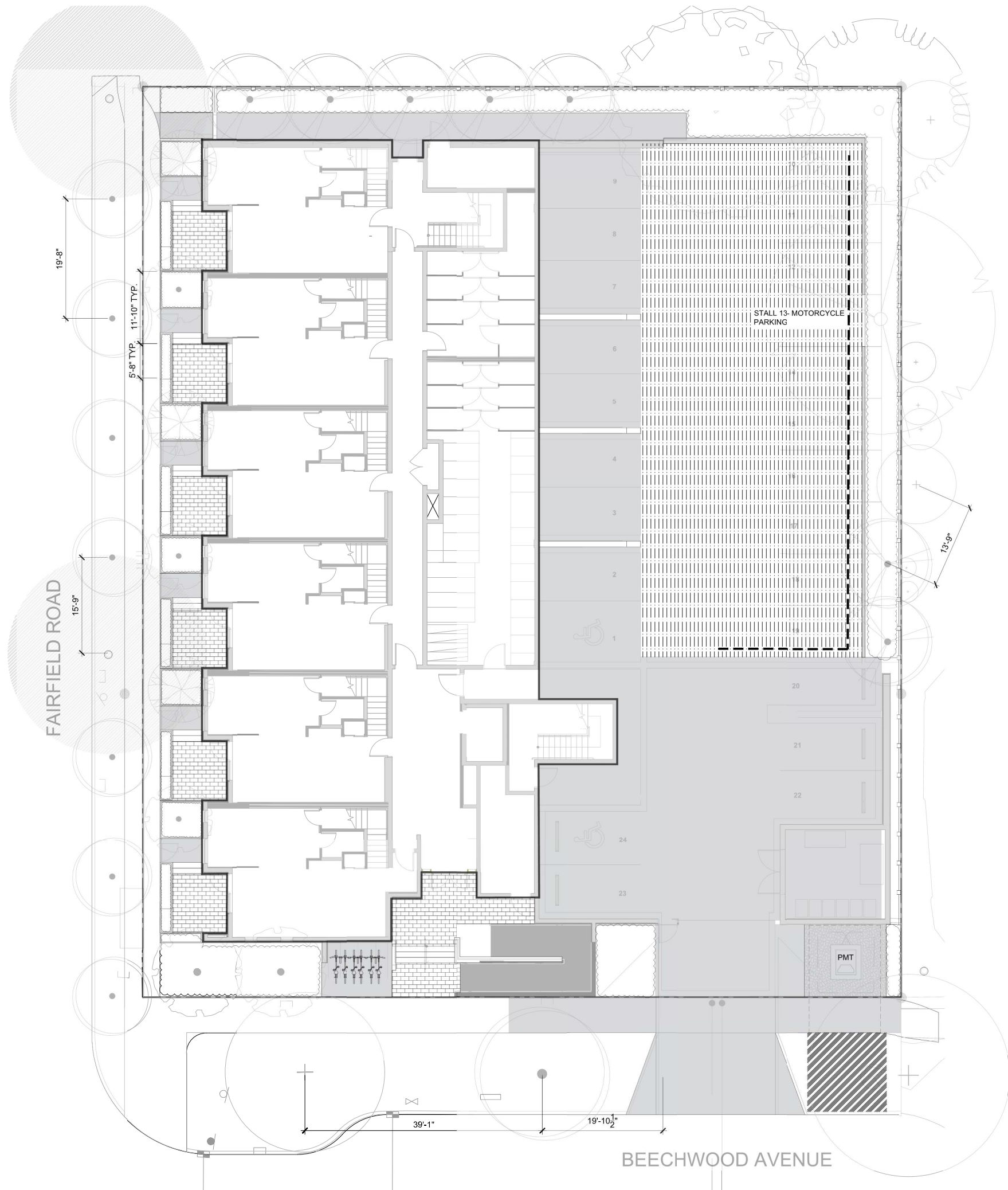


DRAWING TITLE:
LANDSCAPE MATERIALS
AND LAYOUT PLAN
LEVELS 2, 3, & 4

DWG NO:

SCALE: AS NOTED

L1.2



1608 Camosun Street, Victoria BC V8T 3E6
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VICTORIA, BC**

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DRAWN BY: **TYLER YESTAL**

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SEAL



NORTH ARROW

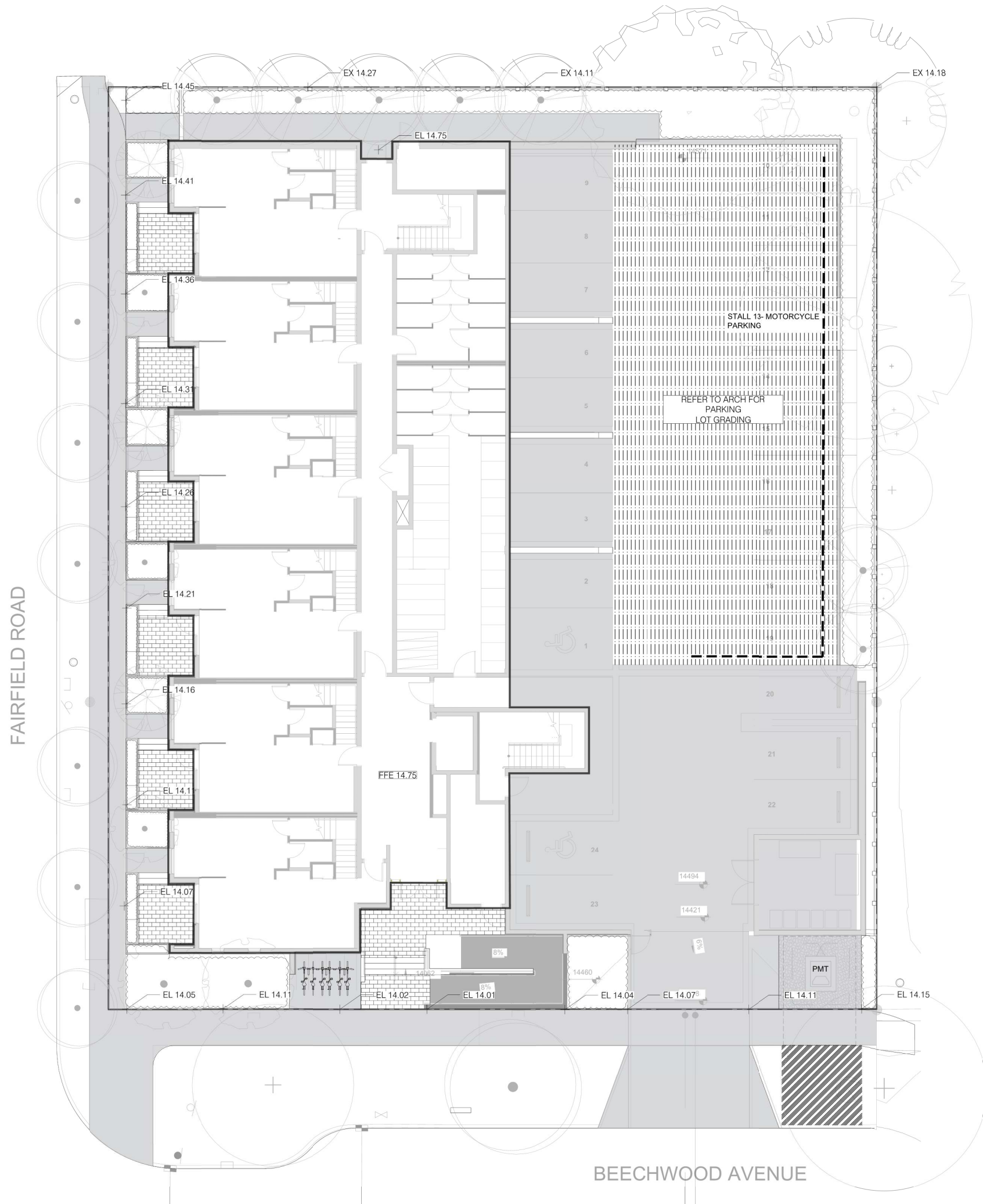


DRAWING TITLE:
LAYOUT PLAN

DWG NO:

L2.1

SCALE: 1:100



- GRADING PLAN NOTES
1. ALL ELEVATIONS ARE NOMINAL. CONTRACTOR TO VERIFY SPOT ELEVATION PRIOR THE START OF CONSTRUCTION.
 2. ALL ELEVATIONS ARE TO TOP OF FINISHED GRADE UNLESS NOTED OTHERWISE.
 3. REFER TO CIV/L FOR ALL OFFSITE AND PARKING LOT GRADING.
 4. REFER TO ARCH FOR ONSITE HARDSCAPE GRADING.



1608 Camosun Street, Victoria BC V8T 3E6
Info@biophilacollective.ca 250 590 1156

OWNER/CLIENT:
ARYZE DEVELOPMENTS

PROJECT ADDRESS:
**1733 FAIRFIELD,
VICTORIA, BC**

DESIGNED BY: **BIANCA BODLEY**
DRAWN BY: **TYLER YESTAL**

NO.	ISSUE	YYMMDD
1	ISSUED FOR DEVELOPMENT TRACKER	22/03/04
2	ISSUED FOR RZ/DP	22/05/02
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5	ISSUED FOR RZ/ DP RESUBMISSION	23/01/24
6	ISSUED FOR RZ/ DP RESUBMISSION	2023/08/18
7	25% BUILDING PERMIT	2023/10/10
8	ISSUED FOR BP COORDINATION	2023/11/24
9	DPIRZ REV 4	2024/01/12
10	DPIRZ REV 5	2024/02/21
11	ISSUED FOR BUILDING PERMIT	2024/03/01
12	DPIRZ REV 6	2024/07/03

SEAL



NORTH ARROW

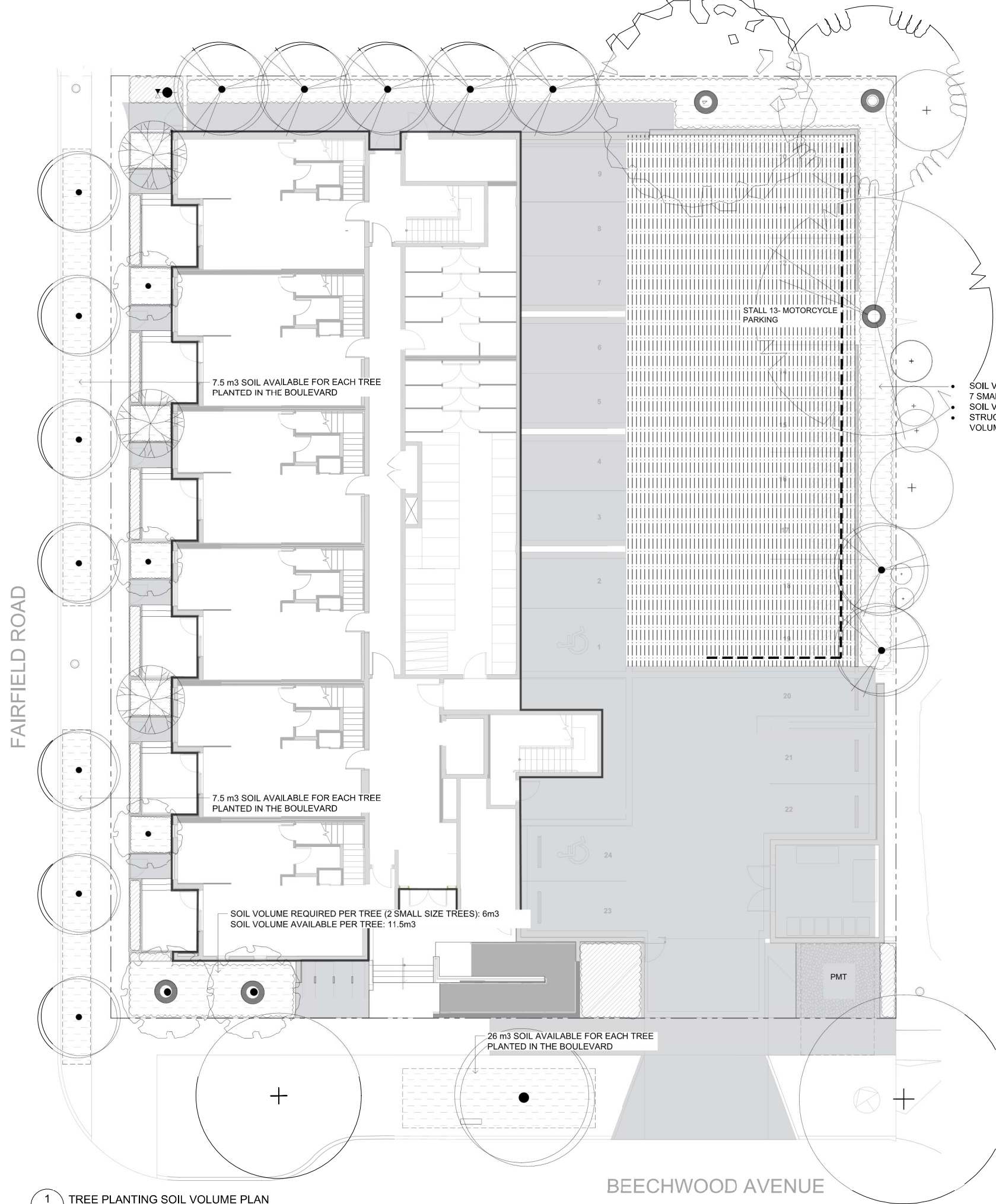


DRAWING TITLE:
GRADING PLAN

DWG NO:

L3.1

SCALE: 1:100



LEGEND

- 1:1 DESIGNATED REPLACEMENT TREE
- 2:1 DESIGNATED REPLACEMENT TREE
- 1.0m DEPTH SOIL IN PLANTING BED
- 0.60m DEPTH SOIL
- 1.0m DEPTH STRUCTURAL SOIL UNDER Hardscape
- TREE PIT WITH 1.0m DEPTH GROWING MEDIUM FOR BOULEVARD TREES

NOTE:
- ALL UPPER LEVELS TO RECEIVE 600mm SOIL DEPTH IN METAL PLANTERS

- SOIL VOLUME REQUIRED PER TREE (2 LARGE SIZE TREES, 1 MEDIUM SIZE TREE, 7 SMALL SIZE TREES): 117 m³
- SOIL VOLUME IN PLANTING BED: 102 m³
- STRUCTURAL SOIL VOLUME COVERAGE AREA: 75m² (COUNTS 20% OF SOIL VOLUME 15 m³)



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SEAL



NORTH ARROW

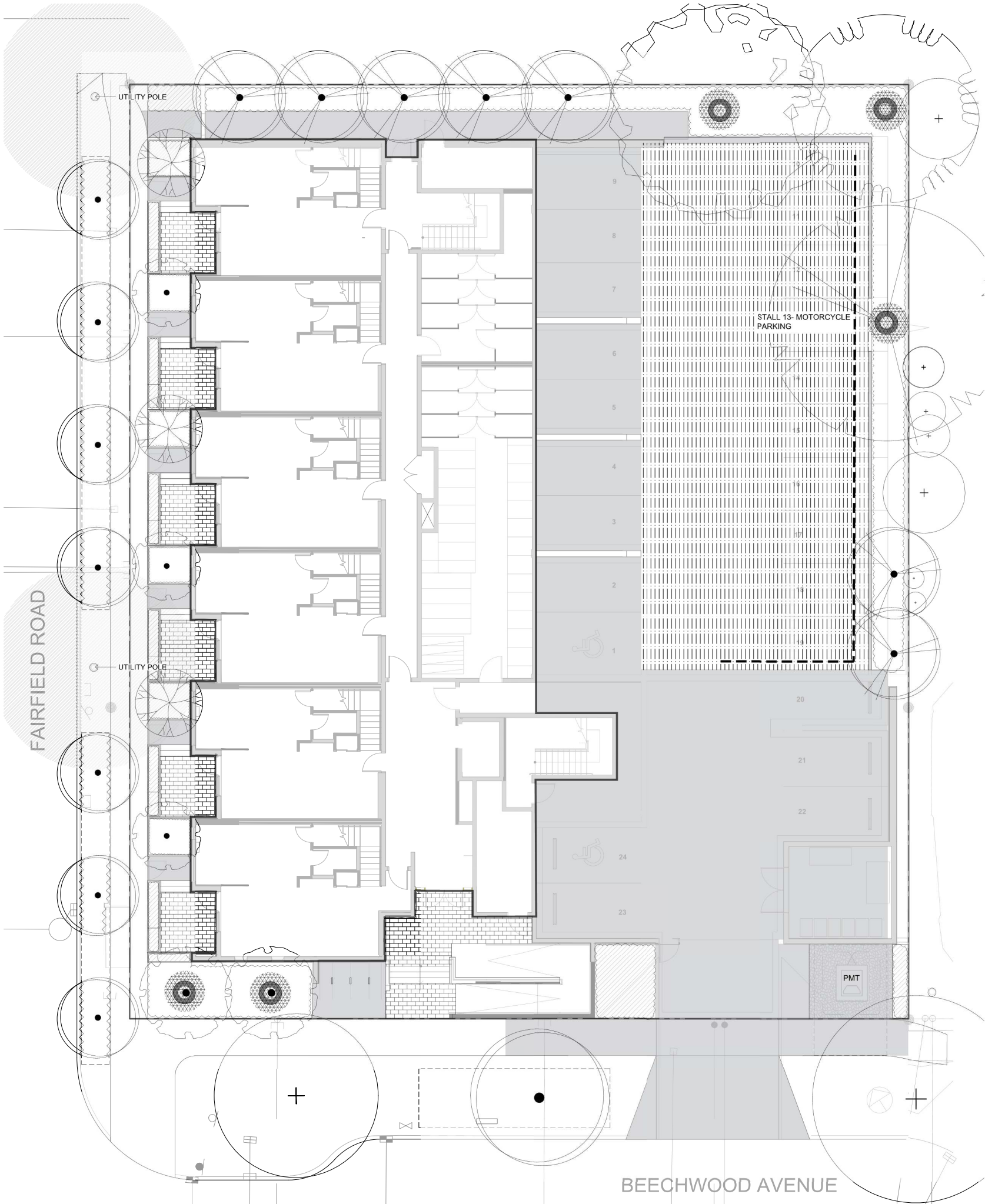


DRAWING TITLE:
SOIL VOLUME PLAN

DWG NO:








