

D.1.a.b1733, 1735 and 1737 Fairfield Road - Rezoning application No. 00821 and Development Permit with Variances Application No.00204 & 1964 Fairfield and 507 Foul Bay Road Rezoning Application No. 00845 and Heritage Designated Amendments (Gonzales)

Moved By Councillor Caradonna

Seconded By Councillor Loughton

OCP Amendments with Rezoning

1. That Council consider who is affected by the proposed changes to the Official Community Plan (OCP) and determine that those within a 200m radius of the subject properties will be affected.
2. That Council provide an opportunity for consultation pursuant to section 475 of the Local Government Act, and direct the Director of Sustainable Planning and Community Development to:
 - a. mail a notice of the proposed OCP Amendments to the persons within a 200m radius of the subject property
 - b. post a notice on the City's website inviting affected persons, organizations and authorities to ask questions of staff and provide written or verbal comments to Council for their consideration.
3. That Council consider that no consultation is necessary with the Capital Regional District Board; Councils of Oak Bay, Esquimalt and Saanich; the Songhees and Esquimalt First Nations; the School District Board; or the provincial or federal governments or their agencies because the proposed OCP Amendments do not affect them.
4. That Council instruct staff to prepare the necessary Official Community Plan Amendment Bylaws in accordance with Section 475 of the Local Government Act and the necessary Zoning Regulation Bylaw Amendments that would authorize the proposed development outlined in Rezoning Application No. REZ00821 for 1733, 1735 and 1737 Fairfield Road and proposed zoning changes outlined in Rezoning Application No. 00845 for 1964 Fairfield Road and 507 Foul Bay Road, that first and second reading of the Zoning Regulation Bylaw Amendments be considered by Council and a public hearing date be set once the following conditions are met:
 - a. the following revisions to the plans for 1733, 1735 and 1737 Fairfield Road to the satisfaction of the Director of Sustainable Planning and Community Development:
 - i. add an additional two-bedroom unit and convert a one-bedroom unit to a two bedroom unit, as outlined in the applicant's letter, dated September 12, 2023
 - ii. improve the transition to lower density buildings (northeast building elevation)
 - iii. increase outdoor amenity space, which may include providing parking underground, reducing parking or adding a rooftop amenity area.

- b. the following revisions to the plans for 1733, 1735 and 1737 Fairfield Road, to the satisfaction of the Director of Engineering and Public Works:
 - i. increase permeable paved area to improve onsite stormwater management
 - ii. to provide 50% of required long term bicycle parking stalls as standard ground mounted stalls.
 - c. plan revision for 1733, 1735 and 1737 Fairfield Road to accommodate more replacement trees required under the Tree Protection Bylaw, to the satisfaction of the Director of Parks, Recreation and Facilities.
 - 5. That subject to approval in principle at the Public Hearing, the applicant prepare and execute the following legal agreements, with form satisfactory to the City Solicitor prior to adoption of the bylaws:
 - a. provision of a 0.86m wide statutory right-of-way for 1733, 1735 and 1737 Fairfield Road, with terms to the satisfaction of the Director of Engineering and Public Works
 - b. provision of no less than nine two-bedroom units, six two-bedroom units with a den and one three-bedroom unit for 1733, 1735 and 1737 Fairfield Road, to the satisfaction of the Director of Sustainable Planning and Community Development
 - c. securing continued public access to the property at 1964 Fairfield Road, consistent with existing public access hours and locations and permitting temporary closures for private events, to the satisfaction of the Director of Sustainable Planning and Community Development
 - d. provision of transportation demand management measures for 1733, 1735 and 1737 Fairfield Road, to the satisfaction of the Director of Engineering and Public Works, including:
 - i. car share memberships and usage credits for all residential units;
 - ii. on-street electric car share vehicle;
 - iii. on-street (Beechwood Avenue), level 2, dual head electrical charger, and all associated infrastructure and connections;
 - iv. bicycle parking to accommodate oversized bicycles (10% of required long-term spaces);
 - v. bicycle parking with access to an electrical outlet (50% of required long-term spaces) and
 - vi. bicycle repair and maintenance station.
 - 6. That the above Recommendations be adopted on the condition that they create no legal rights for the applicant or any other person, or obligation on the part of the City or its officials, and any expenditure of funds is at the risk of the person making the expenditure.

Development Permit with Variances Application (1733, 1735 and 1737 Fairfield Road)

That Council, after giving notice and allowing an opportunity for public comment at a meeting of Council, and after the public hearing for Rezoning Application No. 00821 and Rezoning Application No. 00845, if they are approved, consider the following motion:

1. That subject to the adoption of the necessary Zoning Regulation Bylaw Amendment, Council authorize the issuance of Development Permit with Variances No. 00204 for 1733, 1735 and 1737 Fairfield Road, in accordance with plans submitted to the Planning department and date stamped by Planning on August 22, 2023, subject to:
 - a. the proposed development meeting all City zoning bylaw requirements, except for the following variances:
 - i. reducing the minimum front yard setback (Beechwood Avenue) from 4.00m to 2.70m and increasing stairs projection from 1.80m to 2.73m;
 - ii. reducing the minimum side yard setback (Fairfield Road) from 4.00m to 2.30m;reducing the minimum side yard setback (southeast) from 6.00m to 0.50m;
 - iii. increasing the maximum site coverage from 40% to 65%;
 - iv. decreasing the open site space from 50% to 23%;
 - v. reducing vehicle parking from 40 spaces to 23 spaces;
 - vi. permitting long-term bicycle parking to be provided in a stacked format.
 - b. The property being consolidated into one lot.
2. That the Development Permit with Variances, if issued, lapses two years from the date of this resolution.”

Heritage Designation Amendment Application (1964 Fairfield Road and 507 Foul Bay Road)

That Council instruct the Director of Sustainable Planning and Community Development to:

1. prepare a heritage designation bylaw to amend Heritage Designation (1964 Fairfield Road) Bylaw (No. 530) No. 05-75 to add the property at 507 Foul Bay Road, that first and second reading of the bylaw be considered by Council and that a joint public hearing date be set with Rezoning Application No. 00821 and Rezoning Application No. 00845;
2. add the Statement of Significance for 1964 Fairfield Road and 507 Foul Bay Road, attached as Attachment G to this report, recognizing the building exterior and natural landscape elements as the historic features of the property, to the above noted heritage designation bylaw.

FOR (6): Mayor Alto, Councillor Caradonna, Councillor Coleman,
Councillor Dell, Councillor Loughton, and Councillor Thompson
OPPOSED (2): Councillor Gardiner, and Councillor Hammond

CARRIED (6 TO 2)

F.2 1733, 1735 and 1737 Fairfield Road - Rezoning application No. 00821 and Development Permit with Variances Application No.00204 & 1964 Fairfield and 507 Foul Bay Road Rezoning Application No. 00845 and Heritage Designated Amendments (Gonzales)

Committee received a report dated November 23, 2023 from the Director of Sustainable Planning and Community Development regarding two concurrent proposals: a rezoning application for 1733, 1735 and 1737 Fairfield Road to rezone from the R1-G Zone, Gonzales Single Family Dwelling District to a new site-specific zone to permit a multiple dwelling, and a rezoning application for 1964 Fairfield Road and 507 Foul Bay Road to rezone from the RK-11 Zone and the R1-G Zone to a new site-specific zone to limit permitted uses and reduce the density to the existing density and uses, which include a garden, restaurant, and gift shop, and recommending it proceed to Public Hearing.

Moved By Councillor Caradonna

Seconded By Councillor Dell

OCP Amendments with Rezonings

1. That Council consider who is affected by the proposed changes to the Official Community Plan (OCP) and determine that those within a 200m radius of the subject properties will be affected.
2. That Council provide an opportunity for consultation pursuant to section 475 of the Local Government Act, and direct the Director of Sustainable Planning and Community Development to:
 - a. mail a notice of the proposed OCP Amendments to the persons within a 200m radius of the subject property
 - b. post a notice on the City's website inviting affected persons, organizations and authorities to ask questions of staff and provide written or verbal comments to Council for their consideration.
3. That Council consider that no consultation is necessary with the Capital Regional District Board; Councils of Oak Bay, Esquimalt and Saanich; the Songhees and Esquimalt First Nations; the School District Board; or the provincial or federal governments or their agencies because the proposed OCP Amendments do not affect them.
4. That Council instruct staff to prepare the necessary Official Community Plan Amendment Bylaws in accordance with Section 475 of the Local Government Act and the necessary Zoning Regulation Bylaw Amendments that would authorize the proposed development outlined in Rezoning Application No. REZ00821 for 1733, 1735 and 1737 Fairfield Road and proposed zoning changes outlined in Rezoning Application No. 00845 for 1964 Fairfield Road and 507 Foul Bay Road, that first and second reading of the Zoning Regulation Bylaw Amendments be considered by Council and a public hearing date be set once the following conditions are met:
 - a. the following revisions to the plans for 1733, 1735 and 1737 Fairfield Road to the satisfaction of the Director of Sustainable Planning and Community Development:
 - i. add an additional two-bedroom unit and convert a one-bedroom unit to a two bedroom unit, as outlined in the applicant's letter, dated September 12, 2023

- ii. improve the transition to lower density buildings (northeast building elevation)
 - iii. increase outdoor amenity space, which may include providing parking underground, reducing parking or adding a rooftop amenity area.
 - b. the following revisions to the plans for 1733, 1735 and 1737 Fairfield Road, to the satisfaction of the Director of Engineering and Public Works:
 - i. increase permeable paved area to improve onsite stormwater management
 - ii. to provide 50% of required long term bicycle parking stalls as standard ground mounted stalls.
 - c. plan revision for 1733, 1735 and 1737 Fairfield Road to accommodate more replacement trees required under the Tree Protection Bylaw, to the satisfaction of the Director of Parks, Recreation and Facilities.
5. That subject to approval in principle at the Public Hearing, the applicant prepare and execute the following legal agreements, with form satisfactory to the City Solicitor prior to adoption of the bylaws:
- a. provision of a 0.86m wide statutory right-of-way for 1733, 1735 and 1737 Fairfield Road, with terms to the satisfaction of the Director of Engineering and Public Works
 - b. provision of no less than nine two-bedroom units, six two-bedroom units with a den and one three-bedroom unit for 1733, 1735 and 1737 Fairfield Road, to the satisfaction of the Director of Sustainable Planning and Community Development
 - c. securing continued public access to the property at 1964 Fairfield Road, consistent with existing public access hours and locations and permitting temporary closures for private events, to the satisfaction of the Director of Sustainable Planning and Community Development
 - d. provision of transportation demand management measures for 1733, 1735 and 1737 Fairfield Road, to the satisfaction of the Director of Engineering and Public Works, including:
 - i. car share memberships and usage credits for all residential units;
 - ii. on-street electric car share vehicle;
 - iii. on-street (Beechwood Avenue), level 2, dual head electrical charger, and all associated infrastructure and connections;
 - iv. bicycle parking to accommodate oversized bicycles (10% of required long-term spaces);
 - v. bicycle parking with access to an electrical outlet (50% of required long-term spaces) and
 - vi. bicycle repair and maintenance station.
6. That the above Recommendations be adopted on the condition that they create no legal rights for the applicant or any other person, or obligation on the part of the City or its officials, and any expenditure of funds is at the risk of the person making the expenditure.

Development Permit with Variances Application (1733, 1735 and 1737 Fairfield Road)

That Council, after giving notice and allowing an opportunity for public comment at a meeting of Council, and after the public hearing for Rezoning Application No.

00821 and Rezoning Application No. 00845, if they are approved, consider the following motion:

1. That subject to the adoption of the necessary Zoning Regulation Bylaw Amendment, Council authorize the issuance of Development Permit with Variances No. 00204 for 1733, 1735 and 1737 Fairfield Road, in accordance with plans submitted to the Planning department and date stamped by Planning on August 22, 2023, subject to:
 - a. the proposed development meeting all City zoning bylaw requirements, except for the following variances:
 - i. reducing the minimum front yard setback (Beechwood Avenue) from 4.00m to 2.70m and increasing stairs projection from 1.80m to 2.73m;
 - ii. reducing the minimum rear yard setback from 10.00m to 2.60m;
 - iii. reducing the minimum side yard setback (Fairfield Road) from 4.00m to 2.30m;
 - iv. reducing the minimum side yard setback (southeast) from 6.00m to 0.50m;
 - v. increasing the maximum site coverage from 40% to 65%;
 - vi. decreasing the open site space from 50% to 23%;
 - vii. reducing vehicle parking from 40 spaces to 23 spaces;
 - viii. permitting long-term bicycle parking to be provided in a stacked format.
 - b. The property being consolidated into one lot.
2. That the Development Permit with Variances, if issued, lapses two years from the date of this resolution.”

Heritage Designation Amendment Application (1964 Fairfield Road and 507 Foul Bay Road)

That Council instruct the Director of Sustainable Planning and Community Development to:

1. prepare a heritage designation bylaw to amend Heritage Designation (1964 Fairfield Road) Bylaw (No. 530) No. 05-75 to add the property at 507 Foul Bay Road, that first and second reading of the bylaw be considered by Council and that a joint public hearing date be set with Rezoning Application No. 00821 and Rezoning Application No. 00845;
2. add the Statement of Significance for 1964 Fairfield Road and 507 Foul Bay Road, attached as Attachment G to this report, recognizing the building exterior and natural landscape elements as the historic features of the property, to the above noted heritage designation bylaw.

FOR (7): Mayor Alto, Councillor Caradonna, Councillor Coleman, Councillor Dell, Councillor Kim, Councillor Loughton, and Councillor Thompson
OPPOSED (2): Councillor Gardiner and Councillor Hammond

CARRIED (7 TO 2)



Committee of the Whole Report

For the Meeting of December 7, 2023

To: Committee of the Whole

Date: November 24, 2023

From: Karen Hoese, Director, Sustainable Planning and Community Development

Subject: Rezoning Application No. 00821, associated Official Community Plan Amendment, and Development Permit with Variances Application No. 00204 for 1733, 1735 and 1737 Fairfield Road

Rezoning Application No.00845 and associated Official Community Plan and Heritage Designation Amendments for 1964 Fairfield Road and 507 Foul Bay Road

RECOMMENDATION

OCP Amendments with Rezoning

1. That Council consider who is affected by the proposed changes to the *Official Community Plan* (OCP) and determine that those within a 200m radius of the subject properties will be affected.
2. That Council provide an opportunity for consultation pursuant to section 475 of the *Local Government Act*, and direct the Director of Sustainable Planning and Community Development to:
 - a. mail a notice of the proposed OCP Amendments to the persons within a 200m radius of the subject property
 - b. post a notice on the City's website inviting affected persons, organizations and authorities to ask questions of staff and provide written or verbal comments to Council for their consideration.
3. That Council consider that no consultation is necessary with the Capital Regional District Board; Councils of Oak Bay, Esquimalt and Saanich; the Songhees and Esquimalt First Nations; the School District Board; or the provincial or federal governments or their agencies because the proposed OCP Amendments do not affect them.
4. That Council instruct staff to prepare the necessary Official Community Plan Amendment Bylaws in accordance with Section 475 of the *Local Government Act* and the necessary Zoning Regulation Bylaw Amendments that would authorize the proposed development outlined in Rezoning Application No. REZ00821 for 1733, 1735 and 1737 Fairfield Road and proposed zoning changes outlined in Rezoning Application No. 00845 for 1964 Fairfield Road and 507 Foul Bay Road, that first and second reading of the Zoning

Regulation Bylaw Amendments be considered by Council and a public hearing date be set once the following conditions are met:

- a. the following revisions to the plans for 1733, 1735 and 1737 Fairfield Road to the satisfaction of the Director of Sustainable Planning and Community Development:
 - i. add an additional two-bedroom unit and convert a one-bedroom unit to a two-bedroom unit, as outlined in the applicant's letter, dated September 12, 2023
 - ii. improve the transition to lower density buildings (northeast building elevation)
 - iii. increase outdoor amenity space, which may include providing parking underground, reducing parking or adding a rooftop amenity area.
 - b. the following revisions to the plans for 1733, 1735 and 1737 Fairfield Road, to the satisfaction of the Director of Engineering and Public Works:
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 - ii. to provide 50% of required long term bicycle parking stalls as standard ground mounted stalls.
 - c. plan revision for 1733, 1735 and 1737 Fairfield Road to accommodate more replacement trees required under the *Tree Protection Bylaw*, to the satisfaction of the Director of Parks, Recreation and Facilities.
5. That subject to approval in principle at the Public Hearing, the applicant prepare and execute the following legal agreements, with form satisfactory to the City Solicitor prior to adoption of the bylaws:
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 - b. provision of no less than nine two-bedroom units, six two-bedroom units with a den and one three-bedroom unit for 1733, 1735 and 1737 Fairfield Road, to the satisfaction of the Director of Sustainable Planning and Community Development
 - c. securing continued public access to the property at 1964 Fairfield Road, consistent with existing public access hours and locations and permitting temporary closures for private events, to the satisfaction of the Director of Sustainable Planning and Community Development
 - d. provision of transportation demand management measures for 1733, 1735 and 1737 Fairfield Road, to the satisfaction of the Director of Engineering and Public Works, including:
 - i. car share memberships and usage credits for all residential units;
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 - iv. bicycle parking to accommodate oversized bicycles (10% of required long-term spaces);
 - v. bicycle parking with access to an electrical outlet (50% of required long-term spaces) and
 - vi. bicycle repair and maintenance station.
6. That the above Recommendations be adopted on the condition that they create no legal rights for the applicant or any other person, or obligation on the part of the City or its officials, and any expenditure of funds is at the risk of the person making the expenditure.

Development Permit with Variances Application (1733, 1735 and 1737 Fairfield Road)

That Council, after giving notice and allowing an opportunity for public comment at a meeting of Council, and after the public hearing for Rezoning Application No. 00821 and Rezoning Application No. 00845, if they are approved, consider the following motion:

- “1. That subject to the adoption of the necessary Zoning Regulation Bylaw Amendment, Council authorize the issuance of Development Permit with Variances No. 00204 for 1733, 1735 and 1737 Fairfield Road, in accordance with plans submitted to the Planning department and date stamped by Planning on August 22, 2023, subject to:
 - a. the proposed development meeting all City zoning bylaw requirements, except for the following variances:
 - i. reducing the minimum front yard setback (Beechwood Avenue) from 4.00m to 2.70m and increasing stairs projection from 1.80m to 2.73m;
 - ii. reducing the minimum rear yard setback from 10.00m to 2.60m;
 - iii. reducing the minimum side yard setback (Fairfield Road) from 4.00m to 2.30m;
 - iv. reducing the minimum side yard setback (southeast) from 6.00m to 0.50m;
 - v. increasing the maximum site coverage from 40% to 65%;
 - vi. decreasing the open site space from 50% to 23%;
 - vii. reducing vehicle parking from 40 spaces to 23 spaces;
 - viii. permitting long-term bicycle parking to be provided in a stacked format.
 - b. The property being consolidated into one lot.
2. That the Development Permit with Variances, if issued, lapses two years from the date of this resolution.”

Heritage Designation Amendment Application (1964 Fairfield Road and 507 Foul Bay Road)

That Council instruct the Director of Sustainable Planning and Community Development to:

1. prepare a heritage designation bylaw to amend Heritage Designation (1964 Fairfield Road) Bylaw (No. 530) No. 05-75 to add the property at 507 Foul Bay Road, that first and second reading of the bylaw be considered by Council and that a joint public hearing date be set with Rezoning Application No. 00821 and Rezoning Application No. 00845;
2. add the Statement of Significance for 1964 Fairfield Road and 507 Foul Bay Road, attached as Attachment G to this report, recognizing the building exterior and natural landscape elements as the historic features of the property, to the above noted heritage designation bylaw.

LEGISLATIVE AUTHORITY

This report discusses concurrent rezoning applications involving two nearby properties.

The first is a Rezoning Application (No. 00821), associated Official Community Plan Amendment and a Development Permit with Variances Application for 1733, 1735 and 1737 Fairfield Road.

Relevant Rezoning considerations for this property relate to:

- change of use to allow multiple dwellings
- new regulations pertaining to maximum floor space ratio and maximum floor area.

The relevant Development Permit with Variances considerations relate to:

- consistency with design guidelines
- impact of variances pertaining to setbacks, site coverage, open space and parking.

The second is a Rezoning Application (No. 00845), Heritage Designation Amendment and associated Official Community Plan Amendment for 1964 Fairfield Road and 507 Foul Bay Road.

Relevant Rezoning considerations for this property relate to:

- removal of existing permitted residential uses and addition of garden, restaurant, and gift shop as permitted uses
- new regulations pertaining to maximum floor space ratio, maximum floor area, and vehicle parking.

Enabling Legislation

In accordance with Section 479 of the *Local Government Act*, Council may regulate within a zone the use of land, buildings and other structures, the density of the use of the land, building and other structures, the siting, size and dimensions of buildings and other structures as well as the uses that are permitted on the land and the location of uses on the land and within buildings and other structures.

In accordance with Section 482 of the *Local Government Act*, a zoning bylaw may establish different density regulations for a zone, one generally applicable for the zone and the others to apply if certain conditions are met.

In accordance with Section 489 of the *Local Government Act*, Council may issue a Development Permit in accordance with the applicable guidelines specified in the *Official Community Plan, 2012* (OCP). A Development Permit may vary or supplement the Zoning Regulation Bylaw but may not vary the use or density of the land from that specified in the Bylaw.

In accordance with Section 611 of the *Local Government Act*, Council may designate real property, in whole or in part, as protected property. A heritage designation bylaw may apply to more than one property and may apply to landscape features identified in the bylaw.

EXECUTIVE SUMMARY

The purpose of this report is to present Council with information, analysis and recommendations for two concurrent proposals:

- a Rezoning, OCP Amendment and Development Permit with Variances application for 1733, 1735 and 1737 Fairfield Road, to rezone from the R1-G Zone, Gonzales Single Family Dwelling District to a new site-specific zone to permit a multiple dwelling at this location.
- a Rezoning, OCP Amendment and Heritage Designation Amendment application for 1964 Fairfield Road and 507 Foul Bay Road, to rezone from the RK-11 Zone, Fairfield Townhouse District (1964 Fairfield Road) and the R1-G Zone, Gonzales Single Family Dwelling District (507 Foul Bay Road) to a new site-specific zone to limit permitted uses and reduce the density to the existing density and uses, which include a garden, restaurant, and gift shop.

The proposal for 1964 Fairfield Road and 507 Foul Bay Road has been submitted to support the increase in density for the proposed development at 1733, 1735 and 1737 Fairfield Road as the development exceeds the density envisioned for Traditional Residential Urban Place Designation.

The Development Permit with Variances application for 1733, 1735 and 1737 Fairfield Road pertains to the proposed form, character, exterior design, finishes and landscaping, as well as variances related to setbacks, site coverage, open space and parking for a four-storey building containing 31 dwelling units.

The following points were considered in assessing the concurrent rezoning applications:

- The four-storey multiple dwelling proposed for 1733, 1735 and 1737 Fairfield Road, with a density of 1.77:1 floor space ratio (FSR), requires rezoning to a site-specific zone and an amendment to the *Official Community Plan* (OCP) to increase the density and height envisioned for a Traditional Residential property and allow for a multiple dwelling use.
- The creation of 31 new dwelling units proposed for 1733, 1735 and 1737 Fairfield Road includes two studio, 12 one-bedroom, nine two-bedroom, six two-bedroom with den, and two three-bedroom units, which advances housing objectives to provide a diversity of housing, including family-oriented housing.
- The proposed 0.86m statutory right-of-way (SRW) along Fairfield Road would provide space for a two-metre sidewalk and a planted buffer of small canopy trees between pedestrians and vehicles, which supports the OCP's transportation and placemaking policies.
- The proposal for 1733, 1735 and 1737 Fairfield Road is inconsistent with specific policies in the *Gonzales Neighbourhood Community Plan* (2002) to retain the single-family character and preserve landscape features on public and private properties.
- A rezoning and associated OCP amendment is also proposed for 1964 Fairfield Road and 507 Foul Bay Road to rezone the site to permit only the existing uses and to redesignate from the Traditional Residential Urban Place Designation to the Public Facilities, Institutions, Parks and Open Space Urban Place Designation – this will better align the property's designation with the actual use of the properties.
- The proposal for 1964 Fairfield Road and 507 Foul Bay Road advances OCP objectives to maintain the heritage and cultural value of individual properties, conserve natural and built heritage including the urban forest.
- The application to expand heritage designation to include the maintenance and support area for Abkhazi Garden (located at 507 Foul Bay Road) and the commitment to secure continued public access to Abkhazi Garden will provide heritage protection to the entire site and provide certainty that public access will remain in perpetuity.
- The proposal for 1964 Fairfield Road and 507 Foul Bay Road is consistent with the *Gonzales Neighbourhood Community Plan*, which emphasizes preservation and enhancement of heritage as well as natural landscape features on both private and public properties.