SCALE: AS NOTED

L4.1



LEGEND

- 6kV — 6kV — OVERHEAD POWER LINE
- ~~~~~ ROOT BARRIER
- 1:1 DESIGNATED REPLACEMENT TREE
- 2:1 DESIGNATED REPLACEMENT TREE
- TREE PIT WITH 1.0 m DEPTH GROWING MEDIUM FOR BOULEVARD TREES
- TREE PLANTING RESTRICTIONS
- 5.0 m OFFSET FROM HYDRO OR LIGHT POLE
- 1.0 m OFFSET AREA

TREE SCHEDULE							
Quantity	Symbol	Latin Name	Common Name	Container	Caliper	Size	Native
5		Acer griseum	Paperbark Maple	B&B	6 cm	>2 m	
1		Fraxinus americana 'Autumn Applause'	Autumn Applause white ash	B&B	6 cm	>2 m	
1		Liriodendron tulipifera	Tulip tree	B&B	6 cm	>2 m	
7		Parrotia persica 'Ruby Vase'	Ruby Vase Parrotia	B&B	6 cm	>2 m	
3		Styrax japonicus	Japanese snowbell	B&B	6 cm	>2 m	
1		Quercus palustris	Pin Oak	B&B	6 cm	>2 m	
8		BOULEVARD TREE TO BE DETERMINED BY PARKS AT BP		B&B	TBD		
<p>NOTES: 1. PLANTS IN PLANT LISTS ARE SPECIFIED ACCORDING TO THE CANADIAN NURSERY LANDSCAPE ASSOCIATION CANADIAN STANDARDS FOR NURSERY STOCK AND SECTION 12, CONTAINER GROWN PLANTS FROM THE BC LANDSCAPE STANDARD, CURRENT EDITION.</p>							

TREE IMPACT SUMMARY TABLE						
TREE STATUS	TOTAL	TO BE RETAINED	TO BE REMOVED	REPLACEMENTS REQUIRED	MINIMUM REQUIRED FOR LOT SIZE	PROPOSED
ON-SITE TREES AND SHRUBS BYLAW PROTECTED	7	0	7	7	9	4
MUNICIPAL TREES	3	2	1	1		8
NEIGHBORING TREES, BYLAW PROTECTED	0	0	0	0		0
REPLACEMENT TREES TO BE PLANTED ON SITE	4					
REPLACEMENT TREE SHORTFALL	3					
TOTAL PROPOSED TREES ON SITE (REPLACEMENT (ACTUAL) + OTHER)	18					

ACTUAL NUMBER : 5
2 x LARGE TREE (1:1)
1 x MEDIUM TREE (1:1)
2 x SMALL TREE (2:1)

- NOTES:
- IRRIGATION SYSTEM ON CITY PROPERTY SHALL COMPLY TO CITY OF VICTORIA SUPPLEMENTARY SPECIFICATIONS FOR STREET TREES AND IRRIGATION SCHEDULE C, BYLAW 12-042, SUBDIVISION BYLAW. IRRIGATION DRAWINGS MUST BE SUBMITTED TO PARKS DIVISION FOR REVIEW AND APPROVAL 30 DAYS PRIOR TO INSTALLATION WORK. THE FOLLOWING IRRIGATION AND SLEEVE INSPECTIONS BY PARKS STAFF ARE REQUIRED BY SCHEDULE C.
 - IRRIGATION INSPECTION REQUIREMENTS:
 - THE IRRIGATION SYSTEM AND SLEEVE INSPECTION REQUIREMENTS CAN BE FOUND IN SCHEDULE C OF THE VICTORIA SUBDIVISION AND DEVELOPMENT SERVICING BYLAW NO. 12-042.
 - IRRIGATION SLEEVE PRIOR TO BACKFILLING*
 - OPEN TRENCH MAIN LINE AND PRESSURE TEST
 - OPEN TRENCH LATERAL LINE
 - IRRIGATION SYSTEM, CONTROLLER, COVERAGE TEST, BACKFLOW PREVENTER ASSEMBLY TEST REPORT REQUIRED, BACKFLOW ASSEMBLY IS TO HAVE AN INSPECTION TAG COMPLETED AND ATTACHED.
 - 100MM SDR 28 PIPE WILL BE USED FOR IRRIGATION SLEEVE UNDER HARD SURFACES. INSTALLATIONS WHERE A 90-DEGREE BEND IS REQUIRED SHOULD BE INSTALLED USING 100MM SDR 28 GSX (22.5 DEGREE) LONG SWEEPS. INSTALL WILL BE AT 400MM DEPTH.



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DRAWN BY: TYLER YESTAL

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SEAL



NORTH ARROW



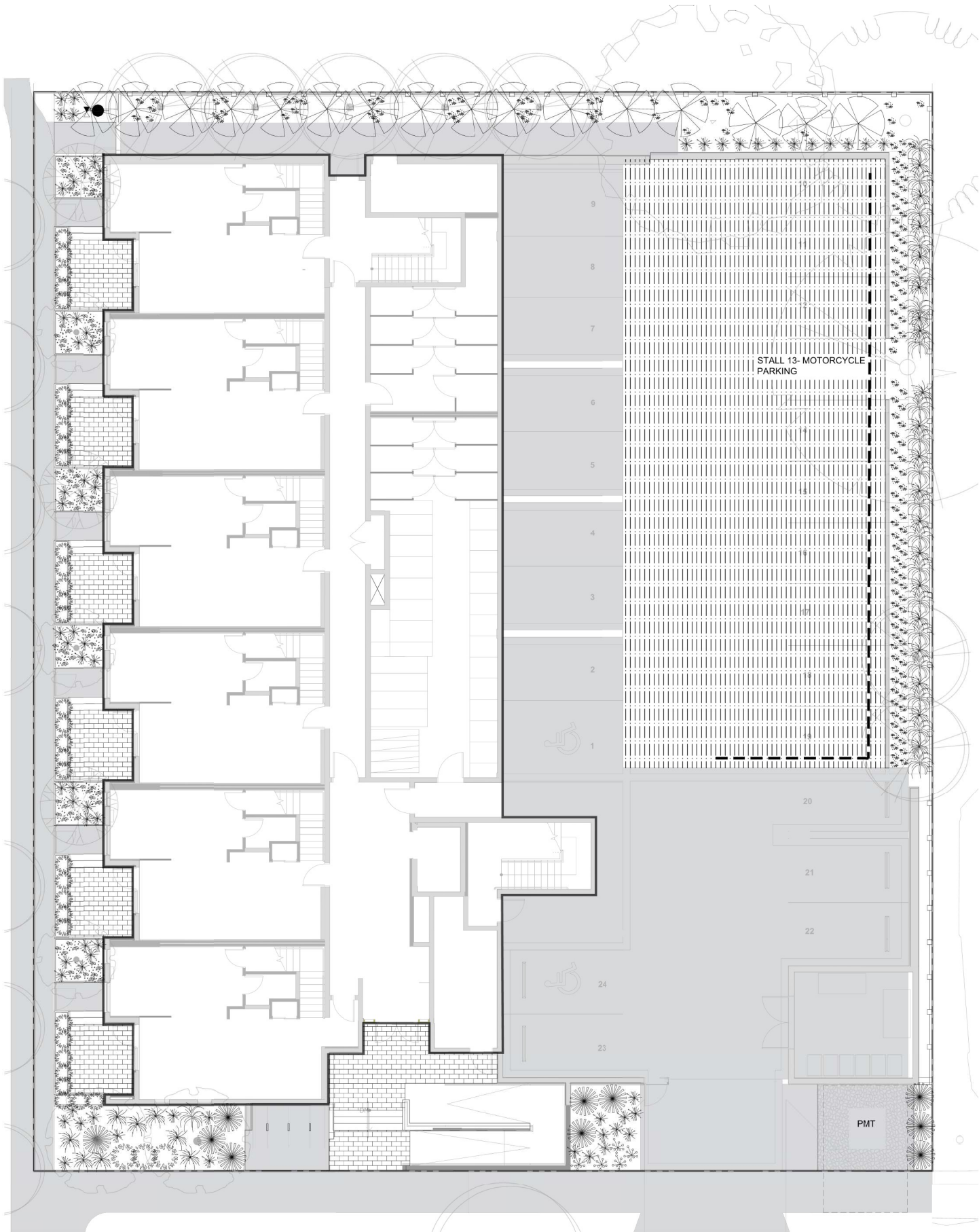
DRAWING TITLE:

TREE PLANTING PLAN

DWG NO:

SCALE: 1:100

L4.2



PLANT SCHEDULE						
Quantity	Symbol	Latin Name	Common Name	Container	Native	Pollinator
12		Armeria maritima 'Alba'	White-flowered Thrift	10cm		y
182		Arctostaphylos Uva-Ursi 'Massachusetts'	Kinnikinnick	10cm	y	y
69		Carex testacea 'Prairie Fire'	New Zealand hair sedge	#1		
10		Cephalotaxus harringtonia 'Prostrata'	Spreading Japanese plum yew	#2		
16		Choisya ternata	Mexican mock orange	#3		y
36		Hebe pinguifolia 'Sutherlandii'	Hebe Sutherlandii	#1		
30		Miscanthus sinensis 'Huron Light'	Huron Light maiden grass	#1		
20		Miscanthus sinensis 'Morning Light'	Chinese silver grass Morning Light	#1		
5		Verbena bonariensis 'Lollipop'	Dwarf purpletop	#1		Y
NOTES: 1. PLANTS IN PLANT LISTS ARE SPECIFIED ACCORDING TO THE CANADIAN NURSERY LANDSCAPE ASSOCIATION CANADIAN STANDARDS FOR NURSERY STOCK AND SECTION 12. CONTAINER GROWN PLANTS FROM THE BC LANDSCAPE STANDARD, CURRENT EDITION.						

- PLANTING PLAN NOTES
- ALL PLANT MATERIAL TO BCSLA STANDARDS.
 - THE SEARCH ARE FOR PLANT MATERIAL IS THE PACIFIC NORTHWEST INCLUDING WASHINGTON, OREGON AND BRITISH COLUMBIA.
 - CONFIRM PLANTING LAYOUT WITH LANDSCAPE ARCHITECT ON SITE.
 - ALL SOIL VOLUMES TO MEET THE STANDARDS OF THE CITY OF VICTORIA. REFER TO SOIL DEPTH PLANS FOR SOIL DEPTH REQUIREMENTS.
 - ALL GROWING MEDIUM PROVISION AND INSTALLATION TO BE IN LINE WITH CANADIAN LANDSCAPE STANDARDS
 - REFER TO CIVIL AND MECHANICAL FOR DRAINS.
 - ALL ONSITE AND OFF-SITE LANDSCAPE AREAS TO BE IRRIGATED BY AN UNDERGROUND AUTOMATIC IRRIGATION SYSTEM. IRRIGATION TO BE SUPPLIED BY CONTRACTOR
 - PLANTING AROUND EXISTING CRITICAL TREE ROOT ZONES TO BE INSTALLED UNDER THE SUPERVISION OF A CERTIFIED ARBORIST.



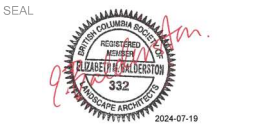
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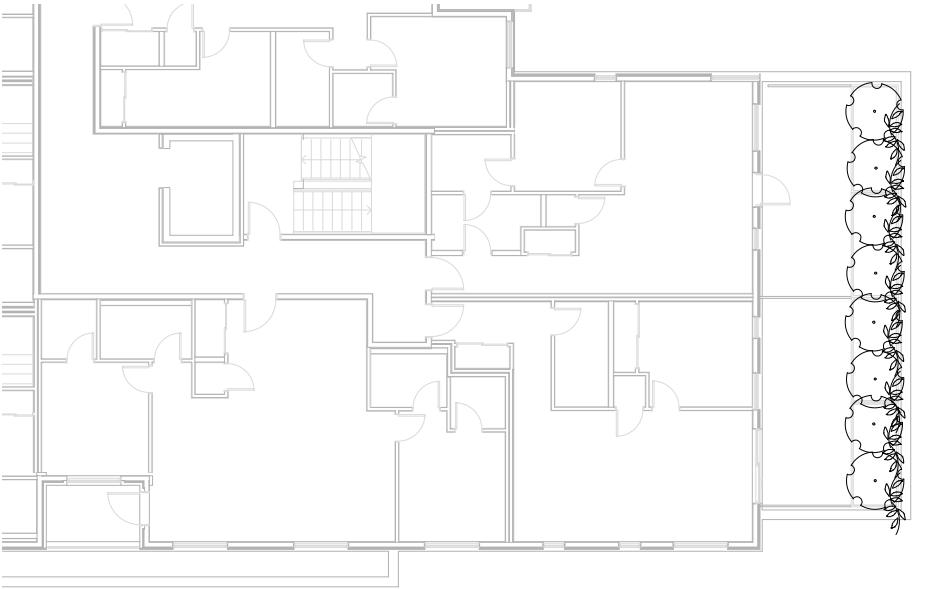
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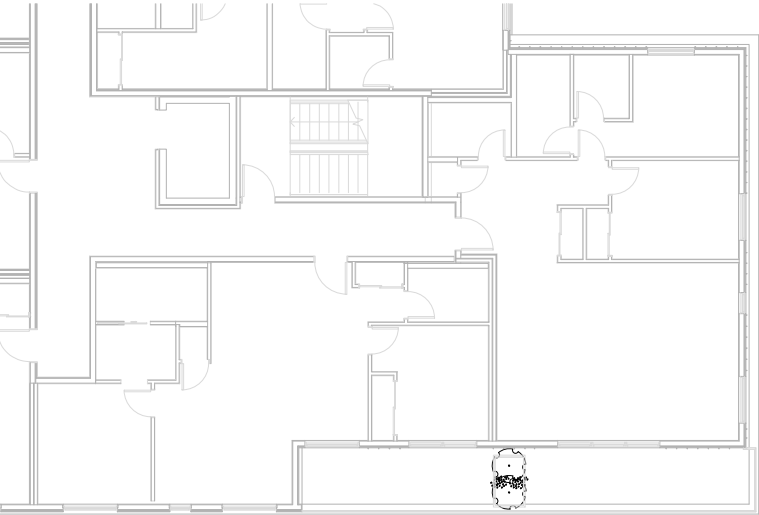
DRAWING TITLE:
SHRUB PLANTING PLAN
LEVEL 1

DWG NO:
L4.3

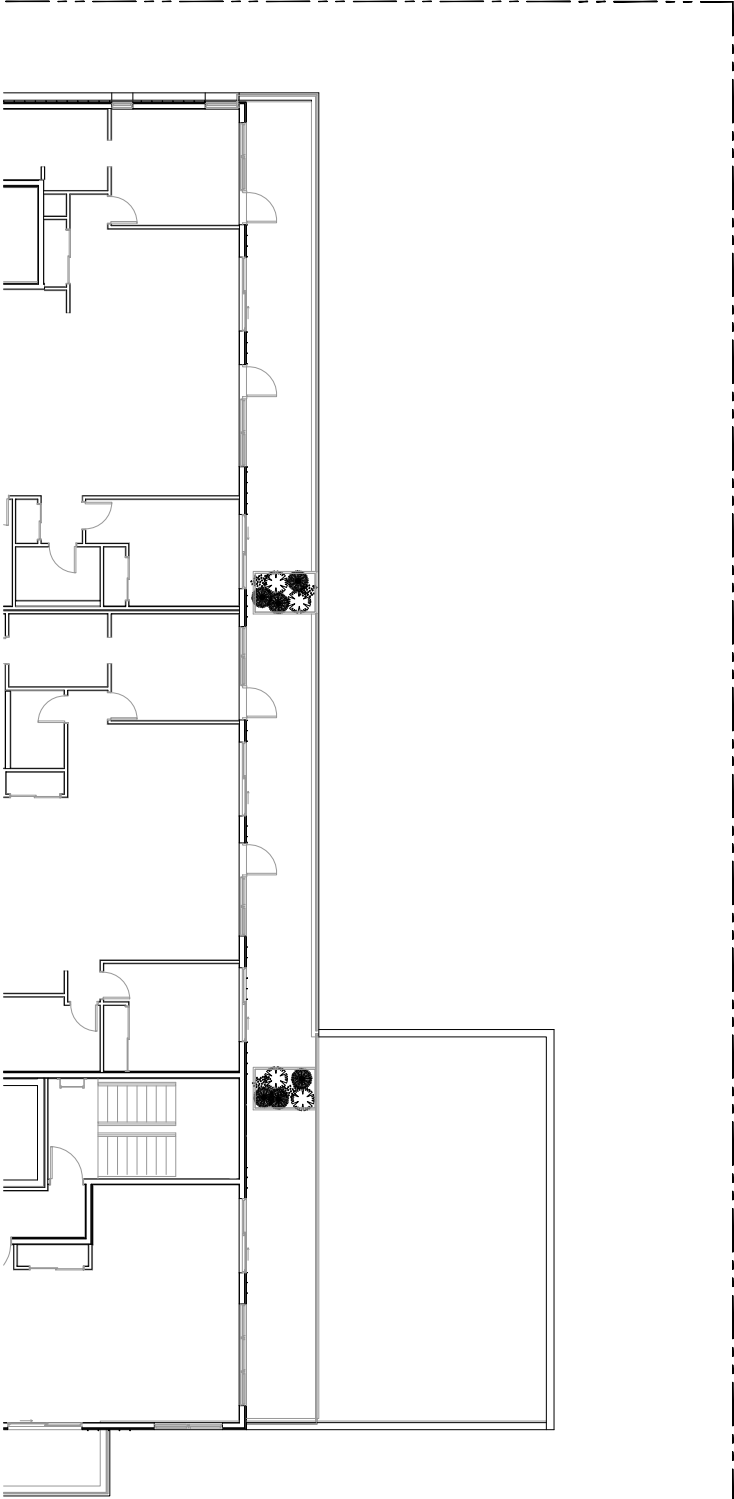
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1 LEVEL TWO PLANTING PLAN
L6 1:100



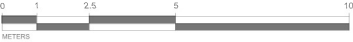
2 LEVEL THREE PLANTING PLAN
L6 1:100



3 LEVEL FOUR PLANTING PLAN
L6 1:100

PLANT SCHEDULE						
Quantity	Symbol	Latin Name	Common Name	Container	Native	Pollinator
6		Armeria maritima 'Alba'	White-flowered Thrift	10cm		y
10		Choisya ternata	Mexican Orange Blossom	#3		y
6		Pinus strobus 'Horsford Dwarf'	Horsford Dwarf Eastern white pine	#2		
4		Rhododendron x	Bloom-A-Thon® White Reblooming Azalea	#1		y
8		Parthenocussus quinquefolia	Virginia Creeper	#2		
NOTES: 1. PLANTS IN PLANT LISTS ARE SPECIFIED ACCORDING TO THE CANADIAN NURSERY LANDSCAPE ASSOCIATION CANADIAN STANDARDS FOR NURSERY STOCK AND SECTION 12, CONTAINER GROWN PLANTS FROM THE BC LANDSCAPE STANDARD, CURRENT EDITION.						

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- ALL PLANT MATERIAL TO BCSLA STANDARDS.
 - THE SEARCH ARE FOR PLANT MATERIAL IS THE PACIFIC NORTHWEST INCLUDING WASHINGTON, OREGON AND BRITISH COLUMBIA.
 - CONFIRM PLANTING LAYOUT WITH LANDSCAPE ARCHITECT ON SITE.
 - ALL SOIL VOLUMES TO MEET THE STANDARDS OF THE CITY OF VICTORIA. REFER TO SOIL DEPTH PLANS FOR SOIL DEPTH REQUIREMENTS.
 - ALL GROWING MEDIUM PROVISION AND INSTALLATION TO BE IN LINE WITH CANADIAN LANDSCAPE STANDARDS
 - REFER TO CIVIL AND MECHANICAL FOR DRAINS.
 - ALL ONSITE AND OFF-SITE LANDSCAPE AREAS TO BE IRRIGATED BY AN UNDERGROUND AUTOMATIC IRRIGATION SYSTEM. IRRIGATION TO BE SUPPLIED BY CONTRACTOR
 - PLANTING AROUND EXISTING CRITICAL TREE ROOT ZONES TO BE INSTALLED UNDER THE SUPERVISION OF A CERTIFIED ARBORIST.



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SEAL



NORTH ARROW

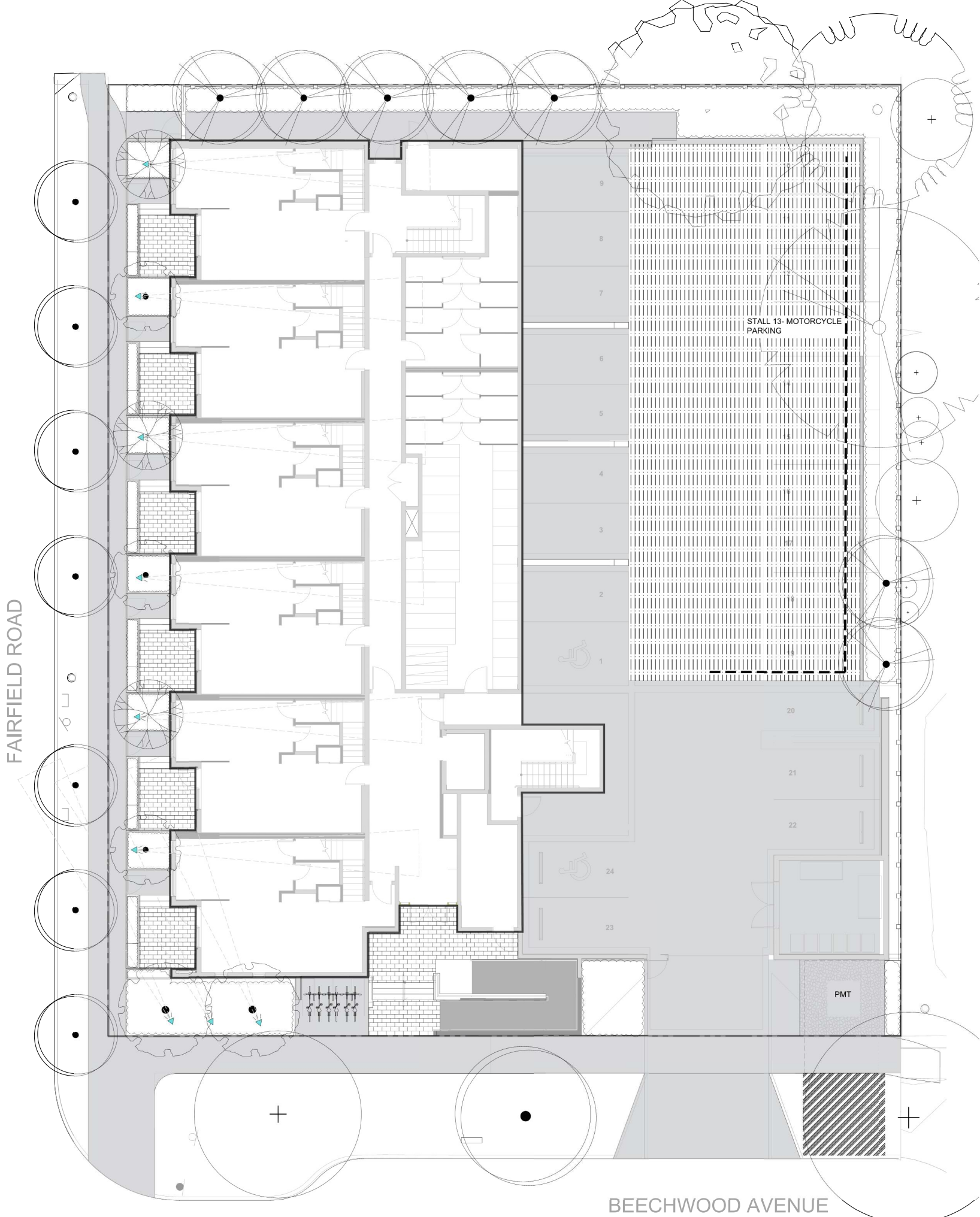


DRAWING TITLE:
SHRUB PLANTING PLAN
LEVELS 2, 3, & 4

DWG NO:

L4.4

SCALE: AS NOTED



LIGHTING SCHEDULE

SYMBOL	MANUFACTURER/MODEL/DESCRIPTION	QTY	DETAIL
	WAC Landscape Lighting 5111 LED Accent Mini Landscape Spotlight Order code: 5111, Aluminum Alloy, (BK) Black on Aluminum, Mounting Stake Lamp: 5111 Integrated LED, 18W/23VA, 2700K, Beamspread: Narrow Accessories: Additional Mounting Stake	9	



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SEAL



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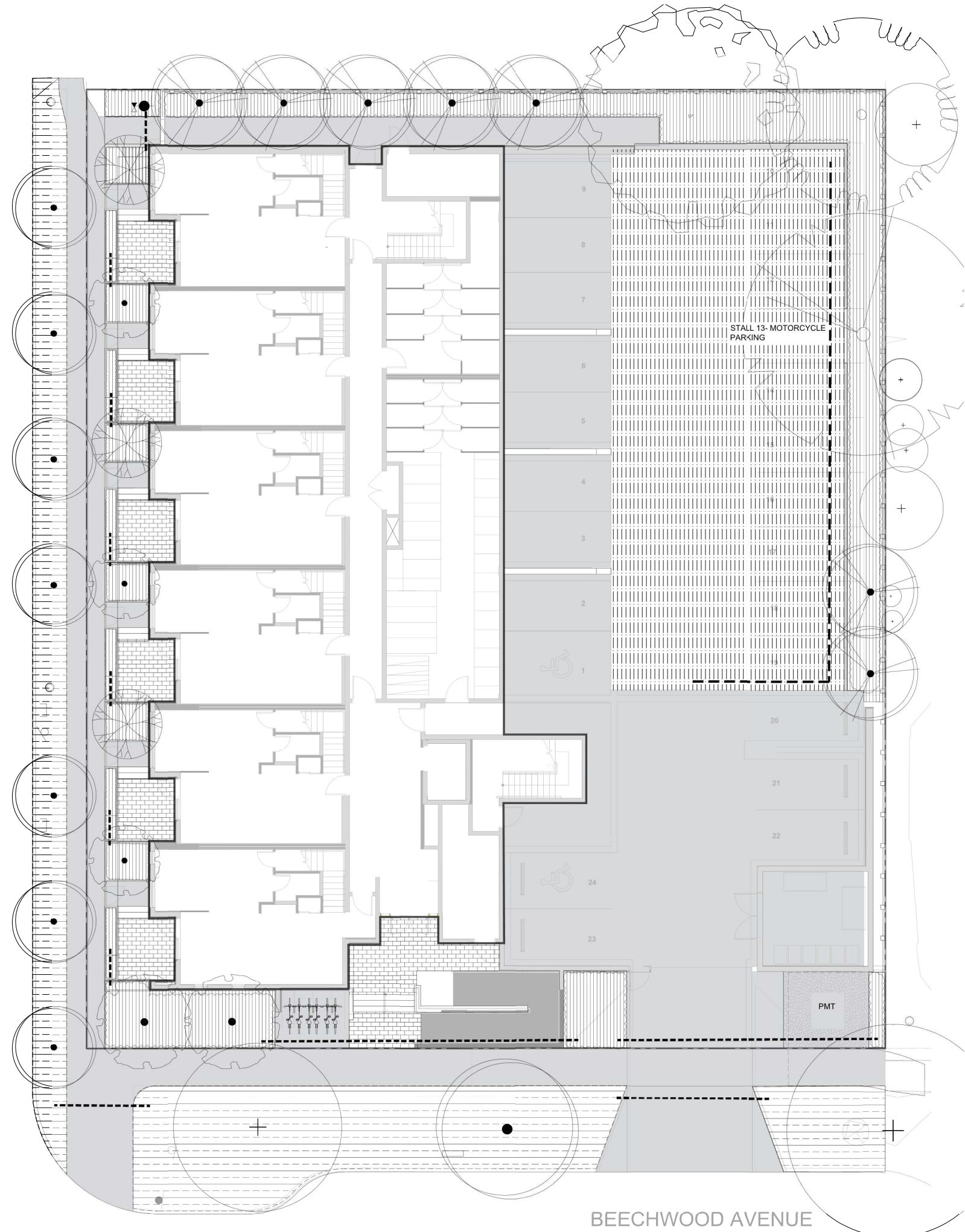
DRAWING TITLE:
LANDSCAPE LIGHTING PLAN
LEVEL 1

DWG NO:

L5.1

SCALE: 1:100

FAIRFIELD ROAD



BEECHWOOD AVENUE

LEGEND

- PIPE SLEEVE: PVC SCHEDULE 40 TWICE THE SIZE OF THE INTERIOR PIPE OF MIN. 50MM DIA.
- HOSE BIB
- SHUT OFF VALVE
- 2" PVC POINT OF CONNECTION
- AREA TO RECEIVE DRIPLINE- RAIN BIRD XFS-CV-09-12 SUB-SURFACE LANDSCAPE DRIPLINE WITH A HEAVY-DUTY .3 BAR CHECK VALVE. 3.41 LPH EMITTERS AT 30CM O.C. DRIPLINE LATERALS SPACED AT 30CM APART. WITH EMITTERS OFFSET FOR TRIANGULAR PATTERN. SPECIFY XF INSERT FITTINGS
- TREES IN THIS AREA TO RECEIVE ROTOR IRRIGATION

NOTES

- IRRIGATION PLANS TO BE UPDATED PRIOR TO IFC.
- OFFSITE TREES TO BE IRRIGATED BY SEPARATE IRRIGATION SYSTEM.
- ALL OFFSITE IRRIGATION TO FOLLOW THE CITY OF VICTORIA STANDARDS
- IRRIGATION DRIPLINE TO BE INSTALLED SUB-SURFACE AND SLEEVED UNDER METAL PLANTERS
- REFER TO MECHANICAL FOR HOSEBIB LOCATIONS



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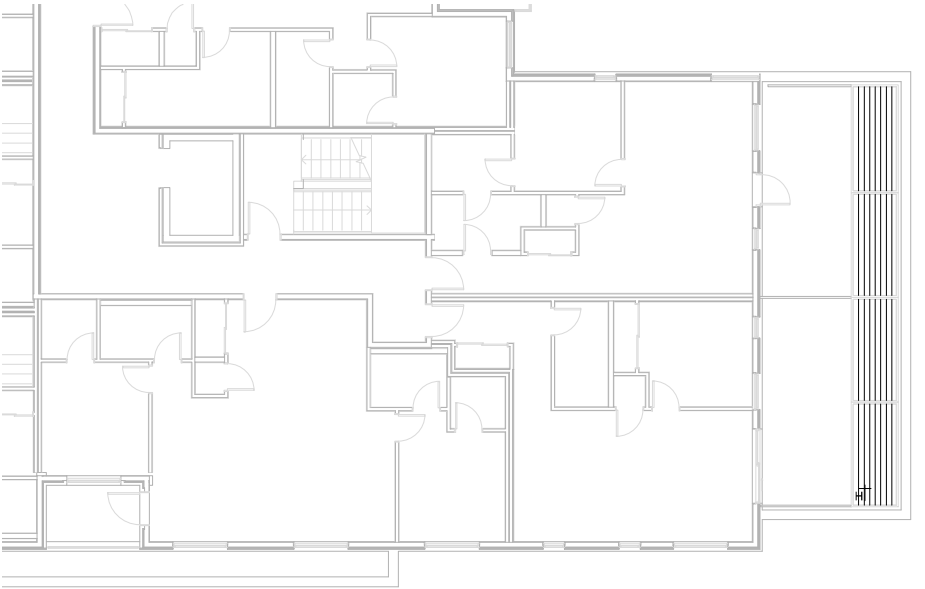


DRAWING TITLE:
IRRIGATED AREAS PLAN
LEVEL 1

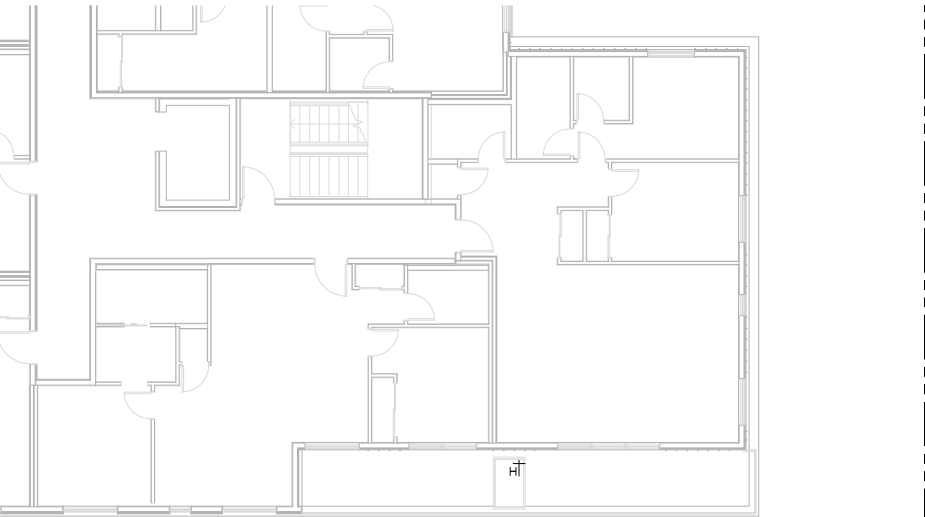
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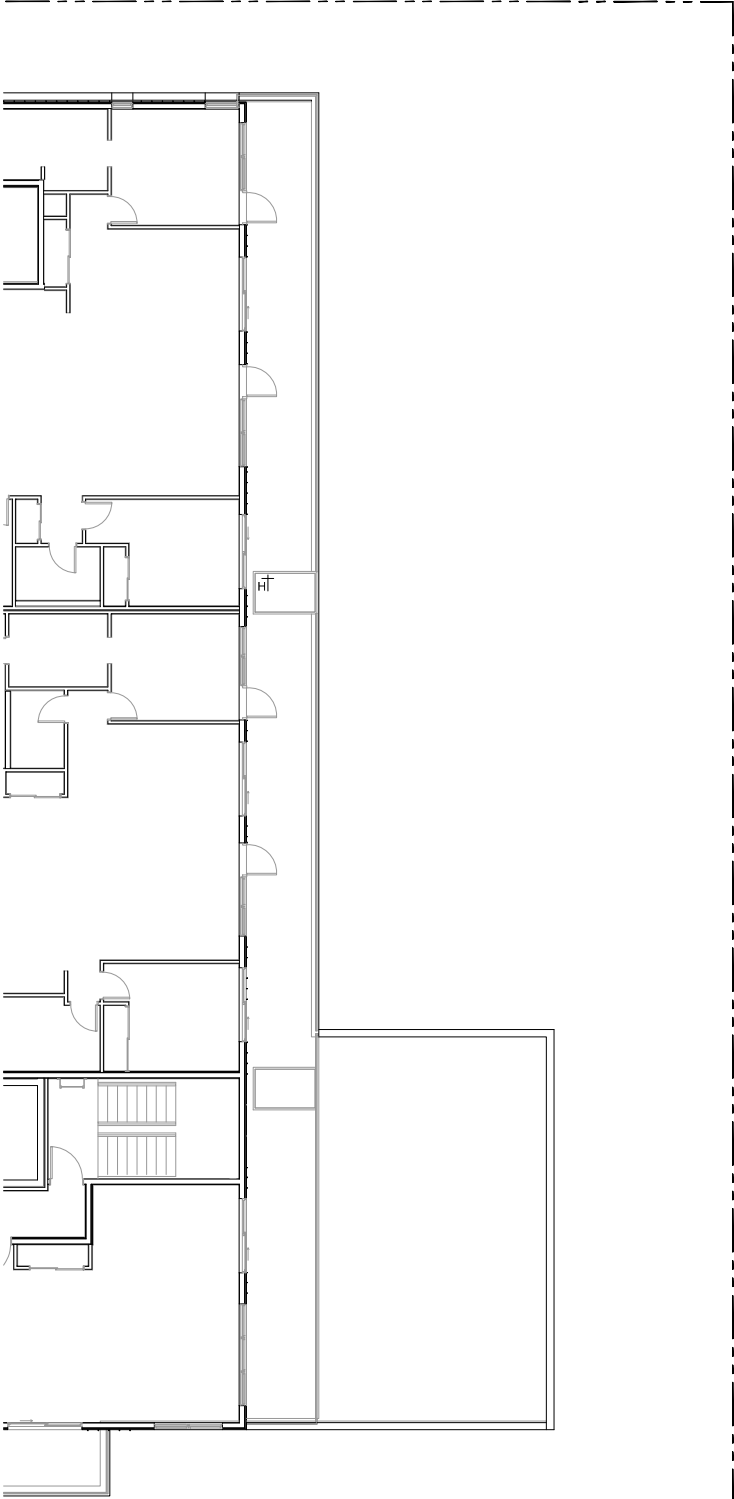
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1 LEVEL TWO IRRIGATION PLAN
L6.2 1:100



2 LEVEL THREE IRRIGATION PLAN
L6.2 1:100



3 LEVEL FOUR IRRIGATION PLAN
L6.2 1:100

LEGEND

PIPE SLEEVE: PVC SCHEDULE 40 TWICE THE SIZE OF THE INTERIOR PIPE OF MIN. 50MM DIA.

HOSE BIB

SHUT OFF VALVE

2" PVC POINT OF CONNECTION

AREA TO RECEIVE DRIPLINE- RAIN BIRD XFS-CV-09-12 SUB-SURFACE LANDSCAPE DRIPLINE WITH A HEAVY-DUTY 3 BAR CHECK VALVE. 3.41 LPH EMITTERS AT 30CM O.C. DRIPLINE LATERALS SPACED AT 30CM APART, WITH EMITTERS OFFSET FOR TRIANGULAR PATTERN. SPECIFY XF INSERT FITTINGS

TREES IN THIS AREA TO RECEIVE ROTOR IRRIGATION

NOTES

- IRRIGATION PLANS TO BE UPDATED PRIOR TO IFC.

- OFFSITE TREES TO BE IRRIGATED BY SEPARATE IRRIGATION SYSTEM.

- ALL OFFSITE IRRIGATION TO FOLLOW THE CITY OF VICTORIA STANDARDS

- IRRIGATION DRIPLINE TO BE INSTALLED SUB-SURFACE AND SLEEVED UNDER METAL PLANTERS

REFER TO MECHANICAL FOR HOSEBIB LOCATIONS



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NORTH ARROW

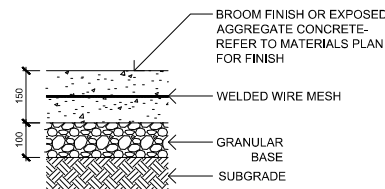


DRAWING TITLE:
IRRIGATED AREAS PLAN
LEVELS 2, 3, & 4

DWG NO:

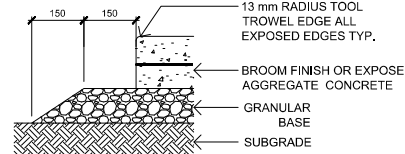
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SCALE: AS NOTED



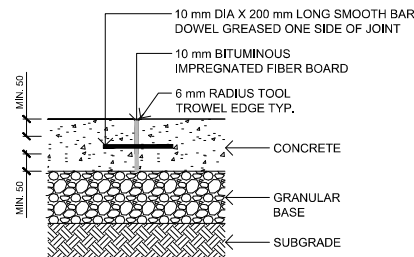
- SUBGRADE AND GRANULAR BASE TO BE COMPACTED TO MIN. 95% MPD.
- CONCRETE SIDEWALK TO MMCD/MUNICIPAL SPECIFICATIONS
- WELDED WIRE REINFORCEMENT MESH TO BE 6 X 6 X W2.9 X W2.9 (152 x 152 MW18.7/18.7) AND TO BE INSTALLED PER NRC NBC AND CAC STANDARDS.
- MIN 50 mm COVER ABOVE AND BELOW MESH.

1 CIP CONCRETE
1:10



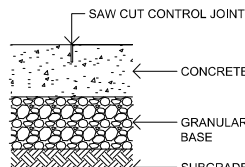
- EXTEND GRANULAR BASE 300 mm PAST EDGE OF SIDEWALK

2 CONCRETE EDGE
1:10



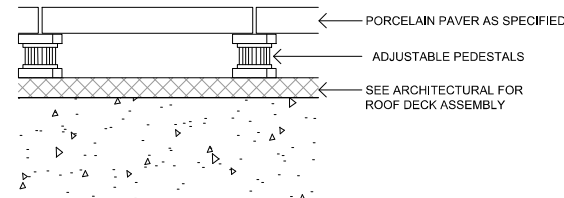
- BAR DOWELS @ 450 mm O.C. AND STARTING 300 mm FROM OUTSIDE EDGE OF CONCRETE - CENTER IN CONCRETE SLAB (MIN. 50 mm COVER)
- ISOLATION JOINT MAXIMUM SPACING 9.0 m
- JOINTING AS PER LAYOUT PLAN AND AT ALL ABUTTING CONCRETE STRUCTURES

3 CONCRETE EXPANSION JOINT
1:10



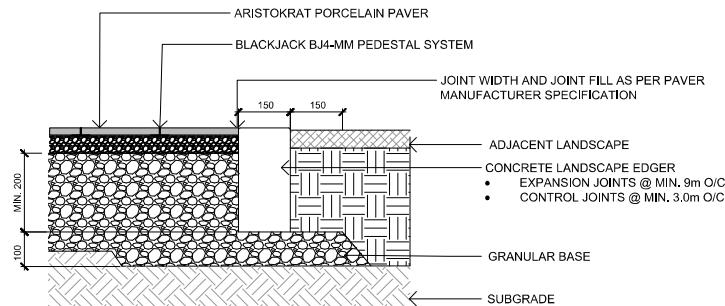
- JOINT DEPTH 1/3 SLAB THICKNESS
- CONTROL JOINT MAXIMUM SPACING 3.0 m
- JOINTING PATTERN AS PER LAYOUT PLAN

4 CONCRETE CONTROL JOINT
1:10



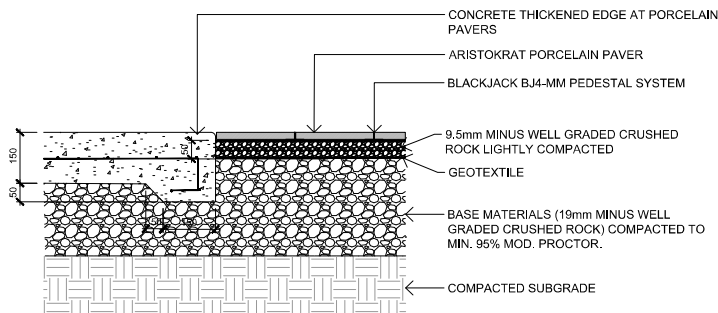
- PRIOR TO INSTALLATION, CONTRACTOR TO CONFIRM WITH ARCHITECTURAL / MEMBRANE CONSULTANT IF PROTECTION BOARD IS REQUIRED FOR PAVER PEDESTAL INSTALLATION; IF PROTECTION BOARD IS REQUIRED AND HAS NOT BEEN INSTALLED BY ARCHITECTURAL, CONTRACTOR TO NOTIFY GENERAL CONTRACTOR.
- ADJUSTABLE PEDESTALS TO BE BlackJack OneStep or BlackJack ScrewJack PEDESTALS AS REQUIRED OR APPROVED EQUAL.
- INSTALLATION TO MANUFACTURER'S SPECIFICATIONS.

5 PORCELAIN PAVER ON PEDESTAL
1:10

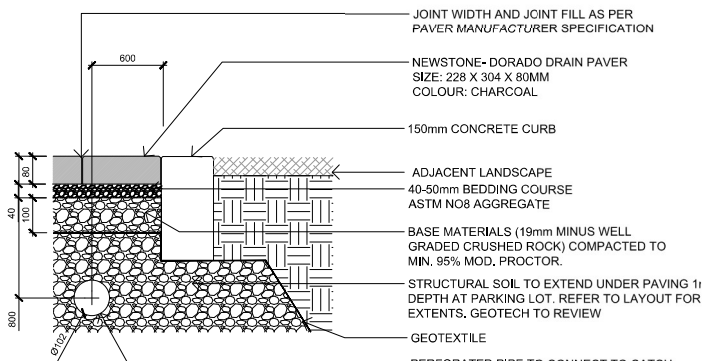


- SUBGRADE AND GRANULAR BASE TO BE COMPACTED TO MIN. 95% MPD.
- CONCRETE TO MMCD SPECIFICATION UNLESS NOTED OTHERWISE.

6 PAVER EDGER
1:10

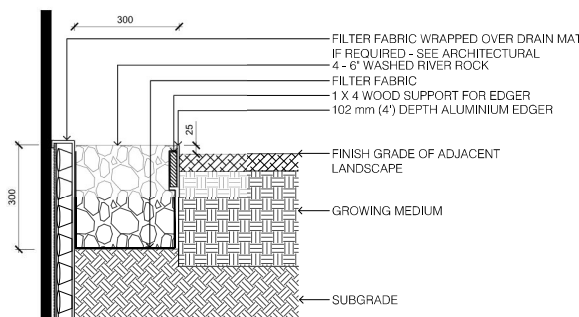
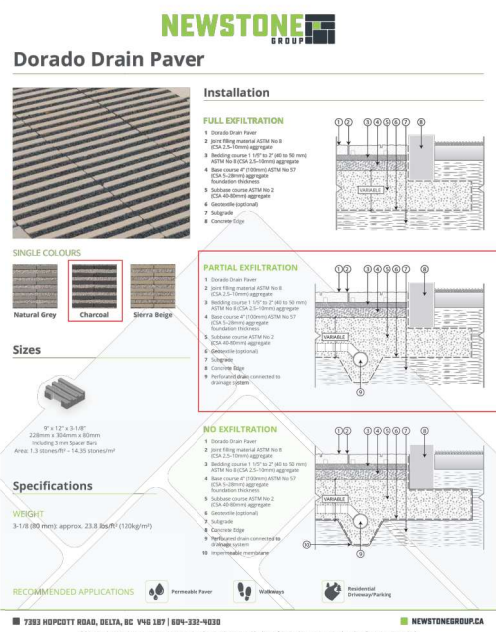


7 PAVER 1
1:10



- SUBGRADE AND GRANULAR BASE TO BE COMPACTED TO MIN. 95% MPD.
- GRANULAR BASE TO BE COMPACTED IN MAX. 100mm LIFTS.
- PAVER AND PAVER INSTALLATION TO MMCD AND MANUFACTURER'S SPECIFICATION UNLESS NOTED OTHERWISE.
- GEOTECH TO REVIEW PRIOR TO INSTALLATION.
- REFER TO CIVIL FOR CATCH BASIN LOCATIONS.
- DIAMETER OF PERFORATED PIPE TO BE DETERMINED BY STORMWATER ENGINEER.

8 PAVER 2
1:10



- LANDSCAPE EDGER TO BE PERMALOC CleanLine Commercial Grade Landscape Edging OR APPROVED EQUAL.
- 4.8mm (3/16") THICKNESS; 102mm (4") HEIGHT; 4.8m (16') LENGTH; NATURAL MILL FINISH; 305mm (12") STAKE.
- INSTALL EDGER AS PER MANUFACTURER'S INSTRUCTIONS.
- 1 X 4 WOOD SUPPORT TO BE STAKED IN PLACE AND USED TO ENSURE EDGER IS INSTALLED IN A STRAIGHT LINE AND MAINTAINS A STRAIGHT ALIGNMENT DURING THE INSTALLATION OF GROWING MEDIUM AND RIVER ROCK. MAY BE SCRAP WOOD AS LONG AS IT IS STRAIGHT.
- FILTER FABRIC TO BE POLYPROPYLENE FIBRE NONWOVEN GEOTEXTILE, SUCH AS NILEX 4551 OR APPROVED EQUAL.

9 DRIP STRIP
1:10

Aristokrat® Series

The Aristokrat® Series is perfect for terraces, roof decks, patios, court yards, swimming pools, water features or any traditional hardscape areas. Porcelain slabs deliver performance, style, versatility and are resistant to staining, fading, marking, chemicals and extreme weather. It is the ideal choice for residential or commercial outdoor applications but can also be used indoors for aesthetic continuity.



COLORS



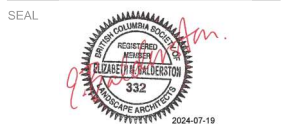
1608 Camosun Street, Victoria BC V8T 3E6
Info@biophilialcollective.ca 250 590 1156

OWNER/CLIENT:
ARYZE DEVELOPMENTS

PROJECT ADDRESS:
1733 FAIRFIELD,
VICTORIA, BC

DESIGNED BY: BIANCA BODLEY
DRAWN BY: TYLER YESTAL

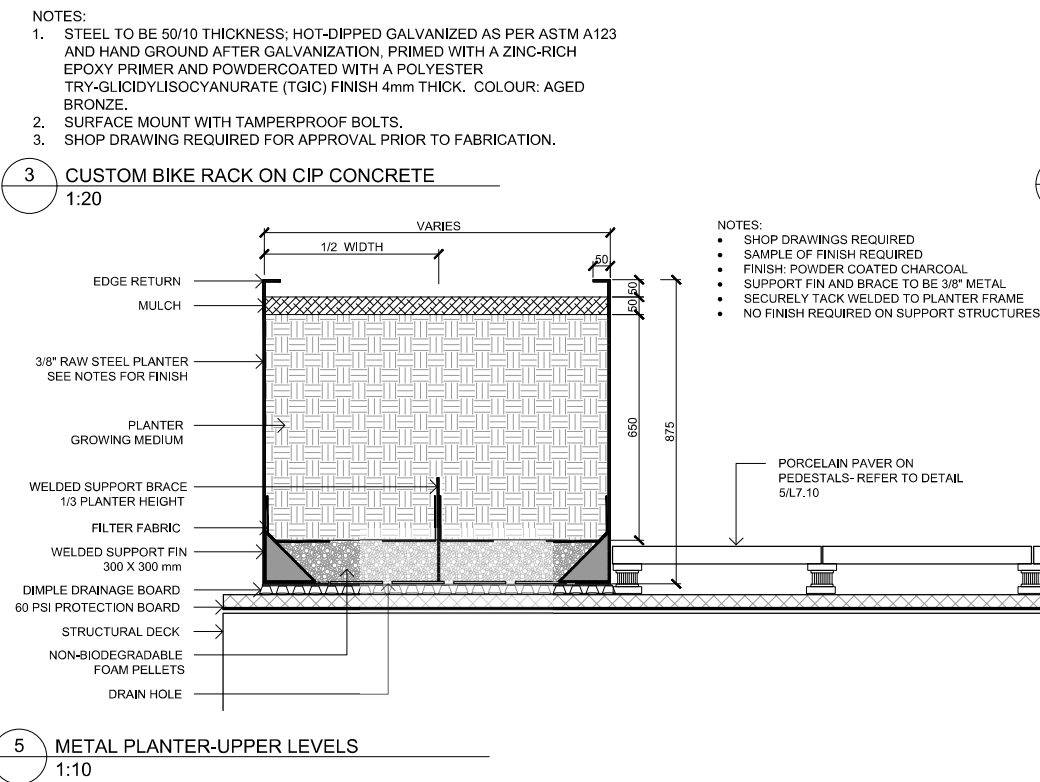
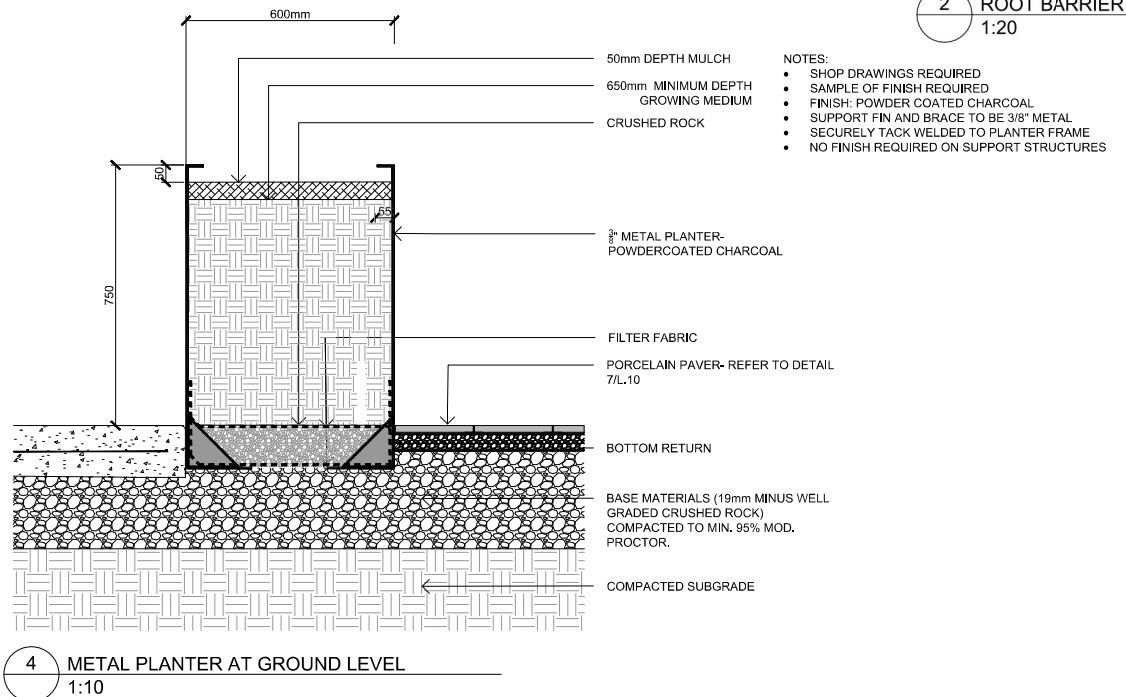
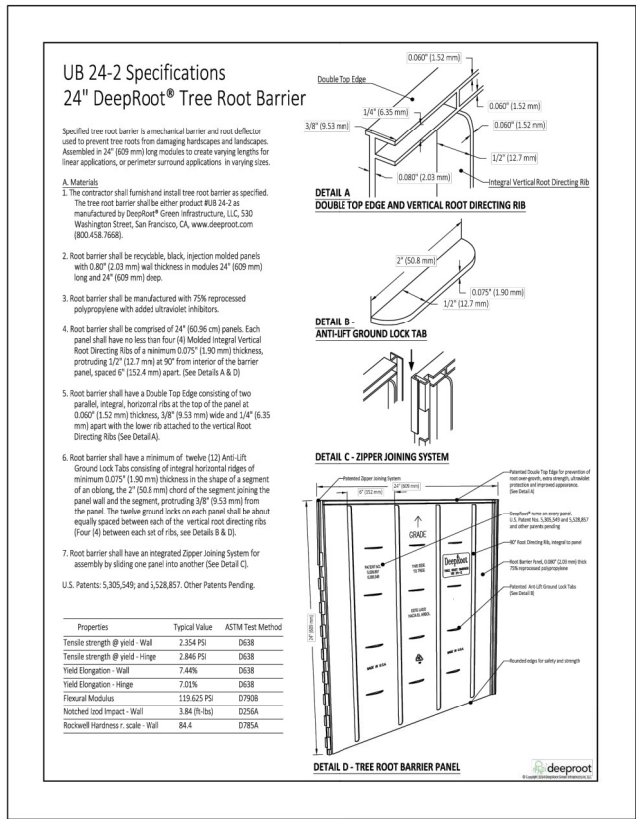
NO.	ISSUE	YY/MM/DD
1	ISSUED FOR DEVELOPMENT TRACKER	22/03/04
2	ISSUED FOR RZ/DP	22/05/02
3	ISSUED FOR COORDINATION	22/07/18
4	ISSUED FOR RZ/ DP	22/07/20
5	ISSUED FOR RZ/ DP RESUBMISSION	23/01/24
6	ISSUED FOR RZ/ DP RESUBMISSION	2023/08/18
7	25% BUILDING PERMIT	2023/10/10
8	ISSUED FOR RZ/ DP REV 5	2024/02/21
8	ISSUED FOR RZ/ DP REV 6	2024/07/19



DRAWING TITLE:
PAVING DETAILS

DWG NO:
L7.1

SCALE: AS NOTED



1608 Camosun Street, Victoria BC V8T 3E6
Info@biophiliccollective.ca 250 590 1156

OWNER/CLIENT:
ARYZE DEVELOPMENTS

PROJECT ADDRESS:

1733 FAIRFIELD,
VICTORIA, BC

DESIGNED BY: **BIANCA BODLEY**
DRAWN BY: **TYLER YESTAL**

NO.	ISSUE	YYMMDD
1	ISSUED FOR DEVELOPMENT TRACKER	22/03/04
2	ISSUED FOR RZ/DP	22/05/02
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5	ISSUED FOR RZ/ DP RESUBMISSION	23/01/24
6	ISSUED FOR RZ/ DP RESUBMISSION	2023/08/18
7	25% BUILDING PERMIT	2023/10/10
8	ISSUED FOR RZ/ DP REV 5	2024/02/21
8	ISSUED FOR RZ/ DP REV 6	2024/07/19

SEAL



NORTH ARROW

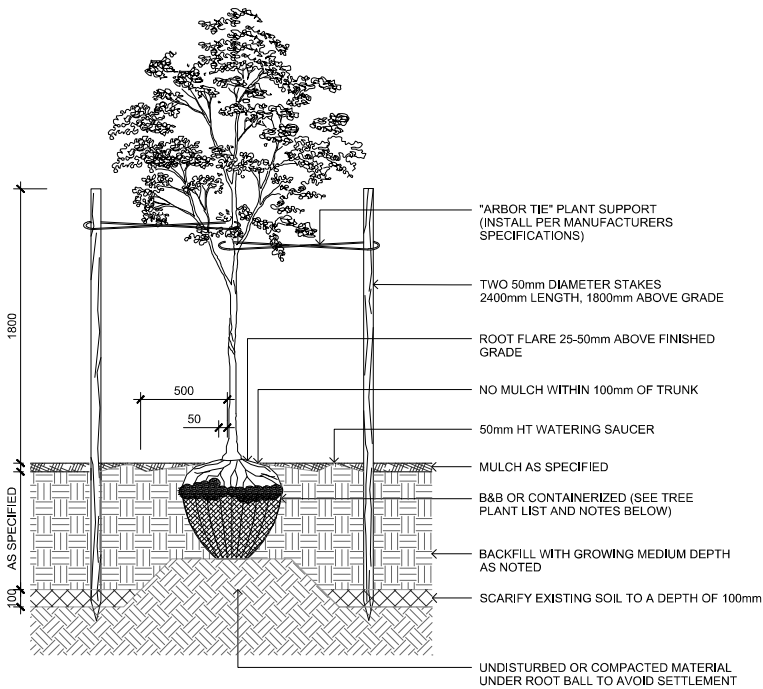
DRAWING TITLE

SITE FURNISHINGS DETAILS

DWG NO

SCALE: AS NOTED

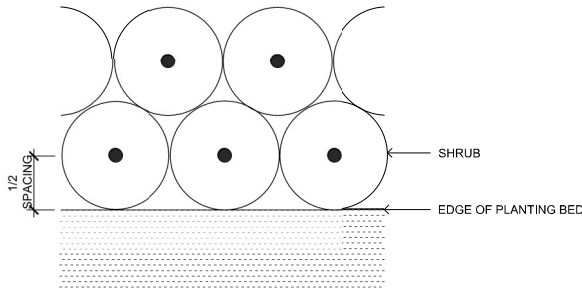
L7.2



- NOTES:
1. CONTRACTOR SHALL INSURE PERCOLATION OF ALL PLANTING PITS PRIOR TO INSTALLATION
 2. FOR B&B: REMOVE TOP 1/3 OF BURLAP AND CAGE PRIOR TO BACKFILLING WITH GROWING MEDIUM
 3. FOR CONTAINERS: LOOSEN ROOT STRUCTURE AND ENSURE NO CIRCLING ROOTS BY LIGHT ROOT PRUNING
 4. INSTALL TREE TIE AT APPROX. 50% OF TREE HT. FOR CONIFERS LESS THAN 3m HT. AND 100mm BELOW LOWEST BRANCH FOR DECIDUOUS LESS THAN 100mm CAL. DO NOT REMOVE OR CONSTRAIN ANY BRANCHES.
 5. STANDARD TREE PLANTING DETAILS TO BE APPLIED ACCORDINGLY TO TREES PLANTED IN PLANTERS
 6. TREE PLANTING ON MUNICIPAL PROPERTY TO MUNICIPAL STANDARD DETAIL

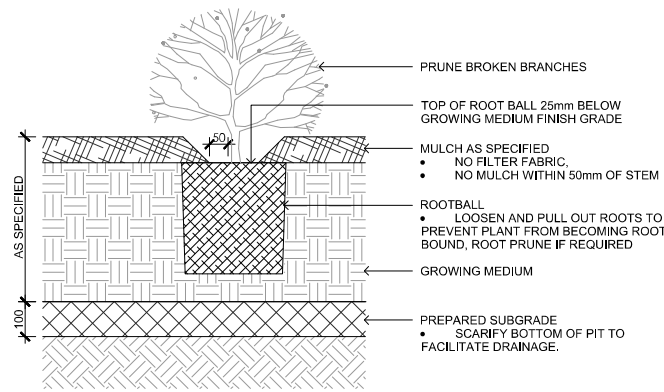
1 TREE PLANTING ON GRADE

1:20



4 PLANT SPACING PLAN

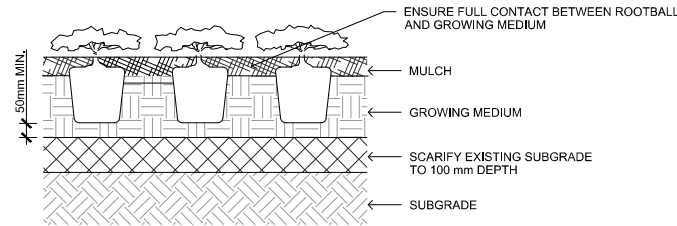
1:10



- NOTES:
1. SHRUB PLANTING AS PER WRITTEN SPECIFICATIONS
 2. GROWING MEDIUM AND MULCH AS PER WRITTEN SPECIFICATIONS

2 SHRUB PLANTING

1:10



- NOTES:
1. PLACE AND COMPACT SUBGRADE AS PER WRITTEN SPECIFICATIONS
 2. SUBGRADE TO BE COMPACTED TO A CONSISTENT 80% M.P.D
 3. GROWING MEDIUM AND MULCH AS PER WRITTEN SPECIFICATIONS

3 GROUNDCOVER PLANTING

1:10



1608 Camosun Street, Victoria BC V8T 3E6
Info@biophiliacollective.ca 250 590 1156

OWNER/CLIENT:
ARYZE DEVELOPMENTS

PROJECT ADDRESS:
1733 FAIRFIELD,
VICTORIA, BC

DESIGNED BY: BIANCA BODLEY
DRAWN BY: TYLER YESTAL

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8	ISSUED FOR RZ/ DP REV 5	2024/02/21
8	ISSUED FOR RZ/ DP REV 6	2024/07/19

SEAL



NORTH ARROW



DRAWING TITLE:
PLANTING DETAILS

DWG NO:
SCALE: AS NOTED

L7.3



Committee of the Whole Report

For the Meeting of December 7, 2023

To: Committee of the Whole **Date:** November 24, 2023

From: Karen Hoese, Director, Sustainable Planning and Community Development

Subject: Rezoning Application No. 00821, associated Official Community Plan Amendment, and Development Permit with Variances Application No. 00204 for 1733, 1735 and 1737 Fairfield Road
Rezoning Application No.00845 and associated Official Community Plan and Heritage Designation Amendments for 1964 Fairfield Road and 507 Foul Bay Road

RECOMMENDATION

OCP Amendments with Rezoning

1. That Council consider who is affected by the proposed changes to the *Official Community Plan* (OCP) and determine that those within a 200m radius of the subject properties will be affected.
2. That Council provide an opportunity for consultation pursuant to section 475 of the *Local Government Act*, and direct the Director of Sustainable Planning and Community Development to:
 - a. mail a notice of the proposed OCP Amendments to the persons within a 200m radius of the subject property
 - b. post a notice on the City's website inviting affected persons, organizations and authorities to ask questions of staff and provide written or verbal comments to Council for their consideration.
3. That Council consider that no consultation is necessary with the Capital Regional District Board; Councils of Oak Bay, Esquimalt and Saanich; the Songhees and Esquimalt First Nations; the School District Board; or the provincial or federal governments or their agencies because the proposed OCP Amendments do not affect them.
4. That Council instruct staff to prepare the necessary Official Community Plan Amendment Bylaws in accordance with Section 475 of the *Local Government Act* and the necessary Zoning Regulation Bylaw Amendments that would authorize the proposed development outlined in Rezoning Application No. REZ00821 for 1733, 1735 and 1737 Fairfield Road and proposed zoning changes outlined in Rezoning Application No. 00845 for 1964 Fairfield Road and 507 Foul Bay Road, that first and second reading of the Zoning

Regulation Bylaw Amendments be considered by Council and a public hearing date be set once the following conditions are met:

- a. the following revisions to the plans for 1733, 1735 and 1737 Fairfield Road to the satisfaction of the Director of Sustainable Planning and Community Development:
 - i. add an additional two-bedroom unit and convert a one-bedroom unit to a two-bedroom unit, as outlined in the applicant's letter, dated September 12, 2023
 - ii. improve the transition to lower density buildings (northeast building elevation)
 - iii. increase outdoor amenity space, which may include providing parking underground, reducing parking or adding a rooftop amenity area.
 - b. the following revisions to the plans for 1733, 1735 and 1737 Fairfield Road, to the satisfaction of the Director of Engineering and Public Works:
 - i. increase permeable paved area to improve onsite stormwater management
 - ii. to provide 50% of required long term bicycle parking stalls as standard ground mounted stalls.
 - c. plan revision for 1733, 1735 and 1737 Fairfield Road to accommodate more replacement trees required under the *Tree Protection Bylaw*, to the satisfaction of the Director of Parks, Recreation and Facilities.
5. That subject to approval in principle at the Public Hearing, the applicant prepare and execute the following legal agreements, with form satisfactory to the City Solicitor prior to adoption of the bylaws:
- a. provision of a 0.86m wide statutory right-of-way for 1733, 1735 and 1737 Fairfield Road, with terms to the satisfaction of the Director of Engineering and Public Works
 - b. provision of no less than nine two-bedroom units, six two-bedroom units with a den and one three-bedroom unit for 1733, 1735 and 1737 Fairfield Road, to the satisfaction of the Director of Sustainable Planning and Community Development
 - c. securing continued public access to the property at 1964 Fairfield Road, consistent with existing public access hours and locations and permitting temporary closures for private events, to the satisfaction of the Director of Sustainable Planning and Community Development
 - d. provision of transportation demand management measures for 1733, 1735 and 1737 Fairfield Road, to the satisfaction of the Director of Engineering and Public Works, including:
 - i. car share memberships and usage credits for all residential units;
 - ii. on-street electric car share vehicle;
 - iii. on-street (Beechwood Avenue), level 2, dual head electrical charger, and all associated infrastructure and connections;
 - iv. bicycle parking to accommodate oversized bicycles (10% of required long-term spaces);
 - v. bicycle parking with access to an electrical outlet (50% of required long-term spaces) and
 - vi. bicycle repair and maintenance station.
6. That the above Recommendations be adopted on the condition that they create no legal rights for the applicant or any other person, or obligation on the part of the City or its officials, and any expenditure of funds is at the risk of the person making the expenditure.

Development Permit with Variances Application (1733, 1735 and 1737 Fairfield Road)

That Council, after giving notice and allowing an opportunity for public comment at a meeting of Council, and after the public hearing for Rezoning Application No. 00821 and Rezoning Application No. 00845, if they are approved, consider the following motion:

- “1. That subject to the adoption of the necessary Zoning Regulation Bylaw Amendment, Council authorize the issuance of Development Permit with Variances No. 00204 for 1733, 1735 and 1737 Fairfield Road, in accordance with plans submitted to the Planning department and date stamped by Planning on August 22, 2023, subject to:
 - a. the proposed development meeting all City zoning bylaw requirements, except for the following variances:
 - i. reducing the minimum front yard setback (Beechwood Avenue) from 4.00m to 2.70m and increasing stairs projection from 1.80m to 2.73m;
 - ii. reducing the minimum rear yard setback from 10.00m to 2.60m;
 - iii. reducing the minimum side yard setback (Fairfield Road) from 4.00m to 2.30m;
 - iv. reducing the minimum side yard setback (southeast) from 6.00m to 0.50m;
 - v. increasing the maximum site coverage from 40% to 65%;
 - vi. decreasing the open site space from 50% to 23%;
 - vii. reducing vehicle parking from 40 spaces to 23 spaces;
 - viii. permitting long-term bicycle parking to be provided in a stacked format.
 - b. The property being consolidated into one lot.
2. That the Development Permit with Variances, if issued, lapses two years from the date of this resolution.”

Heritage Designation Amendment Application (1964 Fairfield Road and 507 Foul Bay Road)

That Council instruct the Director of Sustainable Planning and Community Development to:

1. prepare a heritage designation bylaw to amend Heritage Designation (1964 Fairfield Road) Bylaw (No. 530) No. 05-75 to add the property at 507 Foul Bay Road, that first and second reading of the bylaw be considered by Council and that a joint public hearing date be set with Rezoning Application No. 00821 and Rezoning Application No. 00845;
2. add the Statement of Significance for 1964 Fairfield Road and 507 Foul Bay Road, attached as Attachment G to this report, recognizing the building exterior and natural landscape elements as the historic features of the property, to the above noted heritage designation bylaw.

LEGISLATIVE AUTHORITY

This report discusses concurrent rezoning applications involving two nearby properties.

The first is a Rezoning Application (No. 00821), associated Official Community Plan Amendment and a Development Permit with Variances Application for 1733, 1735 and 1737 Fairfield Road.

Relevant Rezoning considerations for this property relate to:

- change of use to allow multiple dwellings
- new regulations pertaining to maximum floor space ratio and maximum floor area.

The relevant Development Permit with Variances considerations relate to:

- consistency with design guidelines
- impact of variances pertaining to setbacks, site coverage, open space and parking.

The second is a Rezoning Application (No. 00845), Heritage Designation Amendment and associated Official Community Plan Amendment for 1964 Fairfield Road and 507 Foul Bay Road.

Relevant Rezoning considerations for this property relate to:

- removal of existing permitted residential uses and addition of garden, restaurant, and gift shop as permitted uses
- new regulations pertaining to maximum floor space ratio, maximum floor area, and vehicle parking.

Enabling Legislation

In accordance with Section 479 of the *Local Government Act*, Council may regulate within a zone the use of land, buildings and other structures, the density of the use of the land, building and other structures, the siting, size and dimensions of buildings and other structures as well as the uses that are permitted on the land and the location of uses on the land and within buildings and other structures.

In accordance with Section 482 of the *Local Government Act*, a zoning bylaw may establish different density regulations for a zone, one generally applicable for the zone and the others to apply if certain conditions are met.

In accordance with Section 489 of the *Local Government Act*, Council may issue a Development Permit in accordance with the applicable guidelines specified in the *Official Community Plan, 2012* (OCP). A Development Permit may vary or supplement the Zoning Regulation Bylaw but may not vary the use or density of the land from that specified in the Bylaw.

In accordance with Section 611 of the *Local Government Act*, Council may designate real property, in whole or in part, as protected property. A heritage designation bylaw may apply to more than one property and may apply to landscape features identified in the bylaw.

EXECUTIVE SUMMARY

The purpose of this report is to present Council with information, analysis and recommendations for two concurrent proposals:

- a Rezoning, OCP Amendment and Development Permit with Variances application for 1733, 1735 and 1737 Fairfield Road, to rezone from the R1-G Zone, Gonzales Single Family Dwelling District to a new site-specific zone to permit a multiple dwelling at this location.
- a Rezoning, OCP Amendment and Heritage Designation Amendment application for 1964 Fairfield Road and 507 Foul Bay Road, to rezone from the RK-11 Zone, Fairfield Townhouse District (1964 Fairfield Road) and the R1-G Zone, Gonzales Single Family Dwelling District (507 Foul Bay Road) to a new site-specific zone to limit permitted uses and reduce the density to the existing density and uses, which include a garden, restaurant, and gift shop.

The proposal for 1964 Fairfield Road and 507 Foul Bay Road has been submitted to support the increase in density for the proposed development at 1733, 1735 and 1737 Fairfield Road as the development exceeds the density envisioned for Traditional Residential Urban Place Designation.

The Development Permit with Variances application for 1733, 1735 and 1737 Fairfield Road pertains to the proposed form, character, exterior design, finishes and landscaping, as well as variances related to setbacks, site coverage, open space and parking for a four-storey building containing 31 dwelling units.

The following points were considered in assessing the concurrent rezoning applications:

- The four-storey multiple dwelling proposed for 1733, 1735 and 1737 Fairfield Road, with a density of 1.77:1 floor space ratio (FSR), requires rezoning to a site-specific zone and an amendment to the *Official Community Plan* (OCP) to increase the density and height envisioned for a Traditional Residential property and allow for a multiple dwelling use.
- The creation of 31 new dwelling units proposed for 1733, 1735 and 1737 Fairfield Road includes two studio, 12 one-bedroom, nine two-bedroom, six two-bedroom with den, and two three-bedroom units, which advances housing objectives to provide a diversity of housing, including family-oriented housing.
- The proposed 0.86m statutory right-of-way (SRW) along Fairfield Road would provide space for a two-metre sidewalk and a planted buffer of small canopy trees between pedestrians and vehicles, which supports the OCP's transportation and placemaking policies.
- The proposal for 1733, 1735 and 1737 Fairfield Road is inconsistent with specific policies in the *Gonzales Neighbourhood Community Plan* (2002) to retain the single-family character and preserve landscape features on public and private properties.
- A rezoning and associated OCP amendment is also proposed for 1964 Fairfield Road and 507 Foul Bay Road to rezone the site to permit only the existing uses and to redesignate from the Traditional Residential Urban Place Designation to the Public Facilities, Institutions, Parks and Open Space Urban Place Designation – this will better align the property's designation with the actual use of the properties.
- The proposal for 1964 Fairfield Road and 507 Foul Bay Road advances OCP objectives to maintain the heritage and cultural value of individual properties, conserve natural and built heritage including the urban forest.
- The application to expand heritage designation to include the maintenance and support area for Abkhazi Garden (located at 507 Foul Bay Road) and the commitment to secure continued public access to Abkhazi Garden will provide heritage protection to the entire site and provide certainty that public access will remain in perpetuity.
- The proposal for 1964 Fairfield Road and 507 Foul Bay Road is consistent with the *Gonzales Neighbourhood Community Plan*, which emphasizes preservation and enhancement of heritage as well as natural landscape features on both private and public properties.

The following points were considered in assessing the Development Permit with Variances for 1733, 1735 and 1737 Fairfield Road:

- The proposed form and character of the development is largely consistent with design guidelines applicable to Development Permit Area (DPA) 16, General Form and Character; however, revisions are recommended as follows:
 - provide a transition in height on the northeast building elevation
 - reduce space allocated to vehicle parking or provide underground

- increase amenity space, such as a landscaped rooftop amenity.
- The proposed site planning is consistent with specific *Design Guidelines for Multi-Unit Residential, Commercial and Industrial Development*; however, the combination of a reduced 2.6m northeast setback and four-storey height should provide a transition in its form and massing to the abutting neighbour at 1745 Fairfield Road.
- The proposed 65% lot coverage and 23% open space would benefit from a plan revision to either incorporate outdoor amenity space or reduce proposed parking, or a combination of both.
- Requested variances to front and flanking street setbacks are considered supportable to allow building siting that will create a sense of enclosure and to maximize rear and interior side yard setbacks.
- The requested variance to reduce the side yard setback (southeast) is supportable to incorporate the parking entrance into the building envelope, which reduces the visibility of vehicle parking from the public realm.
- Requested reductions to the rear yard setback, site coverage and open space are not supported as proposed but would be considered supportable with revisions as described above.
- The variances to reduce vehicle parking from 40 spaces to 23 spaces and permitting long-term bicycle parking to be provided in a stacked format are considered supportable based on the provision of a comprehensive package of transportation demand management (TDM) measures.

BACKGROUND

Description of Proposals

1733, 1735 and 1737 Fairfield Road

This proposal for 1733, 1735 and 1737 Fairfield Road is to rezone from the R1-G Zone, Gonzales Single Family Dwelling District to a new site-specific zone to permit construction of a four-storey multiple dwelling containing 31 units. The proposal includes demolition of three existing single-family dwellings and consolidation of the three lots. An OCP amendment is required to permit a four-storey multiple dwelling with a density of 1.77:1 FSR on a site within the Traditional Residential Urban Place Designation.

The following differences from the standard URMD Zone, Urban Residential Multiple Dwelling District are being proposed and would be accommodated in the new zone:

- reduce maximum FSR from 2:1 to 1.77:1
- reduce minimum lot area from 1840m² to 1740m².

The associated Development Permit with Variances (DPV) Application is for a four-storey building containing a total of 31 dwelling units, with a 23-space surface parking area.

Specific details include:

- six ground-oriented, two-level units accessed from Fairfield Road, main entry to 25 single-level units on Beechwood Avenue
- unit mix consisting of two studio units, 12 one-bedroom units, nine two-bedroom units, six two-bedroom units with dens, and two three-bedroom units
- partially covered surface parking (23 spaces including visitor and accessible).

Exterior materials include:

- light brick (first three levels)
- light blue metal panel (levels three and four)
- wood grain finish and perforated metal (ground level main entry on Beechwood)
- metal picket balconies.

Landscape elements include:

- 23% of lot area as open space
- 25 units with private balconies, six ground-oriented units with ground level patio space
- permeable parking for a portion of the parking area
- 26 trees identified – 16 on-site, seven off-site and three within Beechwood boulevard
- nine trees retained – the seven offsite private trees and two within Beechwood boulevard
- 17 trees removed- seven bylaw protected trees, 10 non-protected
- 18 new trees proposed on-site and eight boulevard trees (seven on Fairfield, one on Beechwood).

The proposed variances from the standard URMD Zone as well as parking regulations in Schedule C are related to:

- reducing the minimum front yard setback (Beechwood Avenue) from 4.00m to 2.70m and increasing the stair projection from 1.80m to 2.73m
- reducing the minimum rear yard setback from 10.00m to 2.60m
- reducing the minimum side yard setback (Fairfield Road) from 4.00m to 2.30m
- reducing the minimum side yard setback (southeast) from 6.00m to 0.50m
- increasing the maximum site coverage from 40% to 65%
- decreasing the open site space from 50% to 23%
- reducing vehicle parking from 40 spaces to 23 spaces
- permitting long-term bicycle parking to be provided in a stacked format.

1964 Fairfield Road and 507 Foul Bay Road

The concurrent proposal for 1964 Fairfield Road and 507 Foul Bay Road is to rezone the subject properties from the RK-11 Zone, Fairfield Townhouse District (1964 Fairfield Road) and the R1-G Zone, Gonzales Single Family Dwelling District (507 Foul Bay Road) to a new site-specific zone to limit permitted uses and density to allow only for the existing garden, restaurant, and gift shop. In addition, an OCP amendment is proposed to redesignate from the Traditional Residential Urban Place Designation to the Public Facilities, Institutions, Parks and Open Space Urban Place Designation. There is also a proposed amendment to the heritage designation bylaw to add the adjacent property 507 Foul Bay Road, which contains the greenhouse, and the garden's support and maintenance spaces, and to revise the Statement of Significance.

For the property at 1964 Fairfield Road, the following differences from the current RK-11 Zone are being proposed and would be accommodated in the new zone:

- remove existing permitted uses (single-family dwellings with secondary or garden suites, two-family dwellings, attached dwellings, hospitals, nursing homes and home occupations) except public buildings and accessory buildings, and add garden, restaurant, and gift shop as new permitted uses
- reduce maximum FSR from 0.55:1 to 0.04

- add new regulations pertaining to parking.

For the property at 507 Foul Bay Road, the following differences from the current R1-G Zone, Fairfield Townhouse District, are being proposed and would be accommodated in the new zone:

- remove existing permitted uses (single-family dwellings with secondary or garden suites and house conversions) except accessory buildings.

Land Use Context

1733, 1735 and 1737 Fairfield Road

The area, as shown in Figure 1, is characterized by a mix of mostly single-family dwellings as well as older attached dwellings, duplex dwellings, and a recently constructed stacked townhouse development containing 20 dwelling units and density of 0.85:1 FSR. In addition, there are small scale commercial properties and Hollywood Park to the southwest.

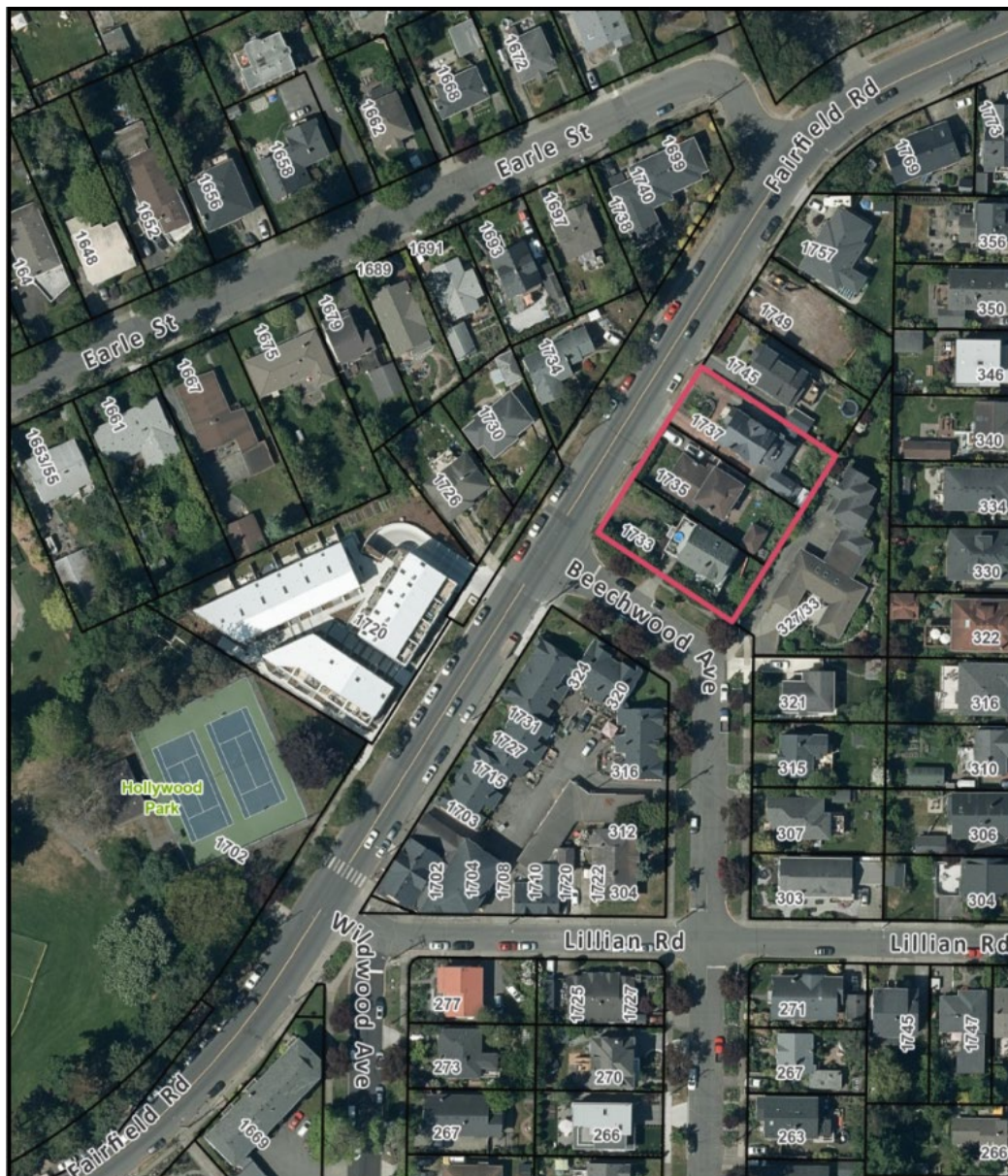
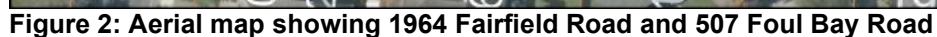


Figure 1: Aerial map showing 1733, 1735 and 1737 Fairfield Road

The area, as shown in Figure 2, is characterized by single-family dwellings some of which are located on similarly large panhandle lots. A five-unit heritage house conversion abuts the property to the north, which is currently proposed for development that would add a 12-unit multiple dwelling development while retaining the heritage building. In addition, Margaret Jenkins Elementary is located to the west of the subject property. Heritage registered and heritage designated properties exist in the surrounding area.



Existing Site Development and Development Potential

1733, 1735 and 1737 Fairfield Road

The site is comprised of three legal lots and is developed with three single-family dwellings. Under the current R1-G Zone, Gonzales Single Family Dwelling District, the three lots could each be developed with a single-family dwelling with either a secondary suite or garden suite, for a total of six dwelling units. In addition, the three lots could potentially be developed under *Missing Middle Regulations* as a combination of corner townhouses and houseplexes with up to 24 dwelling units depending on unit sizes and site design.

1964 Fairfield Road and 507 Foul Bay Road

This site is presently used as a garden with an accessory restaurant and gift shop. The property at 507 Foul Bay Road is limited to use as the maintenance and support area.

Under the current RK-11 Zone, Fairfield Townhouse District, the property at 1964 Fairfield Road permits single-family dwellings with secondary or garden suites, two-family dwellings, attached dwellings, public buildings, hospitals, nursing homes and home occupations. However, the property is heritage designated; therefore, development potential would be limited as the designation protects the existing buildings and the building siting, as well as various site features and views from the existing building.

Data Tables

The following data table compares the proposal for 1733, 1735 and 1737 Fairfield Road with the R1-G Zone, Gonzales Single Family District and the standard URMD Zone, Urban Residential Multiple Dwelling District. An asterisk is used to identify where the proposal does not meet the requirements of the existing Zone.

Zoning Criteria	Proposal	Existing R1-G Zone	Zone Standard (URMD Zone)	OCP
Site area (m2) (min.)	1741.93*	460.00	1840.00	-
Lot width (m) (min.)	38.10	15.00	n/a	-
Combined floor area (m ²) (max.)	3082.22*	300.00	n/a	-
Floor space ratio (ratio) (max.)	1.77:1*	0.50:1	2.00:1	Approximately 1.10:1
Height (m) (max.)	13.39*	7.60	18.50	-
Storeys (max.)	4.00*	2.00/1.50 with Basement	6.00	Approximately 3.00
Roof deck	Yes	Not permitted	n/a	-
Setbacks (m) – minimum				-

Zoning Criteria	Proposal	Existing R1-G Zone	Zone Standard (URMD Zone)	OCP
Front yard (Beechwood)	2.72*	7.50	4.00	-
Projections – porch (max.)	1.026	1.60	1.80	-
Projections – stairs less than 1.7m in height (max.)	2.73*	2.50	1.80	-
Rear (NE)	2.61*	13.72 (30% of lot depth)	10.00	-
Side (Fairfield)	2.32*	5.72 (15% of lot width)	“6.00 – interior lot line 4.0 – flanking street”	-
Side (SE)	0.50*	5.72 (15% of lot width)	“6.00 – interior lot line 4.0 – flanking street”	-
Combined side yards	2.82*	5.40	n/a	-
Site coverage (%) (max.)	65.00*	30.00	40.00	-
Open site space – lot (%) (min.)	23.00*	50.00	50.00	
Parking	21 – Units* 2 – Visitor* 23 Total* (2 Van accessible)	37 – Units 3 – Visitor 40 Total	37 – Units 3 – Visitor 40 Total	-
Bicycle – Long Term	54.00	38.00	38.00	-
Bicycle – Short Term	6.00	6.00 (rack)	6.00 (rack)	-
Driveway/parking slope (%)	6.00	8.00	8.00	-

The following data table compares the concurrent proposal for 1964 Fairfield Road and 507 Foul Bay Road with the R1-G Zone (existing zoning for 507 Foul Bay Road), and the RK-11 Zone, Fairfield Townhouse District (existing zoning for 1964 Fairfield Road). An asterisk is used to identify where the proposal does not meet the requirements of the existing Zone.

Zoning Criteria	Proposal	Zone Standard (R1-G)	Zone Standard (RK-11)
Lot area (m ²) (min.)	5664.60 - 1964 Fairfield Road 793.10 - Foul Bay Road 6,457.70 - Total Site	460	Greater of 555 or 470 per dwelling unit
Use	Garden, Restaurant, and Gift Shop*	Single Family Dwelling	Single-, Two-family, Attached Dwellings
Lot width (m) (min.)	51.24 - Fairfield Road 41.08 - Foul Bay Road	15	18
Total Floor Area	176.79 - Fairfield Road	n/a	n/a
Floor space ratio (max.)	0.03 - Fairfield Road 0.00 - Foul Bay Road	0.5:1	0.55
Storeys (max.)	1	2/1.5 w/ Basement	2.6
Setbacks (m)			
Front	41.81	7.50	6.00
Rear	50.19	9.10 or 30% of lot depth	7.50 - living room 2.50 blank wall/ 4.00 habitable rooms/ 7.50 living room
Side	10.42	1.50 or 15% of lot width	2.5 blank wall/ 4.0 habitable rooms/ 7.5 living room
Parking (min.)	0* (Existing)	Schedule C 6	Schedule C 6
Short term bicycle parking – minimum	14	Schedule C 1	Schedule C 1
Accessory Building Location	Side Yard * Rear Yard	Schedule F Rear Yard	Schedule F Rear Yard

Sustainable Mobility

1733, 1735 and 1737 Fairfield Road

The application proposes the following features which support multi-modal transportation:

- car share memberships and usage credits for all residential units
- on-street electric car share vehicle
- level 2 dual-head on street electric vehicle charger and all associated infrastructure and

connections

- bicycle parking to accommodate oversized bicycles (10% of required long-term spaces)
- bicycle parking with access to an electrical outlet (50% of required long-term spaces)
- bicycle repair and maintenance station.

The application is showing more long-term bicycle parking than is required in the bylaw; however, it should be noted that additional information is required from the applicant to confirm long-term bicycle parking spaces meet minimum dimensions required for long-term bicycle parking. It is possible that the long-term bicycle will need to be marginally reduced to meet the minimum required dimensions.

1964 Fairfield Road and 507 Foul Bay Road

This application proposes new short-term bike parking beyond bylaw requirements, which support multi-modal transportation objectives.

Public Realm

1733, 1735 and 1737 Fairfield Road

The following public realm improvements are proposed in association with this application:

- standard frontage works, including a curb extension at the Beechwood Avenue intersection with Fairfield Road, including pedestrian ramps and tactile domes, to narrow crossing distance and improve pedestrian accessibility and safety
- provision of a level two dual head electric vehicle charging station on Beechwood Avenue, including all necessary electrical connections, for the by the proposed car share vehicle, and for general public use.

The charging station would be secured with a Section 219 covenant, registered on the property's title, prior to Council giving final consideration of the proposed Zoning Regulation Bylaw Amendment.

1964 Fairfield Road and 507 Foul Bay Road

No public realm improvements beyond City standard requirements are proposed in the application for 1964 Fairfield Road and 507 Foul Bay Road.

Community Consultation

1733, 1735 and 1737 Fairfield Road

Consistent with the *Community Association Land Use Committee (CALUC) Procedures for Processing Rezoning and Variance Applications*, prior to submission of the application, it was posted on the Development Tracker along with an invitation to complete a comment form on March 11, 2022. Mailed notification was sent to owners and occupiers of property within 200m of the subject property advising that a consultation process was taking place and that information could be obtained and feedback provided through the Development Tracker. A sign was also posted on site, to notify those passing by of this consultative phase.

The applicant participated in an initial meeting with the CALUC on March 28, 2022. Additional meetings were held on March 27, 2023 and October 23, 2023 to share changes to the application

that increased the density and unit count of the proposal. Summaries of the three meetings along with the comment forms are attached to this report.

The associated application proposes variances, therefore, in accordance with the City's *Land Use Procedures Bylaw*, it requires notice, sign posting and a meeting of Council to consider the variances.

1964 Fairfield Road and 507 Foul Bay Road

Consistent with the *Community Association Land Use Committee (CALUC) Procedures for Processing Rezoning and Variance Applications*, prior to submission of the application, it was posted on the Development Tracker along with an invitation to complete a comment form on March 10, 2023. Mailed notification was sent to owners and occupiers of property within 200m of the subject property advising that a consultation process was taking place and that information could be obtained and feedback provided through the Development Tracker. A sign was also posted on site, to notify those passing by of this consultative phase.

The applicant participated in a meeting with the CALUC on March 27, 2023, which was a combined meeting that included revisions to the proposal at 1733, 1735 and 1737 Fairfield Road. An additional meeting was held on October 23, 2023, to share changes to the concurrent application at 1733, 1735 and 1737 Fairfield Road that increased the density and unit count of that proposal. Summaries of the two meetings along with the comment forms are attached to this report.