The following points were considered in assessing the Development Permit with Variances for 1733, 1735 and 1737 Fairfield Road:

- The proposed form and character of the development is largely consistent with design guidelines applicable to Development Permit Area (DPA) 16, General Form and Character; however, revisions are recommended as follows:
 - provide a transition in height on the northeast building elevation
 - reduce space allocated to vehicle parking or provide underground

- increase amenity space, such as a landscaped rooftop amenity.
- The proposed site planning is consistent with specific *Design Guidelines for Multi-Unit Residential, Commercial and Industrial Development*; however, the combination of a reduced 2.6m northeast setback and four-storey height should provide a transition in its form and massing to the abutting neighbour at 1745 Fairfield Road.
- The proposed 65% lot coverage and 23% open space would benefit from a plan revision to either incorporate outdoor amenity space or reduce proposed parking, or a combination of both.
- Requested variances to front and flanking street setbacks are considered supportable to allow building siting that will create a sense of enclosure and to maximize rear and interior side yard setbacks.
- The requested variance to reduce the side yard setback (southeast) is supportable to incorporate the parking entrance into the building envelope, which reduces the visibility of vehicle parking from the public realm.
- Requested reductions to the rear yard setback, site coverage and open space are not supported as proposed but would be considered supportable with revisions as described above.
- The variances to reduce vehicle parking from 40 spaces to 23 spaces and permitting long-term bicycle parking to be provided in a stacked format are considered supportable based on the provision of a comprehensive package of transportation demand management (TDM) measures.

BACKGROUND

Description of Proposals

1733, 1735 and 1737 Fairfield Road

This proposal for 1733, 1735 and 1737 Fairfield Road is to rezone from the R1-G Zone, Gonzales Single Family Dwelling District to a new site-specific zone to permit construction of a four-storey multiple dwelling containing 31 units. The proposal includes demolition of three existing single-family dwellings and consolidation of the three lots. An OCP amendment is required to permit a four-storey multiple dwelling with a density of 1.77:1 FSR on a site within the Traditional Residential Urban Place Designation.

The following differences from the standard URMD Zone, Urban Residential Multiple Dwelling District are being proposed and would be accommodated in the new zone:

- reduce maximum FSR from 2:1 to 1.77:1
- reduce minimum lot area from 1840m² to 1740m².

The associated Development Permit with Variances (DPV) Application is for a four-storey building containing a total of 31 dwelling units, with a 23-space surface parking area.

Specific details include:

- six ground-oriented, two-level units accessed from Fairfield Road, main entry to 25 single-level units on Beechwood Avenue
- unit mix consisting of two studio units, 12 one-bedroom units, nine two-bedroom units, six two-bedroom units with dens, and two three-bedroom units
- partially covered surface parking (23 spaces including visitor and accessible).

Exterior materials include:

- light brick (first three levels)
- light blue metal panel (levels three and four)
- wood grain finish and perforated metal (ground level main entry on Beechwood)
- metal picket balconies.

Landscape elements include:

- 23% of lot area as open space
- 25 units with private balconies, six ground-oriented units with ground level patio space
- permeable parking for a portion of the parking area
- 26 trees identified – 16 on-site, seven off-site and three within Beechwood boulevard
- nine trees retained – the seven offsite private trees and two within Beechwood boulevard
- 17 trees removed- seven bylaw protected trees, 10 non-protected
- 18 new trees proposed on-site and eight boulevard trees (seven on Fairfield, one on Beechwood).

The proposed variances from the standard URMD Zone as well as parking regulations in Schedule C are related to:

- reducing the minimum front yard setback (Beechwood Avenue) from 4.00m to 2.70m and increasing the stair projection from 1.80m to 2.73m
- reducing the minimum rear yard setback from 10.00m to 2.60m
- reducing the minimum side yard setback (Fairfield Road) from 4.00m to 2.30m
- reducing the minimum side yard setback (southeast) from 6.00m to 0.50m
- increasing the maximum site coverage from 40% to 65%
- decreasing the open site space from 50% to 23%
- reducing vehicle parking from 40 spaces to 23 spaces
- permitting long-term bicycle parking to be provided in a stacked format.

1964 Fairfield Road and 507 Foul Bay Road

The concurrent proposal for 1964 Fairfield Road and 507 Foul Bay Road is to rezone the subject properties from the RK-11 Zone, Fairfield Townhouse District (1964 Fairfield Road) and the R1-G Zone, Gonzales Single Family Dwelling District (507 Foul Bay Road) to a new site-specific zone to limit permitted uses and density to allow only for the existing garden, restaurant, and gift shop. In addition, an OCP amendment is proposed to redesignate from the Traditional Residential Urban Place Designation to the Public Facilities, Institutions, Parks and Open Space Urban Place Designation. There is also a proposed amendment to the heritage designation bylaw to add the adjacent property 507 Foul Bay Road, which contains the greenhouse, and the garden's support and maintenance spaces, and to revise the Statement of Significance.

For the property at 1964 Fairfield Road, the following differences from the current RK-11 Zone are being proposed and would be accommodated in the new zone:

- remove existing permitted uses (single-family dwellings with secondary or garden suites, two-family dwellings, attached dwellings, hospitals, nursing homes and home occupations) except public buildings and accessory buildings, and add garden, restaurant, and gift shop as new permitted uses
- reduce maximum FSR from 0.55:1 to 0.04

- add new regulations pertaining to parking.

For the property at 507 Foul Bay Road, the following differences from the current R1-G Zone, Fairfield Townhouse District, are being proposed and would be accommodated in the new zone:

- remove existing permitted uses (single-family dwellings with secondary or garden suites and house conversions) except accessory buildings.

Land Use Context

1733, 1735 and 1737 Fairfield Road

The area, as shown in Figure 1, is characterized by a mix of mostly single-family dwellings as well as older attached dwellings, duplex dwellings, and a recently constructed stacked townhouse development containing 20 dwelling units and density of 0.85:1 FSR. In addition, there are small scale commercial properties and Hollywood Park to the southwest.

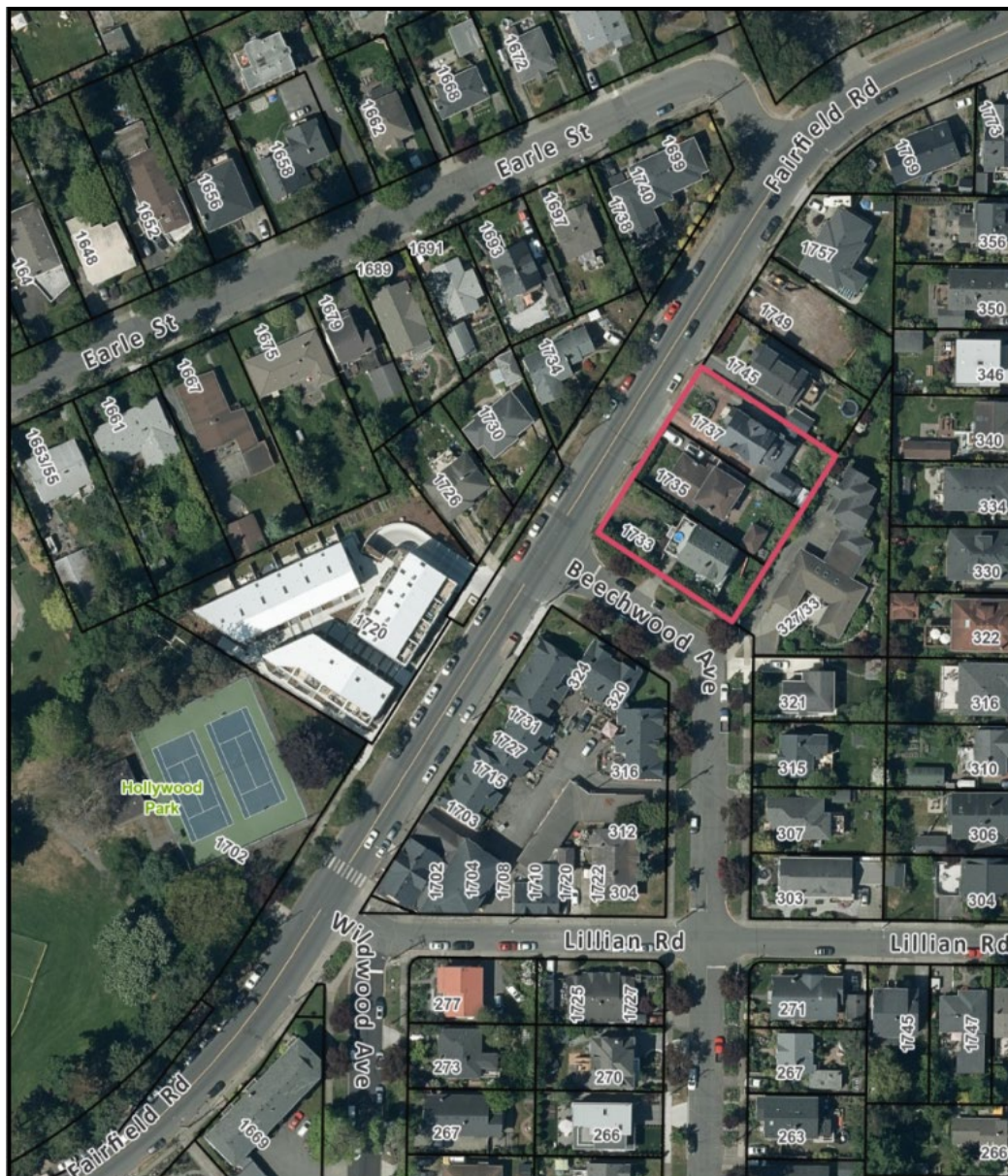


Figure 1: Aerial map showing 1733, 1735 and 1737 Fairfield Road

The area, as shown in Figure 2, is characterized by single-family dwellings some of which are located on similarly large panhandle lots. A five-unit heritage house conversion abuts the property to the north, which is currently proposed for development that would add a 12-unit multiple dwelling development while retaining the heritage building. In addition, Margaret Jenkins Elementary is located to the west of the subject property. Heritage registered and heritage designated properties exist in the surrounding area.



Existing Site Development and Development Potential

1733, 1735 and 1737 Fairfield Road

The site is comprised of three legal lots and is developed with three single-family dwellings. Under the current R1-G Zone, Gonzales Single Family Dwelling District, the three lots could each be developed with a single-family dwelling with either a secondary suite or garden suite, for a total of six dwelling units. In addition, the three lots could potentially be developed under *Missing Middle Regulations* as a combination of corner townhouses and houseplexes with up to 24 dwelling units depending on unit sizes and site design.

1964 Fairfield Road and 507 Foul Bay Road

This site is presently used as a garden with an accessory restaurant and gift shop. The property at 507 Foul Bay Road is limited to use as the maintenance and support area.

Under the current RK-11 Zone, Fairfield Townhouse District, the property at 1964 Fairfield Road permits single-family dwellings with secondary or garden suites, two-family dwellings, attached dwellings, public buildings, hospitals, nursing homes and home occupations. However, the property is heritage designated; therefore, development potential would be limited as the designation protects the existing buildings and the building siting, as well as various site features and views from the existing building.

Data Tables

The following data table compares the proposal for 1733, 1735 and 1737 Fairfield Road with the R1-G Zone, Gonzales Single Family District and the standard URMD Zone, Urban Residential Multiple Dwelling District. An asterisk is used to identify where the proposal does not meet the requirements of the existing Zone.

Zoning Criteria	Proposal	Existing R1-G Zone	Zone Standard (URMD Zone)	OCP
Site area (m2) (min.)	1741.93*	460.00	1840.00	-
Lot width (m) (min.)	38.10	15.00	n/a	-
Combined floor area (m ²) (max.)	3082.22*	300.00	n/a	-
Floor space ratio (ratio) (max.)	1.77:1*	0.50:1	2.00:1	Approximately 1.10:1
Height (m) (max.)	13.39*	7.60	18.50	-
Storeys (max.)	4.00*	2.00/1.50 with Basement	6.00	Approximately 3.00
Roof deck	Yes	Not permitted	n/a	-
Setbacks (m) – minimum				-

Zoning Criteria	Proposal	Existing R1-G Zone	Zone Standard (URMD Zone)	OCP
Front yard (Beechwood)	2.72*	7.50	4.00	-
Projections – porch (max.)	1.026	1.60	1.80	-
Projections – stairs less than 1.7m in height (max.)	2.73*	2.50	1.80	-
Rear (NE)	2.61*	13.72 (30% of lot depth)	10.00	-
Side (Fairfield)	2.32*	5.72 (15% of lot width)	“6.00 – interior lot line 4.0 – flanking street”	-
Side (SE)	0.50*	5.72 (15% of lot width)	“6.00 – interior lot line 4.0 – flanking street”	-
Combined side yards	2.82*	5.40	n/a	-
Site coverage (%) (max.)	65.00*	30.00	40.00	-
Open site space – lot (%) (min.)	23.00*	50.00	50.00	
Parking	21 – Units* 2 – Visitor* 23 Total* (2 Van accessible)	37 – Units 3 – Visitor 40 Total	37 – Units 3 – Visitor 40 Total	-
Bicycle – Long Term	54.00	38.00	38.00	-
Bicycle – Short Term	6.00	6.00 (rack)	6.00 (rack)	-
Driveway/parking slope (%)	6.00	8.00	8.00	-

The following data table compares the concurrent proposal for 1964 Fairfield Road and 507 Foul Bay Road with the R1-G Zone (existing zoning for 507 Foul Bay Road), and the RK-11 Zone, Fairfield Townhouse District (existing zoning for 1964 Fairfield Road). An asterisk is used to identify where the proposal does not meet the requirements of the existing Zone.

Zoning Criteria	Proposal	Zone Standard (R1-G)	Zone Standard (RK-11)
Lot area (m ²) (min.)	5664.60 - 1964 Fairfield Road 793.10 - Foul Bay Road 6,457.70 - Total Site	460	Greater of 555 or 470 per dwelling unit
Use	Garden, Restaurant, and Gift Shop*	Single Family Dwelling	Single-, Two-family, Attached Dwellings
Lot width (m) (min.)	51.24 - Fairfield Road 41.08 - Foul Bay Road	15	18
Total Floor Area	176.79 - Fairfield Road	n/a	n/a
Floor space ratio (max.)	0.03 - Fairfield Road 0.00 - Foul Bay Road	0.5:1	0.55
Storeys (max.)	1	2/1.5 w/ Basement	2.6
Setbacks (m)			
Front	41.81	7.50	6.00
Rear	50.19	9.10 or 30% of lot depth	7.50 - living room 2.50 blank wall/ 4.00 habitable rooms/ 7.50 living room
Side	10.42	1.50 or 15% of lot width	2.5 blank wall/ 4.0 habitable rooms/ 7.5 living room
Parking (min.)	0* (Existing)	Schedule C 6	Schedule C 6
Short term bicycle parking – minimum	14	Schedule C 1	Schedule C 1
Accessory Building Location	Side Yard * Rear Yard	Schedule F Rear Yard	Schedule F Rear Yard

Sustainable Mobility

1733, 1735 and 1737 Fairfield Road

The application proposes the following features which support multi-modal transportation:

- car share memberships and usage credits for all residential units
- on-street electric car share vehicle
- level 2 dual-head on street electric vehicle charger and all associated infrastructure and

connections

- bicycle parking to accommodate oversized bicycles (10% of required long-term spaces)
- bicycle parking with access to an electrical outlet (50% of required long-term spaces)
- bicycle repair and maintenance station.

The application is showing more long-term bicycle parking than is required in the bylaw; however, it should be noted that additional information is required from the applicant to confirm long-term bicycle parking spaces meet minimum dimensions required for long-term bicycle parking. It is possible that the long-term bicycle will need to be marginally reduced to meet the minimum required dimensions.

1964 Fairfield Road and 507 Foul Bay Road

This application proposes new short-term bike parking beyond bylaw requirements, which support multi-modal transportation objectives.

Public Realm

1733, 1735 and 1737 Fairfield Road

The following public realm improvements are proposed in association with this application:

- standard frontage works, including a curb extension at the Beechwood Avenue intersection with Fairfield Road, including pedestrian ramps and tactile domes, to narrow crossing distance and improve pedestrian accessibility and safety
- provision of a level two dual head electric vehicle charging station on Beechwood Avenue, including all necessary electrical connections, for the by the proposed car share vehicle, and for general public use.

The charging station would be secured with a Section 219 covenant, registered on the property's title, prior to Council giving final consideration of the proposed Zoning Regulation Bylaw Amendment.

1964 Fairfield Road and 507 Foul Bay Road

No public realm improvements beyond City standard requirements are proposed in the application for 1964 Fairfield Road and 507 Foul Bay Road.

Community Consultation

1733, 1735 and 1737 Fairfield Road

Consistent with the *Community Association Land Use Committee (CALUC) Procedures for Processing Rezoning and Variance Applications*, prior to submission of the application, it was posted on the Development Tracker along with an invitation to complete a comment form on March 11, 2022. Mailed notification was sent to owners and occupiers of property within 200m of the subject property advising that a consultation process was taking place and that information could be obtained and feedback provided through the Development Tracker. A sign was also posted on site, to notify those passing by of this consultative phase.

The applicant participated in an initial meeting with the CALUC on March 28, 2022. Additional meetings were held on March 27, 2023 and October 23, 2023 to share changes to the application

that increased the density and unit count of the proposal. Summaries of the three meetings along with the comment forms are attached to this report.

The associated application proposes variances, therefore, in accordance with the City's *Land Use Procedures Bylaw*, it requires notice, sign posting and a meeting of Council to consider the variances.

1964 Fairfield Road and 507 Foul Bay Road

Consistent with the *Community Association Land Use Committee (CALUC) Procedures for Processing Rezoning and Variance Applications*, prior to submission of the application, it was posted on the Development Tracker along with an invitation to complete a comment form on March 10, 2023. Mailed notification was sent to owners and occupiers of property within 200m of the subject property advising that a consultation process was taking place and that information could be obtained and feedback provided through the Development Tracker. A sign was also posted on site, to notify those passing by of this consultative phase.

The applicant participated in a meeting with the CALUC on March 27, 2023, which was a combined meeting that included revisions to the proposal at 1733, 1735 and 1737 Fairfield Road. An additional meeting was held on October 23, 2023, to share changes to the concurrent application at 1733, 1735 and 1737 Fairfield Road that increased the density and unit count of that proposal. Summaries of the two meetings along with the comment forms are attached to this report.

ANALYSIS

Official Community Plan (OCP) Amendment Applications

An amendment to the *Official Community Plan (OCP)* is required for the proposal at 1733, 1735 and 1737 Fairfield Road to increase the density and height envisioned for a Traditional Residential property and allow for a multiple dwelling use. The additional proposed OCP amendment pertaining to 1964 Fairfield Road and 507 Foul Bay Road is to redesignate the subject properties from the Traditional Residential Urban Place Designation to the Public Facilities, Institutions, Parks and Open Space Urban Place Designation.

The two proposed amendments to the OCP have been submitted to permit construction of a four-storey multiple dwelling containing 31 units at 1733, 1735 and 1737 Fairfield Road while reducing permitted density and permitted uses to that which exists currently for 1964 Fairfield Road and 507 Foul Bay Road. Staff recommend that the proposals, when considered as a whole, are supportable if revisions are made to the proposal for 1733, 1735 and 1737 Fairfield Road to address concerns with the massing and height in the northeast elevation as well as a lack of usable amenity space. The rationale for considering the proposals supportable based on OCP policy is outlined in detail below. The rationale for the recommended revisions is contained in the analysis of the Development Permit with Variances Application to follow.

The proposed development for 1733, 1735 and 1737 Fairfield Road is to construct a four-storey multiple dwelling with a density of 1.77:1 FSR. The development would exceed the envisioned density and height of up to approximately 1.1:1 FSR and three-storeys for Traditional Residential areas, and the multiple dwelling use is not a ground-oriented building form as defined in the OCP because less than half the total number of units have direct access to the outdoors. Further, an OCP strategic direction for Gonzales is to maintain and enhance neighbourhood character including the heritage character, encouraging a ground-oriented Traditional Residential area.

However, the OCP emphasizes that designations are intended as general guidance and there are a range of uses, densities and built forms that may be approved depending on the existing and envisioned context of the site, block, and neighbourhood.

The proposal is located along Fairfield Road between two Small Urban Villages, namely Fairfield Plaza and Fairfield at Irving. The OCP strategic directions for Gonzales also include encouraging opportunities for enhancement of the small urban villages on Fairfield Road. In this application, the addition of 31 units will contribute to enhancement of these urban villages, each being approximately 300m to the west and east of the site, respectively. Further, Fairfield Road is identified as a secondary arterial west of Foul Bay Road, a cumulative frequent service transit route and a greenway, which supports consideration of increased density.

The OCP supports encouraging heritage conservation through incentives and allowances such as financial incentives, bonus density provision and variances. Also, where a proposal is achieving heritage conservation objectives, development can depart from the OCP's envisioned uses and densities. Although the site proposed for redevelopment will not protect heritage features at 1733, 1735 and 1737 Fairfield Road, the concurrent proposal for 1964 Fairfield Road and 507 Foul Bay Road will strengthen the level of heritage protection for another site within the neighbourhood through the proposed rezoning, OCP amendment and heritage designation amendment.

The OCP also envisions Community Amenity Contributions (CACs) where possible as part of new development proposals and the Inclusionary Housing and Community Amenity Policy sets out expectations for CACs. The applicant for 1733, 1735 and 1737 Fairfield Road has not provided CACs in the form of inclusionary housing units or cash contributions to be directed to City reserve funds; however, the applicant has proposed an alternative for Council's consideration, which is described in more detail below.

The proposed amendment to redesignate the subject properties for 1964 Fairfield Road and 507 Foul Bay Road is supportable because the amendment will align the land use designation to the existing use, proposed site-specific zoning and long-term vision for this unique heritage asset. In addition, the amendment and associated rezoning and heritage designation amendment proposals will advance OCP objectives to conserve the heritage value of a prominent cultural landscape in the neighbourhood.

OCP Consultation

The *Local Government Act* (LGA) Section 475 requires a Council to provide one or more opportunities it considers appropriate for consultation with persons, organizations and authorities it considers will be affected by an amendment to the OCP. Consistent with Section 475 of the LGA, Council must further consider whether consultation should be early and ongoing. This statutory obligation is in addition to the Public Hearing requirements. In this instance, if Council moves the application forward, staff recommend for Council's consideration that notifying owners and occupiers of land located within 200 metres of the subject site along with posting a notice on the City's website will provide adequate opportunities for consultation with those affected.

Through the Community Association Land Use Committee (CALUC) Community Meeting process all owners and occupiers within a 200m radius of the site were notified and invited to participate in a Community Meeting; therefore, the consultation proposed at this stage in the process is recommended as adequate and consultation with specific authorities, under Section 475 of the LGA, is not recommended as necessary.

Should Council support the OCP amendment, Council is required to consider consultation with the Capital Regional District Board; Councils of Oak Bay, Esquimalt and Saanich; the Songhees and Esquimalt First Nations; the School District Board and the provincial government and its agencies. However, further consultation is not recommended as necessary for this amendment to the Urban Place Designation as this matter can be considered under policies in the OCP.

Council is also required to consider OCP Amendments in relation to the City's *Financial Plan* and the *Capital Regional District Liquid Waste Management Plan* and the *Capital District Solid Waste Management Plan*. This proposal will have no impact on any of these plans.

Rezoning Application

Official Community Plan

1733, 1735 and 1737 Fairfield Road

The *Official Community Plan, 2012* (OCP) Traditional Residential Urban Place Designation supports a range of ground-oriented residential uses. The OCP envisions densities of approximately 1.1:1 FSR for Traditional Residential properties, with building heights up to approximately three-storeys. As noted, the proposal for 1733, 1735 and 1737 Fairfield Road is not consistent with the envisioned use, density or height for this designation, which necessitates an OCP amendment. The proposal does advance OCP objectives, including improvements to the public realm and providing a diversity of housing.

1964 Fairfield Road and 507 Foul Bay Road

The OCP seeks to maintain and enhance neighbourhood character of Gonzales, including the heritage value and the cultural landscape of individual properties such as Abkhazi Garden. The OCP also supports maintaining community assets that contribute to ecological functions, attract investment and support economic activity, including natural and built heritage and green infrastructure. Additionally, the OCP prioritizes conservation of the green space that supports a healthy urban forest. The rezoning will provide another layer of protection to ensure the continuation of the current use on the subject properties.