ANALYSIS

Official Community Plan (OCP) Amendment Applications

An amendment to the *Official Community Plan (OCP)* is required for the proposal at 1733, 1735 and 1737 Fairfield Road to increase the density and height envisioned for a Traditional Residential property and allow for a multiple dwelling use. The additional proposed OCP amendment pertaining to 1964 Fairfield Road and 507 Foul Bay Road is to redesignate the subject properties from the Traditional Residential Urban Place Designation to the Public Facilities, Institutions, Parks and Open Space Urban Place Designation.

The two proposed amendments to the OCP have been submitted to permit construction of a four-storey multiple dwelling containing 31 units at 1733, 1735 and 1737 Fairfield Road while reducing permitted density and permitted uses to that which exists currently for 1964 Fairfield Road and 507 Foul Bay Road. Staff recommend that the proposals, when considered as a whole, are supportable if revisions are made to the proposal for 1733, 1735 and 1737 Fairfield Road to address concerns with the massing and height in the northeast elevation as well as a lack of usable amenity space. The rationale for considering the proposals supportable based on OCP policy is outlined in detail below. The rationale for the recommended revisions is contained in the analysis of the Development Permit with Variances Application to follow.

The proposed development for 1733, 1735 and 1737 Fairfield Road is to construct a four-storey multiple dwelling with a density of 1.77:1 FSR. The development would exceed the envisioned density and height of up to approximately 1.1:1 FSR and three-storeys for Traditional Residential areas, and the multiple dwelling use is not a ground-oriented building form as defined in the OCP because less than half the total number of units have direct access to the outdoors. Further, an OCP strategic direction for Gonzales is to maintain and enhance neighbourhood character including the heritage character, encouraging a ground-oriented Traditional Residential area.

However, the OCP emphasizes that designations are intended as general guidance and there are a range of uses, densities and built forms that may be approved depending on the existing and envisioned context of the site, block, and neighbourhood.

The proposal is located along Fairfield Road between two Small Urban Villages, namely Fairfield Plaza and Fairfield at Irving. The OCP strategic directions for Gonzales also include encouraging opportunities for enhancement of the small urban villages on Fairfield Road. In this application, the addition of 31 units will contribute to enhancement of these urban villages, each being approximately 300m to the west and east of the site, respectively. Further, Fairfield Road is identified as a secondary arterial west of Foul Bay Road, a cumulative frequent service transit route and a greenway, which supports consideration of increased density.

The OCP supports encouraging heritage conservation through incentives and allowances such as financial incentives, bonus density provision and variances. Also, where a proposal is achieving heritage conservation objectives, development can depart from the OCP's envisioned uses and densities. Although the site proposed for redevelopment will not protect heritage features at 1733, 1735 and 1737 Fairfield Road, the concurrent proposal for 1964 Fairfield Road and 507 Foul Bay Road will strengthen the level of heritage protection for another site within the neighbourhood through the proposed rezoning, OCP amendment and heritage designation amendment.

The OCP also envisions Community Amenity Contributions (CACs) where possible as part of new development proposals and the Inclusionary Housing and Community Amenity Policy sets out expectations for CACs. The applicant for 1733, 1735 and 1737 Fairfield Road has not provided CACs in the form of inclusionary housing units or cash contributions to be directed to City reserve funds; however, the applicant has proposed an alternative for Council's consideration, which is described in more detail below.

The proposed amendment to redesignate the subject properties for 1964 Fairfield Road and 507 Foul Bay Road is supportable because the amendment will align the land use designation to the existing use, proposed site-specific zoning and long-term vision for this unique heritage asset. In addition, the amendment and associated rezoning and heritage designation amendment proposals will advance OCP objectives to conserve the heritage value of a prominent cultural landscape in the neighbourhood.

OCP Consultation

The *Local Government Act* (LGA) Section 475 requires a Council to provide one or more opportunities it considers appropriate for consultation with persons, organizations and authorities it considers will be affected by an amendment to the OCP. Consistent with Section 475 of the LGA, Council must further consider whether consultation should be early and ongoing. This statutory obligation is in addition to the Public Hearing requirements. In this instance, if Council moves the application forward, staff recommend for Council's consideration that notifying owners and occupiers of land located within 200 metres of the subject site along with posting a notice on the City's website will provide adequate opportunities for consultation with those affected.

Through the Community Association Land Use Committee (CALUC) Community Meeting process all owners and occupiers within a 200m radius of the site were notified and invited to participate in a Community Meeting; therefore, the consultation proposed at this stage in the process is recommended as adequate and consultation with specific authorities, under Section 475 of the LGA, is not recommended as necessary.

Should Council support the OCP amendment, Council is required to consider consultation with the Capital Regional District Board; Councils of Oak Bay, Esquimalt and Saanich; the Songhees and Esquimalt First Nations; the School District Board and the provincial government and its agencies. However, further consultation is not recommended as necessary for this amendment to the Urban Place Designation as this matter can be considered under policies in the OCP.

Council is also required to consider OCP Amendments in relation to the City's *Financial Plan* and the *Capital Regional District Liquid Waste Management Plan* and the *Capital District Solid Waste Management Plan*. This proposal will have no impact on any of these plans.

Rezoning Application

Official Community Plan

1733, 1735 and 1737 Fairfield Road

The *Official Community Plan, 2012* (OCP) Traditional Residential Urban Place Designation supports a range of ground-oriented residential uses. The OCP envisions densities of approximately 1.1:1 FSR for Traditional Residential properties, with building heights up to approximately three-storeys. As noted, the proposal for 1733, 1735 and 1737 Fairfield Road is not consistent with the envisioned use, density or height for this designation, which necessitates an OCP amendment. The proposal does advance OCP objectives, including improvements to the public realm and providing a diversity of housing.

1964 Fairfield Road and 507 Foul Bay Road

The OCP seeks to maintain and enhance neighbourhood character of Gonzales, including the heritage value and the cultural landscape of individual properties such as Abkhazi Garden. The OCP also supports maintaining community assets that contribute to ecological functions, attract investment and support economic activity, including natural and built heritage and green infrastructure. Additionally, the OCP prioritizes conservation of the green space that supports a healthy urban forest. The rezoning will provide another layer of protection to ensure the continuation of the current use on the subject properties.

Gonzales Neighbourhood Community Plan

1733, 1735 and 1737 Fairfield Road

The proposal is inconsistent with the *Gonzales Neighbourhood Community Plan, (2002)*, which encourages maintaining existing zoning and the detached dwelling character of the neighbourhood. However, the plan also envisions a range of housing options to meet the needs of people with different needs and incomes, which the proposal advances through a mix of units in a multiple dwelling housing form. The plan emphasizes preservation and enhancement of the natural landscape on private and public properties as well as boulevards. Removal of all existing trees and shrubs onsite is proposed; however, the landscape plan proposes 18 new trees, including seven new boulevard trees on the Fairfield Road frontage with adequate soil volumes.

1964 Fairfield Road and 507 Foul Bay

The concurrent proposal is generally consistent with the *Gonzales Neighbourhood Community Plan (2002)*, which emphasizes preservation and enhancement of the heritage assets and of the natural landscape on both private and public properties.

Housing

The application for 1733, 1735 and 1737 Fairfield Road, if approved, would add 31 new residential strata units, which would increase the overall supply of housing in the area and contribute to the targets set out in the *Victoria Housing Strategy*.

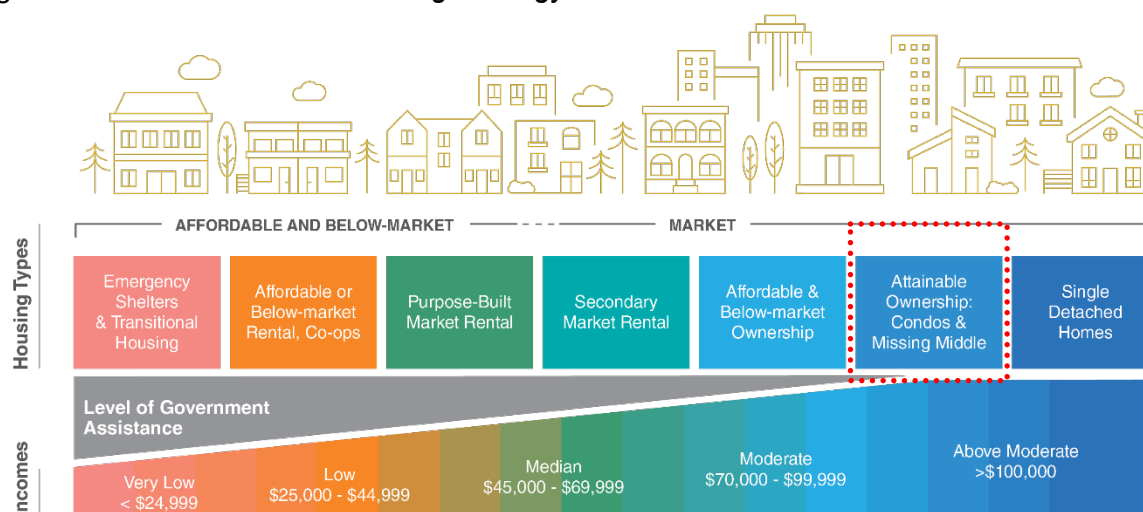


Figure 3. Housing Continuum

Housing Mix

At present there is no policy that provides targets regarding housing mix and unit type is not regulated or secured. However, the OCP identifies a mix of units as an objective and identifies the need for a diverse range of housing units including family housing. As submitted, this application for 1733, 1735 and 1737 Fairfield Road proposes two studio, twelve one-bedroom, nine two-bedroom, six two-bedroom with den and two three-bedroom units. The applicant has agreed to secure a mix of two-bedroom, two-bedroom and a den and three-bedroom units through a legal agreement; however, flexibility is requested by the applicant to allow one of the three-bedroom units to be converted to a two-bedroom with a den, which is reflected in the recommendation.

Existing Tenants

The proposal for 1733, 1735 and 1737 Fairfield Road is to demolish three existing buildings which would result in a loss of seven existing residential rental units. Consistent with the Tenant Assistance Policy, the applicant has provided a Tenant Assistance Plan, which is attached to this report.

Statutory Right of Way

The applicant for 1733, 1735 and 1737 Fairfield Road is proposing to provide a 0.86m wide statutory right-of-way along Fairfield Road to help achieve a 2m sidewalk and small canopy trees with adequate soil volumes that will act as a planted buffer between pedestrians and motor vehicles.

Inclusionary Housing and Community Amenity Policy

The Inclusionary Housing and Community Amenity Policy outlines the expectations for providing CACs and helps steer the rezoning process for new residential developments. While the policy does not request CACs for properties designated as Traditional Residential, the required increase in density and associated OCP amendment for 1733, 1735 and 1737 Fairfield Road classifies the proposal as an atypical application where a contribution is justified, and an economic analysis was requested. The policy accommodates alternatives to the provision of inclusionary housing units or cash contributions defined in the policy, with any appropriate alternative to be based on identified community needs and demonstrating a value equivalent to 75% of the value of the increased bonus density.

The applicant for 1733, 1735 and 1737 Fairfield Road has not provided CACs in the form of inclusionary housing units or cash contributions to the City's reserve funds; rather, as outlined in the applicant letters, the applicant has entered into an agreement with The Land Conservancy (TLC), the owner of 1964 Fairfield Road and 507 Foul Bay Road, which is intended to strengthen protection of Abkhazi Garden from future redevelopment.

The concurrent OCP amendment, rezoning and heritage designation amendment of 1964 Fairfield Road and 507 Foul Bay Road is proposed as an alternative to providing inclusionary housing units or cash contributions. The following measures would be secured through legal agreements and bylaws, as applicable:

- rezone the sites at 1964 Fairfield Road and 507 Foul Bay Road to remove existing residential permitted uses and limit zoning to permit only the existing uses
- amend the OCP designation to reflect the existing use
- expand heritage designation to encompass the entire site
- secure continued public access to Abkhazi Garden in perpetuity.

In addition, the applicant is proposing a monetary donation to TLC in the amount of \$350,000; however, the donation will not be secured as part of either rezoning application.

The applicant provided an independent economic analysis to demonstrate that the amount of the donation is consistent with the Inclusionary Housing and Community Amenity Policy's recommended method for determining a cash contribution, which is to provide 75% of the estimated increase in land value from what would be anticipated under existing zoning.

Development Permit with Variances Application

Official Community Plan Design Guidelines

The OCP identifies the properties at 1733, 1735 and 1737 Fairfield Road as within Development Permit Area (DPA) 16, General Form and Character. The objectives of this DPA are to integrate new developments to compliment and enhance the established place character through architecture, landscape and urban design. Other objectives include providing sensitive transitions to adjacent properties with built form of three storeys or lower, and to achieve human-scaled design, quality of open spaces and accessibility. The applicable guidelines include the *Design Guidelines for Multi-Unit Residential, Commercial and Industrial Development* (2012, revised 2019), *Guidelines for Fences, Gates and Shutters* (2010), and the *Advisory Design Guidelines for Buildings, Signs and Awnings* (1981).

Site Design

The *Design Guidelines for Multi-Unit Residential, Commercial and Industrial Development* includes objectives to ensure design of multiple dwelling development provides a transition in form and massing to lower density building forms. The southeast elevation of the proposed development for 1733, 1735 and 1737 Fairfield Road effectively provides transition in form and massing; however, the northeast setback of 2.6m, combined with the proposed four-storey building height, would benefit from revisions to improve the transition to the northeast neighbour (1745 Fairfield Road).

The guidelines prioritize open space as part of site design, which should be usable, attractive, and well-integrated, and should preserve existing vegetation where possible. The proposed open space area does not meet the minimum required in the standard URMD Zone, largely due to the proposed surface parking. When combined with 65% lot coverage, the surface parking does not preserve existing established landscaping or provide adequate space for replacement trees. The recommendation includes a condition that parking be reduced, located underground and/or a landscaped amenity space be provided to better align with the guidelines. In addition, a two-space reduction in vehicle parking is recommended at a minimum to provide additional space to provide required replacement trees as required in the *Tree Protection Bylaw No. 21-035*.

Parking should be located underground or at the rear of the property to minimize the impact on streetscape and maximize ground level space for landscaping. Where it is unavoidable to locate driveways in building frontages, consideration should be given to the incorporation of these elements into the building. Also, the location and design of service functions, such as parking and hydro infrastructure, should not be prominent from the street. The proposed parking is effectively concealed from the public realm through innovative building design; however, hydro infrastructure is visible from Beechwood Avenue.

The guidelines encourage vertical disruptions along pedestrian routes be avoided and vehicle and pedestrian conflicts be minimized through site design. The proposal adequately achieves this accessibility objective by providing a ramp to the main entrance to allow access without stairs or other vertical disruptions.

The guidelines also encourage the use of high quality, permeable paving materials in parking and pedestrian areas in order to improve on-site stormwater management. This is particularly important where a development occupies more than 40% of the site and includes less than 50% open space, which are minimum requirements in the standard URMD Zone. The proposal occupies 65% of the site area and provides 23% open space, with less than half being landscaped area; therefore, the use of permeable pavement should be a priority. While the proposal does include permeable paving material for a portion of the parking area, it is recommended that this be increased to cover the entire parking area, or at a minimum, the portions of the parking area that is uncovered.

The applicant for 1733, 1735 and 1737 Fairfield Road provided a supplementary letter, dated September 12, 2023, which outlines that they intend to revise the proposal from what is shown in the attached plans, which would add an additional dwelling unit and convert a one-bedroom unit to a two-bedroom unit, which would result in the removal of the only common amenity space provided in the development. Similar to above-noted concerns related to provision surface parking, a reduced rear yard setback and provision of open space, it is recommended that the applicant consider a landscaped rooftop amenity area to provide additional usable outdoor space.

Building Design

The design guidelines encourage that overall building design be sensitive and innovative in response to context. In addition, multiple dwellings directly abutting lower density buildings should provide a height transition. The contemporary design for the dwelling at 1733, 1735 and 1737 Fairfield Road compliments the variety of architectural styles along Fairfield Road and massing is sensitive to surrounding lower density development, apart from the northeast elevation where a transition in form and massing to the abutting single-family dwelling is recommended.

The proposed building contributes to both streetscapes and adds interest to the streetscape through variations in building height, roofline and massing. The street level design, with individual entrances facing Fairfield Road and a prominent shared entryway fronting Beechwood Avenue, encourages interaction with the street and public sidewalk, consistent with the design guidelines.

Porches and other design features are encouraged to make transitions from the public to the private realm, which is achieved along Fairfield Road with landscaping to define the transition to private open space. Also, the exterior building materials are high quality and durable, with use of light brick on lower levels.

It should be noted that the applicant is proposing additional revisions that will add a two-bedroom unit (from 30 units to 31 units), convert a one-bedroom unit to a two-bedroom unit, result in the loss of the common amenity space, and will marginally reduce the step back of the third storey in the southeast elevation. Given the proposed additional revision will result in a loss of the only shared amenity space in the building, it is recommended that a landscaped rooftop amenity be added through revisions to be submitted.

Variances (1733, 1735 and 1737 Fairfield Road)

Although a site-specific zone is sought, variances are recommended for the 1733, 1735 and 1737 Fairfield Road application (instead of inclusion in the new zone) where the proposal is not consistent with the standard URMD Zone, Urban Residential Multiple Dwelling District, and the Off-Street Parking Regulations (Schedule C) of the *Zoning Regulation Bylaw*. This ensures that if this proposal is not built, any potential future redevelopment would require Council's consideration and approval for these specific aspects.

Setbacks

Variances are required for all building setbacks. A reduction to the minimum front yard setback is required from 4m to 2.70m as well as an increase in stairs projection from 1.80m to 2.73m. In addition, a reduction to the minimum flanking street setback (Fairfield Road) from 4.00m to 2.30m is proposed. These variances are considered supportable as each result from the design objectives to site the building to frame fronting public streets, create a sense of enclosure and maximize rear and interior side yard setbacks to ensure a buffer with abutting properties.

A variance to reduce the minimum side yard setback (southeast) from 6m to 0.50m is also requested, which is a result of the incorporation of driveway access into the building in order to conceal the surface parking area from the public realm. This variance is considered supportable because the portion of the building within the required setbacks effectively minimizes the visual impact of the parking area and the reduced setback is limited to the portion of the building near Beechwood Avenue.

The proposal requires a reduction to the minimum rear yard setback (northeast) from 10m to

2.60m, which is not considered supportable because the 10m rear yard requirement is intended to ensure usable open space and, when combined with surface parking, the reduction of all building setbacks results in limited open space to provide a buffer to abutting properties. Additionally, when combined with the four-storey building height, the reduced rear yard setback may impact neighbouring properties. The variance would be considered supportable with revision to the building massing to soften the height transition and provision of amenity space above the ground floor, specifically a shared rooftop amenity that is possible with a flat roof design.

Site Coverage and Open Space

There are required variances from the standard URMD Zone to increase the maximum site coverage from 40% to 65% and decrease open site space from 50% to 23%. Similar to the concern with reductions to all setbacks, limited open space is proposed for buffers and amenity area. This variance would also be considered supportable with the revisions outlined above.

Parking

A variance is required to reduce vehicle parking from 40 spaces to 23 spaces. The applicant has submitted a parking study, and the variance is considered supportable based on the comprehensive TDM package that is expected to offset the parking shortfall. Also, a further two-space reduction in vehicle parking is recommended to provide additional space to provide required replacement trees, as noted above. It should be noted that the applicant has indicated that they are not amenable to reducing the number of parking spaces beyond the current proposal of 23 spaces, as this will impact the marketability of the project. Staff maintain that this is advised, and a condition has been included in the recommendation. The requested variance to permit long-term bicycle parking provided in a stacked format is considered supportable as the applicant has provided details showing that the stacked bike storage will still accommodate larger bicycles, with a lift assist mechanism to ensure ease of use.