Gonzales Neighbourhood Community Plan

1733, 1735 and 1737 Fairfield Road

The proposal is inconsistent with the *Gonzales Neighbourhood Community Plan, (2002)*, which encourages maintaining existing zoning and the detached dwelling character of the neighbourhood. However, the plan also envisions a range of housing options to meet the needs of people with different needs and incomes, which the proposal advances through a mix of units in a multiple dwelling housing form. The plan emphasizes preservation and enhancement of the natural landscape on private and public properties as well as boulevards. Removal of all existing trees and shrubs onsite is proposed; however, the landscape plan proposes 18 new trees, including seven new boulevard trees on the Fairfield Road frontage with adequate soil volumes.

1964 Fairfield Road and 507 Foul Bay

The concurrent proposal is generally consistent with the *Gonzales Neighbourhood Community Plan (2002)*, which emphasizes preservation and enhancement of the heritage assets and of the natural landscape on both private and public properties.

Housing

The application for 1733, 1735 and 1737 Fairfield Road, if approved, would add 31 new residential strata units, which would increase the overall supply of housing in the area and contribute to the targets set out in the *Victoria Housing Strategy*.

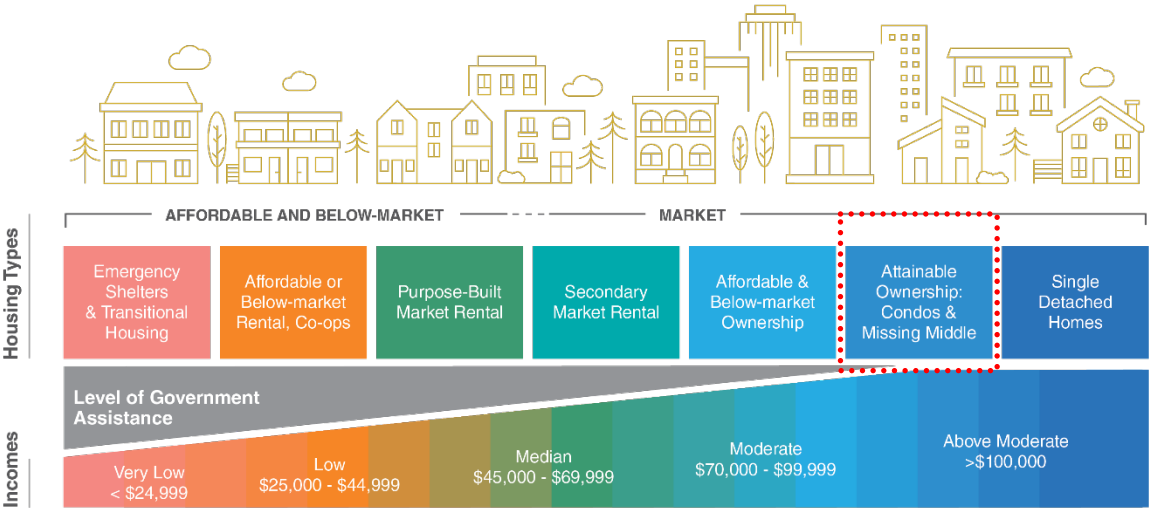


Figure 3. Housing Continuum

Housing Mix

At present there is no policy that provides targets regarding housing mix and unit type is not regulated or secured. However, the OCP identifies a mix of units as an objective and identifies the need for a diverse range of housing units including family housing. As submitted, this application for 1733, 1735 and 1737 Fairfield Road proposes two studio, twelve one-bedroom, nine two-bedroom, six two-bedroom with den and two three-bedroom units. The applicant has agreed to secure a mix of two-bedroom, two-bedroom and a den and three-bedroom units through a legal agreement; however, flexibility is requested by the applicant to allow one of the three-bedroom units to be converted to a two-bedroom with a den, which is reflected in the recommendation.

Existing Tenants

The proposal for 1733, 1735 and 1737 Fairfield Road is to demolish three existing buildings which would result in a loss of seven existing residential rental units. Consistent with the Tenant Assistance Policy, the applicant has provided a Tenant Assistance Plan, which is attached to this report.

Statutory Right of Way

The applicant for 1733, 1735 and 1737 Fairfield Road is proposing to provide a 0.86m wide statutory right-of-way along Fairfield Road to help achieve a 2m sidewalk and small canopy trees with adequate soil volumes that will act as a planted buffer between pedestrians and motor vehicles.

Inclusionary Housing and Community Amenity Policy

The Inclusionary Housing and Community Amenity Policy outlines the expectations for providing CACs and helps steer the rezoning process for new residential developments. While the policy does not request CACs for properties designated as Traditional Residential, the required increase in density and associated OCP amendment for 1733, 1735 and 1737 Fairfield Road classifies the proposal as an atypical application where a contribution is justified, and an economic analysis was requested. The policy accommodates alternatives to the provision of inclusionary housing units or cash contributions defined in the policy, with any appropriate alternative to be based on identified community needs and demonstrating a value equivalent to 75% of the value of the increased bonus density.

The applicant for 1733, 1735 and 1737 Fairfield Road has not provided CACs in the form of inclusionary housing units or cash contributions to the City's reserve funds; rather, as outlined in the applicant letters, the applicant has entered into an agreement with The Land Conservancy (TLC), the owner of 1964 Fairfield Road and 507 Foul Bay Road, which is intended to strengthen protection of Abkhazi Garden from future redevelopment.

The concurrent OCP amendment, rezoning and heritage designation amendment of 1964 Fairfield Road and 507 Foul Bay Road is proposed as an alternative to providing inclusionary housing units or cash contributions. The following measures would be secured through legal agreements and bylaws, as applicable:

- rezone the sites at 1964 Fairfield Road and 507 Foul Bay Road to remove existing residential permitted uses and limit zoning to permit only the existing uses
- amend the OCP designation to reflect the existing use
- expand heritage designation to encompass the entire site
- secure continued public access to Abkhazi Garden in perpetuity.

In addition, the applicant is proposing a monetary donation to TLC in the amount of \$350,000; however, the donation will not be secured as part of either rezoning application.

The applicant provided an independent economic analysis to demonstrate that the amount of the donation is consistent with the Inclusionary Housing and Community Amenity Policy's recommended method for determining a cash contribution, which is to provide 75% of the estimated increase in land value from what would be anticipated under existing zoning.

Development Permit with Variances Application

Official Community Plan Design Guidelines

The OCP identifies the properties at 1733, 1735 and 1737 Fairfield Road as within Development Permit Area (DPA) 16, General Form and Character. The objectives of this DPA are to integrate new developments to compliment and enhance the established place character through architecture, landscape and urban design. Other objectives include providing sensitive transitions to adjacent properties with built form of three storeys or lower, and to achieve human-scaled design, quality of open spaces and accessibility. The applicable guidelines include the *Design Guidelines for Multi-Unit Residential, Commercial and Industrial Development* (2012, revised 2019), *Guidelines for Fences, Gates and Shutters* (2010), and the *Advisory Design Guidelines for Buildings, Signs and Awnings* (1981).

Site Design

The *Design Guidelines for Multi-Unit Residential, Commercial and Industrial Development* includes objectives to ensure design of multiple dwelling development provides a transition in form and massing to lower density building forms. The southeast elevation of the proposed development for 1733, 1735 and 1737 Fairfield Road effectively provides transition in form and massing; however, the northeast setback of 2.6m, combined with the proposed four-storey building height, would benefit from revisions to improve the transition to the northeast neighbour (1745 Fairfield Road).

The guidelines prioritize open space as part of site design, which should be usable, attractive, and well-integrated, and should preserve existing vegetation where possible. The proposed open space area does not meet the minimum required in the standard URMD Zone, largely due to the proposed surface parking. When combined with 65% lot coverage, the surface parking does not preserve existing established landscaping or provide adequate space for replacement trees. The recommendation includes a condition that parking be reduced, located underground and/or a landscaped amenity space be provided to better align with the guidelines. In addition, a two-space reduction in vehicle parking is recommended at a minimum to provide additional space to provide required replacement trees as required in the *Tree Protection Bylaw No. 21-035*.

Parking should be located underground or at the rear of the property to minimize the impact on streetscape and maximize ground level space for landscaping. Where it is unavoidable to locate driveways in building frontages, consideration should be given to the incorporation of these elements into the building. Also, the location and design of service functions, such as parking and hydro infrastructure, should not be prominent from the street. The proposed parking is effectively concealed from the public realm through innovative building design; however, hydro infrastructure is visible from Beechwood Avenue.

The guidelines encourage vertical disruptions along pedestrian routes be avoided and vehicle and pedestrian conflicts be minimized through site design. The proposal adequately achieves this accessibility objective by providing a ramp to the main entrance to allow access without stairs or other vertical disruptions.

The guidelines also encourage the use of high quality, permeable paving materials in parking and pedestrian areas in order to improve on-site stormwater management. This is particularly important where a development occupies more than 40% of the site and includes less than 50% open space, which are minimum requirements in the standard URMD Zone. The proposal occupies 65% of the site area and provides 23% open space, with less than half being landscaped area; therefore, the use of permeable pavement should be a priority. While the proposal does include permeable paving material for a portion of the parking area, it is recommended that this be increased to cover the entire parking area, or at a minimum, the portions of the parking area that is uncovered.

The applicant for 1733, 1735 and 1737 Fairfield Road provided a supplementary letter, dated September 12, 2023, which outlines that they intend to revise the proposal from what is shown in the attached plans, which would add an additional dwelling unit and convert a one-bedroom unit to a two-bedroom unit, which would result in the removal of the only common amenity space provided in the development. Similar to above-noted concerns related to provision surface parking, a reduced rear yard setback and provision of open space, it is recommended that the applicant consider a landscaped rooftop amenity area to provide additional usable outdoor space.

Building Design

The design guidelines encourage that overall building design be sensitive and innovative in response to context. In addition, multiple dwellings directly abutting lower density buildings should provide a height transition. The contemporary design for the dwelling at 1733, 1735 and 1737 Fairfield Road compliments the variety of architectural styles along Fairfield Road and massing is sensitive to surrounding lower density development, apart from the northeast elevation where a transition in form and massing to the abutting single-family dwelling is recommended.

The proposed building contributes to both streetscapes and adds interest to the streetscape through variations in building height, roofline and massing. The street level design, with individual entrances facing Fairfield Road and a prominent shared entryway fronting Beechwood Avenue, encourages interaction with the street and public sidewalk, consistent with the design guidelines.

Porches and other design features are encouraged to make transitions from the public to the private realm, which is achieved along Fairfield Road with landscaping to define the transition to private open space. Also, the exterior building materials are high quality and durable, with use of light brick on lower levels.

It should be noted that the applicant is proposing additional revisions that will add a two-bedroom unit (from 30 units to 31 units), convert a one-bedroom unit to a two-bedroom unit, result in the loss of the common amenity space, and will marginally reduce the step back of the third storey in the southeast elevation. Given the proposed additional revision will result in a loss of the only shared amenity space in the building, it is recommended that a landscaped rooftop amenity be added through revisions to be submitted.

Variances (1733, 1735 and 1737 Fairfield Road)

Although a site-specific zone is sought, variances are recommended for the 1733, 1735 and 1737 Fairfield Road application (instead of inclusion in the new zone) where the proposal is not consistent with the standard URMD Zone, Urban Residential Multiple Dwelling District, and the Off-Street Parking Regulations (Schedule C) of the *Zoning Regulation Bylaw*. This ensures that if this proposal is not built, any potential future redevelopment would require Council's consideration and approval for these specific aspects.

Setbacks

Variances are required for all building setbacks. A reduction to the minimum front yard setback is required from 4m to 2.70m as well as an increase in stairs projection from 1.80m to 2.73m. In addition, a reduction to the minimum flanking street setback (Fairfield Road) from 4.00m to 2.30m is proposed. These variances are considered supportable as each result from the design objectives to site the building to frame fronting public streets, create a sense of enclosure and maximize rear and interior side yard setbacks to ensure a buffer with abutting properties.

A variance to reduce the minimum side yard setback (southeast) from 6m to 0.50m is also requested, which is a result of the incorporation of driveway access into the building in order to conceal the surface parking area from the public realm. This variance is considered supportable because the portion of the building within the required setbacks effectively minimizes the visual impact of the parking area and the reduced setback is limited to the portion of the building near Beechwood Avenue.

The proposal requires a reduction to the minimum rear yard setback (northeast) from 10m to

2.60m, which is not considered supportable because the 10m rear yard requirement is intended to ensure usable open space and, when combined with surface parking, the reduction of all building setbacks results in limited open space to provide a buffer to abutting properties. Additionally, when combined with the four-storey building height, the reduced rear yard setback may impact neighbouring properties. The variance would be considered supportable with revision to the building massing to soften the height transition and provision of amenity space above the ground floor, specifically a shared rooftop amenity that is possible with a flat roof design.

Site Coverage and Open Space

There are required variances from the standard URMD Zone to increase the maximum site coverage from 40% to 65% and decrease open site space from 50% to 23%. Similar to the concern with reductions to all setbacks, limited open space is proposed for buffers and amenity area. This variance would also be considered supportable with the revisions outlined above.

Parking

A variance is required to reduce vehicle parking from 40 spaces to 23 spaces. The applicant has submitted a parking study, and the variance is considered supportable based on the comprehensive TDM package that is expected to offset the parking shortfall. Also, a further two-space reduction in vehicle parking is recommended to provide additional space to provide required replacement trees, as noted above. It should be noted that the applicant has indicated that they are not amenable to reducing the number of parking spaces beyond the current proposal of 23 spaces, as this will impact the marketability of the project. Staff maintain that this is advised, and a condition has been included in the recommendation. The requested variance to permit long-term bicycle parking provided in a stacked format is considered supportable as the applicant has provided details showing that the stacked bike storage will still accommodate larger bicycles, with a lift assist mechanism to ensure ease of use.

The TDM package is to include:

- car share memberships and usage credits for all residential units
- electric car share vehicle and dual head electric vehicle charging station on Beechwood Road
- bicycle parking to accommodate oversized bicycles (10% of required long-term spaces)
- bicycle parking with access to an electrical outlet (50% of required long-term spaces)
- bicycle repair and maintenance station.

Accessibility

The proposed walkways surrounding the building and to the building entrances are designed to be accessible, with a ramp required to the main entrance and an elevator that will provide access to units.

Advisory Design Panel Review

The application for 1733, 1735 and 1737 Fairfield Road was reviewed by the Advisory Design Panel on October 26, 2022. At that meeting, the following motion was passed:

“That Development Permit with Variance(s) Application No. 000204 for 1733-1737 Fairfield Road be approved with the following changes:

Consideration to simplifying the roof form to be more sympathetic to the neighbouring context.

MINORITY REPORT: Those that voted against believe the building is not consistent with the density, height and use envisioned for traditional areas in the OCP.

Carried 4:2"

In response, the applicant has revised the roofline, specifically removing the mansard roof to better reflect the neighbourhood context, incorporated a step-back of the upper floors along Fairfield Road, and extended the proposed use of brick to emphasize the ground-oriented brick base of the building. In addition, at the time of submitting these revisions in response to staff and panel concerns, the applicant revised the building to create smaller units and increase the unit count from 19 to 30 units, which resulted in a marginal increase in total floor space.

Tree Preservation Bylaw and Urban Forest Master Plan

The goals of the *Urban Forest Master Plan* include protecting, enhancing, and expanding Victoria's urban forest and optimizing community benefits from the urban forest in all neighbourhoods. The application for 1733, 1735 and 1737 Fairfield Road was received after July 1, 2021, so *Tree Protection Bylaw No. 21-035* applies. The application at 1964 Fairfield Road and 507 Foul Bay has no associated tree impacts.

A total of 20 trees and six hedges have been inventoried. Of these, ten trees and six hedges are located on the subject lot, and six trees and one of the hedges are bylaw protected. There are three existing municipal trees on the Beechwood Avenue frontage. Six bylaw protected trees and one bylaw protected hedge will require removal as they are in the building area or immediately adjacent to an area where excavation will occur. One small municipal tree would be removed for service installation. All off-site trees and two municipal trees can be retained following the mitigation measures outlined in the arborist report.

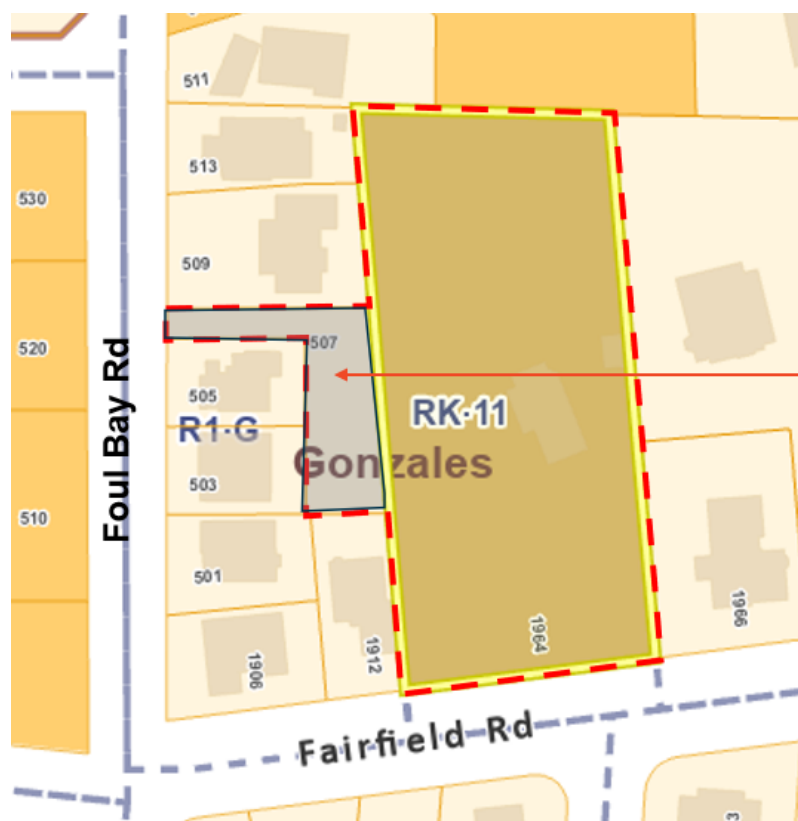
The applicant is proposing to plant 18 new trees on the subject lot, five of which will be replacement trees planted towards requirements under the Bylaw. Under the current proposal, the applicant would be required to pay \$10,000 for cash-in-lieu towards the City's Tree Reserve Fund (\$2,000 X five replacement trees not planted). Seven new municipal trees are proposed on the Fairfield Road frontage with adequate soil volumes. Currently there are no municipal trees on the Fairfield Road frontage. One municipal tree is proposed on the Beechwood Avenue frontage.

Heritage Designation Amendment Application (507 Foul Bay Road)

The purpose of this portion of the report is to provide information and analysis regarding the proposed amendment to the existing heritage designation bylaw for 1964 Fairfield Road. This amendment would include the addition of the adjacent property 507 Foul Bay Road, which contains the greenhouse, and the garden's support and maintenance spaces.

Description of Historic Place

The historic place comprises a designed domestic evolving garden, garden buildings, and an early example of a modern bungalow, set around and upon a rocky knoll, in Victoria's Gonzales neighbourhood. The house was designed for Peggy and Nicholas Abkhazi, by Victoria-based Modernist architect John Wade, in 1946-47. For a complete description of the heritage value and character-defining elements, see attached Statement of Significance.



Add Garden
Support Area / Lot
507 to Designation
Bylaw No. 23-007

Relevant History

The original application was reviewed by the Heritage Advisory Panel at its April 12, 2005 meeting, and a motion was made to recommend that Council consider approving the heritage designation of the existing house, summer house, garden shed and garden, known as Abkhazi Garden at 1964 Fairfield, as a municipal heritage site, including the birch paneling and oak flooring on the interior of the main house.

More recently, the Statement of Significance has been updated to include additional character-defining elements, make specific corrections, and add the abutting property at 507 Foul Bay, which acts as an important maintenance and support area for the gardens, including propagation activities. As noted in the Statement of Significance, the garden is valued for its rare plant

conservation and the development of new hybrids, therefore the area identified as 507 Foul Bay Road, even though it is a working back-of-house space, is integral to the functioning of the gardens, its evolving nature, and ongoing maintenance. Therefore, it is considered important to ensure the whole of the gardens is acknowledged as significant through heritage designation.

CONCLUSIONS

The proposed four-storey multiple dwelling with a density of 1.77:1 FSR is not consistent with the use, density and height envisioned for Traditional Residential properties in the OCP.

However, the proposed development would advance housing objectives by providing 31 new dwelling units, including some family-oriented housing. Further, the concurrent rezoning and OCP amendment includes proposed measures to strengthen heritage protection of Abkhazi Garden and secure continued public access.

While the form and character of the development is largely consistent with the design guidelines, revisions are recommended to improve the height transition from the neighbouring home and the northeast building elevation. Also, a revision to provide parking underground, a further reduction in parking or a common rooftop amenity is recommended to offset the space allocated to surface parking and resulting reduced open space. The application is recommended to proceed to a public hearing, subject to the conditions outlined in the recommendation.

ALTERNATE MOTIONS

Alternate Option 1 - Decline

That Council decline Application No. 00821 and associated Official Community Plan Amendment for the property located at 1733, 1735 and 1737 Fairfield Road as well as Rezoning Application No.00845 associated Official Community Plan and Heritage Designation Amendments for 1964 Fairfield Road and 507 Foul Bay Road.

Respectfully submitted,

Patrick Carroll
Senior Planner
Development Services Division

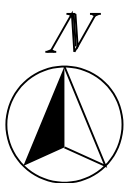
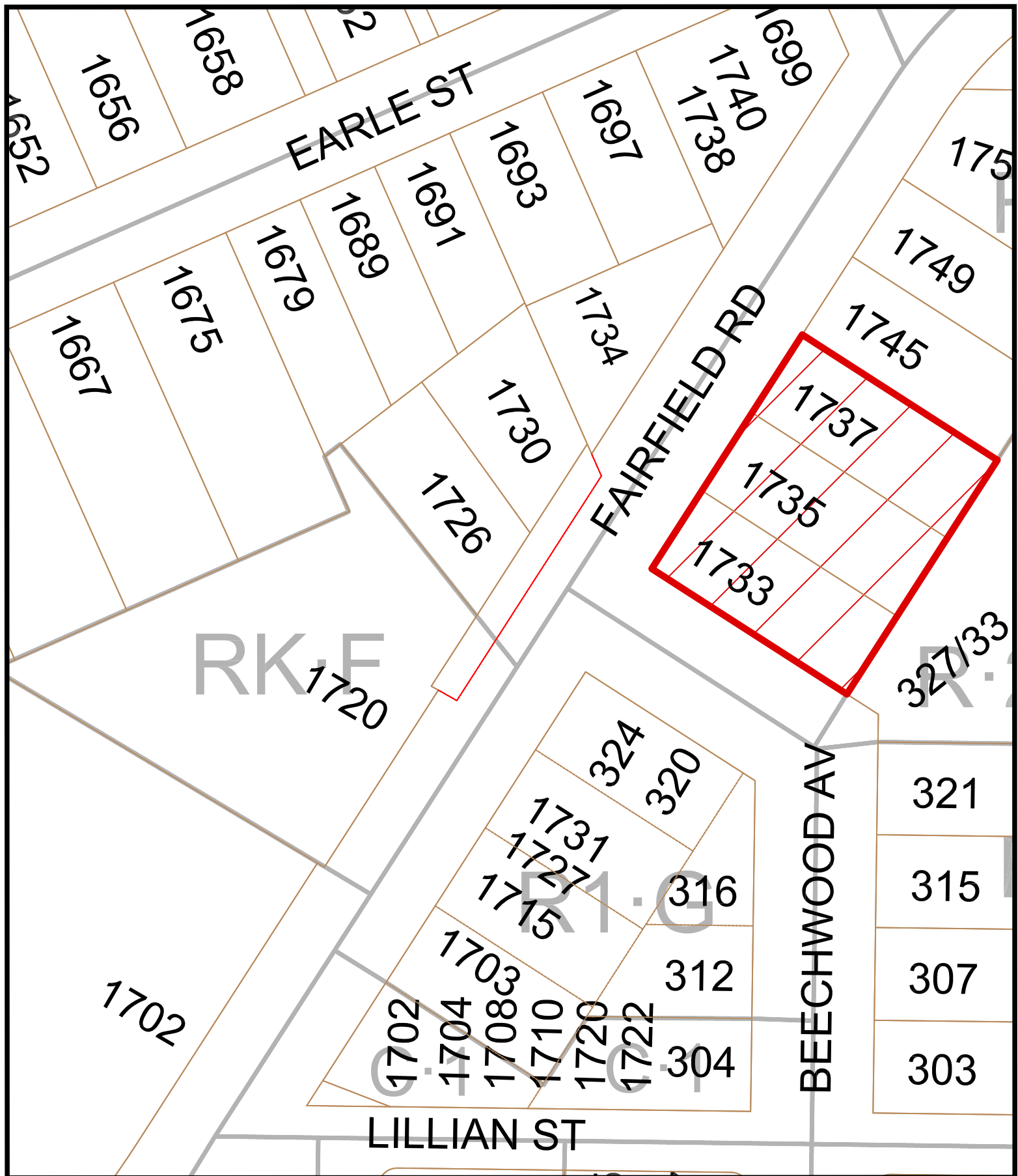
Karen Hoesle, Director
Sustainable Planning and Community
Development Department

Report accepted and recommended by the City Manager.