The TDM package is to include:

- car share memberships and usage credits for all residential units
- electric car share vehicle and dual head electric vehicle charging station on Beechwood Road
- bicycle parking to accommodate oversized bicycles (10% of required long-term spaces)
- bicycle parking with access to an electrical outlet (50% of required long-term spaces)
- bicycle repair and maintenance station.

Accessibility

The proposed walkways surrounding the building and to the building entrances are designed to be accessible, with a ramp required to the main entrance and an elevator that will provide access to units.

Advisory Design Panel Review

The application for 1733, 1735 and 1737 Fairfield Road was reviewed by the Advisory Design Panel on October 26, 2022. At that meeting, the following motion was passed:

“That Development Permit with Variance(s) Application No. 000204 for 1733-1737 Fairfield Road be approved with the following changes:

Consideration to simplifying the roof form to be more sympathetic to the neighbouring context.

MINORITY REPORT: Those that voted against believe the building is not consistent with the density, height and use envisioned for traditional areas in the OCP.

Carried 4:2”

In response, the applicant has revised the roofline, specifically removing the mansard roof to better reflect the neighbourhood context, incorporated a step-back of the upper floors along Fairfield Road, and extended the proposed use of brick to emphasize the ground-oriented brick base of the building. In addition, at the time of submitting these revisions in response to staff and panel concerns, the applicant revised the building to create smaller units and increase the unit count from 19 to 30 units, which resulted in a marginal increase in total floor space.

Tree Preservation Bylaw and Urban Forest Master Plan

The goals of the *Urban Forest Master Plan* include protecting, enhancing, and expanding Victoria's urban forest and optimizing community benefits from the urban forest in all neighbourhoods. The application for 1733, 1735 and 1737 Fairfield Road was received after July 1, 2021, so *Tree Protection Bylaw No. 21-035* applies. The application at 1964 Fairfield Road and 507 Foul Bay has no associated tree impacts.

A total of 20 trees and six hedges have been inventoried. Of these, ten trees and six hedges are located on the subject lot, and six trees and one of the hedges are bylaw protected. There are three existing municipal trees on the Beechwood Avenue frontage. Six bylaw protected trees and one bylaw protected hedge will require removal as they are in the building area or immediately adjacent to an area where excavation will occur. One small municipal tree would be removed for service installation. All off-site trees and two municipal trees can be retained following the mitigation measures outlined in the arborist report.

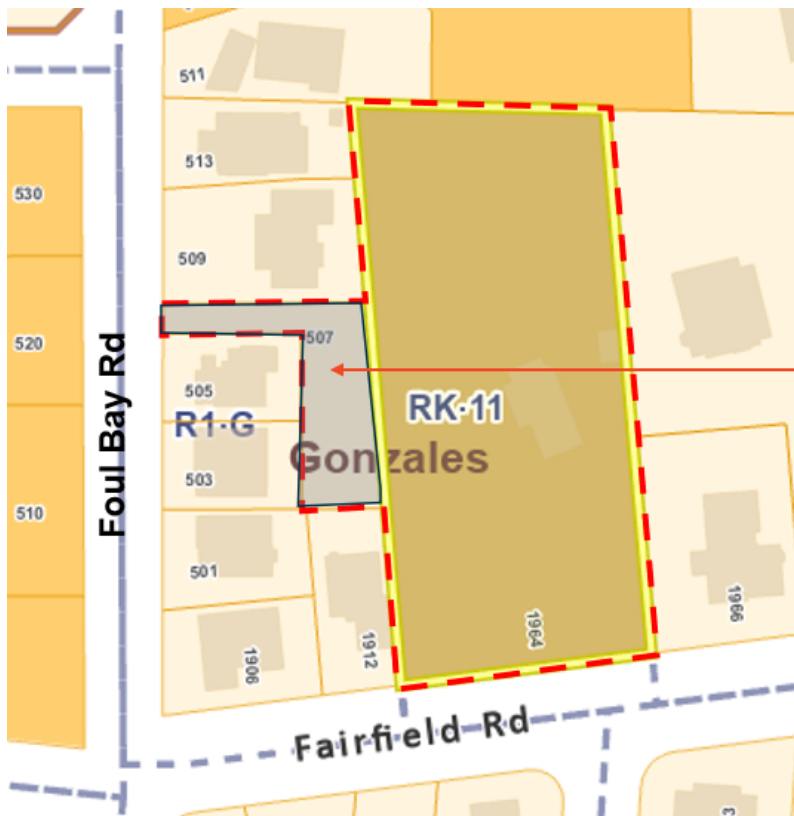
The applicant is proposing to plant 18 new trees on the subject lot, five of which will be replacement trees planted towards requirements under the Bylaw. Under the current proposal, the applicant would be required to pay \$10,000 for cash-in-lieu towards the City's Tree Reserve Fund (\$2,000 X five replacement trees not planted). Seven new municipal trees are proposed on the Fairfield Road frontage with adequate soil volumes. Currently there are no municipal trees on the Fairfield Road frontage. One municipal tree is proposed on the Beechwood Avenue frontage.

Heritage Designation Amendment Application (507 Foul Bay Road)

The purpose of this portion of the report is to provide information and analysis regarding the proposed amendment to the existing heritage designation bylaw for 1964 Fairfield Road. This amendment would include the addition of the adjacent property 507 Foul Bay Road, which contains the greenhouse, and the garden's support and maintenance spaces.

Description of Historic Place

The historic place comprises a designed domestic evolving garden, garden buildings, and an early example of a modern bungalow, set around and upon a rocky knoll, in Victoria's Gonzales neighbourhood. The house was designed for Peggy and Nicholas Abkhazi, by Victoria-based Modernist architect John Wade, in 1946-47. For a complete description of the heritage value and character-defining elements, see attached Statement of Significance.



Add Garden
Support Area / Lot
507 to Designation
Bylaw No. 23-007

Relevant History

The original application was reviewed by the Heritage Advisory Panel at its April 12, 2005 meeting, and a motion was made to recommend that Council consider approving the heritage designation of the existing house, summer house, garden shed and garden, known as Abkhazi Garden at 1964 Fairfield, as a municipal heritage site, including the birch paneling and oak flooring on the interior of the main house.

More recently, the Statement of Significance has been updated to include additional character-defining elements, make specific corrections, and add the abutting property at 507 Foul Bay, which acts as an important maintenance and support area for the gardens, including propagation activities. As noted in the Statement of Significance, the garden is valued for its rare plant

conservation and the development of new hybrids, therefore the area identified as 507 Foul Bay Road, even though it is a working back-of-house space, is integral to the functioning of the gardens, its evolving nature, and ongoing maintenance. Therefore, it is considered important to ensure the whole of the gardens is acknowledged as significant through heritage designation.

CONCLUSIONS

The proposed four-storey multiple dwelling with a density of 1.77:1 FSR is not consistent with the use, density and height envisioned for Traditional Residential properties in the OCP.

However, the proposed development would advance housing objectives by providing 31 new dwelling units, including some family-oriented housing. Further, the concurrent rezoning and OCP amendment includes proposed measures to strengthen heritage protection of Abkhazi Garden and secure continued public access.

While the form and character of the development is largely consistent with the design guidelines, revisions are recommended to improve the height transition from the neighbouring home and the northeast building elevation. Also, a revision to provide parking underground, a further reduction in parking or a common rooftop amenity is recommended to offset the space allocated to surface parking and resulting reduced open space. The application is recommended to proceed to a public hearing, subject to the conditions outlined in the recommendation.

ALTERNATE MOTIONS

Alternate Option 1 - Decline

That Council decline Application No. 00821 and associated Official Community Plan Amendment for the property located at 1733, 1735 and 1737 Fairfield Road as well as Rezoning Application No.00845 associated Official Community Plan and Heritage Designation Amendments for 1964 Fairfield Road and 507 Foul Bay Road.

Respectfully submitted,

Patrick Carroll
Senior Planner
Development Services Division

Karen Hoesle, Director
Sustainable Planning and Community
Development Department

Report accepted and recommended by the City Manager.

List of Attachments

- Attachment A: Subject Maps
- Attachment B: Plans for 1733, 1735 and 1737 Fairfield Road date stamped August 22, 2023
- Attachment C: Letter from applicant to Mayor and Council dated August 21, 2023 (REZ00821)
- Attachment D: Applicant Letter re: Design Changes dated September 12, 2023
- Attachment E: Letter from applicant to Mayor and Council dated November 23, 2023 (REZ00845)
- Attachment F: ADP Minutes from the meeting held October 26, 2022
- Attachment G: Statement of Significance for 1964 Fairfield Road and 507 Foul Bay Road

- Attachment H: Tenant Assistance Plan
- Attachment I: Community Amenity Contribution Analysis dated June 25, 2023
- Attachment J: Parking Variance Report dated January 18, 2023
- Attachment K: Community Association Land Use Committee Reports dated March 28, 2022, March 27, 2023 and October 23, 2023
- Attachment L: Pre-Application Consultation Comments from Online Feedback Forms
- Attachment M: Correspondence (Letters received from residents)

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: Opposition to the INCREASE to 4 storey building proposed for 1733,1735 and 1737 Fairfield Road
Date: June 11, 2024 12:37:49 PM

Dear Patrick,

First of all, I would like to state upfront that we are not anti-development.

However it is becoming increasingly clear that there is no integrity in the Aryze development proposed for the properties at 1733,1735 and 1737 Fairfield Road. We, The Ray Family, are the immediate next door neighbours, residing at 1745 Fairfield Road and have recently received a notice of an amendment to the development set to be constructed next to us. The developer came to our house multiple times and told us, to our faces, that this project was to be no more than 3 stories. He came back when it crept up to 3.5 stories. It was, at that time, when I confronted this discrepancy, and at that point when I point blank asked him if this was going to 4 stories and he said no, to my face. When I said that this project didn't look at all like what was initially proposed, he said that was not true and unfair to say. Well, I would like to say to you now, that this project is not what was presented to us, not what we agreed to be on board with and agreed we would not protest against. I tell you today, that sadly with this letter that Aryze has not come to us again to tell of us this new change to their plans, our goodwill is now gone.

As I mentioned, we live next door and this new proposed 4 storey project will dwarf our home. It will take our light. It will be built right up to the sidewalk which will in turn block our sight lines to get out of our driveway - a dangerous proposition for a young family with 3 small kids and a parent in a wheelchair. We live on a busy main street, with access to bus routes and close proximity to many desirable amenities. This is, of course, the place where density should live, but it needs to be done in the right manner that fits with the neighbourhood. This is by no means the project we were sold the 5 times the company came to our house about. They came to make sure we were on board. They came to promise things that we can no longer believe will be upheld. My father was killed by a flat bed truck a year ago across the street from his home on a quiet street. We live on a main street and as mentioned we have 3 small kids and my husband in a wheelchair. I fear for not just the construction phase of this sizeable project. I fear that the recommendations to uphold our sight lines and noise mitigation that were recommended by the City of Victoria will be disregarded. I fear that the height of this building is just the start of the dishonesty. Again, this was to be a 2.5 -3 storey project, not 4.

So, with all that being said, we would like to firmly request and very much appreciate **that you not pass this amendment.** We have no illusions that this letter will stop the project and that is not our intent. What we would like is for the project to be what we were originally sold, **a 2.5 -3 storey property** with character and aesthetic fit for our neighbourhood. We implore you to uphold this, to hold developers to be accountable for their promises and their words. These are the things we teach our children, to be someone of your word. The City of Victoria needs to hold them to this on our behalf. The integrity of Aryze is gone for us. We had heard this about Aryze. They develop at all costs. They promise things and then discard those promises when building starts. This begs the question, why bother with all the community engagement, the home visits, the community representatives making sure we were on board if all along the goal was to do something different?

Thank you for your time.

With warmest regards,

Cathy Ray

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: Feedback 1733, 1735, 1737 Fairfield resident of beechwood
Date: June 10, 2024 7:13:18 PM

Patrick,

I am a resident on Beechwood avenue (267 beechwood avenue to be exact).

I am fairly pro- density in neighbourhoods as a whole, but have one concern that seems to be lacking as far as being paired with building homes in what have been single family dwelling neighbourhoods.

I have lived in many other neighbourhoods of Victoria and although I LOVE my current neighbourhood, I have NEVER lived in a neighbourhood that has so few amenities! If this neighbourhood is attempting to become an urban village, is the city doing anything to encourage mixed used buildings?

I currently have to go to grocery stores outside my neighbourhood to get BASICS because the only grocery store within a half hour walk is Thrifties and it is on a daily basis sold out of BASIC items like milk!

Other neighbourhoods are easily live, walk because they have a variety of stores small and mid size. There is not even a convenience store within a 25 min walk from the Hollywood corners!

I am not sure if this is the venue for bringing this up but the development on Fairfield mentioned I expect will be trying to house up to 70-90 people? If 29 units? Where will these people getting their basics from? On average houses on my street have 2 minimum cars and 3 is normal, with 5 not being unheard of because every person in a household has to drive to get out of the neighbourhood!

The bus route is close but at peak times it is often full (I had a roommate for 10 years who had to get a car to get to the college and then to downtown for work because most days the bus would pass by full). Services and amenities are severely lacking in this neighbourhood!

To reiterate- I am NOT opposed to density... just wondering how to get a better balance for residents + amenities in this neighbourhood.

Rebecca Lang
Resident 267 Beechwood Avenue

Business owner: Any Thyme Gardening
[REDACTED]

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: Rezoning proposal No. 00821 (1733-1737 Fairfield Road)
Date: June 11, 2024 3:45:36 PM

Hi Patrick,

This is regarding rezoning proposal No. 00821 (1733-1737 Fairfield Road). I am writing to express my concerns regarding the proposed development aimed at densifying our neighborhood. While I understand the need for urban development and growth, I firmly believe that any such development should be undertaken with careful consideration for the existing community and its residents. The proposed densification project, as it stands, raises significant concerns that I urge you to consider before making any decisions. Overall, we've been supportive of densification in Fairfield to-date, however, we feel the community has limitations of how much more it can accept, particularly with the surrounding infrastructure and services. We are seeing impacts of recent projects and decisions by the city which has had negative impacts on our neighbourhood which I don't feel the city has fully considered. As residents who has lived in Fairfield for 10+ years, we cherish the urban character of our community. We believe that the proposed project and densification could have detrimental effects on both the aesthetic appeal and quality of life in our neighbourhood.

We live very close to the proposed development at 1785 Fairfield Road and bike and walk along Fairfield Road daily. In recent years we have seen other developments and densification in the neighbourhood have a detrimental impact. The proposed densification could exacerbate existing issues such as traffic, parking shortages and overcrowded public services. Our neighborhood is already struggling to accommodate the needs of its current residents and adding 29 more housing units in such a condensed area without adequate infrastructure upgrades would only exacerbate these problems. What is the city's plan to support the already overcrowded infrastructure and services in the neighbourhood?

We have seen considerable changes to the traffic density and safety of Fairfield Road because of the increased densification and redirecting of traffic from other roads in the area. I use Fairfield Road for my bike commute daily (Richardson is out of my way) and we are concerned that this development will further saturate the neighbourhood and add to the issues we're seeing today. The closure of Richardson to local / bike traffic only along with other developments have put stress on Fairfield Road, which is a very narrow street with parking along both sides for the majority (Richardson is a wider street which makes one wonder why they would close it off as an option from downtown into Fairfield and push all the traffic to a narrow and busy Fairfield Road?). What action is the city going to take to reduce traffic on Fairfield Road and ensure safety on the street? Adding 29 residential units to a space where 4 currently exist is a considerable increase that will dramatically add to the number of vehicles driving and parking in the area. What is the plan to provide off-street parking for the 29 units, their residents, and guests?

Also, the height of this development poses a significant issue. The proposed 4-storey development would be significantly taller than the surrounding structures, well outside the norm for height and FSR in the neighborhood. This will have an impact on the look and feel of the residential neighbourhood and dramatically impact the sightlines and aesthetics. Instead of pursuing a one-size-

fits-all approach to development, I urge you to consider alternative solutions that prioritize the preservation of our neighborhood's unique character and the well-being of its residents. This could include exploring options for infill development that complement the existing architectural style and scale of our community, as well as investing in sustainable infrastructure improvements to support modest growth without sacrificing our quality of life.

I ask that you carefully weigh the potential consequences of the proposed densification project and to prioritize the long-term interests of our community in the decision. We want to ensure that our neighborhood remains a vibrant and desirable place to live for generations to come.

I appreciate the opportunity to provide local resident input and look forward to your response.

Regards,
Jordan Semeschuk
1785 Fairfield Road, Victoria.

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: Amendment to OCP Fairfield Road
Date: June 12, 2024 2:19:53 PM

As a resident of Rhodo at 1720 Fairfield Road, I fully support amending the OCP Bylaw for the properties at 1733, 1735, 1737 Fairfield Road. I support the increased density and height! We desperately need this type of housing in beautiful Fairfield.

Thank you,
Kelly Galitzine
Sent from my iPhone

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: 1733-1737 Fairfield Rd
Date: June 14, 2024 11:26:01 AM

Mr. Carroll-

I would like to add my thoughts and responses to this proposal from Aryze Developments. I have owned and lived in my home at 311 Robertson Street since 2000.

*The proposal contravenes our Community Plan, that so many of my neighbours and myself worked on and developed.

*It's too tall - 4 stories. The OCP and the Missing Middle plan call for 2-3 stories .

*The Density is not acceptable. 1.79:1 floor space ration and not the OCP density of UP to 1:1.1 FSR.

*The project has virtually no landscaping, trees, yard or land. This is a huge building with a parking lot that has no natural space around it. It replaces 3 homes that had yards, gardens, trees, and space for kids. This proposal has none of these.

*This proposal does not add anything to improve our community. Conversely, it diminishes our community. The developers sacrifice natural space to put in more units, to make more profits for themselves.

We should never support this building height, paving and building density, knowing that it sacrifices the land in the process.

I ask that the City of Victoria not approve this building as presented. It's too dense, is 4 stories tall and contravenes our Official Community Plan that we the community wrote.

Thank you-
Linda Maasch

[REDACTED]

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: 1733-1737 Fairfield Rd Feedback
Date: June 14, 2024 11:32:34 AM

Dear Patrick Carroll,

My family and I live in close proximity to the proposed development at 1733-1737 Fairfield Rd. On the whole, we are in support of this project. We feel it aligns well with the Provincial government direction for increasing housing, and recognize that Victoria needs to diversify the types of housing that is available to residents. With little in between a downtown condo or a single family home, we see this development as filling a much needed gap in our housing supply. Further, we appreciate, based on information that we've heard from the developers, that this development will not be using any natural gas fuels with a focus on electric, renewable fuels for the building.

Despite our support, we do have a couple of concerns with the development. Firstly, the vehicle access on Beechwood Ave is concerning and has the potential to lead to significantly increased vehicle traffic on our street. Already there is considerable vehicle congestion on this portion of Beechwood Ave. With many of our neighbours opting for more than one vehicle, the space on the road is already limited with the abundance of vehicles parked on the street. Adding 20+ new residents and their vehicles to this section of Beechwood will have a negative impact and we worry about the street safety of pedestrians and young children. If the Community Plan can be amended to allow this development to move forward, a second amendment to whatever piece of policy required to allow for parking access on Fairfield Rd would be appreciated.

Related to parking, we were disappointed to see that this development would not feature underground parking, but would have a paved parking surface on the ground level. We understand the cost implications of underground parking, however, surface level paved parking is a waste of valuable space and contributes to heat islands. Given our city's recent experience with extreme heat and the likelihood of experiencing similar heat events going forward, allowing a development that includes paved ground level parking would be disappointing.