List of Attachments

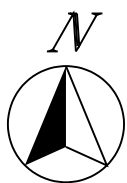
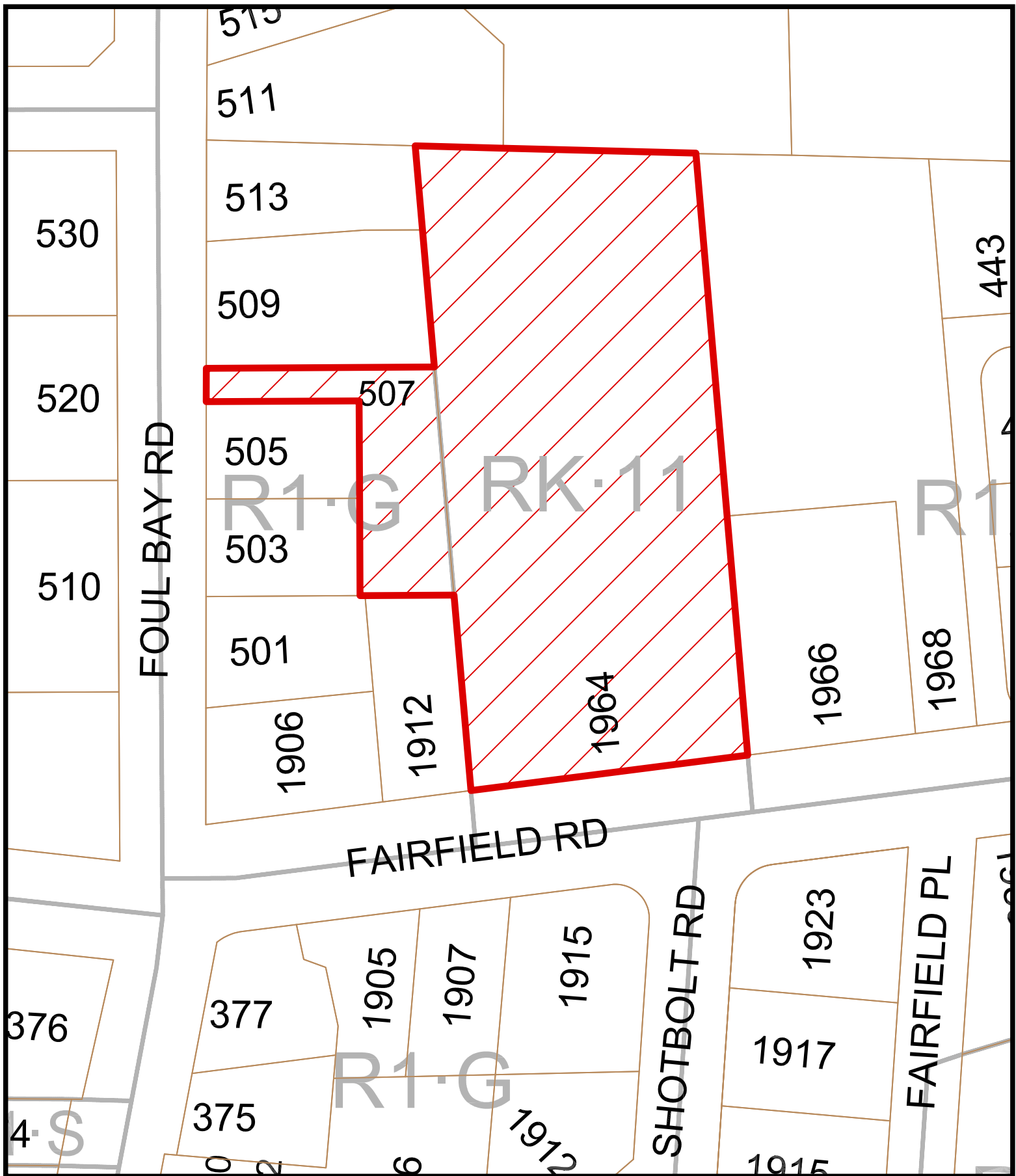
- Attachment A: Subject Maps
- Attachment B: Plans for 1733, 1735 and 1737 Fairfield Road date stamped August 22, 2023
- Attachment C: Letter from applicant to Mayor and Council dated August 21, 2023 (REZ00821)
- Attachment D: Applicant Letter re: Design Changes dated September 12, 2023
- Attachment E: Letter from applicant to Mayor and Council dated November 23, 2023 (REZ00845)
- Attachment F: ADP Minutes from the meeting held October 26, 2022
- Attachment G: Statement of Significance for 1964 Fairfield Road and 507 Foul Bay Road

- Attachment H: Tenant Assistance Plan
- Attachment I: Community Amenity Contribution Analysis dated June 25, 2023
- Attachment J: Parking Variance Report dated January 18, 2023
- Attachment K: Community Association Land Use Committee Reports dated March 28, 2022, March 27, 2023 and October 23, 2023
- Attachment L: Pre-Application Consultation Comments from Online Feedback Forms
- Attachment M: Correspondence (Letters received from residents)



1733-1737 Fairfield Road
Rezoning No.00821





1964 Fairfield Road & 507 Foul Bay Road
Rezoning No.00845






PROJECT LOCATION PLAN (NTS)



PROJECT SITE (NTS)

CONTACTS

OWNER Aryze Developments Inc. 1839 Fairfield Road Victoria, BC V85 1G9		Juan Crespi juan@aryze.ca 250.661.4493
ARCHITECT Cascadia Architects 101 - 804 Broughton Street Victoria, BC V8W 1E4		Gregory Damant, Architect AIBC greg@cascadiaarchitects.ca 250.590.3223
LANDSCAPE DESIGNER Biophilia 1608 Camosun Street Victoria, BC V8T 3E6		Bianca Bodley bianca@biophiliacollective.ca 250.590.1156
CIVIL ENGINEER McElhanney 500 - 3960 Quadra Street Victoria, BC V8X 4A3		Nathan Dunlop ndunlop@mcelhanney.com 778.746.7417



Revisions

Received Date:
August 22, 2023

LIST OF DRAWINGS

ARCHITECTURAL	
A-000	COVER
A-009	PERSPECTIVES
A-010	PERSPECTIVE & WINDOW OVERLAY
A-011	CONTEXT ELEVATIONS & PERSPECTIVES
A-012	SHADOW STUDIES
A-050	SITE PLAN & PROJECT DATA
A-051	ZONING CALCULATIONS
A-052	CODE REVIEW
A-053	SPATIAL SEPARATION
A-101	LEVEL 1 PROPOSED PLAN
A-102	LEVEL 2 PROPOSED PLAN
A-103	LEVEL 3 PROPOSED PLAN
A-104	LEVEL 4 PROPOSED PLAN
A-200	EXTERIOR ELEVATIONS
A-201	EXTERIOR ELEVATIONS
A-301	BUILDING SECTIONS
CIVIL	
C-1	PRELIMINARY CIVIL PLAN
LANDSCAPE	
L-1	LANDSCAPE SITE PLAN
L-1.1	PAVEMENT SPECIFICATIONS
L-2	UPPER LEVEL LANDSCAPE PLAN
L-3	TREE REMOVAL PLAN
L-4	TREE PLANTING PLAN
L-4.1	SOIL VOLUME ANALYSIS
L- 5	GROUND FLOOR PLANTING PLAN
L- 6	UPPER LEVELS PLANTING PLAN
L- 7	DETAILS

ARYZE
1733-1737 Fairfield Rd
AUGUST 21, 2023

Project #	2123	Date	AUGUST 21, 2023
Sheet #	A-000	Revision	5



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INC
101 - 804 Broughton Street
Victoria BC V8W 3J6 Canada
250.590.3223
cascadiaarchitects.ca



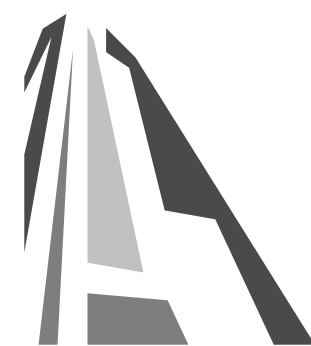


1 BEECHWOOD ELEVATION PERSPECTIVE
SCALE = 1 : 100



2 FAIRFIELD PEDESTRIAN VIEW
SCALE = 1 : 100

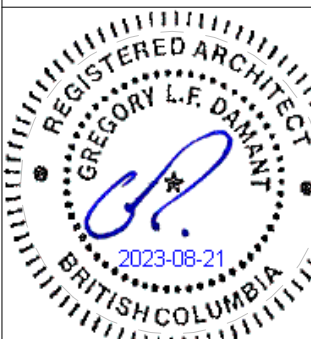
4	DPIRZ Rev2	Jan 26, 2023
3	DPIRZ Rev1	July 20, 2022
2	DPIRZ Submission	April 29, 2022
1	Development Tracker Application	March 7, 2022
NO.	DESCRIPTION	DATE

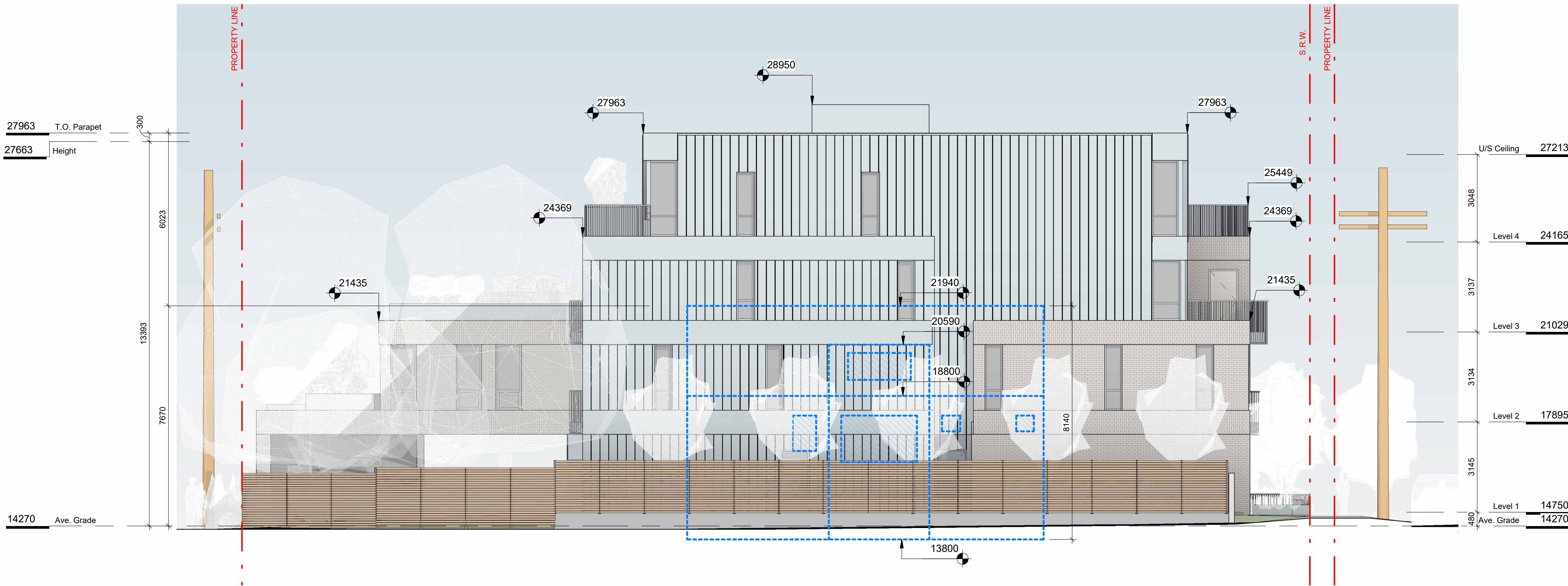


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Project		1733-1737 Fairfield Rd	
		ARYZE	
Sheet Name		PERSPECTIVES	
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Scale	1 : 100	Project #	2123
		Revision	4
		Jan 26, 2023	
		Sheet #	A-009



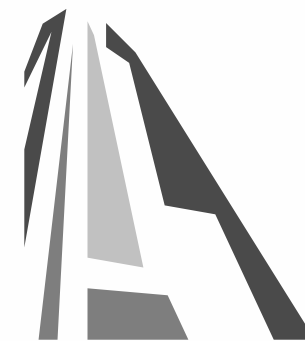


1 WINDOW OVERLAY NE ELEVATION
SCALE = 1 : 100



2 FAIRFIELD NORTH PERSPECTIVE
SCALE = 1 : 100

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3	DPIRZ Rev1	July 20, 2022
NO.	DESCRIPTION	DATE

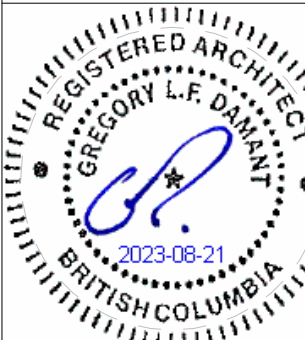


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Project	
1733-1737 Fairfield Rd	
ARYZE	
Sheet Name	
PERSPECTIVE & WINDOW OVERLAY	
Date	
AUGUST 21, 2023	
Scale	Project #
1 : 100	2123
Revision	4
Jan 26, 2023	
Sheet #	A-010

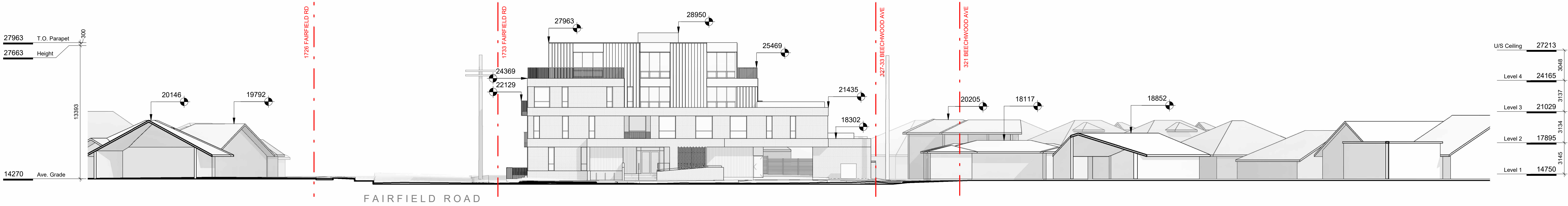
DRAFT





1 CONTEXT PERSPECTIVE FAIRFIELD ROAD
SCALE = 1 : 100

2 CONTEXT PERSPECTIVE BEECHWOOD AVE
SCALE = 1 : 100



3 CONTEXT ELEVATION - BEECHWOOD AVENUE
SCALE = 1 : 200



4 CONTEXT ELEVATION - FAIRFIELD ROAD
SCALE = 1 : 200

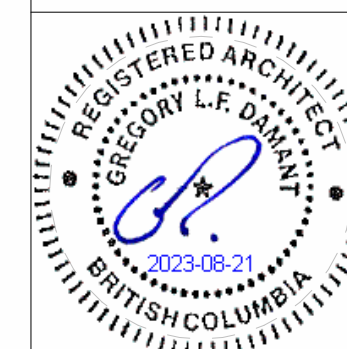
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2	DPIRZ Submission	April 29, 2022
1	Development Tracker Application	March 7, 2022
NO.	DESCRIPTION	DATE

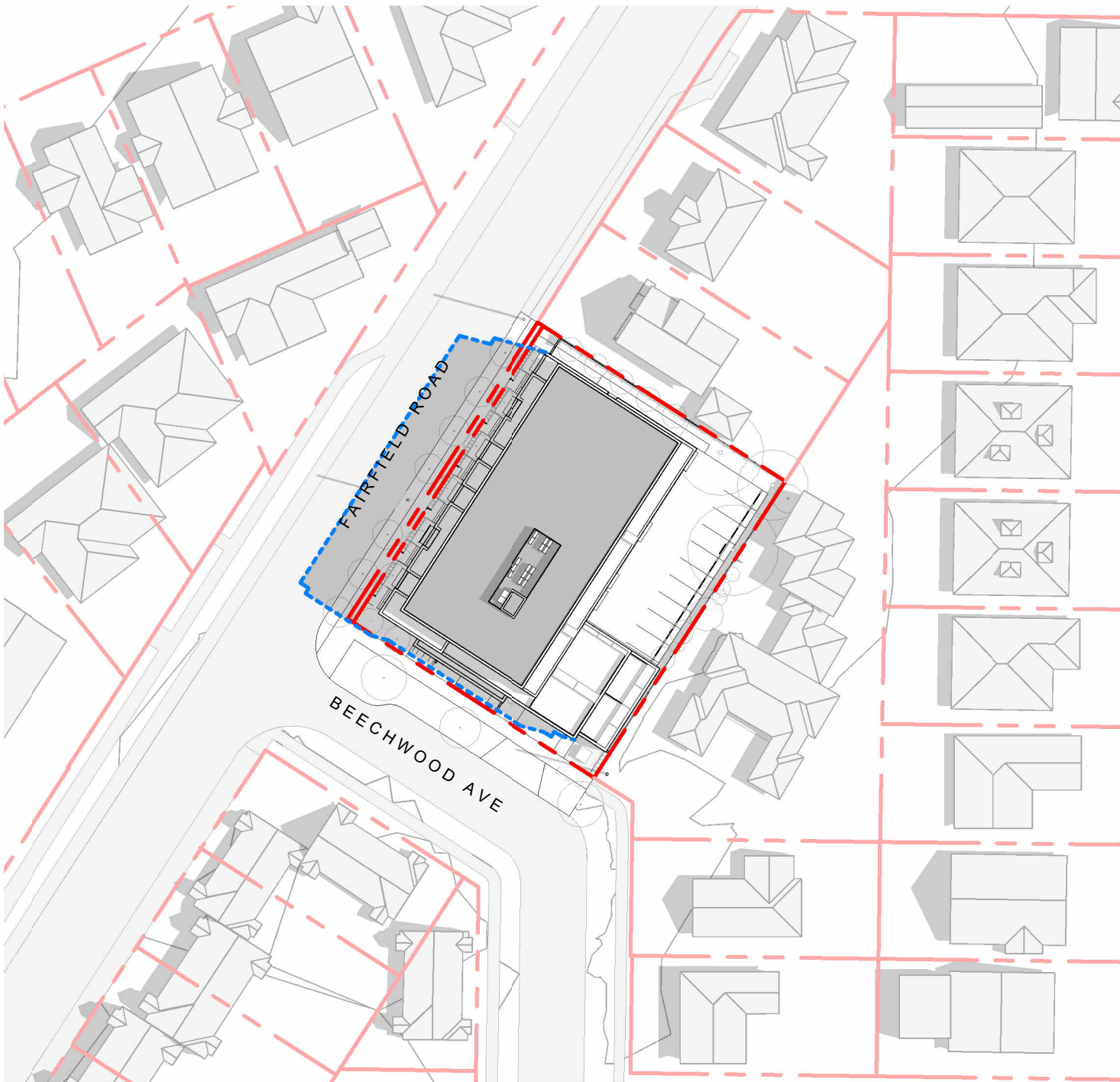


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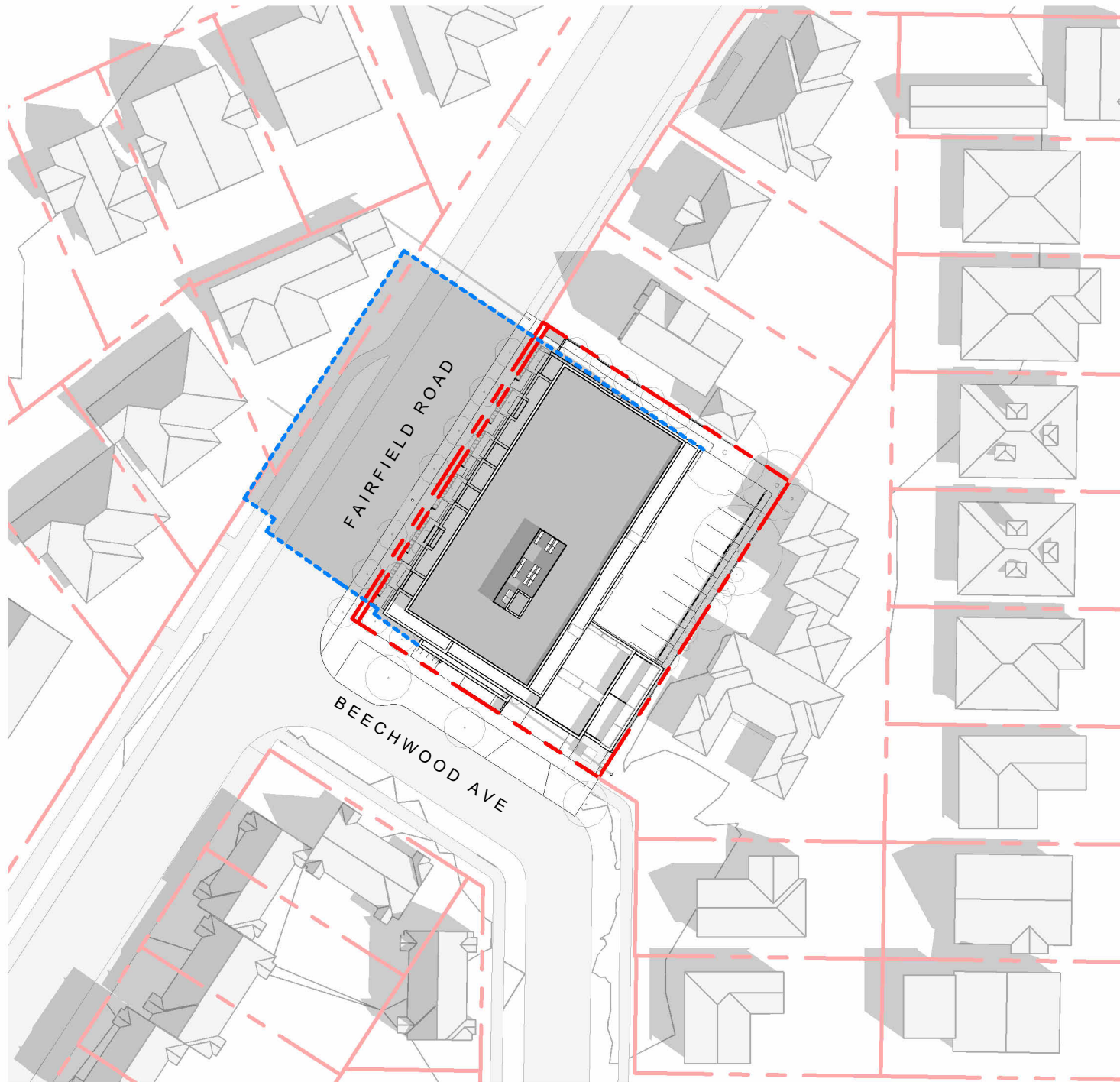
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Project		1733-1737 Fairfield Rd	
		ARYZE	
Sheet Name			
CONTEXT ELEVATIONS & PERSPECTIVES			
Date		AUGUST 21, 2023	
Scale		Project #	2123
As indicated		Revision	4
		Jan 26, 2023	
		Sheet #	A-011