Despite our concerns, which we hope will be addressed, we are in support of this development and the required Official Community Plan amendment to ensure this project can move forward.

Regards,

Miranda and Matthew Andrews
321 Beechwood Ave

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: 1733/1725/1737 Fairfield
Date: June 16, 2024 10:08:35 AM

Love it! Keep building! Glad to see more density along the Fairfield corridor, hope to see similar closer to the Plaza. And some day... the Plaza itself getting a rebuild with residential? Would be great for the neighborhood.

Thanks!

Paul Ramsey
1684 Chandler Ave

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: 1733-1735 Fairfield Road Proposed Development: FEEDBACK
Date: June 18, 2024 8:27:19 PM
Attachments: [image1.png](#)

We are AGAINST the current proposed development, for the following reasons:

1) HEIGHT - IT IS TOO TALL

4 above ground floors will **tower** over existing homes.

Solution: maximum 3 floors (just like the Rhodo development a block away)

2) PARKING

The current plan has surface parking behind the building. This is adjacent to many back yards. The pollution and noise directly affecting adjacent properties is not acceptable.

Solution: underground parking (just as the Rhodo Development has).

3) COMPLETE LOSS OF GREEN SPACE

Balance again needed.

Solution - underground parking so that back part of development is gardens, food source (communal veggies garden), trees, maybe even a water source. Current proposal is pretty much building and pavement.

Here is a photo of the Rhodo which is a block away from this proposed development - as you can see, there is virtually no greenspace (more like “token” greenspace and lots of concrete).

To allow this type of development to continue when our climate is in a crisis seems untenable.





4) Building mechanicals/ vents etc on roof

Our understanding is some of the building “systems” will be on the roof which also poses noise issues to adjacent properties.

Solution: move to below ground (another reason for underground parking)

5) Loss of privacy and sunlight

These should be consideration to neighbouring properties. For some of us, access to sunlight in our homes is critical for health (mental health/SAD) as is privacy. 4 floors will mean we will have at least one full floor and their back decks / windows staring directly into our homes and yards.

Solution: max 3 floors

It is all about balance. We would love to see diverse housing and this CAN be done, it just needs to be reasonable and thoughtful in terms of the CURRENT surroundings.

Comment: these 3 properties would be IDEAL for Missing Middle Housing providing 22 units (6 allowed on 2 of the lots and 10 on the corner if I remember the MMH numbers correctly).

I sincerely hope that council will read all letters sent when prior to this latest request for feedback.

Thank you for your consideration of our families concerns and feedback.

Joanna Betts

Created and sent from my iPhone with my “iThumbs” so please excuse typos!

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: Proposal on 1733-1735-1737 Fairfield Road
Date: June 19, 2024 9:30:57 AM

Hello Mr. Carroll,

I got a letter about your new proposal on Fairfield Road. I have almost nothing against this proposal. My only concern is that the street parking be considered. As a homeowner in the surrounding area, I don't want people to park in front of other residences. When designing this complex, I only ask that you have some sort of parking system for all of the residents in this new residential building. Whether that is through underground parking or some other method. Please also consider that the city may add bike paths in front of the building, in the coming years, making parking in front of the building nearly impossible. This is a family neighborhood and by adding such buildings we lose that sense of community.

Thanks for your consideration

Peter Meyer

Peter Meyer
[REDACTED]

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: 1733, 1735 and 1737 Fairfield Road
Date: June 23, 2024 6:30:19 PM

Hello Patrick/City Council,

I would like to comment on the proposed development at 1733, 1735 and 1737 Fairfield Road.

I understand and support the need to increase the housing supply and to achieve more residential density in the Greater Victoria area, however, a four-story multi-unit residential building is too large for the proposed lot. There are no other residential buildings of that size in the area. A building of that size would have a significant negative impact on the owners of single-family homes beside and behind that location.

My family and I live at 350 Robertson Street. We enjoy time spent in the privacy of our backyard. We would certainly be negatively impacted by a four-story building going up in that location.

My neighbours and I also share concerns that this new development may negatively affect our property values and increase traffic on Fairfield Road.

With the privacy of the surrounding homes in mind, I believe a two to three-story building or a townhouse style complex built to the same height as the roofline of the existing homes on the subject properties, has the potential to be a reasonable compromise for all parties involved.

We currently view our home as our forever home. What ends up being built on those properties could have a big impact on how we feel about that.

Thanks for your consideration.

Sincerely,

Jordan Anderson
350 Robertson Street
Victoria

From: [Alison Trembath](#)
To: [Patrick Carroll](#)
Subject: Aryze Proposal 1733, 1735 and 1737 Fairfield Rd.
Date: July 3, 2024 12:52:50 PM

Objection to Proposal for Residential Intensification in Gonzales Neighbourhood

I am writing to formally object to the proposal for residential intensification in the Gonzales neighbourhood, specifically regarding the plan to increase density beyond current zoning regulations and the associated impacts on parking capacity and neighborhood character.

1. **Density Concerns:** The proposal seeks to exceed the allowable Floor Space Ratio (FSR) as defined in the Official Community Plan for Traditional Residential areas. While the proponents argue for the necessity of increased housing options, the significant increase in density to 1.73 FSR is not justified. This level of density is incompatible with the existing character of the neighbourhood, which predominantly features single-family homes. Such intensification could lead to overcrowding, strain on local infrastructure, and a loss of community ambiance that residents cherish.
2. **Parking Capacity:** The Parking Capacity Study cited in the proposal indicates a peak demand that already exceeds available parking spaces in the vicinity. Despite assurances that there are currently vacant spaces during peak periods, the reality for local residents contradicts this claim. As a resident of the area, I can attest to the chronic shortage of parking, which is exacerbated by the proposal's intent to introduce additional housing units without adequate provisions for parking. This situation poses a direct inconvenience and safety risk to current residents, particularly concerning for families with young children.
3. **Neighbourhood Character and Quality of Life:** Gonzales is valued for its tranquil atmosphere, green spaces, and accessibility to amenities such as parks and schools. The proposed increase in density and associated changes threaten to alter the neighborhood's character irreversibly. The removal of green spaces and the strain on local services diminishes the quality of life for existing residents and undermines the very reasons families choose to live in Gonzales.
4. **Community Engagement and Trust:** The proposal fails to adequately address the concerns raised by the community regarding the impacts of increased density. Mayor Alto's recent remarks highlight a growing sentiment of frustration and distrust towards developers seeking significant amendments after initial approvals. The lack of consistency and reliability in project proposals erodes trust in the planning process and undermines the city's commitment to sustainable development.

Additional Objection: Parking Lot Access on Residential Streets

I would also like to raise a specific concern regarding the proposal's plan to have the parking lot empty onto a quiet residential street rather than utilizing the main corridor which is better suited to handle increased traffic flow.

1. **Traffic Management and Safety:** Directing parking lot access onto a quiet residential street raises significant concerns regarding traffic management and safety. Residential streets are typically designed to accommodate local traffic and pedestrian activity, not the influx of vehicles associated with commercial or high-density residential

developments. Introducing such traffic onto a quiet street not only disrupts the peaceful environment but also poses safety risks, especially for children playing and pedestrians.

2. **Impact on Neighborhood Tranquility:** Gonzales is cherished for its peaceful atmosphere and residential charm. Routing parking lot access onto a residential street undermines this tranquility by introducing noise, congestion, and potential safety hazards. Residents rely on these streets for their daily activities and recreational purposes, and the proposed traffic flow would disrupt their quality of life.
3. **Alternative Access Solutions:** The main corridor, Fairfield Road, is designed to handle higher traffic volumes and is more suitable for commercial and residential access. It provides safer conditions for vehicular movement and minimizes disruption to residential areas. Reconsidering the access point to utilize Fairfield Road would align with responsible urban planning principles and mitigate adverse impacts on neighborhood livability.

In conclusion, while acknowledging the need for responsible urban planning and housing diversity, the current proposal for residential intensification in Gonzales is unsuitable and detrimental to the well-being of the neighborhood. I urge the council to reconsider the proposal in light of its impact on density, parking, neighborhood character, and the overall quality of life for current residents.

In consideration of these concerns, I strongly urge the council to reassess the proposal's plan for parking lot access and prioritize solutions that preserve the residential character and safety of Gonzales.

Sincerely,
Alison and Toby Trembath

July 3rd, 2024

From: [REDACTED]
To: [Patrick Carroll; Victoria Mayor and Council](#)
Subject: Feedback to Mayor & Council re Aryze Development Proposal at 1733, 1735, 1737 Fairfield Rd.
Date: July 3, 2024 12:56:36 PM

Mayor and Council,

Our family resides on Beechwood Avenue, in close proximity to the proposed development at 1733, 1735, 1737 Fairfield Road. Our submission to you is in response to the letter we received from the City of Victoria this past June, requesting input on the development.

It is important that we highlight the fact that we are not opposed to increased housing and development in the area, including the site in question. We do, however, respectfully request that Council limit approval of any multi-unit residential building at that location to one with a design in height and density that is **consistent with the Official Community Plan**. This would result in a development that still increases housing while ensuring height and density aligns with the OCP and existing buildings in the neighborhood.

Our request of Council is based on the following considerations and overall rationale:

The Official Community Plan (OCP)

As stated in the "It's Your Neighbourhood" letter received from the City of Victoria, The Official Community Plan (OCP) currently identifies the property within the Traditional Residential urban place designation, which supports residential uses that include the missing middle housing, ground-oriented multi-unit, attached, duplex, and single detached dwelling buildings, with heights generally ranging from two to three storeys. For this urban place designation, the OCP supports a density of up to approximately 1:1.1 FSR. The current proposal far exceeds that with an application for an overall density of 1.79:1 FSR.

It should be noted that the original Working Group for the Gonzales Neighbourhood Plan included Ryan Goodman from Aryze Developments. The plan included *Key Moves #1 to Add housing that fits the neighbourhood character ... and spoke to limiting apartment/townhomes up to 3 storeys along Fairfield Road between St. Charles and Foul Bay Road.*

It also included *Key Move # 4 - Celebrate Neighbourhood Heritage* commenting that "Many places in the neighbourhood have strong heritage value, and there is a desire to protect the historic character of special homes and streets. This plan seeks to conserve the special historic character of Gonzales by: Encouraging new types of housing, such as a main house + suite + garden suite, for new heritage designated properties."

The main point we want to emphasize here is to limit height and density as intended in the OCP, and not to disregard a community plan the developer was directly involved in.

Mayor and Council should be aware that concerns regarding height and density and

the importance of the OCP dates back two years as documented in the March 28, 2022 CALUC Meeting Report, amongst others. Those concerns were communicated to Aryze when their proposal called for tearing down three (3) homes to construct six (6) townhomes and thirteen (13) condos, for a total of nineteen (19) new housing units. Despite community concerns and feedback, Aryze responded by authoring their August 21, 2023 Application Brief stating "... *we have received valuable feedback from the community* ..." (p.2) but increased both height and density by proposing to build thirty (30) units consisting of six (6) townhomes and twenty-four (24) apartments. An increase of eleven (11) additional units, despite concerns and feedback from community members who are personally impacted by the proposed changes. This constitutes an even further departure from both the spirit and intent of the OCP.

Meeting the Objectives of the Missing Middle Initiative

While we realize that the goal of the missing middle initiative is not "Affordable Housing", caution should be taken in terms of buying into a narrative that new developments should disregard current zoning and Official Community Plans by overtly increasing height and density to achieve the missing middle objectives. More specifically, the caution relates to proposed outcomes vs realized outcomes. Case in point, when the Rhodo was built by Aryze on Fairfield Road, the housing "crisis" and "missing middle" narrative was also very present. Fast forward to today when two townhouses in the Rhodo are listed for sale as follows:

[REALTOR.CA](https://www.realtor.ca) (as of July 2, 2024)

MLS #967978

\$1,549,000

118-1720 Fairfield Rd. (RHODO by Aryze)

3 bedroom Townhome

Property Taxes \$4,769

Maintenance Fees \$675 Monthly

The current rate for a 5-year fixed rate mortgage amortized over 25 years is 4.74%. With a \$309,980 (20%) downpayment, the monthly mortgage payment calculates to **\$7,029/month**. When strata fees and property taxes are included, the monthly cost for the \$1,549,000 property further increases to **\$8,101/month**.

MLS #965263

\$1,200,000

112-1720 Fairfield Rd. (RHODO by Aryze)

2 bedroom Townhome

Property Taxes \$3,679

Maintenance Fees \$440 Monthly

The current rate for a 5-year fixed rate mortgage amortized over 25 years at 4.74%. With a \$240,000 (20%) downpayment, the monthly mortgage payment calculates to **\$5442/month**. When strata fees and property taxes are included, the monthly cost for the \$1,200,000 property further increases to **\$6,188/month**.

The purpose of these examples is simply to illustrate that densification does not, by default, meet the objectives of the missing middle. We mention this within the context that the missing middle seeks to provide opportunities for housing in areas where purchasing a single family residential house may not be financially feasible for middle income earners. In our neighbourhood, single family older homes have sold for \$1.2 to \$1.4 million, many of which have secondary suites to provide additional income. Newer townhomes, however, have cost as much or more in some cases (an MLS listing on June 15, 2024 showed another Townhouse in the Rhodo for sale at \$1,750,000). In the first example provided above, it does not sound reasonable that middle income earners are able to pay **\$8,101 per month** for a mortgage and related costs.

We would also like to add that a homeowner who lives next door to the Rhodo, advised that some units were purchased by people outside of Victoria who bought them as secondary investment properties. If this is the case, it demonstrates there are no certainties in regard to who actually purchases new properties. This is not to suggest they should not be built, but highlights the importance of keeping things in perspective within the context that higher, denser multi-unit residential structures are not guaranteed to meet the objectives of the missing middle, nor are they necessarily justified in overriding Official Community Plans as they are distinctly a for profit business venture, not below market or lower income housing initiatives. There is nothing wrong with being in a for profit business, but such developments need to be kept in perspective.

Impact on Traffic Volume and Parking

It is important for the Mayor and Council to be aware that the volume of traffic and related parking on Fairfield, Beechwood, and Lillian continues to be impacted by development and other factors. On Beechwood specifically, the majority of homes have secondary suites and tenants, which normally results in the entire street lined up with parked cars at various times of the day, weekends, and most notably in the evenings. This also occurs during the day on Lillian Road due to businesses located near Wildwood. This, combined with the fact that Lillian is a narrow road that runs east/west and only permits parking on one side of the roadway, adds to the parking congestion.

To further aggravate the current parking situation, people who visit Hollywood Park for baseball games, tennis, and other activities are frequently unable to park on Fairfield Road resulting in an overflow of parking on Lillian and Beechwood. This will be further complicated by the proposed development which, unlike the Rhodo: a) will not have any underground parking; b) includes a plan with very limited above ground parking with fewer spaces than living units, and c) is designed such that on site parking access/egress is on Beechwood which is a residential side street. Parking for the Rhodo is accessed from Fairfield Road, but not the proposed development.

It should also be noted that the north end of Beechwood has a narrower roadway where the driveway for the proposed development will be located. The location of the driveway, narrow roadway, and limited street parking will further aggravate the overall parking congestion on Lillian and Beechwood. Additionally, the number of parking spaces is not only limited for the proposed development site, it fails to take into account the potential for more than one car per

family, in addition to volume from visitors, deliveries etc.

The higher the volume of occupancy and visitation at the proposed development site, the more congested parking will be on Beechwood, Lillian, and Fairfield Rd. While it's admirable that the development will have numerous places for bicycle parking, the fact is that area residents, tenants and those attending the local businesses primarily operate vehicles, and many of our local homeowners are driving electric cars which will be the future for vehicular transportation.

It is also critical to take into consideration the fact that Montague Court is a large mixed residential commercial property that borders Fairfield, Beechwood, and Lillian. It is a large site across from the proposed development that, in due course, will be completely redeveloped similar to the proposed development by Aryze, and this will significantly increase the volume of traffic, parking and overall activity in the area. It is very important that Council is aware of this as the future redevelopment of Montague Court will also have a significant impact on the area.

In addition to parking, traffic volume is also a consideration as increased density and height for the proposed development will result in increased vehicular traffic in the area which is already exacerbated due to the closure of Richardson at Foul Bay Rd. More specifically, traffic volume westbound from McNeill Avenue in Oak Bay is unable to continue westbound onto Richardson and have to reroute south or north on Foul Bay Road. Those who proceed southbound drive to Fairfield Road, turn right and pass by the elementary school, then proceed westbound on Fairfield Road towards the city. This has increased the volume of traffic on Fairfield Road, especially during workday hours and when Margaret Jenkins elementary school is in session.

Additionally, traffic from Oak Bay that choose to turn right off McNeill Avenue to proceed northbound on Foul Bay Road can no longer turn left on Quamicham Rd (the site of another Aryze Development). That road closure has also increased traffic volume in both directions on Foul Bay Road which has also added to increased traffic on Fairfield Road.

Overall traffic volume is a significant consideration in this area as there is a large amount of homes with families, children, and seniors, in addition to Margaret Jenkins Elementary School, Glengarry Hospital, Hollywood Park, and Fairfield Plaza, all of which are in close proximity.

Ensuring a Balanced Approach to Development in Fairfield/Gonzales

A May 13, 2022 Times Colonist article by Andrew Duffy commented on the *goal of the missing-middle housing program and the importance of ensuring new developments suit the character of neighbourhoods and preserve heritage*. While development is important and more housing is needed, it is also important for Council to ensure that land-use procedures and Official Community Plans are aligned. This can be accomplished by considering both the present and the future through decisions that strike a balance between development and community overall well-being. More specifically, Community Plans seek input and are authored for a reason, they seek to ensure new

developments in residential family oriented neighborhoods are reasonable in size and scope, limit impact on vehicular traffic, sewer, garbage, energy draw, carbon emissions, and overall balance (mass, height, general form, parking, greenspace, privacy of neighbouring homes, consistency with the neighbourhood).

Concerns regarding the height and density of the proposed development, are not dissimilar to those expressed by the community in relation to 349 Kipling and 1400 Fairfield in relation to Rezoning Application No. 00702 and Development Permit with Variances Application 000555 (Fairfield). That development did not proceed when staff and Council considered the nature and character of the existing housing and Community Plan.

The proposed development for 1733, 1735, 1737 Fairfield Road is a similar situation in that there are no four (4) storey multi-unit residential developments in the immediate neighbourhood, nor does the Community Plan support them. The original plan for 1733, 1735, 1737 Fairfield Road was to tear down the three existing residential homes to build nineteen (19) units. We were part of a local community group that were supportive of increased density through the construction of townhomes (similar to the Rhodo). In 2022 we made Aryze (Matthew Jardine and Ryan Goodman) aware that we supported a new development of townhouses at the site in question but stressed the importance of limiting overall height and density in a manner consistent with the OCP. That support has not wavered but we do not support the current design and proposal.

Concluding Remarks

In closing, providing input and asking questions in an effort to ensure a balanced approach to local development is both reasonable and necessary and should be encouraged. Everyone should have a voice through a process that is mutually respectful of the opinions of all involved, that is why Official Community Plans are developed.

Whenever our family has been involved with community discussions regarding input on developments there is often a lot of judgment and shaming from individuals outside the community who use the term NIMBY in an effort to silence local tax paying citizens who have paid mortgages for 20, 30, and even 40 years in order to raise families in Victoria neighborhoods. Being in favour of reasonable and prudent development that aligns with Community Plans is both normal and encouraged, it is not Nimbyism.

We'd like to thank the Mayor, Council, and staff at the City of Victoria for the opportunity to provide input and we remain hopeful that our input will be given consideration. Our neighborhood is not opposed to development, we just ask for a balanced approach that takes into consideration the interests of all stakeholders, including the local community who are directly impacted.

Thank you,
David Green
266 Beechwood Avenue

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: 1733 Fairfield Road
Date: July 3, 2024 3:06:30 PM

Regarding development at 1733, 1755 and 1737 Fairfield Road

Dear Mr. Carroll

I am opposed to the 4 storey residential building proposed for this site for many reasons.

This proposal has nothing to benefit and will only be to the detriment of the Gonzales Beach neighbourhood. It brings more people and cars into the neighbourhood without contributing anything. No shops, no progressive energy proposals, no life, nothing. It is like resource extraction, move in, decimate the area, make money, move on.

On top of this, it is a shockingly poor building, not in the slightest in keeping with the neighbourhood. So many opportunities for this site and the developer has only managed something reminiscent of the sterile buildings of the 1960's with a dash of office block stuck on. As well, the site is entirely built over with only tiny patches of greenery in total contrast to the rest of the neighbourhood.

When the City began proposals for the Community Plan, the discussions were about allowing a variety of developments that would enhance the neighbourhood for the existing inhabitants as well as bring new housing. This development is only about housing and will impact very negatively on our neighbourhood. There are existing interesting and useful small shops close by that benefit from being outside the Fairfield strip mall and, as I understood the Community Plan, the point was to promote developments that included a variety of uses.

It is understandable that the city wishes to deter car ownership but simply reducing off road car parking at this point in time is not workable. More cars will be parking on the local roads and this is a big problem for everyone, children and cyclists in particular. I regularly cycle on Fairfield Road and, as it is narrow and without a cycle path, parked cars are a major hazard.

The building should:

- be a maximum of 3 stories
- have more green space
- include more parking, preferably underground

- include retail / business space.

There is nothing about this building that says Community. We will have to live with this poor building looming over our homes for the rest of our time here. The developer must do better.

Regards

Stephen Brown
310 Robertson Street

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: 1733, 1735, 1737 Fairfield Road Proposed Development
Date: July 4, 2024 2:20:02 PM

Dear sir,

I have not been able to access this development online.

From information I've received by mail and the meetings I have attended my understanding is this is now a 31 unit development with 22 parking spaces and 1 Modo car share on the street.

1. Parking is already an issue in this area as many of the houses already have suites, and 2 of the old houses on Beechwood Avenue are triplexes.

At least 4 street parking spots on the 300 block of Beechwood will disappear for the sidewalk widening and 3 minute parking zone for the development.

2. The initial plan was for 19 units in March of 2022 with 22 parking spots.

3. March 2023 the plan was 30 units and 22 parking spots. The dirty deal between City Hall, Aryze Developments, and The Land Conservancy(TLC) seemed to enable this addition.

Who knew that the zoning had never been changed on Abkhazi Garden, and a \$300,000 "gift" (bribe) to the TLC to help them with their debts could add on another 11 suites!

Double insult to me as I had donated money to buy Abkhazi Gardens from being developed into townhouses in 2000!

4. October 2023 another suite added to the development for a total of 31. One modo car share spot added to the already crowded street parking.

5. The home at 1745 Fairfield Road is totally overwhelmed by this development, and they will get little to no sunlight. This family has a suite in their house. The spouse has a disability. The house is set up for this family to live in.

I am not anti development, and was quite happy with the 19 homes in the initial plan other than the fact that the facade of the building looked like a bunker, and did not fit into the neighbourhood. The Cottages across Beechwood are heritage, and surely an architect/developer could do better.

How many renters will be displaced when the 3 houses are torn down? I'd say at least 5.

None of these "homes" are for rent, or lower-middle income to buy. It is all for huge profits. How does this help the Missing Middle?

Shame on City of Victoria if this development goes through as planned.

Mary Sutton,
251 Beechwood Avenue, Victoria B.C. V8S 3W6
[REDACTED]

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: Fwd: Proposed Development - 1733, 1735, and 1737 Fairfield Road
Date: July 5, 2024 9:39:35 AM

Patrick, thank you for the opportunity to provide feedback.

The amended proposals for the development at 1733, 1735, and 1737 Fairfield Road add even more unwarranted density and provide less parking. I trust City Council will consider the impact to our neighbourhood; and in so doing so reject the proposal.

Aryze is proposing a looming four-story apartment building that is over 20 feet higher than the surrounding houses and will become the highest structure in all of Gonzales. There are no front or rear setbacks, no useable ground level outdoor space and minimal landscaping. The building is highly disruptive to the neighbourhood because of its height and mass, density, layout, appearance, number of units, parking, no greenspace and impact on the neighbor's privacy.

Aryze has not demonstrated any added community amenities to merit the proposed density transfer nor does it provide a convincing case that the receiver site is suitable. The developer has simply bought density from a third party to maximize profit and usurp city planning bylaws well beyond what should be considered reasonable for the site.

The requested density is far beyond what the site and neighborhood can accommodate. It does not comply with the OCP and amendments will be needed to increase the height beyond the three stories maximum required in a Traditional Neighbourhood designation (Section 6.1.5 and Map 23) and Floor Space ratio (FSR) from 1.1 to 1.79, as well as front/back/side setback variances.

I believe densification efforts in Gonzales should provide quality housing options for families. Any new development needs to be compatible with neighbors, have respectful front and rear yard distances, usable rear yards, access to outdoor open green space, consistent massing, adequate underground parking and consistent character. In other words, all infill buildings in Traditional Residential areas of Gonzales should be ground-oriented dwellings that are limited to two and a half story houseplexes, duplexes and townhouses. Victoria could give families more choice to live in something other than a condo.

Thank you for your consideration.

Kevin Warren
356 Robertson Street

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: Proposed Development - 1733, 1735 and 1737 Fairfield Road
Date: July 5, 2024 10:51:48 AM


Thank you for the opportunity to provide feedback on the proposed development at 1733, 1735 and 1737 Fairfield Road. As a close neighbor to this site, I have followed the application process closely over the past two years and have provided previous feedback to the City of Victoria and the developers expressing concern about the height and density. I am aware that many of my neighbors have communicated the same concerns. Every revision by the developer seems to propose an even higher level densification for this site and disregard previous feedback on a range of issues.

The proposed density transfer appears to benefit the developer with little benefit to the city, and a significant detriment to the Gonzales neighborhood. The developer has not demonstrated that the receiver site can accommodate this level of density, which is far beyond current OCP requirements. The four story apartment-style building design with no front or rear setbacks, no usable ground level outdoor space and paved outdoor areas with minimal landscaping, is not aligned with principles of family-focussed attainable housing or green space enhancement.

I am very supportive of densification efforts for this site and elsewhere in Gonzales that provide quality housing for families that is compatible with the character of our neighborhood, respectful of neighbors, and protects our greenspace. In Traditional Residential areas I would like to see ground-oriented dwellings that are limited to two and a half story houseplexes, duplexes and townhouses.

The height, mass and density proposed for 1733, 1735 and 1737 Fairfield Road far exceeds what is reasonable in this neighbourhood. It will be highly disruptive, both to close neighbours and the wider community, who use nearby parks, shopping and schools. I respectfully request Victoria City Council reject the revised proposal for this site.

Janice Linton
356 Robertson Street
[REDACTED]

From: 
To: [Patrick Carroll](#)
Subject: 1733,1735,1737 Fairfield Road
Date: July 5, 2024 11:06:35 AM

Dear Mr.Carroll,

I reside at 327 Beechwood Ave, directly behind the proposed development at the above address. I have several concerns regarding the proposed development, the most pressing are as follows:

1. Height -

The 4 story design will be very imposing on my property. The fourth story looks down into my private patio and will severely compromise my privacy.

The height of the building will also block the light coming into the property, affecting the garden and plants on my lot. My wellbeing will also be affected by the shadow of this building.

There are already 4 apartment/condominium developments in this 2 - 3 block area, but at this time they have been held to 3 stories, which makes them more compatible with the height of the existing buildings. I do not think that this OCP bylaw amendment will bring any benefit to anyone other than the developer (See point below).

2. Density -

An additional 29 units will be 9.7 units per lot, far and away above what has been intended for this neighbourhood. I use units per lot rather than the FSR as this provides more clarity to someone who is not a developer. The notion of the trade of density to "save Abkazi gardens" is laughable, as is the notion that "this will provide housing for the missing middle". The condos in the Rhodo at 1712 - 1720 are already reselling for more than the "missing middle" can afford. I have two adult children with young families who are no longer able to live in Victoria. Much to their and my dismay, homeownership for them is unreachable here. Yes, the city does need to increase density, and is currently doing so with buildings going up everywhere. Fairfield Road however, is shouldering too much of the density without any additional infrastructure to support it.

3. Traffic -

The Rhodo development at 1712 - 1720 Fairfield Ave has just been completed on the opposite

side of Fairfield Road within the block and has had a very unfavourable effect on the traffic in the area. It is no longer safe to make a left turn onto Fairfield Road from Beechwood Ave or 1 block south from Lillian Road. The parking on Beechwood Ave has already been overwhelmed. 29 additional units will bring 29 - 58 additional cars to this block which is unacceptable. Traffic congestion along Fairfield Road is severe.

4. Property Value -

My realtor tells me this development will decrease my property value.

In summary -

I am of the opinion that this area of Fairfield Ave already has enough multiunit housing, however, if it must be, then I would like to see a development that remains at 3 stories or below and with density that conforms to the current community plan. As well, I would like to see at least one parking spot per unit. The parking along Fairfield Ave and Beechwood Ave is already so maxed out that it is difficult for the residents of and the visitors to Beechwood Ave to find parking. We are already at a place where there is a need to consider installing a traffic light at Beechwood Ave and Fairfield Ave, or decreasing the speed limit to 30 km on Fairfield Road to increase safety at these already dangerous intersections (Beechwood Ave and Lillian St).

With Respect,

Liza (Elizabeth) Pelzer
327 Beechwood Ave
Victoria, BC
V8S 3W8

[REDACTED]

[REDACTED]

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: Public hearing for 1733-1737 Fairfield rd
Date: July 5, 2024 8:12:15 PM

Hello,

Please see our feedback below for the proposal for 1733-1737 Fairfield rd

Our family, me, my husband and our toddler, live at 1734 Fairfield Rd; directly across the street from the proposed development of 1733, 1735 and 1737 Fairfield Road into a multi-residential building consisting of 29 units with a height of four stories. We have concerns about this development not only as neighbors that will be directly affected and impacted but also as members of the community.

The first concern that we have is that Aryze Developments is constantly developing housing with the promise that there will be affordable housing in these developments but without following through with this promise. We are more than supportive of an increase in our housing stock in Victoria and in all communities within Greater Victoria. However, we are concerned, especially with Aryze's history, that these projects and the units built will be out of range for middle income families and will be priced well out of any affordable price range and, if they are rented out, their rents will be unaffordable as well.

The *Rhodo* which was developed by Aryze and is down the street from our house on Fairfield has three units that have gone up for sale in the past month with prices of up to of \$1.7 million. This is of course out of range for any middle income family to afford so my question for the Council is what are you actually doing to help increase the affordable housing stock in Victoria and address this "missing middle"? Will you hold these developers to the promises they made? We are not talking about subsidized housing but affordable housing so families can live in all neighborhoods of Greater Victoria comfortably without their rent being 70% of their income or so that purchasing a house is only a pipe dream. We have little confidence that Aryze will follow through on the promise of affordable units so when are they going to be held responsible for breaking this promise multiple times?

Another concern we have is of course parking. We rent the top floor of our house and have three tenants, most of the houses in our block of Fairfield also have multiple tenants. In the meeting that was held late last year Aryze's response to the question about parking was that it is not their responsibility where trades park. This is not acceptable, it is their responsibility to come up with a reasonable plan so that everyone has access to parking including the homeowners, renter and the trades workers. Aryze needs to take more responsibility to be a good neighbor as this will affect many people on this block including renters, homeowners, people who have

caregivers who visit daily, people who have home businesses etc. It is the Council's responsibility to hold developers like Aryze to the standard that they promised when these projects are proposed, how many times are these promises going to be broken before the Council decides to do anything about it?

Furthermore, if the community plan is amended to accommodate the proposed Aryze development of 1733, 1735 and 1737 Fairfield Road we would like to know whether that amendment affects or applies to other properties in the community. Can you please clarify whether this amendment has broader application throughout the community.

Thank you

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: Re: 1733,1735, & 1737 Fairfield Road Project
Date: July 6, 2024 6:55:49 AM

On Jul 5, 2024, at 10:15 AM, David Wilks [REDACTED] wrote:

On Jul 4, 2024, at 9:51 AM, David Wilks [REDACTED] wrote:

Hello Mr. Carroll,

We received a notice from the city seeking input on this project.

We would like to advise that we are opposed to 4 story apartments being situated in single family neighbourhoods. That will result in loss of privacy for neighbours; parking issues; more vehicle congestion on Fairfield Road making it less safe for everyone, more emissions, less green space and will these units be affordable.

Also, we are not supportive of the up zoning of all Victoria neighbourhoods for 4, 6 and larger apartment buildings as proposed in the community plan survey. This proposal is even higher than the density that is legislated by the NDP government. Victoria's density in 2021 was 4,722, seventh highest in the country. With Victoria's population in 2023 being close to 100,000, the density is over 5,100. Victoria seems to be doing more than its fair share in accommodating population increases in the CRD and BC. Increased density has not helped downtown businesses or ended the chaos on the downtown streets. This up zoning will increase land costs. Also, what about infrastructure, parks, recreation centres- one for over 100,000 people, schools, loss of tree canopy and health care.

It just seems we are going to pave over what makes Victoria a great place to live!

Thank you.
David Wilks and Linda Park

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: Proposed Development - 1733, 1735 and 1737 Fairfield Road
Date: July 6, 2024 11:06:20 AM

This note is in response to the latest proposed development for 1733, 1735 and 1737 Fairfield Road. If this development goes through, I would be a neighbour (living at 417 St. Charles St.)

I find that the proposed development far exceeds the limits noted in the Official Community Plan (OCP), and I question why such a development proposal is being considered.

Given that the OCP states two to three storeys for a development in a Traditional urban space (and four storeys is being proposed), and that the OCP also states an FSR of 1:1.1 (but a much denser 1.79:1 is being proposed), I think that the development proposal should have been rejected right away. Why are proposals that far exceed the OCP even being entertained?

I think that we will also find (and are currently finding out) that these types of new developments are not supporting missing middle housing. I.e. the developers are not selling the new units at a cost that the "missing middle" can afford.

Needless to say, I strongly oppose this current proposal and I hope that such concerns are taking into account this time around.

Thank you.

Michelle Crompton
417 St. Charles St.
[REDACTED]

July 6, 2024

Dear Mr Carroll,

Please consider our feedback to DENY changing the Traditional Residential urban plan designation for properties at 1733, 1735 and 1737 Fairfield Road.

Our reasons follow:

Social Impact/Accessibility-The proposed change in urban plan designation does not help alleviate the need for affordable housing or address other social issues that most city councillors made election issues. In addition, the building would be near a school but not accessible for most families. The long term affect is that when fewer families reside in a catchment area, schools close. Neighbourhoods loose the energy and vitality of students using the playgrounds. Vandalism follows..

Tourism-A four story building is completely out of place in our neighbourhood. If each unit is 12 feet high or more and the facade has a modern design this big building will disrupt the charm that visitors expect as they tour historic Gonzales Fairfield.

Precedent-Approving the proposed changes will result in other developers expecting carte blanche to erect building four stories and more. The downtown is changing from Victoria to Condoria. Dont let that trend extend to pictuesque neighbourhoods that tourists see in advertisements..

Parking 1-Since the original proposal the number of parking stalls has been decreased but one Modo stall added. One of us is a Modo member who bought a car because the demand for Modo bookings in Gonzales Fairfield exceeds availability. One Modo at the 4 story development would not change the residents' perceived need for cars.

Parking 2-We live near the apartments at Fairfield and Lillian where there are only a few parking stalls. Almost every day renters and their visitors block our driveway. There is going to be an increase in parking bylaw offenses throughout our neighbourhood if large multi-family residences are constructed without a parking space designated for each dwelling.