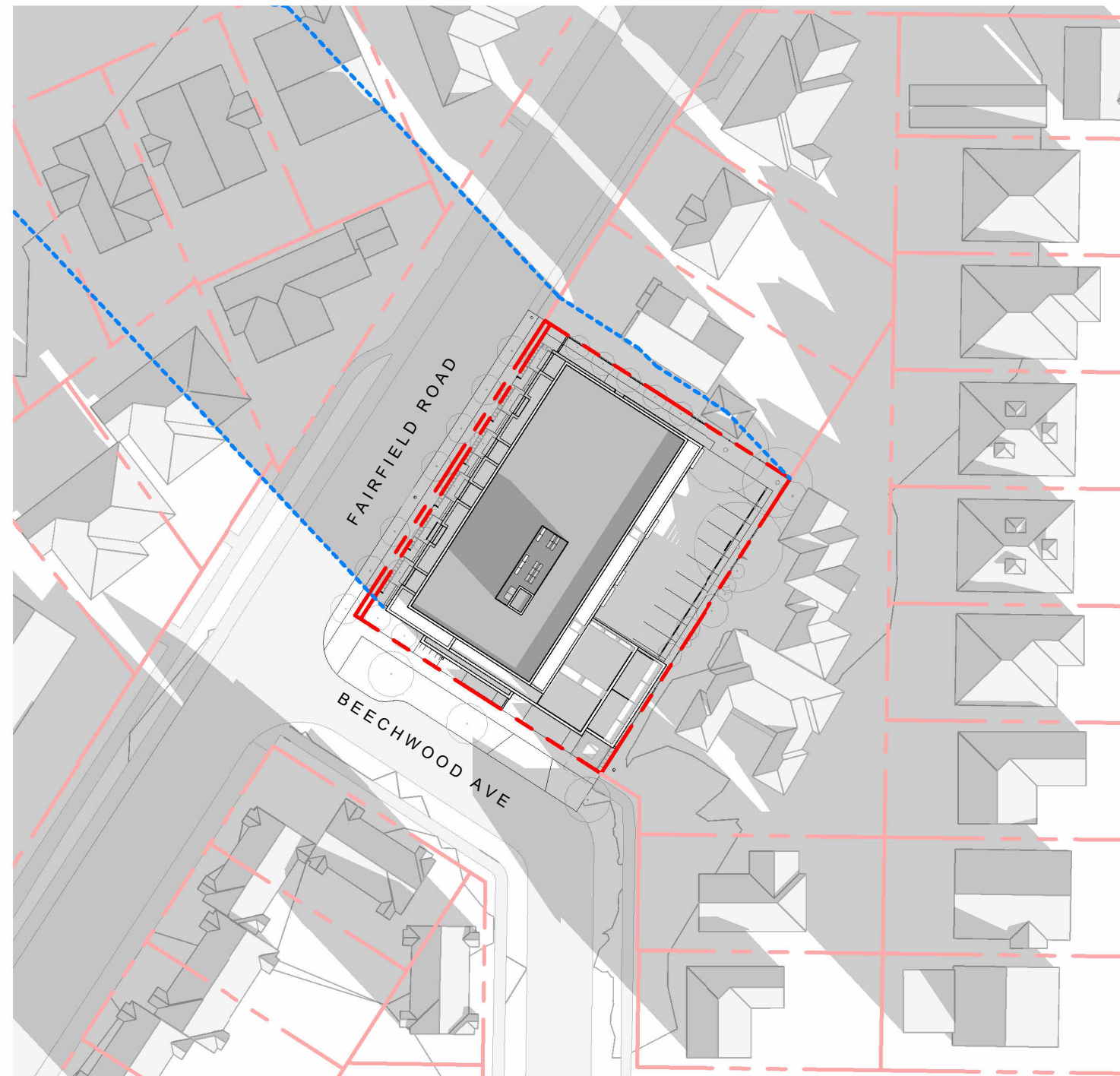




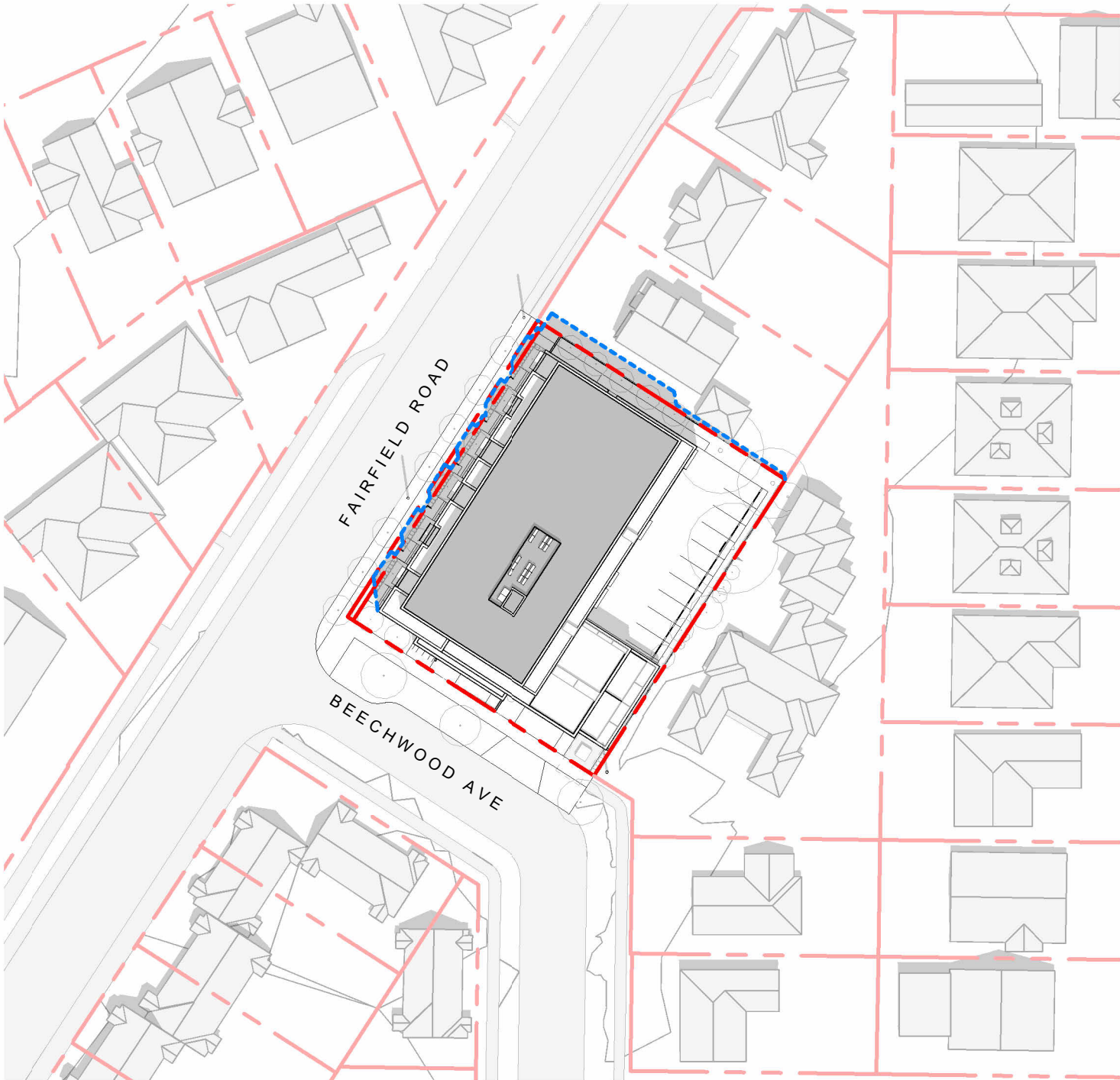
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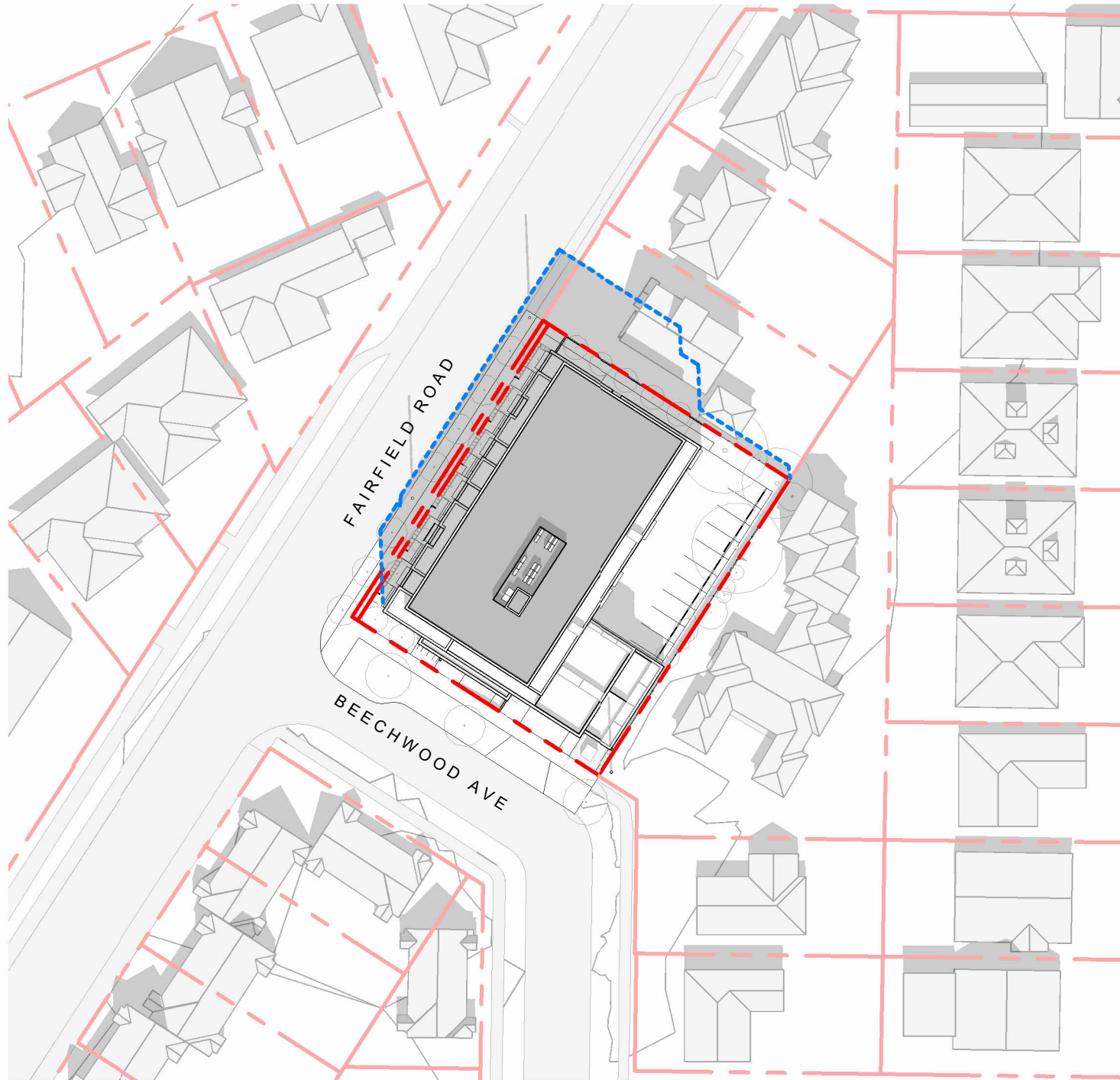
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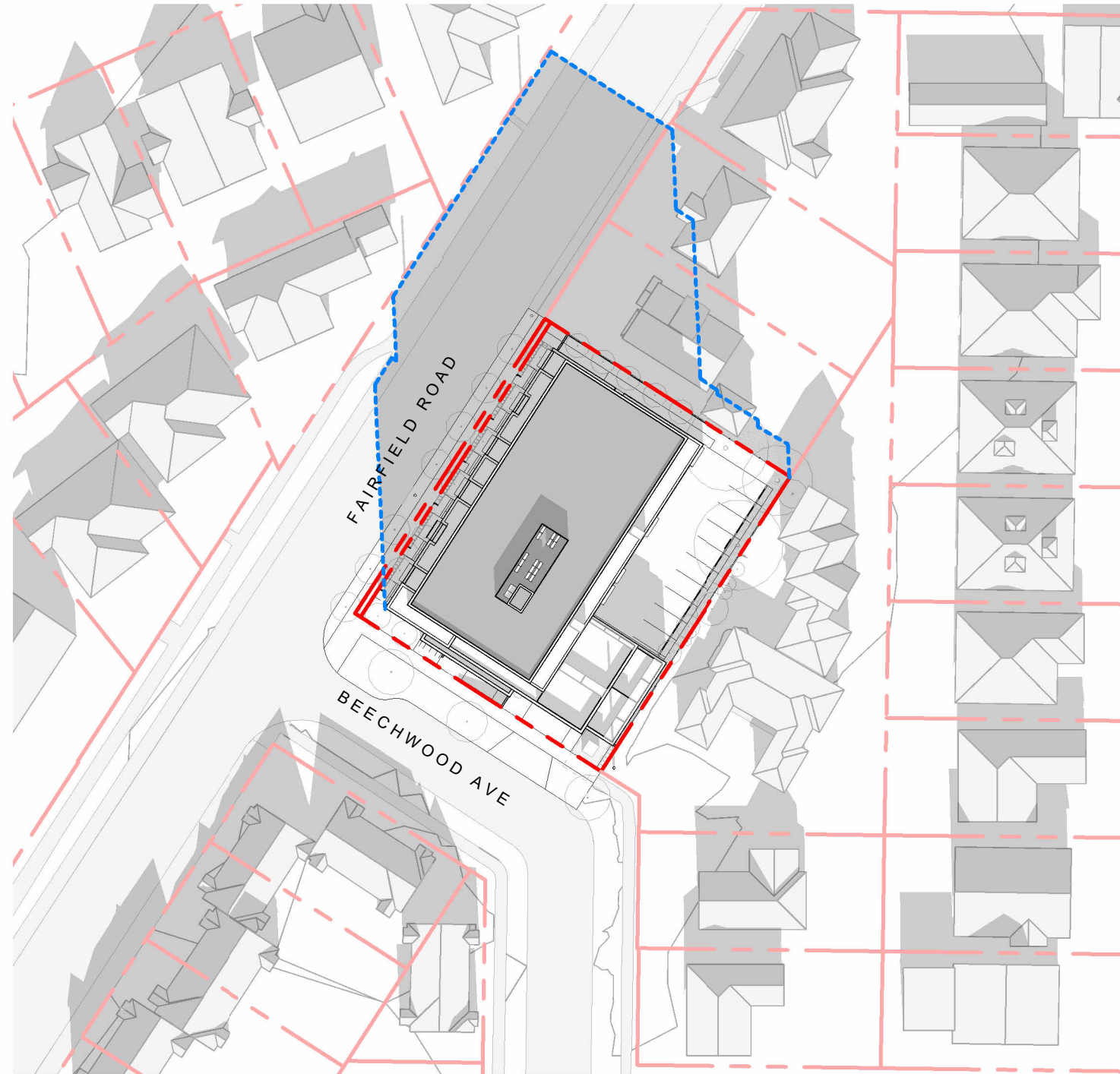
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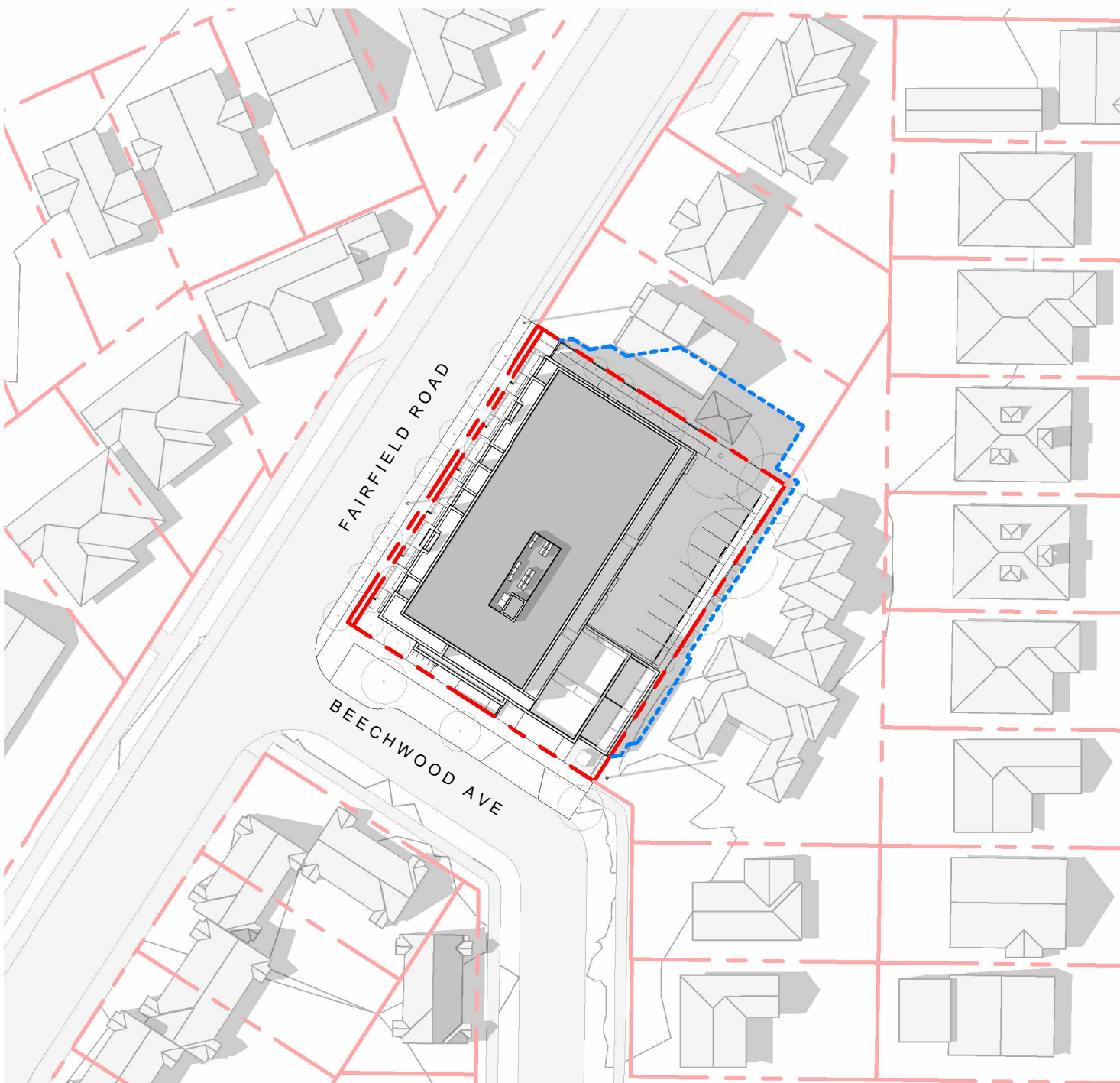
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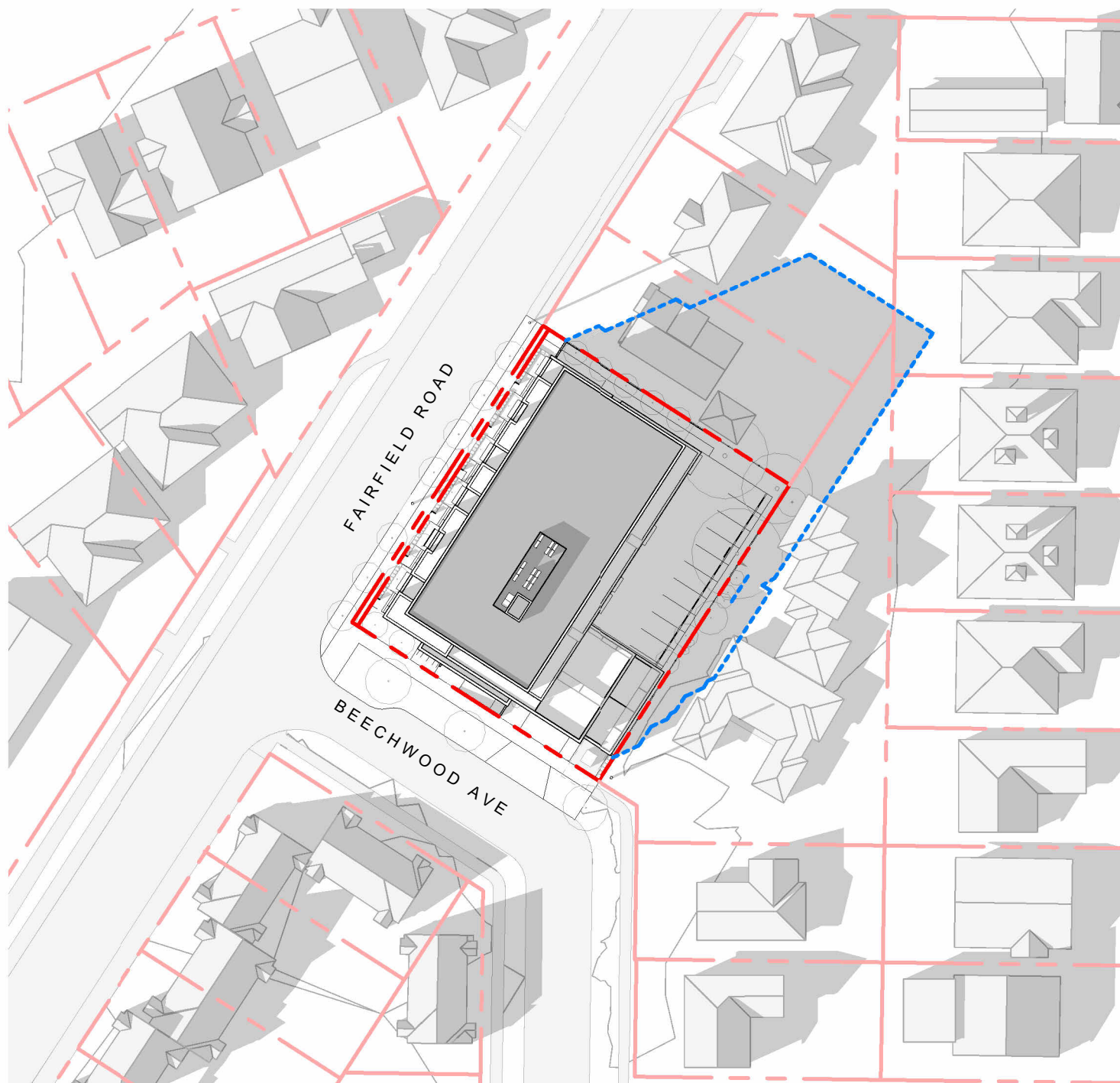
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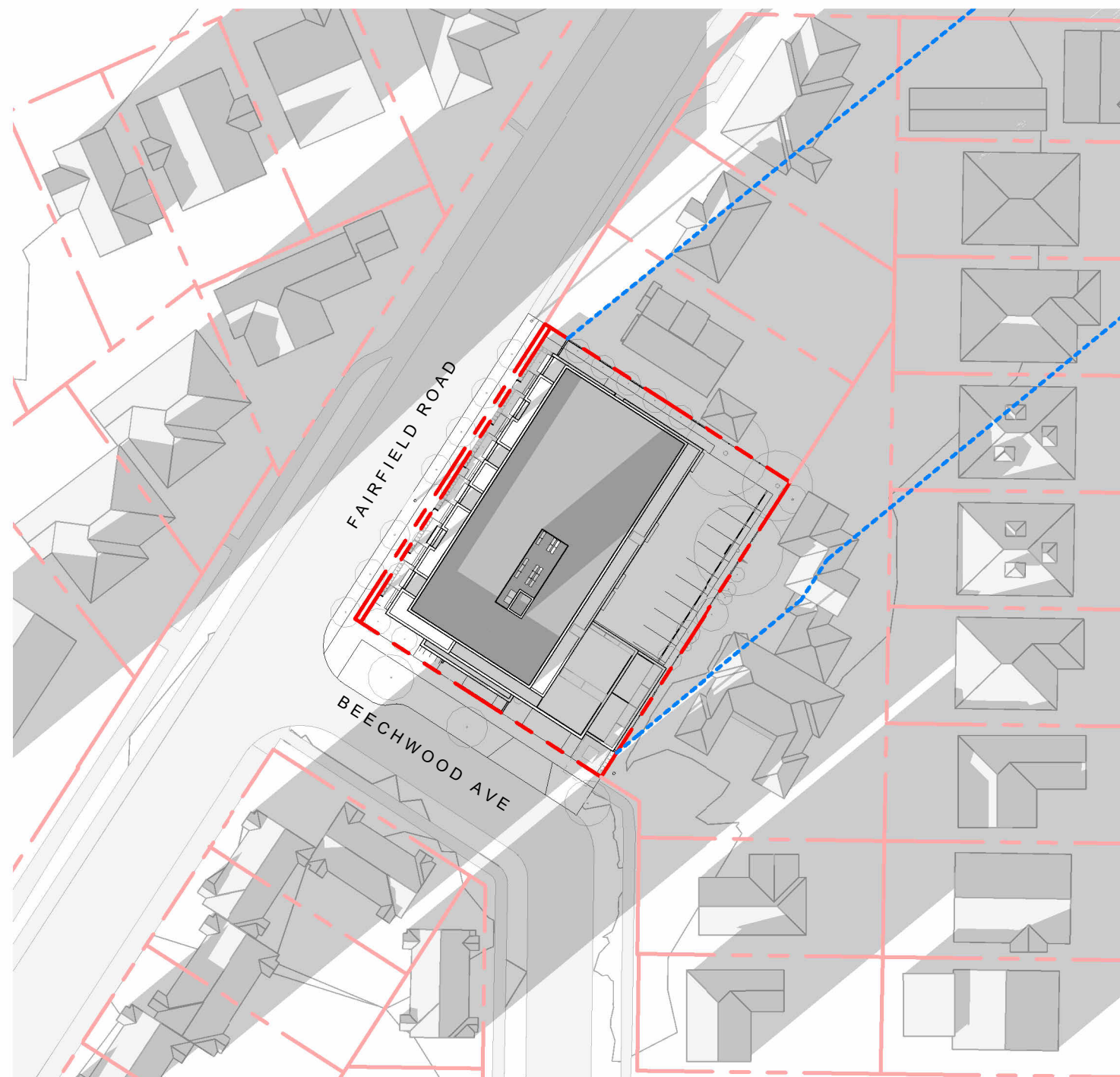
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SUMMER SOLSTICE 4:00 PM