Stormwater-Problems already exist with perimeter drain overload in Gonzales, Fairfield. Changing the existing 3 permeable yards to a four story block of concrete surfaces will increase storm water flowing to adjacent properties.

Sewage-A four story building housing more people equals more crap in the same sewers that serviced three homes.

Finally, the proposed trading scheme with the Abakazi Gardens is ludicrous. If Aryze Developments is granted approval to build their four stories on partly imaginary land, we'd like to pay our property tax invoices with imaginary money. (LOL)

Sincere Regards

Maureen Eley-Round and Leon Sinclair
Owner residents of 267 Wildwood Ave.
Phone [REDACTED]

Subject: 1964 Fairfield Road and 507 Foul Bay Road
Date: June 14, 2024 8:42:16 AM

Hi Patrick,

I'm writing to express my support for the OCP amendment of the noted address (Abkazi Gardens). My family and I live at 501 Foul Bay Road and regularly visit the gardens. It's always busy, well used and appreciated by the community.

While I am typically in favour of higher density land-use in the City, I am happy to see the reduction in density for this site and change in permitted land-use.

I've cc'd my wife Meghan Walker who is also in support.

Thanks,
Colin

Colin Harper

Principal | Architect AIBC

COLIN HARPER ARCHITECT

██████████

██████████

To:

[Patrick Carroll](#)

Subject:

Zoning Fairfield and Foul Bay Roads

Date:

June 11, 2024 2:28:32 PM

Dear Mr. Carroll.

We just received your letter about the rezoning of 1964 Fairfield Road and 507 Foul Bay Road (Abkhazi Garden). Actually we were surprised to receive it as we had thought it was all settled quite awhile ago. Anyway, we certainly hope it can be left as the beautiful garden and other features for the public and members to enjoy for many years to come. I volunteered there many years ago and attended the council meeting when it was first saved as a green space or park. While the property is greatly enjoyed by residents of Victoria, it also attracts visitors and tourists from out-of-town to our city. We have seen several bus loads of tour groups come to see the Garden over the years.

As Victoria becomes more built up and populated, the need for more greenspaces such as this one should certainly be part of the Official Plan. Greenspaces can help to promote good health and are the "lungs" of our city. We hope the city encourages builders to provide more green space around their structures than some projects around here do. Perhaps an increase in setbacks is possible for the future.

Yours truly

Joyce Harrison

Adrian Harrison

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: 1964 Fairfield Road and 507 Foul Bay Road
Date: June 14, 2024 9:13:33 AM

Dear Mr. Carroll,

We are writing to express our support for the proposed Official Community Plan amendment, for the above-noted addresses, to change the urban place designation from Traditional Residential to Public Facilities, Institutions, Parks and Open Spaces.

Thank you for the opportunity to comment on this proposal.

Sincerely,

Shan Marcus and Jacqueline Pierce
2007 Romney Road

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: Note for clarification re Abkhazi
Date: June 16, 2024 10:45:07 AM

The new site-specific zone limiting use and density for Abkhazi Gardens would allow additional floor area for a future accessory building. File is associated with REZ00821. I cannot find anything about future accessory buildings and the regulations that would apply. The gardens back onto the private rear yards of many homes and I would like assurance as to the permitted maximum height and setbacks from property lines of accessory buildings that could be permitted. In normal residential development rear yards are back to back and a minimum of 30 feet rear yard is expected and therefore adjacent residential buildings would be a further 30 feet distant and sheds etc would have a separate height limitation and setback requirements.

The gardens have been good neighbours however the proposed composting facility is immediately adjacent to my rear garden area where I have seating. Composting does take place already and only occasionally have I experienced odours from this composting. I would like assurance that composting facilities will not be expanded and that steps are taken in accordance with recommendations given in _

Controlling Composting Odors - BioCycle

<https://www.biocycle.net/controlling-composting-odors/#:~:text=Composting%20is%20never%20odor%2Dfree.,odors%20are%20going%20to%20form.>
There has been a BC Government review - chrome-extension://oemmnadbldboiebfnladdacbfmadadm/https://www2.gov.bc.ca/assets/gov/environment/air-land-water/air/reports-pub/odour_mgt_final_june13_05.pdf

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: Rezoning No. 00845
Date: June 23, 2024 4:41:39 PM

Good day,

My properties are neighbors to the proposed rezoning of 1964 Fairfield Rd and 507 Foul Bay Road.

We support this rezoning strongly as that oasis of nature is so important to us. Q

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: Rezoning No.00845
Date: June 23, 2024 5:25:39 PM

Hello Patrick, We totally agree with proposed rezoning amendment.

How could anyone want to risk losing or changing such an iconic property that attracts so many local and visiting people year round to Abkhazi Gardens.

Clive and Anne Sawdon

361 Foul Bay Road

[REDACTED]

From: [REDACTED]
To: [Patrick Carroll](#)
Subject: 1964 Fairfield Rd
Date: July 1, 2024 12:29:04 PM

To: Patrick Carroll, Senior Planner,
City of Victoria
July 1, 2024

Dear Patrick,

As close neighbours to the Abkhazi Garden, we're writing in support of the proposed zoning change for the property. The neighbourhood and the volunteer community have worked hard to keep this garden open to the public for many years. Rezoning and placing the garden in the protection of Parks will hopefully preserve this historic site for continued public use through the future.

Thank you,
Virginia and Jeff Errick
615 Foul Bay Rd

Sent from my iPad

NO. 24-055

A BYLAW OF THE CITY OF VICTORIA

The purpose of this Bylaw is to amend the Official Community Plan to support increased density for a portion of the Traditional Residential Urban Place area at the northeast corner of Fairfield Road and Beechwood Avenue.

Under its statutory powers, including Part 14, Division 4 of the *Local Government Act*, the Council of the Corporation of the City of Victoria in an open meeting enacts the following provisions:

- 1 This Bylaw may be cited as the “OFFICIAL COMMUNITY PLAN BYLAW, 2012, AMENDMENT BYLAW (NO. 54)”.
- 2 Bylaw No. 12-013, the Official Community Plan Bylaw, 2012, is amended by adding the following immediately after Section 21.12:
 - 21.12A Place-specific departures from the Urban Place Designation guidance in this neighbourhood include:
 - > For the Traditional Residential area on the northeast corner of Fairfield Road and Beechwood Avenue, consider supporting a maximum density of approximately 1.8 floor space ratio.

READ A FIRST TIME the	day of	2024
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READ A SECOND TIME the	day of	2024
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Public hearing held on the	day of	2024
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READ A THIRD TIME the	day of	2024
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CITY CLERK

MAYOR

A BYLAW OF THE CITY OF VICTORIA

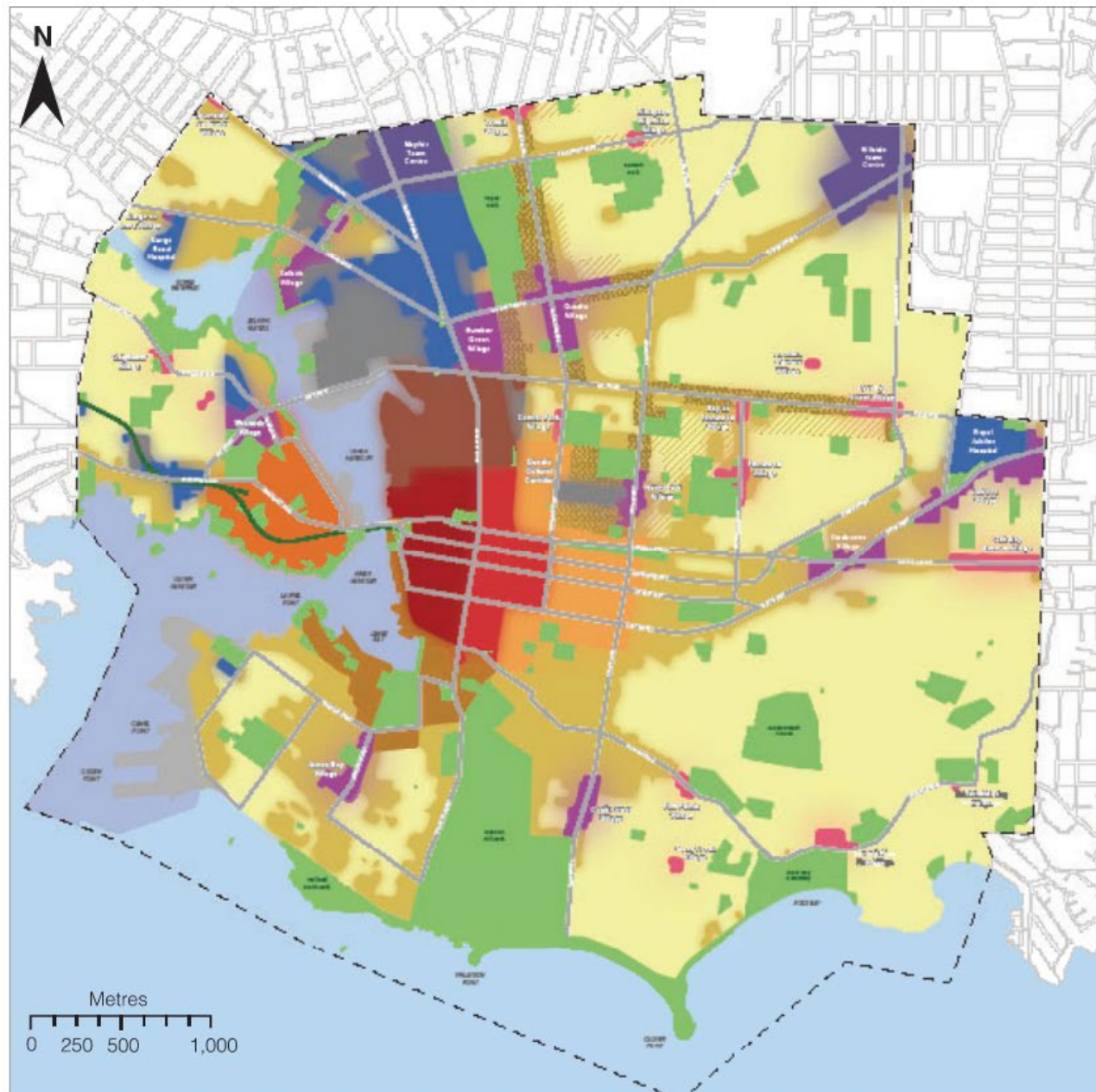
The purpose of this Bylaw is to amend the Official Community Plan to change the urban place designation for 1964 Fairfield Road and 507 Foul Bay Road from Traditional Residential to Public Facilities, Institutions, Parks and Open Space.

Under its statutory powers, including Part 14, Division 4 of the *Local Government Act*, the Council of the Corporation of the City of Victoria in an open meeting enacts the following provisions:

- 1 This Bylaw may be cited as the “OFFICIAL COMMUNITY PLAN BYLAW, 2012,
AMENDMENT BYLAW (NO. 55)”.
- 2 Bylaw No. 12-013, the Official Community Plan Bylaw, 2012, is amended by repealing
Map 2: Urban Place Designations and replacing it with a new Map 2: Urban Place
Designations, attached to this Bylaw in Schedule 1.
- 3 Bylaw No. 12-013, the Official Community Plan Bylaw, 2012, is amended by repealing
Map 11: Parks, Open Space and Recreational Facilities and replacing it with a new Map
11: Parks, Open Space and Recreational Facilities, attached to this Bylaw in Schedule 2.
- 4 Bylaw No. 12-013, the Official Community Plan Bylaw, 2012, is amended by repealing
Map 25: Gonzales Strategic Directions and replacing it with a new Map 25: Gonzales
Strategic Directions, attached to this Bylaw in Schedule 3.

ADOPTED on the _____ day of _____ 2024

MAYOR





MAP 11

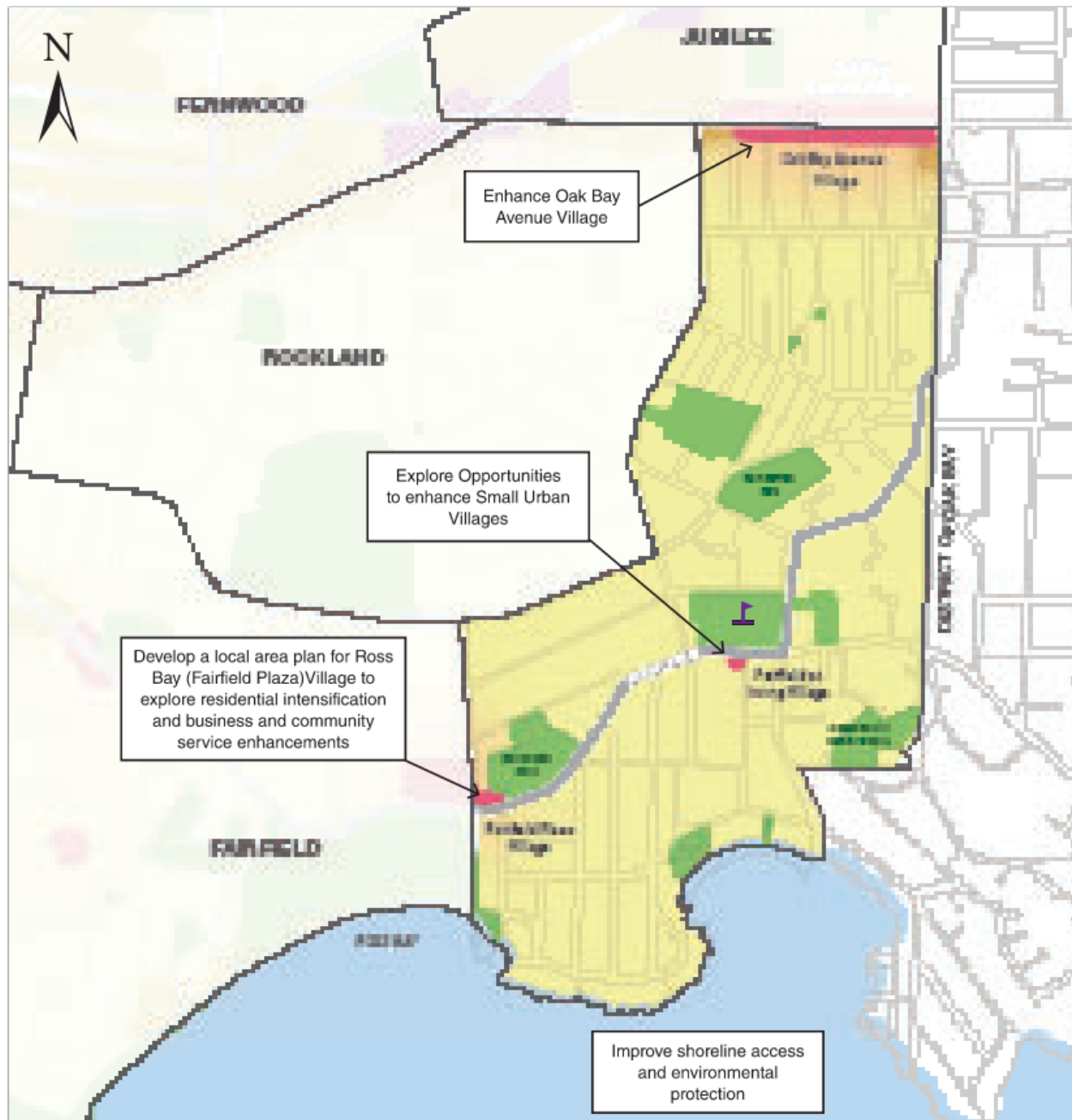
Parks, Open Space and Recreational Facilities

- C** Community Centre
- R** Recreation Centre
- S** Seniors Centre
- A** Arena
- ⌵** Existing Public School
- ⊙** Proposed Park (approximate location)
- Green** Park
- Yellow** Open Space
- Green Line** Designated Greenway
- Black Line** Rail Corridor

Park: Land managed by the City of Victoria as a park.

Open Space: Land that is generally publicly-accessible, other than City parks. Includes private lands, public lands and other City-held property, such as greens and street rights of way.

This map is provided for information purposes only. Please refer to Map 2 and Figure 8 for designation information.



MAP 25

Gonzales

Strategic Directions

Urban Place Designations*

- Small Urban Village
- Urban Residential
- Traditional Residential
- Public Facilities, Institutions, Parks and Open Space
- Marine

Public Facilities

- Existing Public School

*Urban Place Designations are provided for information only. Please refer to Map 2 and Section 6 for designation information.

A BYLAW OF THE CITY OF VICTORIA

The purposes of this Bylaw are to amend the Zoning Regulation Bylaw by creating the FRMD Zone, Fairfield Road Multiple Dwelling District and to rezone land known as 1733 Fairfield Road, 1735 Fairfield Road and 1737 Fairfield Road from the R1-G Zone, Gonzales Single Family Dwelling District to the FRMD Zone, Fairfield Road Multiple Dwelling District.

The Council of The Corporation of the City of Victoria in an open meeting enacts the following provisions:

- 1 This Bylaw may be cited as the “ZONING REGULATION BYLAW, AMENDMENT BYLAW (NO. 1343)”.
- 2 Bylaw No. 80-159, the Zoning Regulation Bylaw, is amended in the Table of Contents of Schedule “B” under the caption PART 3 – MULTIPLE DWELLING ZONES by adding the following words:

“3.157 FRMD Zone, Fairfield Road Multiple Dwelling District”
- 3 The Zoning Regulation Bylaw is also amended by adding to Schedule B after Part 3.156 the provisions contained in Schedule 1 of this Bylaw.
- 4 The land specified below and shown hatched on the attached map, is removed from the R1-G Zone, Gonzales Single Family Dwelling District, and placed in the FRMD Zone, Fairfield Road Multiple Dwelling District:
 - (a) 1733 Fairfield Road, legally described as PID 001-887-955, Lot 9, Section 68, Victoria District, Plan 1280;
 - (b) 1735 Fairfield Road, legally described as PID 002-493-802, Lot 10, Section 68, Victoria District, Plan 1280; and
 - (c) 1737 Fairfield Road, legally described as PID 007-628-323, Lot 11, Section 68, Victoria District, Plan 1280.

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Public hearing held on the	day of	2024
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ADOPTED on the

day of

2024

CITY CLERK

MAYOR

PART 3.157 – FRMD Zone, Fairfield Road Multiple Dwelling District**3.157.1 Permitted Uses in this Zone**

The following uses are the only uses permitted in this Zone:

- a. Uses permitted in the R1-G Zone, Gonzales Single Family Dwelling District, subject to the regulations set out in Part 1.6 of the Zoning Regulation Bylaw
- b. Multiple dwelling

3.157.2 Lot Area

- a. Lot area (minimum) 1740.00m²
- b. Lot width (minimum) 38.00m

3.157.3 Floor Area, Floor Space Ratio

- a. Floor space ratio (maximum) 1.79:1.00

3.157.4 Height, Storeys

- a. Principal building height (maximum) 13.45m
- b. Storeys (maximum) 4.00m

3.157.5 Setbacks, Projections

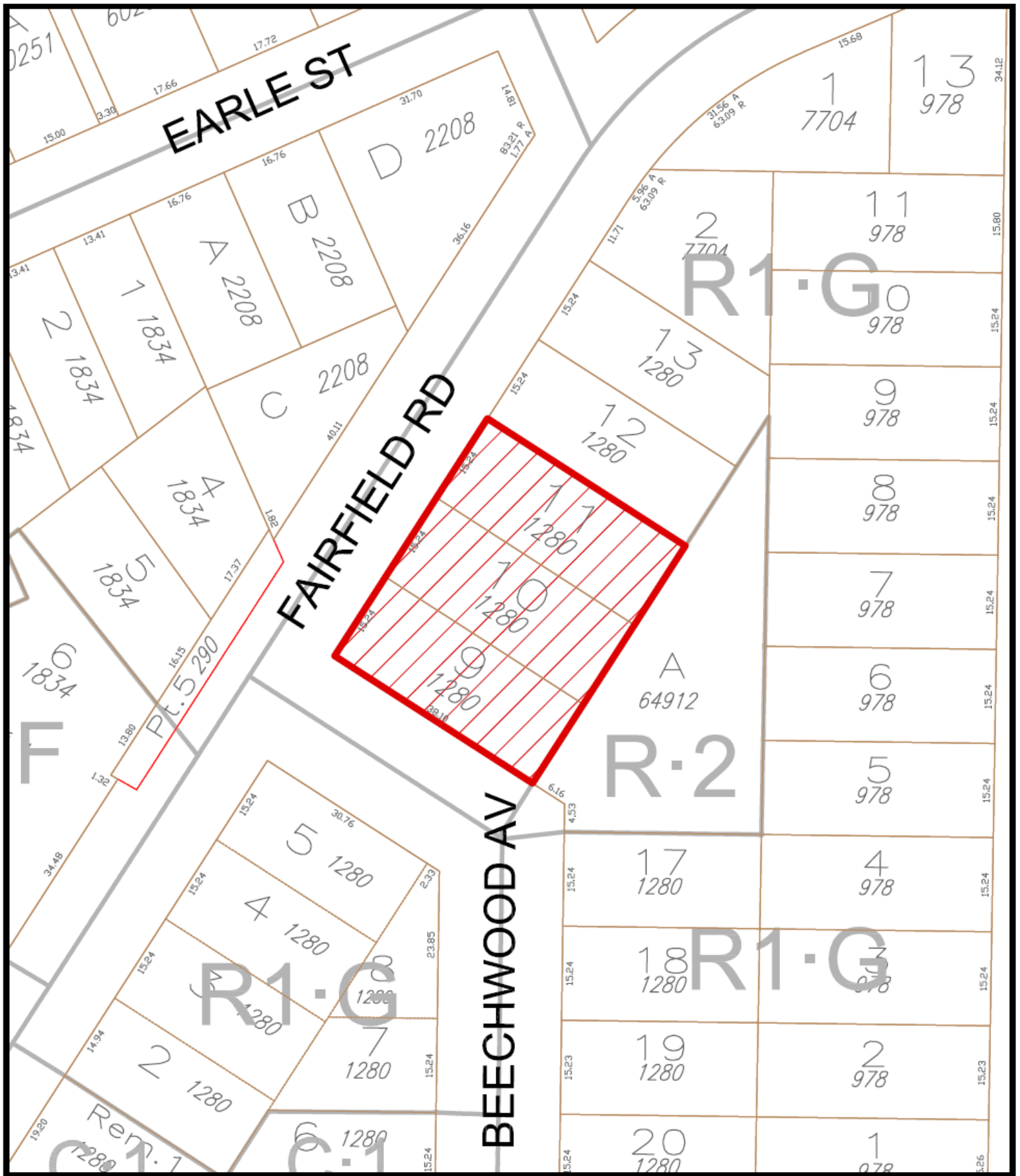
- a. Front yard setback (minimum) 4.00m
 Except for the following maximum projections into the setback:
 - Steps less than 1.7m in height 1.80m
 - porch 1.70m
- b. Rear yard setback (minimum) 10.00m
- c. Side yard setback from interior lot lines (minimum) 6.00m
- d. Side yard setback on a flanking street for a corner lot (minimum) 4.00m
- e. Any balcony or deck that faces a street boundary may project into a setback (maximum) 1.70m

PART 3.157 – FRMD Zone, Fairfield Road Multiple Dwelling District**3.157.6 Site Coverage, Open Site Space**

- | | |
|-------------------------------------|--------|
| a. <u>Site Coverage</u> (maximum) | 40.00% |
| b. <u>Open site space</u> (minimum) | 50.00% |

3.157.7 Vehicle and Bicycle Parking

- | | |
|------------------------------|--|
| a. Vehicle parking (minimum) | Subject to the regulations in Schedule "C" |
| b. Bicycle parking (minimum) | Subject to the regulations in Schedule "C" |



1733-1737 Fairfield Road
Rezoning No.00821



A BYLAW OF THE CITY OF VICTORIA

The purposes of this Bylaw are to amend the Zoning Regulation Bylaw by creating the AGH Zone, Abkhazi Garden Heritage District, and to rezone land known as 1964 Fairfield Road and 507 Foul Bay Road from the RK-11 Zone, Fairfield Townhouse District (1964 Fairfield Road) and the R1-G Zone, Gonzales Single Family Dwelling District (507 Foul Bay Road) to the AGH Zone, Abkhazi Garden Heritage District.

The Council of The Corporation of the City of Victoria in an open meeting enacts the following provisions:

- 1 This Bylaw may be cited as the “ZONING REGULATION BYLAW, AMENDMENT BYLAW (NO. 1344)”.
- 2 Bylaw No. 80-159, the Zoning Regulation Bylaw, is amended in the Table of Contents of Schedule “B” under the caption PART 9 – OTHER ZONES by adding the following words:

“9.6 AGH Zone, Abkhazi Garden Heritage District”
- 3 The Zoning Regulation Bylaw is also amended by adding to Schedule B after Part 9.5 the provisions contained in Schedule 1 of this Bylaw.
- 4 The land known as 1964 Fairfield Road, legally described as PID 005-896-444, Lot 1, Section 68, Victoria District, Plan 6009 and shown hatched on the attached map, is removed from the RK-11 Zone, Fairfield Townhouse District, and placed in the AGH Zone, Abkhazi Garden Heritage District.
- 5 The land known as 507 Foul Bay Road, legally described as PID 001-039-857, Lot 3, Section 68, Victoria District, Plan 37953 and shown hatched on the attached map, is removed from the R1-G Zone, Gonzales Single Family Dwelling District, and placed in the AGH Zone, Abkhazi Garden Heritage District.

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Public hearing held on the	day of	2024
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CITY CLERK

MAYOR

PART 9.6 – AGH Zone, Abkhazi Garden Heritage District**9.6.1 Permitted Uses in this Zone**

The following uses are the only uses permitted in this Zone:

- a. Recreational garden, which may include the following accessory uses:
 - i. Restaurant
 - ii. Retail sales
- b. Accessory Buildings subject to the regulations in Schedule “F”
- c. Notwithstanding paragraphs a. and b., the only use permitted on a lot with a lot area less than 5664.60m² is an accessory building

9.6.2 Floor Area, Floor Space Ratio

- a. Floor space ratio (maximum) 0.04:1

9.6.3 Height

- a. Storeys (maximum) 1.00

9.6.4 Setbacks

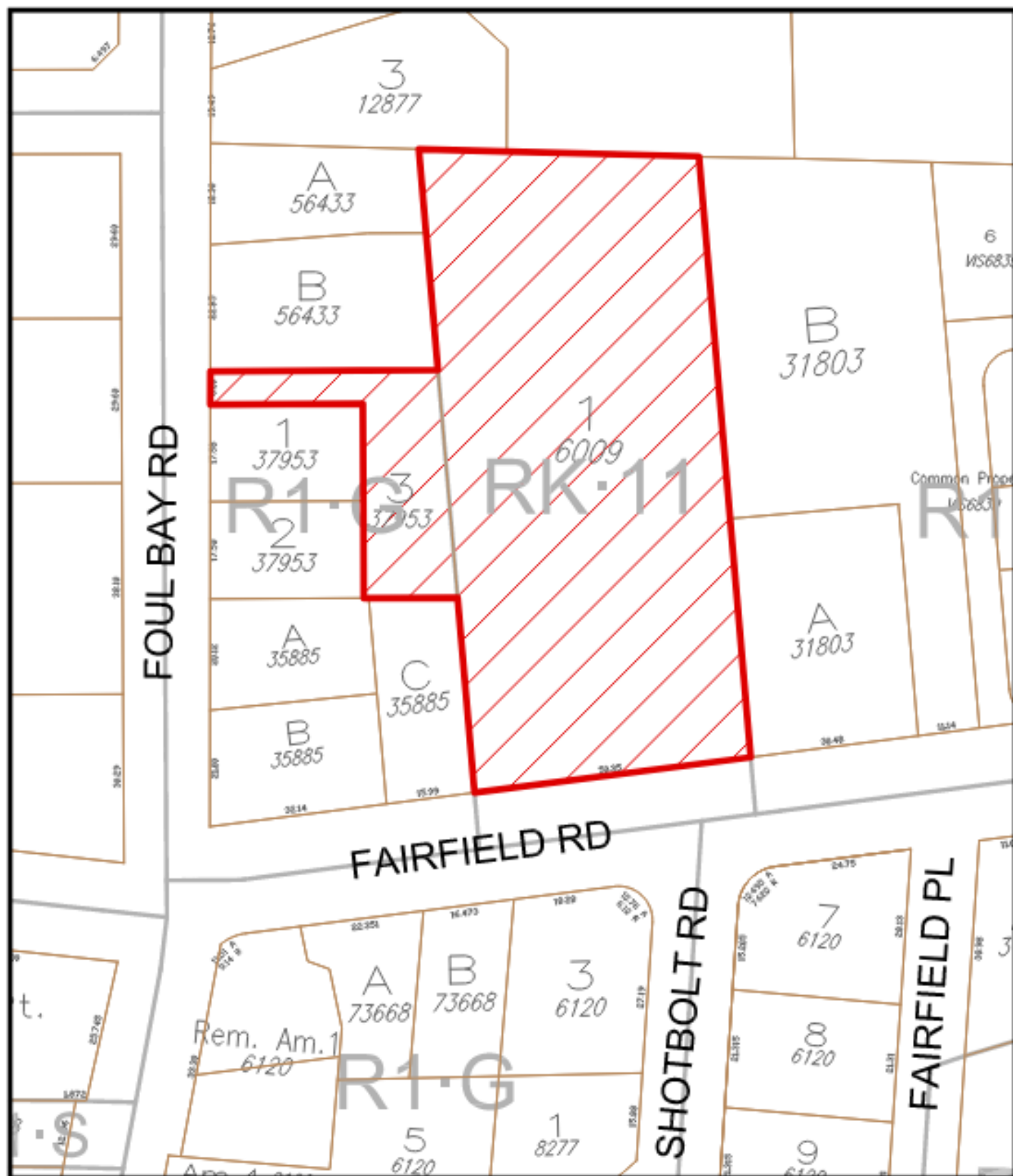
- a. Front yard setback (minimum) 41.00m
- b. Rear yard setback (minimum) 49.00m
- c. Side yard setback (east) (minimum) 10.00m
- d. Side yard setback (west) (minimum) 25.00m

9.6.5 Site Coverage

- a. Site Coverage (maximum) 3.50%

9.6.6 Vehicle and Bicycle Parking

- a. Vehicle parking Subject to the regulations in Schedule “C” except as otherwise specified by the regulations in this Part
- b. Bicycle parking (minimum) Subject to the regulations in Schedule “C”
- c. Notwithstanding Section 1.2(1) of Schedule “C”, no off-street vehicle parking spaces are required.



1964 Fairfield Road & 507 Foul Bay Road
Rezoning No.00845



A BYLAW OF THE CITY OF VICTORIA

The purpose of this bylaw is to amend Bylaw No. 05-75 Heritage Designation (1964 Fairfield Road) Bylaw (No. 530) to add the property known as 507 Foul Bay Road to the heritage designation at 1964 Fairfield Road and add the Statement of Significance to the bylaw.

Whereas:

- a. Council considers that heritage designation of the property known as 507 Foul Bay Road is necessary and desirable for the conservation of the protected heritage property at 1964 Fairfield Road known as Abkhazi Garden; and
- b. the owner of 507 Foul Bay Road has consented to heritage designate the real property and has waived their right to compensation for such heritage designation.

Under its statutory powers, including Section 611 of the *Local Government Act*, the Municipal Council of The Corporation of the City of Victoria in an open meeting enacts the following provisions:

1. This Bylaw may be cited as the “HERITAGE DESIGNATION (1964 FAIRFIELD ROAD) AMENDMENT BYLAW (NO. 2)”.
2. Bylaw No. 05-75 Heritage Designation (1964 Fairfield Road) Bylaw (No. 530) is amended by:
 - a) deleting the purpose statement and replacing it with:

“The purpose of this Bylaw is to designate the existing house, the summer house, garden shed and gardens known as Abkhazi Garden, located at 1964 Fairfield Road and the adjacent ancillary garden support area which forms part of Abkhazi Garden, located at 507 Foul Bay Road, as protected heritage real property.”
 - b) inserting the words “as described in the Statement of Significance attached to this Bylaw at Schedule A,” after the words “known as Abkhazi Garden,” in section 2;
 - c) adding the following new section 3 immediately after section 2:

“3. The adjacent ancillary garden support area which forms part of Abkhazi Garden, as described in the Statement of Significance attached to this Bylaw as Schedule A, and which is located at 507 Foul Bay Road, legally described as PID 001-039-857, Lot 3, Section 68, Victoria District, Plan 37953, is designated protected heritage real property.”
 - d) inserting the Schedule A – Statement of Significance at page 2 and attached to this bylaw at Appendix 1.

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Public Hearing Held On the	day of	2024
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CITY CLERK

MAYOR

Appendix 1

Schedule A

Statement of Significance

1964 Fairfield Road – Abkhazi Gardens

Original version from 2005 – written by Steve Barber

Amended – October 2023 by Kristal Stevenot, Senior Heritage Planner, City of Victoria

Description of Historic Place

The historic place comprises a designed domestic garden, garden buildings, and an early modern bungalow set around and upon a rocky knoll, in Victoria's Gonzales neighbourhood.

Heritage Value

The historic place, begun in 1946 and still evolving, is valued as one of the very earliest expressions of Modernism in both garden design and domestic architecture, in post-war Victoria and for what its design tells us about its creators' lives. The garden is valued for its long tradition of plant conservation.

Peggy (nee Pemberton-Carter) and Nicholas Abkhazi's approach to the planning and design of their home and garden represents an isolated example of an international shift in design thinking, that was, in part, a reaction to war, but which also sought to celebrate new technology, often itself a by-product of war. The garden contains references to, and is seen by many as a metaphor, the lives of its creators. The layering, texture and colouring – a reference to Chinese art and landscape – reflects the influence of Peggy's time living in China; the tranquility and privacy – a response to lives shattered by wartime internment; for Peggy in a Japanese camp, and for Nicholas, an exiled Georgian Prince, in a German prisoner of war camp; and the energy that went into the creation of the garden on a difficult site – an expression of their love that began in Paris in 1920 and triumphed over the adversity of war to be rekindled when they met again after the war and moved to Canada.

The summerhouse (1946), and the house (1947), are important surviving examples of the domestic work of accomplished Victoria-based Modernist architect John Wade and are valued for the way the planning and detailing responds, not to the tyranny of precedent, but rather to the spirit of place, the integration of house and garden, the function of space, and the celebration of post-war construction technology.

Continuing a course followed by the Abkhazis until Nicholas' death and in 1987 and Peggy's in 1994, the garden continues to evolve to survive, reflecting new realities such as the drier climate and public accessibility. It is valued as a setting for rare plant conservation, for important rescued specimens from Vancouver Island collections, and of hybrids developed by key Island plantsmen including Ed Lohbrunner, Herman Vaartnou, and Joe Harvey.

Character-Defining Elements

- the single-storey house with deep overhanging enclosed eaves
- the ratio of window openings to walls, and the location and size of the window openings in relation to the landscape
- the distinctive wide beveled siding
- the stone chimney stack
- the stone revetments, stone steps, stone walls on the house and garden shed, and stone foundation retaining walls
- the view from the kitchen to the garden looking north-west
- the position of the bay window opening in the sitting room that marks the location of the original arrangement of French doors flanking a window
- the remains of the roman brick fireplace
- the fitted cupboards at the rear entrance, which was once the mud room
- the position of the kitchen sink
- the acoustic tiles on a vaulted ceiling
- birch-faced plywood wall coverings
- oak parquet floors and under floor heating infrastructure
- lighting integrated into the window valences
- the original interior wood doors (swing and sliding)
- the underlying natural landscape elements including the rocky outcrops and the Garry oaks
- the lawns, the incised concrete walkways and their signs of a once painted finish, stone and gravel paths and stone terrace, and outbuildings
- the ever-evolving three ponds and their surrounding benches, rocks and plantings
- the evolving borders resulting from the addition and translocation of rare and threatened species and specimens from other gardens in Victoria, and experimentation with hardier and drought tolerant plants, see Head Gardener's landscape plan and plant inventory
- garden buildings including the summerhouse and the tool shed
- the Rhododendron woodland garden
- the Georgian horn beam hedge along the perimeter of the garden
- Lot 3 at 507 Foul Bay Road – adjacent property and ancillary support area where the green house, compost area, propagation area and gardener's shed are located.