EQUINOX 4:00 PM



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2	DPIRZ Submission	April 29, 2022
1	Development Tracker Application	March 7, 2022
NO.	DESCRIPTION	DATE



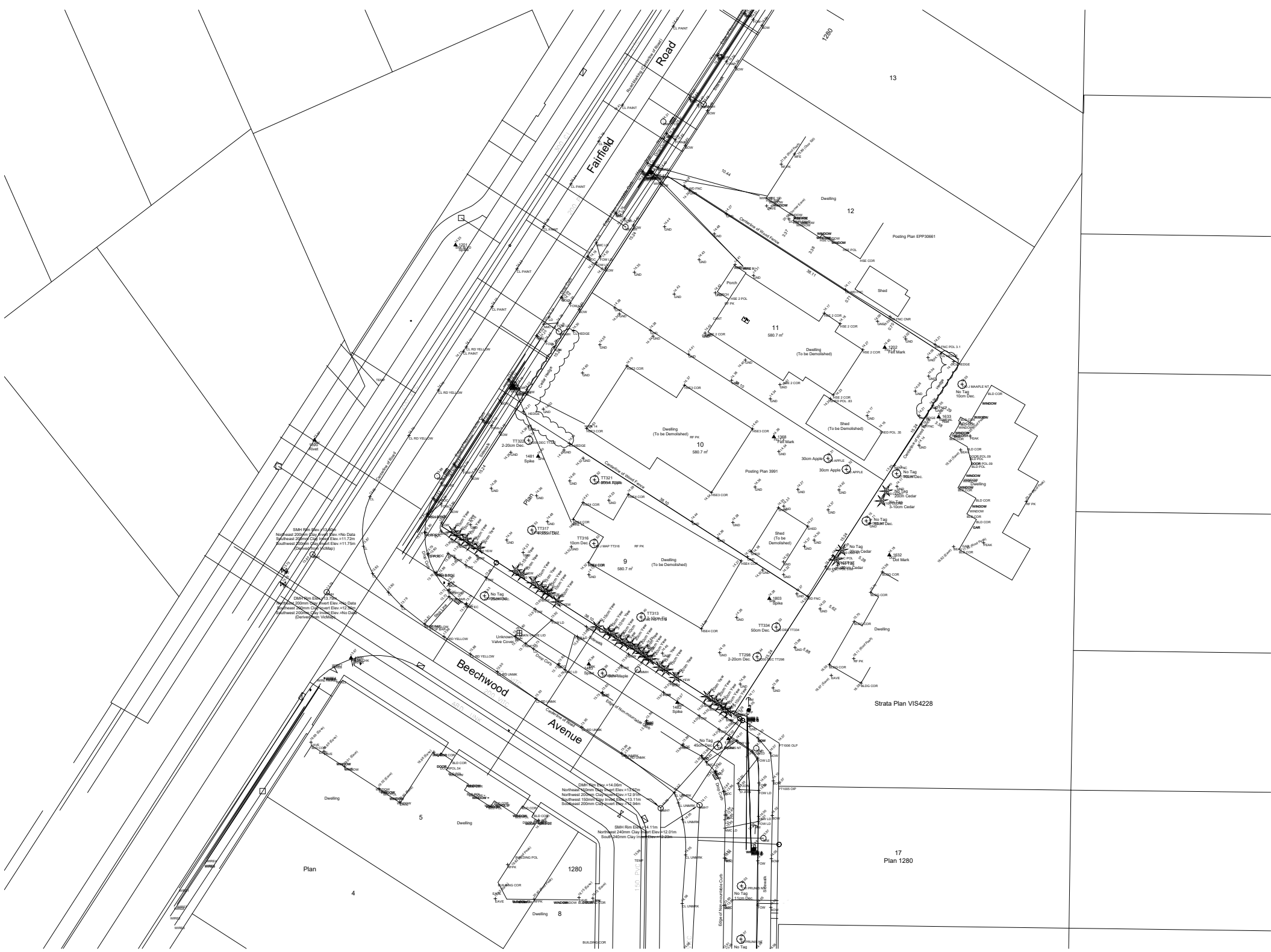
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Project	1733-1737 Fairfield Rd	
	ARYZE	
Sheet Name	SHADOW STUDIES	
Date	AUGUST 21, 2023	
Scale	1 : 750	Project # 2123
	Revision	4
	Jan 26, 2023	
	Sheet #	A-012



2023-08-21 9:45:12 AM



SCALE = 1 : 500

MUNICIPALITY	CITY OF VICTORIA
MUNICIPAL ADDRESS	1733-37 FAIRFIELD ROAD, VICTORIA BC
LEGAL DESCRIPTION	LOT 9, 10, 11, PLAN 1280, SECTION 68 VICTORIA
PROJECT DESCRIPTION	4 STOREY MULTI UNIT RESIDENTIAL BUILDING

ZONE (EXISTING)	R1-G	
PROPOSED ZONE	UNKNOWN	
SITE AREA (m ²)	1,741.93m ²	
TOTAL FLOOR AREA (m ²)	2985.20m ²	
COMMERCIAL FLOOR AREA (m ²)	0	
FLOOR SPACE RATIO	1.71:1	
SITE COVERAGE (%)	65%	
OPEN SITE SPACE (%)	23%	
HEIGHT (m)	13.39m	
NUMBER OF STOREYS	4	
	REQUIRED	PROVIDED
PARKING STALLS (#) ON SITE	3	23
PARKING STALLS - VISITOR (#) ON SITE	3	2
BICYCLE PARKING (#) SHORT TERM	6	6
BICYCLE PARKING (#) LONG TERM	37	54

	PROPOSED
FRONT YARD (BEECHWOOD AVE)	2.73m
FRONT YARD (TO AWNING/LANDING)	1.70m
INTERNAL SIDE YARD (TO BALCONY)	0.50m
INTERNAL SIDE YARD (TO BUILDING FACE)	4.78m
SIDE YARD (FAIRFIELD RD)	2.33m
REAR YARD	2.62m
COMBINED SIDE YARDS	2.83m



TOTAL NUMBER OF UNITS	30
UNIT TYPE	TOWNHOMES (6), STUDIO (2), 1 BEDS (13), 2 BEDS (7), 3 BEDS (2)
GROUND-ORIENTATED UNITS	6
MINIMUM UNIT FLOOR AREA (m ²)	41m ²
TOTAL RESIDENTIAL FLOOR AREA (m ²)	1975m ²

5	DP/RZ Rev3	Aug 18, 2023
4	DP/RZ Rev2	Jan 26, 2023
3	DP/RZ Rev1	July 20, 2022
2	DP/RZ Submission	April 29, 2022
1	Development Tracker Application	March 7, 2022
NO.	DESCRIPTION	DATE

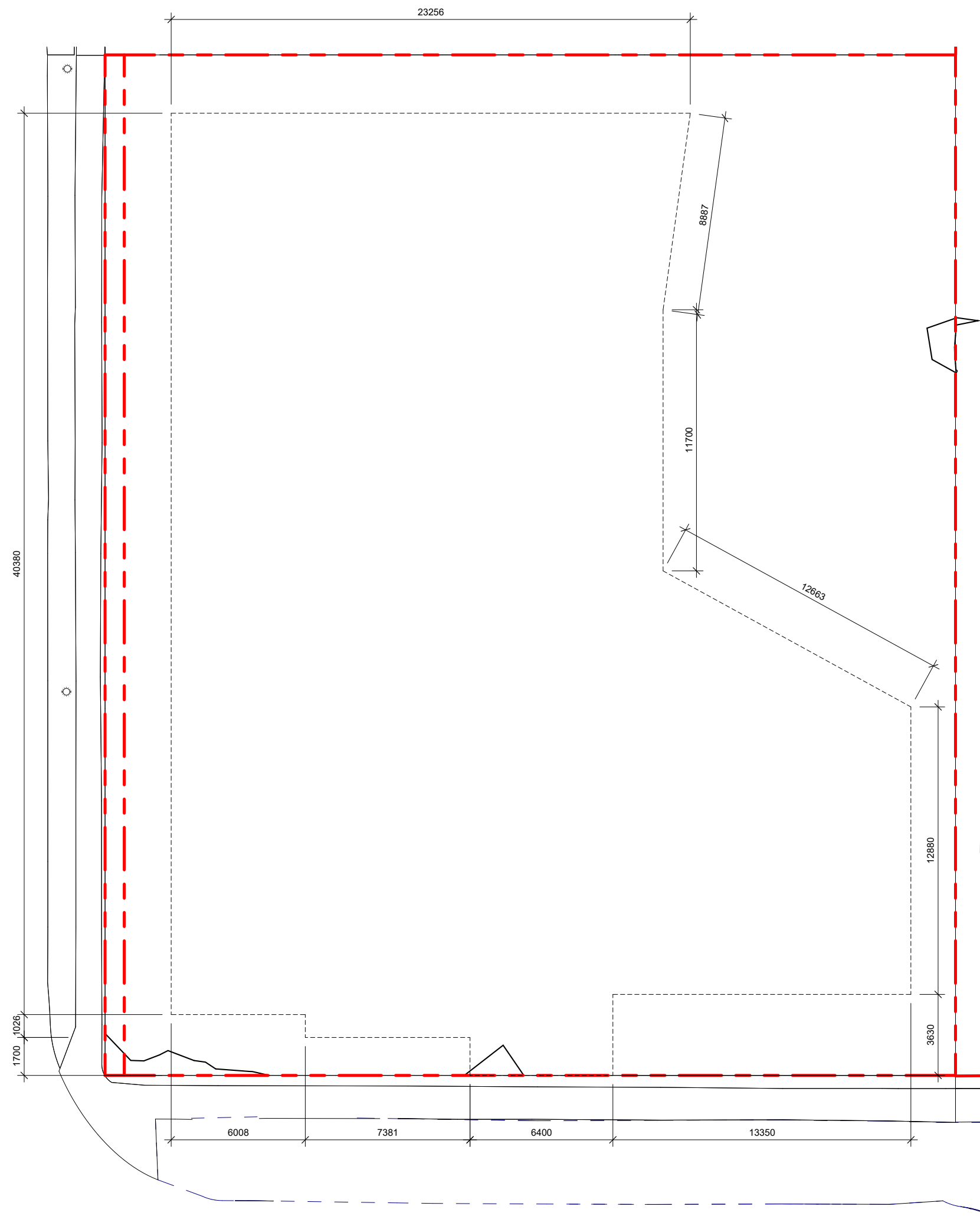


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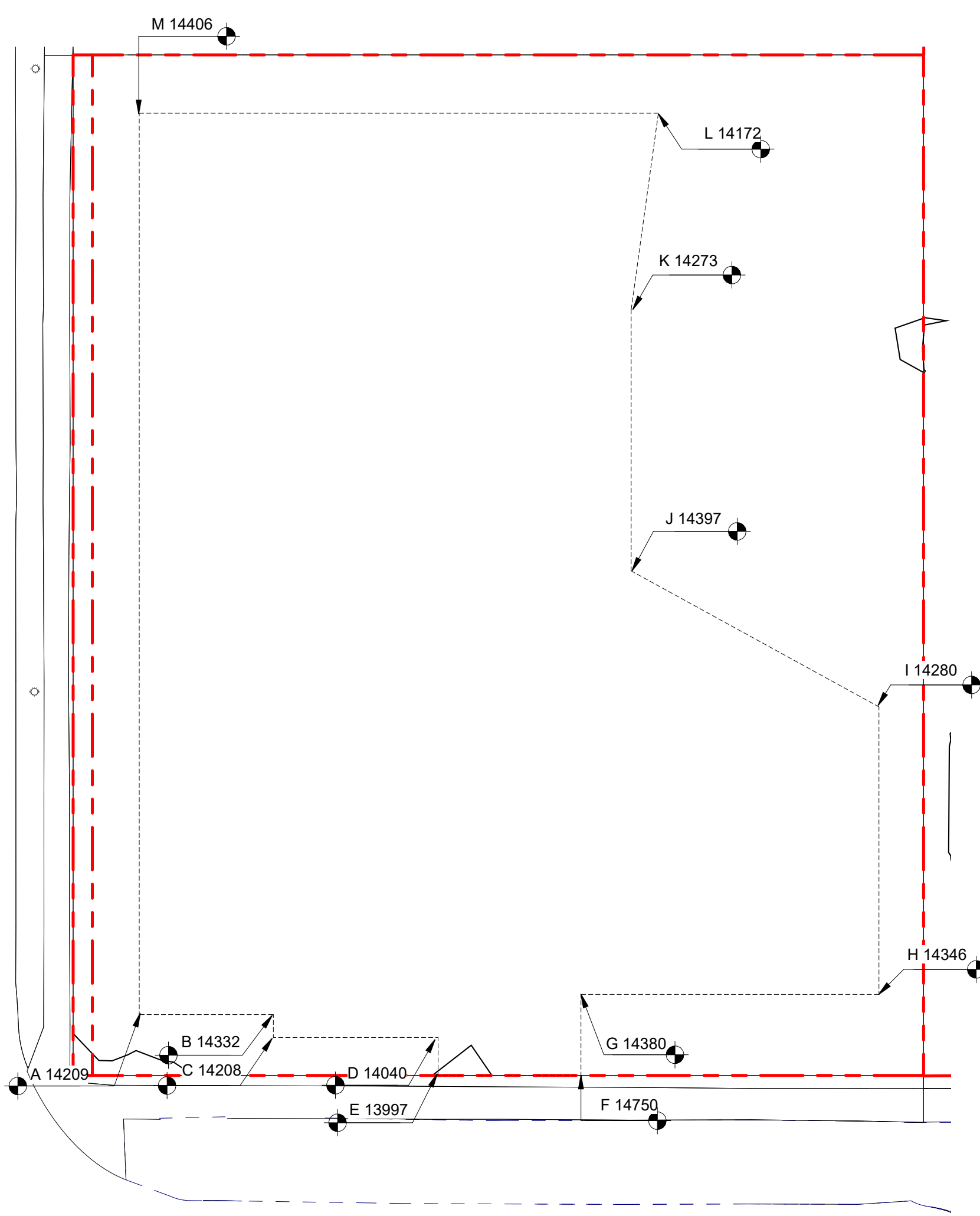
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Project		1733-1737 Fairfield Rd		ARYZE	
Sheet Name					
SITE PLAN & PROJECT DATA					
Date					
AUGUST 21, 2023					
Scale		As indicated		Project #	
				2123	
		Revision			
		Aug 18, 2023		5	
		Sheet #		A-050	

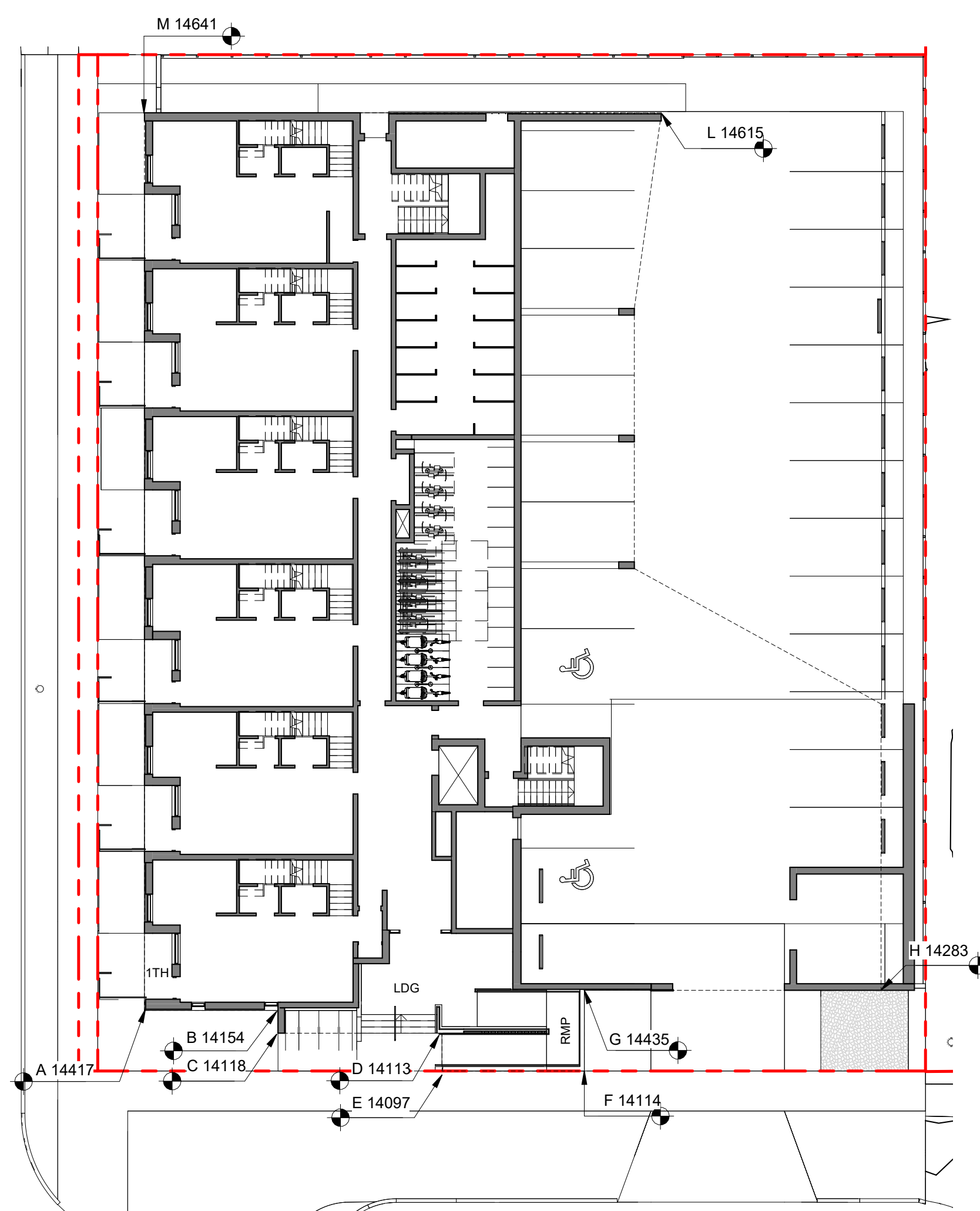
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1 DISTANCE BTWN POINTS KEY PLAN
SCALE = 1 : 200



2 NATURAL GRADE KEY PLAN
SCALE = 1 : 200

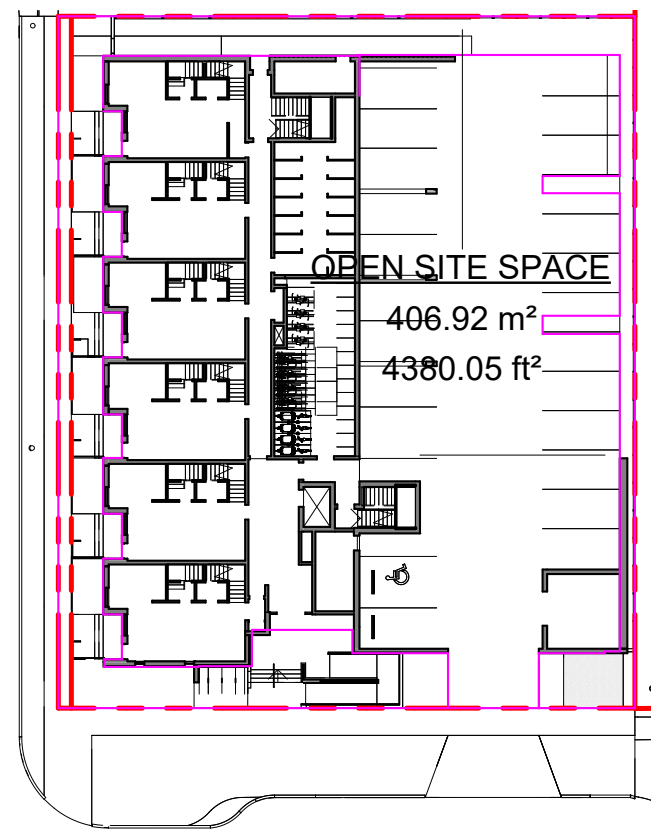


3 PROPOSED GRADE KEY PLAN
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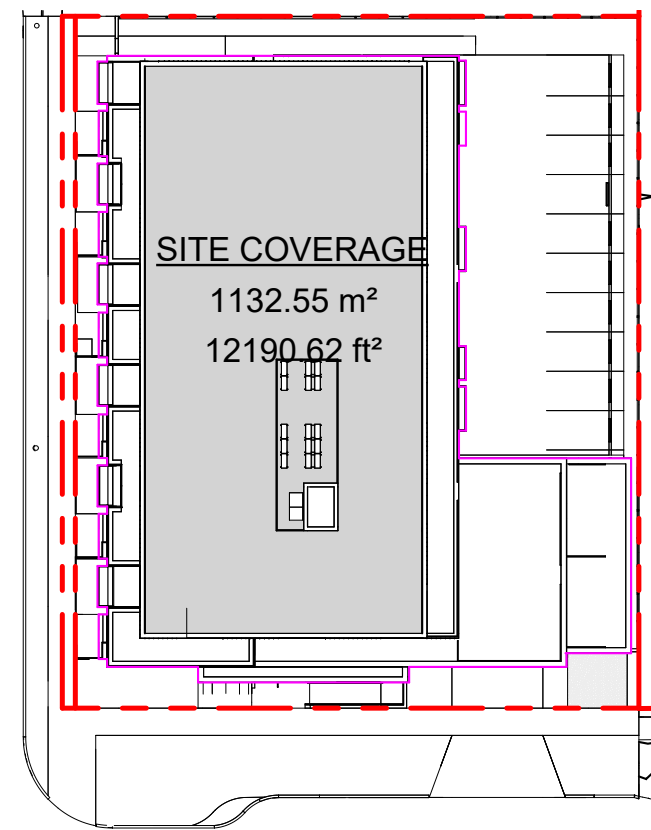
POINT NAME	NATURAL	PROPOSED	DISTANCE BETWEEN POINTS		AVE. OF POINTS
A	14.209	14.42	Point A & B	6.008	85.20
B	14.332	14.15	Point B & C	1.026	14.50
C	14.208	14.12	Point C & D	7.381	103.92
D	14.04	14.11	Point D & E	1.7	23.83
E	13.997	14.10	Point E & F	6.4	89.96
F	14.75	14.11	Point F & G	3.63	51.72
G	14.38	14.44	Point G & H	13.35	191.33
H	14.346	14.28	Point H & I	12.88	183.90
I	14.273	14.41	Point I & J	12.663	181.52
J	14.397	14.69	Point J & K	11.7	167.72
K	14.273	14.64	Point K & L	8.887	126.40
L	14.172	14.62	Point L & M	23.256	332.30
M	14.406	14.64	Point M & N	40.38	577.74

Total	149.26	2130.03
Average Grade	14.27	

*NOTE: Lowest grade point governs per Schedule A definition



4 OPEN SITE SPACE
SCALE = 1 : 500



5 SITE COVERAGE
SCALE = 1 : 500

SUPPORTING CALCULATIONS

ZONING REGULATION BYLAW FLOOR AREAS

Level 1	542.56 m²
Level 2	23.69 m²
Level 3	951.08 m²
Level 4	785.79 m²
Level 5	682.07 m²
Grand total:	2985.20 m²

FLOOR SPACE RATIO

2985.20m² / 1741.93m² = 1.714
FSR - 1.71 : 1

SITE COVERAGE

1132.55m² / 1741.93m² = 0.65 (65%)

OPEN SITE SPACE

406.92m² / 1741.93m² = 0.23 (23%)

HEIGHT OF PATIOS, STAIRS, & RAMP TO EXISTING & PROPOSED GRADE REFER TO GRADE KEY PLANS

1TH PATIO

(WORST CASE GRADING ALONG FAIRFIELD RD - THEREFORE ALL <600mm)

14750 - 14200 (PROPOSED)	= 550mm
14750 - 14171 (EXISTING)	= 579mm

LDG (LANDING AT PRIMARY ENTRANCE)

14750 - 14114 (PROPOSED)	= 636mm
14750 - 14040 (EXISTING)	= 710mm

RMP (INTERMEDIARY RAMP LANDING)

14507 - 14114 (PROPOSED)	= 393mm
14507 - 14280 (EXISTING)	= 227mm

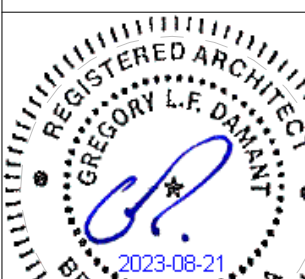
5	DPIRZ Rev3	Aug 18, 2023
4	DPIRZ Rev2	Jan 26, 2023
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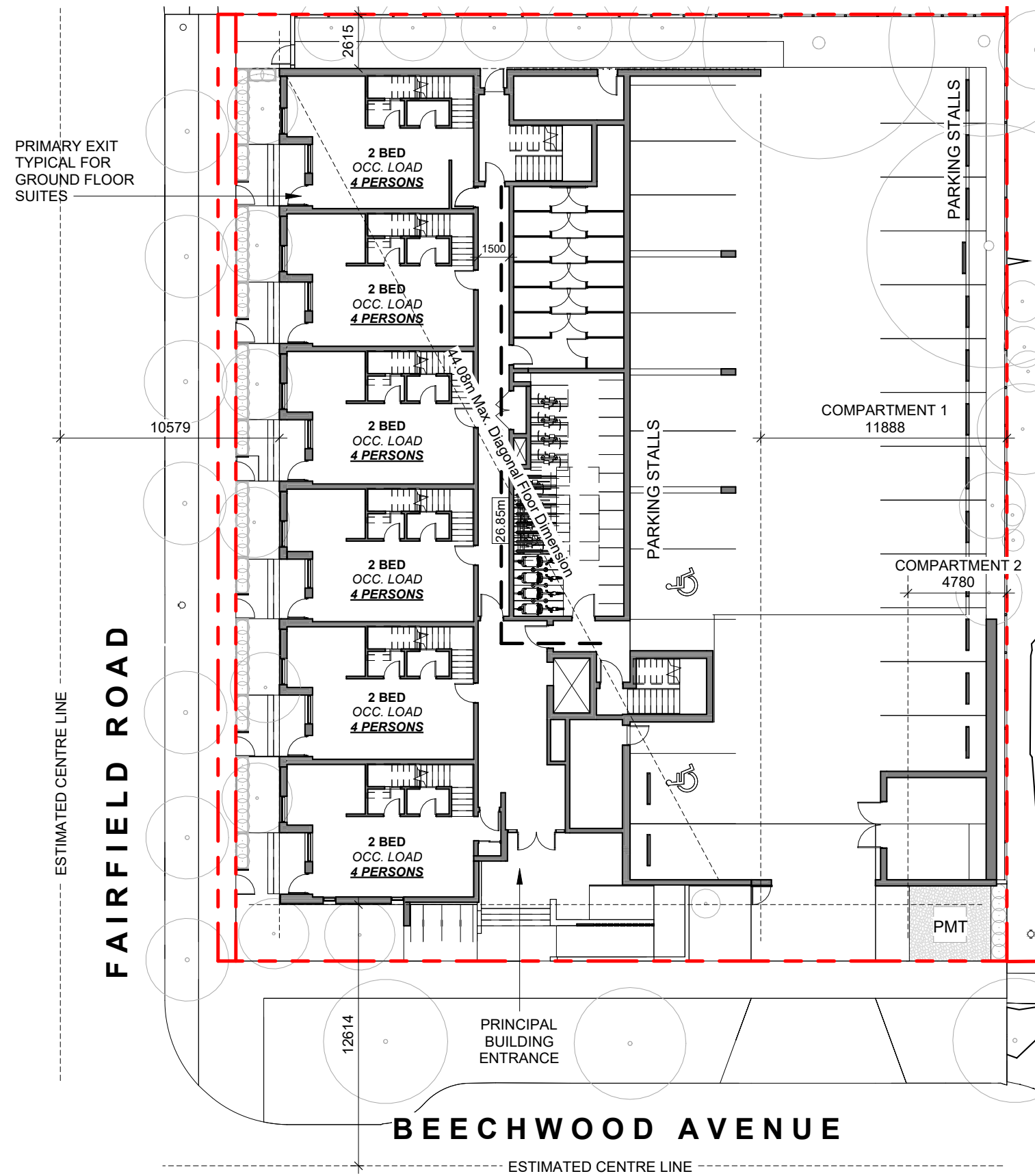
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Project	
1733-1737 Fairfield Rd	
ARYZE	
Sheet Name	
ZONING CALCULATIONS	
Date	Project #
AUGUST 21, 2023	2123
Scale	Revision
As indicated	Aug 18, 2023
Sheet #	
A-051	



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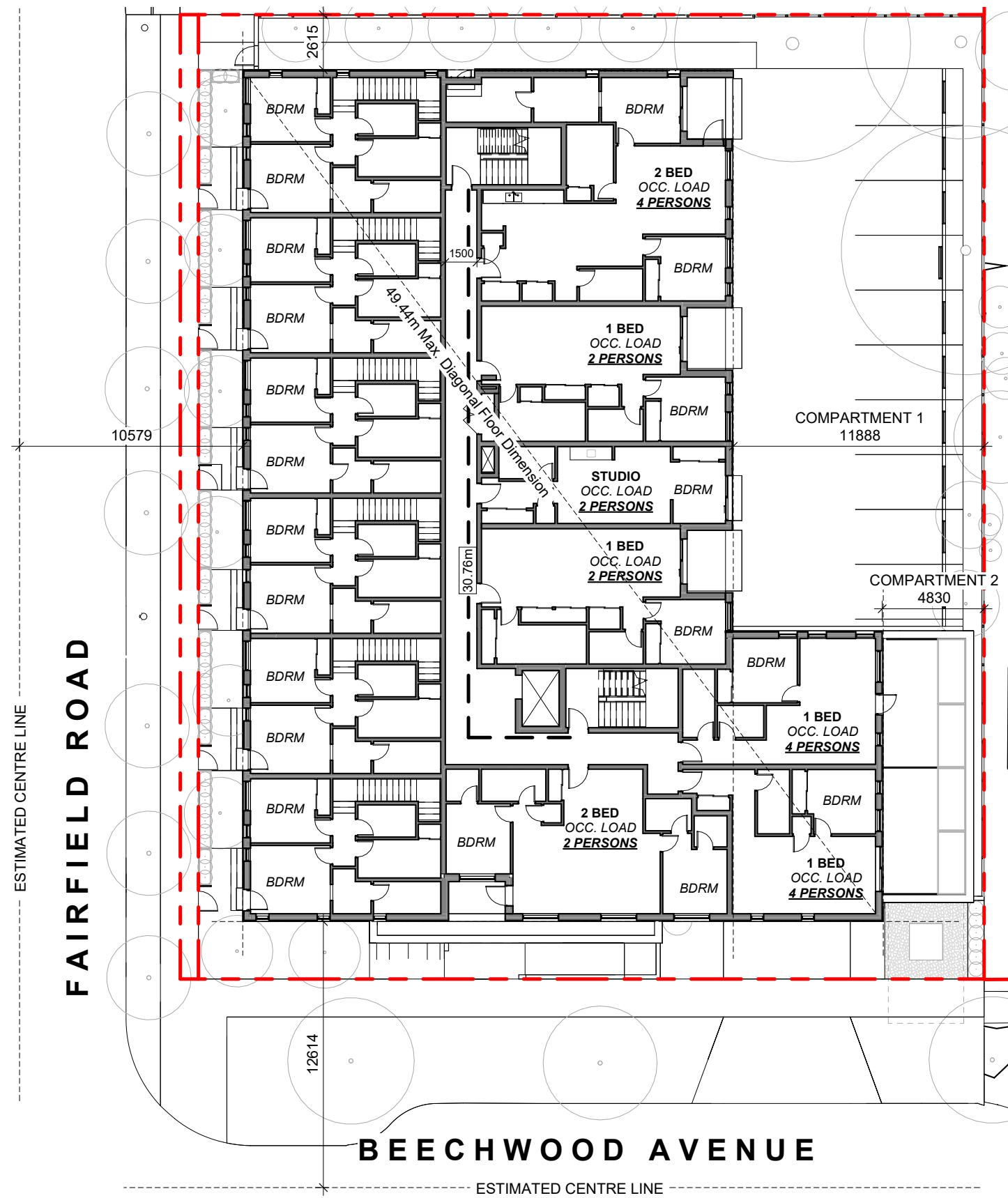


OCCUPANT LOAD:
Occupancy: Group C
12 Bedrooms x 2 persons/bedroom
= 24 persons

MINIMUM EXIT WIDTH:
Ramps, Corridors, Passageways
the greater of 6.1mm x 24 = 146.4mm
or 1100mm (minimum 1500mm for
accessible path of travel)

Stairs
the greater of 8mm x 24 = 192.0mm
or 1100mm

1 L1 CODE REVIEW KEY PLAN
SCALE = 1 : 250



OCCUPANT LOAD:
Occupancy: Group C
9 Bedrooms x 2 persons/bedroom
= 18 persons

MINIMUM EXIT WIDTH:
Ramps, Corridors, Passageways
the greater of 6.1mm x 18 = 109.8mm
or 1100mm (minimum 1500mm for
accessible path of travel)

Stairs
the greater of 8mm x 18 = 144.0mm
or 1100mm

2 L2 CODE REVIEW KEY PLAN
SCALE = 1 : 250

BUILDING CODE ANALYSIS

PROJECT TYPE	<input checked="" type="checkbox"/> NEW CONSTRUCTION	<input type="checkbox"/> RENOVATION	<input type="checkbox"/> ADDITION
GOVERNING BUILDING CODE	2018 BC BUILDING CODE	<input checked="" type="checkbox"/> PART 3	<input type="checkbox"/> PART 9
MAJOR OCCUPANCY	A1 A2 A3 A4 B1 B2 B3 C D E F1 F2 F3	<input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input checked="" type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/>	3.1.2.1
BUILDING AREA	1051	m ² (approx)	1.4.1.2
GRADE	18.75	m geodetic	1.4.1.2
BUILDING HEIGHT (STOREYS)	4	STOREYS ABOVE GRADE	1.4.1.2
	0	STOREYS BELOW GRADE	

BUILDING FIRE SAFETY & CONSTRUCTION CLASSIFICATION

CLASSIFICATION	GROUP C, UP TO 4 STOREYS, SPRINKLERED	3.2.2.51
MAXIMUM BUILDING AREA	1800	m ² 3.2.2.51
No. OF STREETS FACING	1	(BEECHWOOD AVE UNDER 9M MINIMUM STREET WIDTH) 3.2.2.10
CONSTRUCTION TYPES PERMITTED	<input checked="" type="checkbox"/> COMBUSTIBLE <input type="checkbox"/> NON-COMBUSTIBLE	3.2.2.51
FIRE ALARM SYSTEM	YES NO	3.2.4.1
STANDPIPE SYSTEM	YES NO	3.2.5.8
HIGH BUILDING	YES NO	3.2.6.1
INTERCONNECTED FLOOR SPACE	YES NO EXCEPT WITHIN AT GRADE RESIDENTIAL UNITS	3.2.8

FIRE RESISTANCE RATINGS

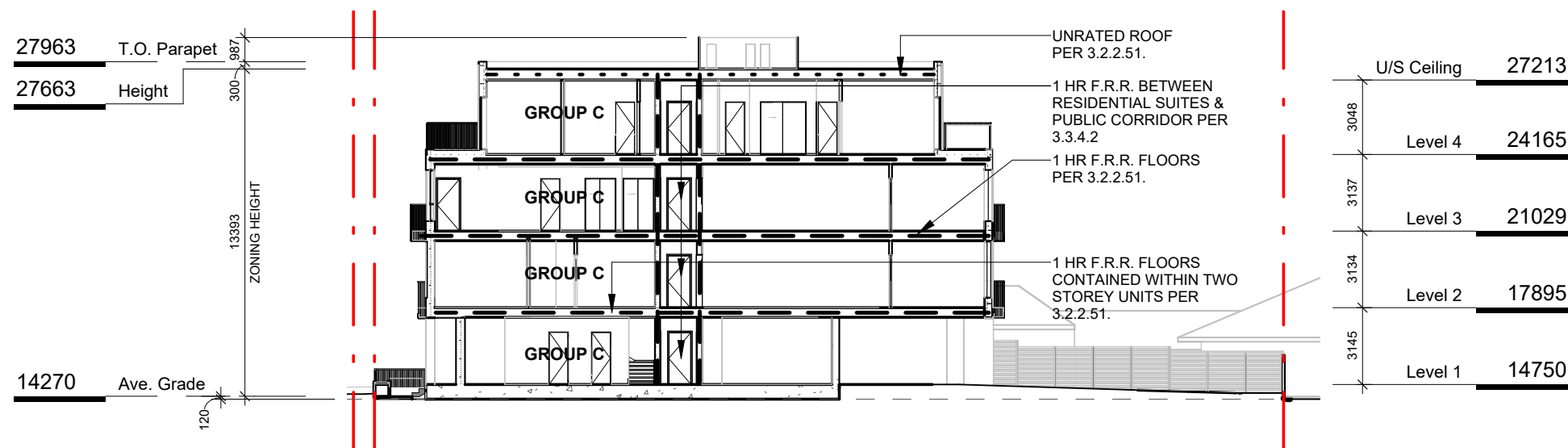
HORIZONTAL SEPARATIONS	1 hr FLOORS 1 hr MEZZANINE Unrated ROOF	3.2.2.51
LOADBEARING WALLS, COLUMNS & ARCHES	1 hr (NOT LESS THAN THAT REQUIRED FOR SUPPORTED STRUCTURE)	3.2.2.51
EXITS	1 hr	3.4.4.1

EXITS FROM FLOOR AREAS

NUMBER OF EXITS REQUIRED	2	3.4.2.1
SEPARATION OF EXITS (MIN.)	1/2 MAX. DIAGONAL FLOOR AREA, BUT NEED NOT BE > 9 m	3.4.2.3
MAX. TRAVEL DISTANCE ALLOWED	45 m, GROUP C	3.4.2.5
MEZZANINE	YES NO	3.2.8

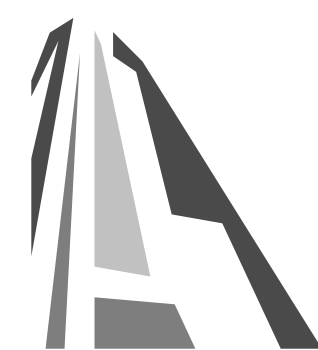
FIRE RESISTANCE RATING - KEY

----	UNRATED FIRE SEPERATIONS
----	45 MIN
----	1 HOUR
----	1.5 HOUR
-----	2 HOUR



5 CODE REVIEW KEY SECTION
SCALE = 1 : 250

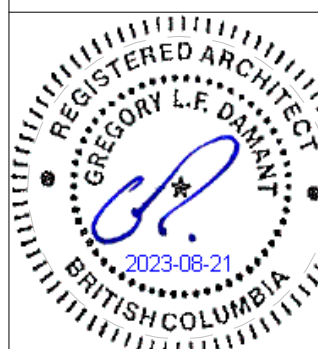
4	DPIRZ Rev2	Jan 26, 2023
2	DPIRZ Submission	April 29, 2022
NO.	DESCRIPTION	DATE



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Project	1733-1737 Fairfield Rd	ARYZE
Sheet Name	CODE REVIEW	
Date	AUGUST 21, 2023	
Scale	As indicated	Project # 2123
Revision	Jan 26, 2023	4
Sheet #	A-052	



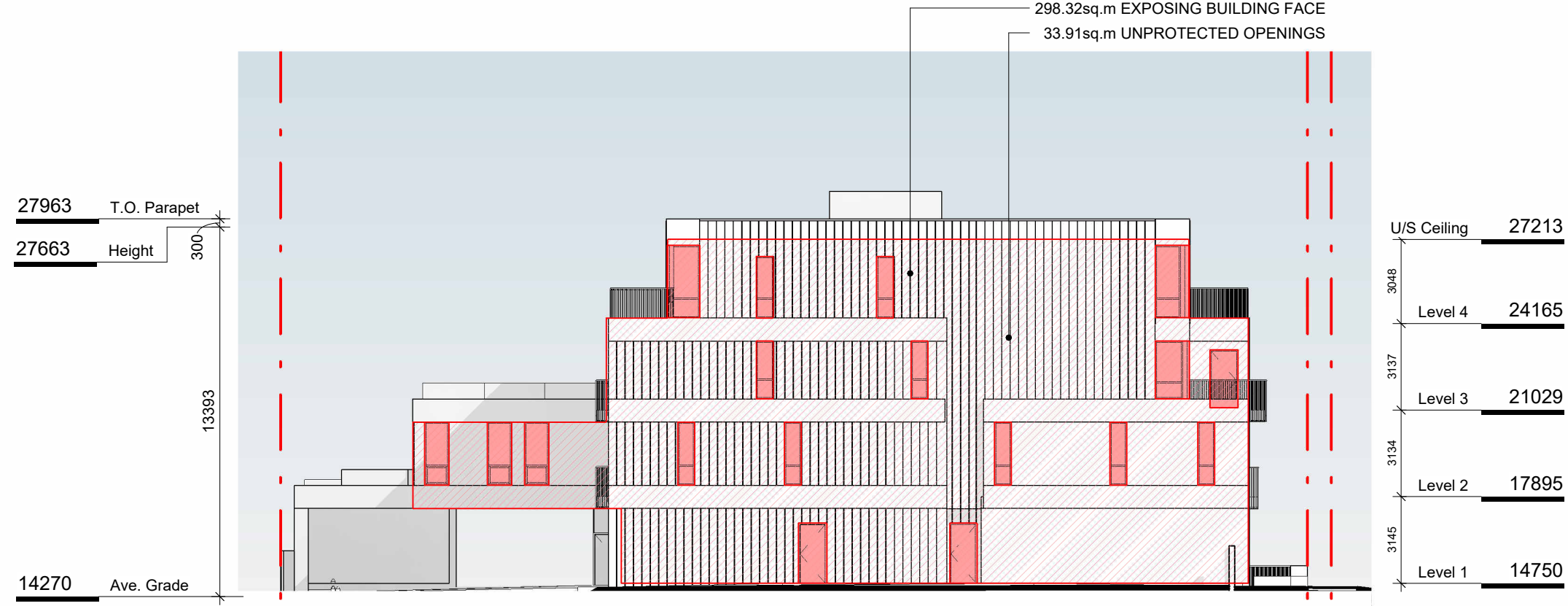
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NW ELEVATION

TABLE 3.2.3.1.-D	LIMITING DISTANCE (m)	ALLOWABLE OPENINGS (%)	AREA OF EXPOSING BUILDING FACE (sq.m)	PROPOSED OPENINGS (sq.m)	PROPOSED OPENINGS (%)
	10.60m (>9m)	100	496.26	213.77	43
TABLE 3.2.3.7	LIMITING DISTANCE (m)	ALLOWABLE OPENINGS (%)	REQUIRED FRR	REQUIRED TYPE OF CONSTRUCTION	REQUIRED TYPE OF CLADDING
	10.12m (>9m)	100	45 minutes	Any	Any

1 NW ELEVATION - LD KEY
SCALE = 1 : 200



NE ELEVATION

TABLE 3.2.3.1.-D	LIMITING DISTANCE (m)	ALLOWABLE OPENINGS (%)	AREA OF EXPOSING BUILDING FACE (sq.m)	PROPOSED OPENINGS (sq.m)	PROPOSED OPENINGS (%)
	2.62m (>2m)	16	298.32	33.91	11.4
TABLE 3.2.3.7	OCCUPANCY CLASSIFICATION	ALLOWABLE OPENINGS (%)	REQUIRED FRR	REQUIRED TYPE OF CONSTRUCTION	REQUIRED TYPE OF CLADDING
	GROUP C	10 - 25	1 hr	Any	Noncombustible

2 NE ELEVATION - LD KEY
SCALE = 1 : 200



SE ELEVATION

TABLE 3.2.3.1.-D COMPARTMENT 1	LIMITING DISTANCE (m)	ALLOWABLE OPENINGS (%)	AREA OF EXPOSING BUILDING FACE (sq.m)	PROPOSED OPENINGS (sq.m)	PROPOSED OPENINGS (%)
	11.89m (>9m)	100	453.14	141.26	31.2
TABLE 3.2.3.7 COMPARTMENT 1	OCCUPANCY CLASSIFICATION	ALLOWABLE OPENINGS (%)	REQUIRED FRR	REQUIRED TYPE OF CONSTRUCTION	REQUIRED TYPE OF CLADDING
	GROUP C	50 - 100	45 minutes	Any	Any
TABLE 3.2.3.1.-D COMPARTMENT 2	LIMITING DISTANCE (m)	ALLOWABLE OPENINGS (%)	AREA OF EXPOSING BUILDING FACE (sq.m)	PROPOSED OPENINGS (sq.m)	PROPOSED OPENINGS (%)
	4.78m (>4m)	56	43.20	17.70	41.0
TABLE 3.2.3.7 COMPARTMENT 2	OCCUPANCY CLASSIFICATION	ALLOWABLE OPENINGS (%)	REQUIRED FRR	REQUIRED TYPE OF CONSTRUCTION	REQUIRED TYPE OF CLADDING
	GROUP C	50 - 100	45 minutes	Any	Any

4 SE ELEVATION - LD KEY
SCALE = 1 : 200



SW ELEVATION

TABLE 3.2.3.1.-D	LIMITING DISTANCE (m)	ALLOWABLE OPENINGS (%)	AREA OF EXPOSING BUILDING FACE (sq.m)	PROPOSED OPENINGS (sq.m)	PROPOSED OPENINGS (%)
	12.61m (>9m)	100	315.54	84.24	26.7
TABLE 3.2.3.7	OCCUPANCY CLASSIFICATION	ALLOWABLE OPENINGS (%)	REQUIRED FRR	REQUIRED TYPE OF CONSTRUCTION	REQUIRED TYPE OF CLADDING
	GROUP C	100	45 minutes	Any	Any

3 SW ELEVATION - LD KEY
SCALE = 1 : 200

5	DPIRZ Rev3	Aug 18, 2023
4	DPIRZ Rev2	Jan 26, 2023
3	DPIRZ Rev1	July 20, 2022
2	DPIRZ Submission	April 29, 2022
NO.	DESCRIPTION	DATE



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Project		1733-1737 Fairfield Rd	
		ARYZE	
Sheet Name			
SPATIAL SEPARATION			
Date			
AUGUST 21, 2023			
Scale		Project #	
1 : 200		2123	
Revision		5	
Aug 18, 2023			
Sheet #		A-053	





5	DPIRZ Rev3	Aug 18, 2023
4	DPIRZ Rev2	Jan 26, 2023
3	DPIRZ Rev1	July 20, 2022
2	DPIRZ Submission	April 29, 2022
1	Development Tracker Application	March 7, 2022
NO.	DESCRIPTION	DATE



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Project		1733-1737 Fairfield Rd	
		ARYZE	
Sheet Name			
LEVEL 1 PROPOSED PLAN			
Date			
AUGUST 21, 2023			
Scale		Project #	
1 : 100			2123
		Revision	
		Aug 18, 2023	
		Sheet #	
		A-101	






5	DPIRZ Rev3	Aug 18, 2023
4	DPIRZ Rev2	Jan 26, 2023
3	DPIRZ Rev1	July 20, 2022
2	DPIRZ Submission	April 29, 2022
1	Development Tracker Application	March 7, 2022
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Project		1733-1737 Fairfield Rd	
		ARYZE	
Sheet Name			
LEVEL 2 PROPOSED PLAN			
Date			
AUGUST 21, 2023			
Scale		Project #	
1 : 100			2123
		Revision	
		Aug 18, 2023	
		Sheet #	
		A-102	



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4	DPIRZ Rev2	Jan 26, 2023
3	DPIRZ Rev1	July 20, 2022
2	DPIRZ Submission	April 29, 2022
1	Development Tracker Application	March 7, 2022
NO.	DESCRIPTION	DATE



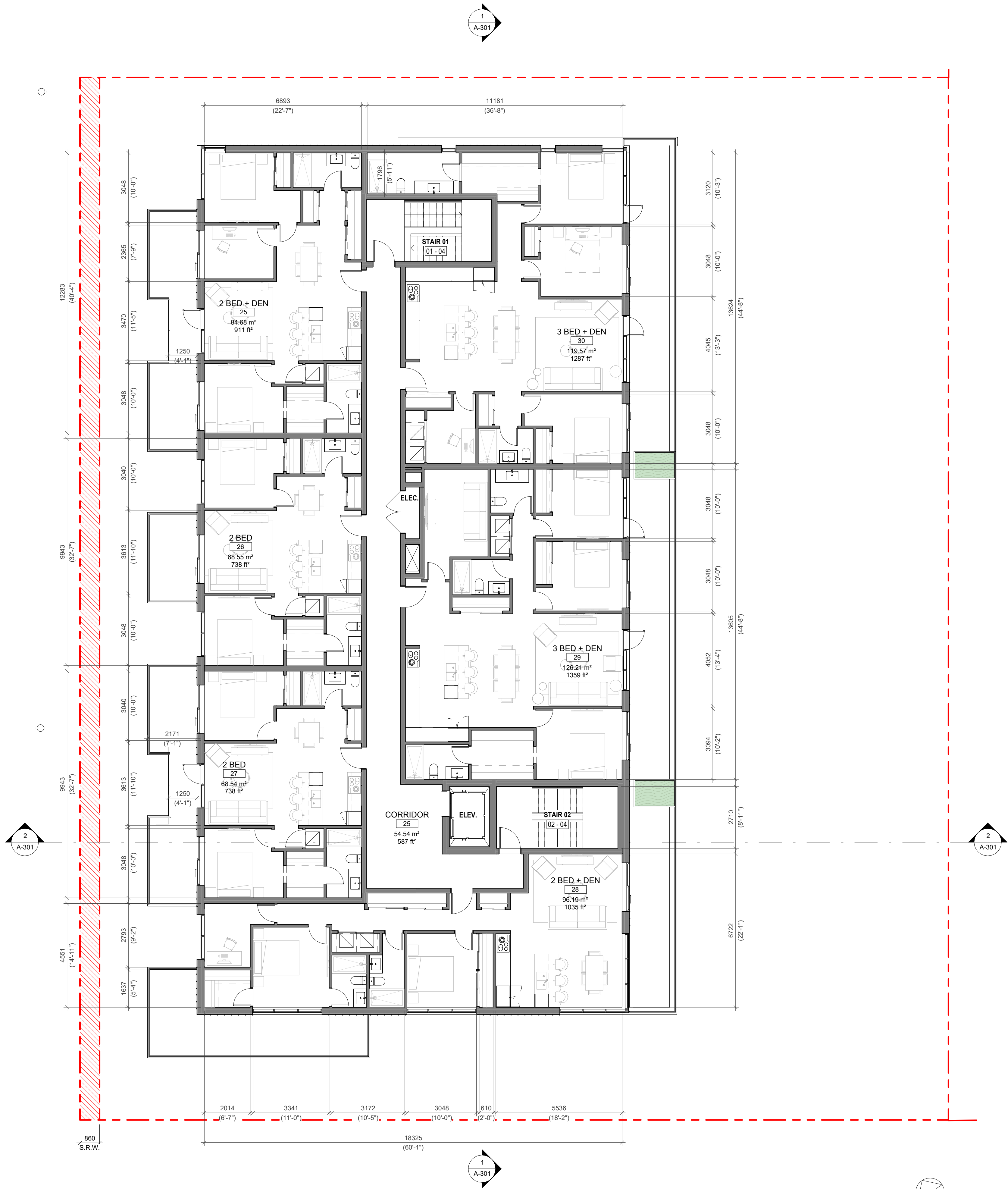
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Project	1733-1737 Fairfield Rd	
	ARYZE	
Sheet Name	LEVEL 3 PROPOSED PLAN	
Date	AUGUST 21, 2023	
Scale	1 : 100	Project # 2123
	Revision	4
	Sheet #	A-103

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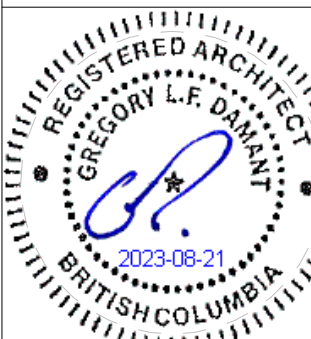
4	DPIRZ Rev2	Jan 26, 2023
3	DPIRZ Rev1	July 20, 2022
2	DPIRZ Submission	April 29, 2022
1	Development Tracker Application	March 7, 2022
NO.	DESCRIPTION	DATE



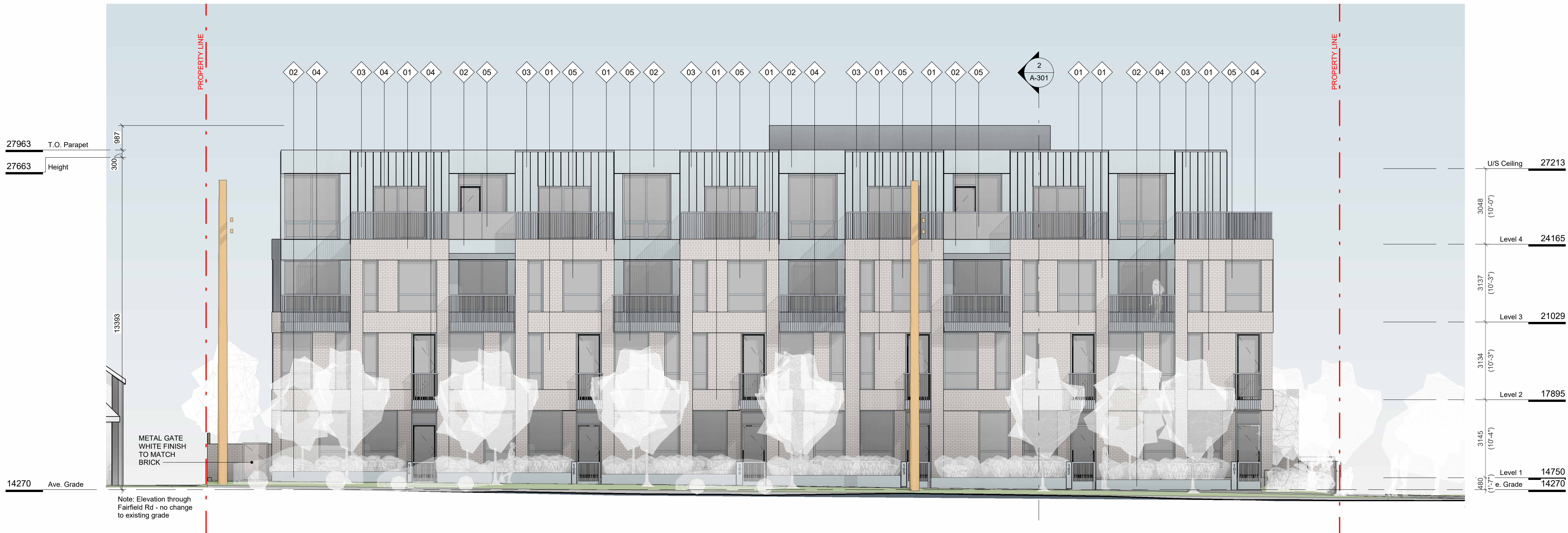
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Project		1733-1737 Fairfield Rd	
		ARYZE	
Sheet Name			
LEVEL 4 PROPOSED PLAN			
Date			
AUGUST 21, 2023			
Scale		Project #	
1 : 100			2123
		Revision	
		Jan 26, 2023	
		Sheet #	
		A-104	



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1 NW ELEVATION
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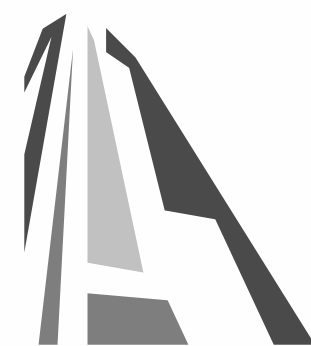


2 SW ELEVATION
SCALE = 1 : 100

FINISH SCHEDULE

01. BRICK
LIGHT FINISH
02. PREFINISHED METAL
FLAT PANEL
03. PREFINISHED METAL
STANDING SEAM
04. METAL PICKET GUARD
05. GLAZING
06. WOOD GRAIN FINISH
07. HORIZONTAL WOOD FENCE

5	DPIRZ Rev3	Aug 18, 2023
4	DPIRZ Rev2	Jan 26, 2023
3	DPIRZ Rev1	July 20, 2022
2	DPIRZ Submission	April 29, 2022
1	Development Tracker Application	March 7, 2022
NO.	DESCRIPTION	DATE



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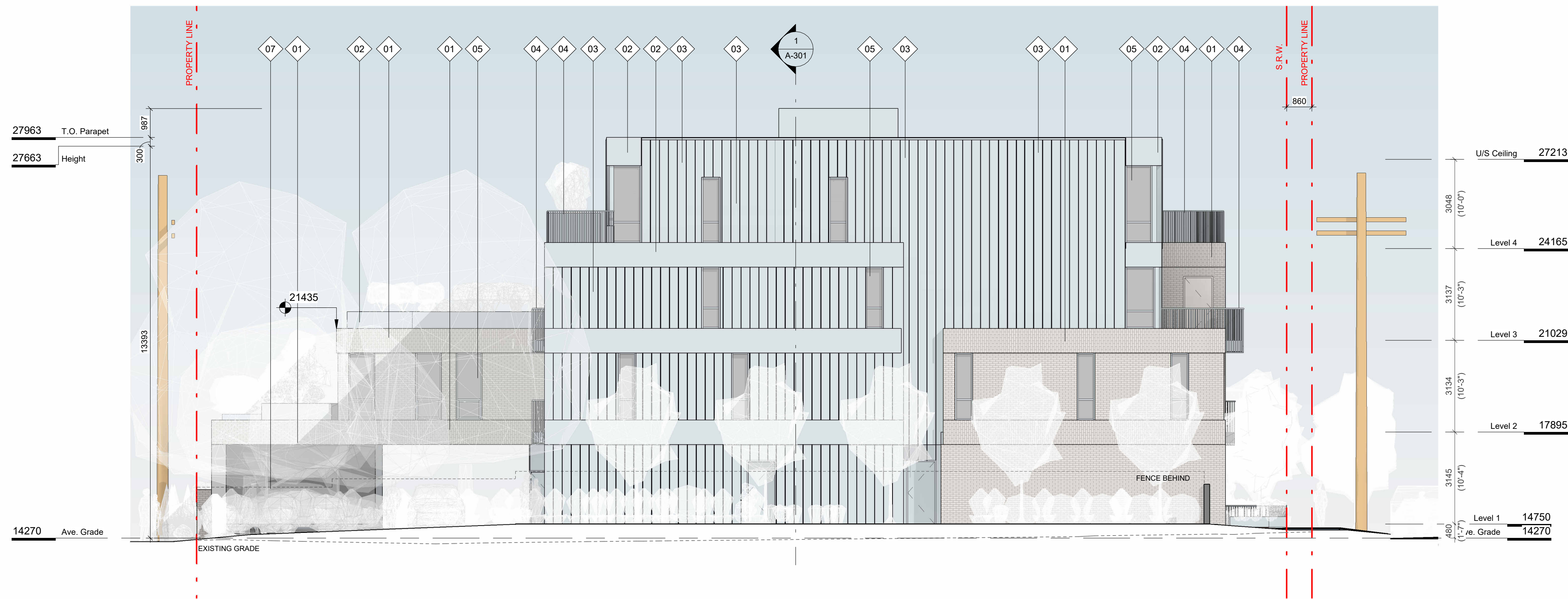
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Project 1733-1737 Fairfield Rd ARYZE		
Sheet Name EXTERIOR ELEVATIONS		
Date AUGUST 21, 2023		
Scale As indicated	Project # 2123	Revision Aug 18, 2023 5
Registered Architect GREGORY L.F. DANN 2023-08-21 BRITISH COLUMBIA		Sheet # A-200

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1 SE ELEVATION
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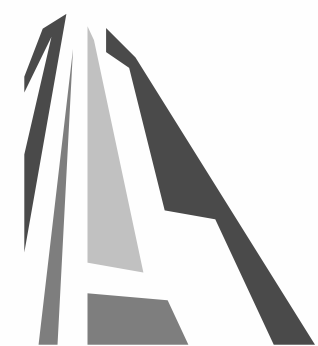


2 NE ELEVATION
SCALE = 1 : 100

FINISH SCHEDULE

01. BRICK
LIGHT FINISH
02. PREFINISHED METAL
FLAT PANEL
03. PREFINISHED METAL
STANDING SEAM
04. METAL PICKET GUARD
05. GLAZING
06. WOOD GRAIN FINISH
07. HORIZONTAL WOOD FENCE

5	DPIRZ Rev3	Aug 18, 2023
4	DPIRZ Rev2	Jan 26, 2023
3	DPIRZ Rev1	July 20, 2022
2	DPIRZ Submission	April 29, 2022
1	Development Tracker Application	March 7, 2022
NO.	DESCRIPTION	DATE

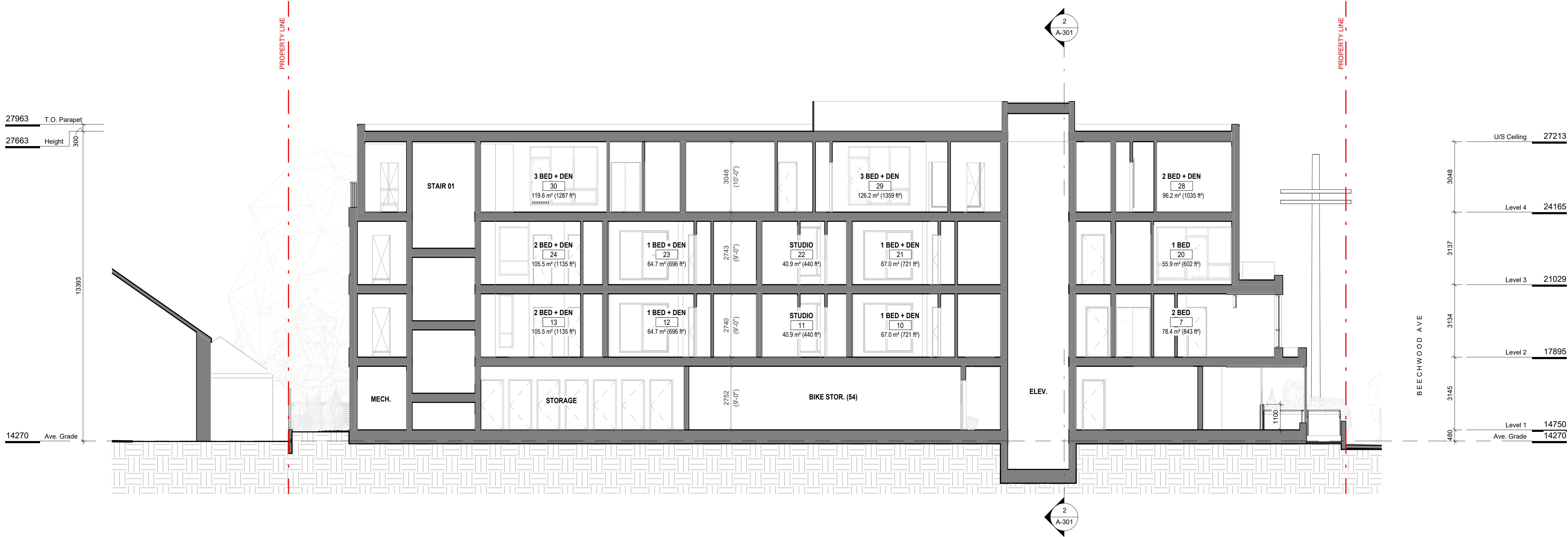


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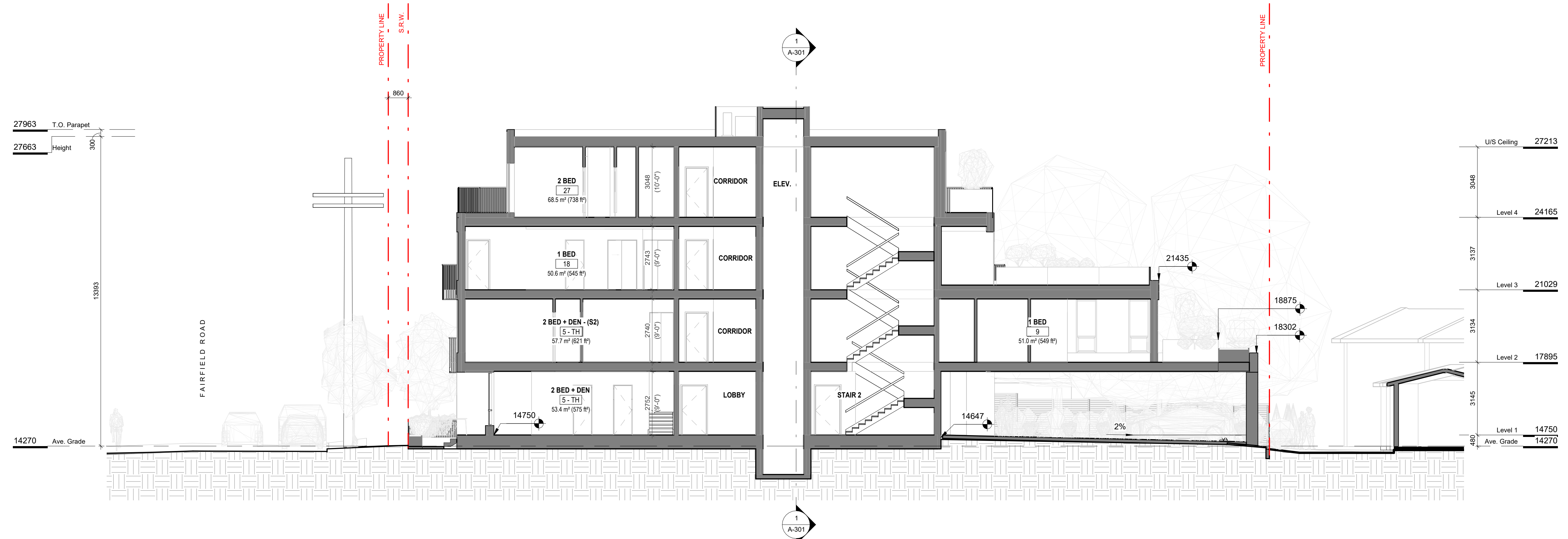
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Project 1733-1737 Fairfield Rd ARYZE		
Sheet Name EXTERIOR ELEVATIONS		
Date	AUGUST 21, 2023	
Scale	As indicated	Project # 2123
	Revision	Aug 18, 2023 5
	Sheet #	A-201

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1 BUILDING SECTION 1
SCALE = 1 : 100



2 BUILDING SECTION 2
SCALE = 1 : 100

4	DPIRZ Rev2	Jan 26, 2023
3	DPIRZ Rev1	July 20, 2022
2	DPIRZ Submission	April 29, 2022
1	Development Tracker Application	March 7, 2022
NO.	DESCRIPTION	DATE



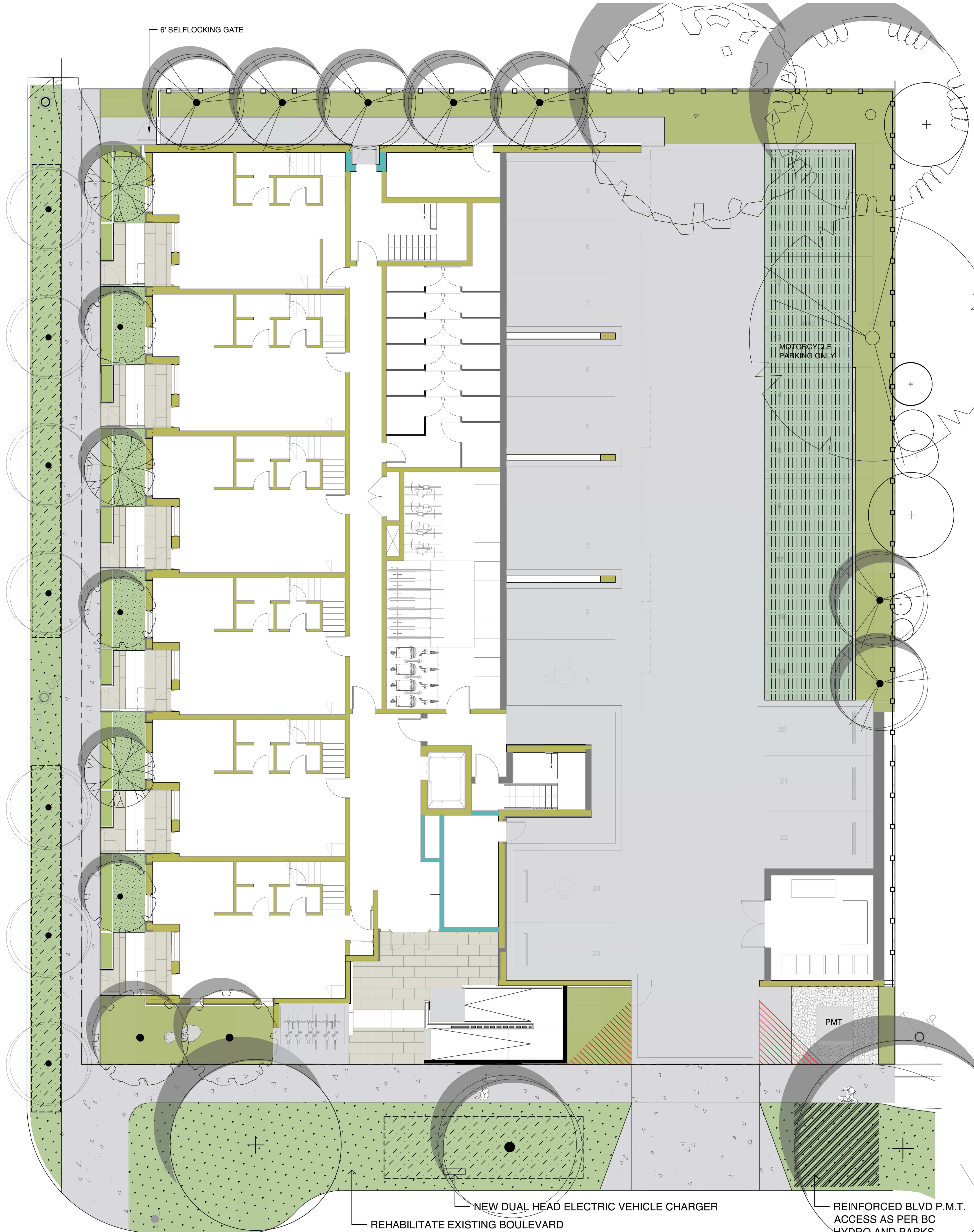
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Project 1733-1737 Fairfield Rd ARYZE		
Sheet Name BUILDING SECTIONS		
Date	AUGUST 21, 2023	
Scale	1 : 100	Project # 2123
	Revision	4
	Jan 26, 2023	
Sheet # A-301		

FAIRFIELD ROAD

BEECHWOOD AVENUE

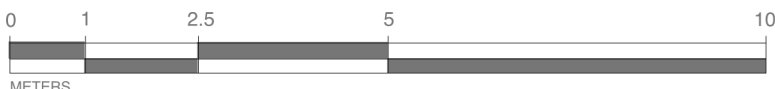


LEGEND

- PROPERTY LINE
ROOF OVERHANG
- ON-SITE LANDSCAPE AREA
- 1000 mm DEPTH PLANTING AREAS GROWING MEDIUM (MMCD)
 - 50 mm DEPTH MULCH
- RAISED PLANTER
- 450 mm DEPTH PLANTER GROWING MEDIUM
 - 50 mm DEPTH MULCH
- PARKING STALL PAVER
- NEWSTONE DORADO DRAIN PAVERS
 - COLOR: CHARCOAL
- GROUND FLOOR PATIO AND UPPER LEVEL BALCONY PAVER
- ARISTOKRAT™ SERIES
 - COLOR: CASCADE GRANITE
 - SIZE: 24" X 48"
- SOD MUNICIPAL BOULEVARD AREA
- 150 mm DEPTH HIGH TRAFFIC LAWN GROWING MEDIUM (MMCD)
 - SOD
- SOD MUNICIPAL BOULEVARD AREA
- 1000 mm DEPTH LOW TRAFFIC LAWN GROWING MEDIUM (MMCD)
 - SOD
- SOD ON SITE
- 1000 mm DEPTH PLANTING AREAS GROWING MEDIUM (MMCD) FOR LAWN AREA WITH TREES + SOD
 - 150 mm DEPTH HIGH TRAFFIC LAWN GROWING MEDIUM (MMCD) FOR AREA WITHOUT TREES + SOD
- MUNICIPAL SIDEWALK
- CIP CONCRETE WITH BROOM FINISH TO CITY OF VICTORIA MUNICIPAL STANDARDS
- CONCRETE PATH AND DRIVEWAY
- TREE PIT WITH 1000mm DEPTH GROWING MEDIUM FOR BOULEVARD TREES
- BIKE RACK
- SURFACE MOUNT ON CONCRETE PAD
- LANDSCAPE BOULDER
- VANCOUVER ISLAND GRANITE OR BASALT
- FENCE
- 6' HT SOLID CEDAR WOOD FENCE
 - STAIN: HEMLOCK GREY
- EXISTING BOULEVARD TREE TO BE RETAINED
- PROPOSED BOULEVARD TREE TO BE SPECIFIED BY CITY OF VICTORIA PARKS
- PROPOSED TREES

SITE PERMEABILITY

TOTAL SITE AREA 1703 m²
LANDSCAPED AREA 223 m²
PERMEABLE PAVED AREA 484 m²
SITE PERMEABILITY RATIO: 41.6%



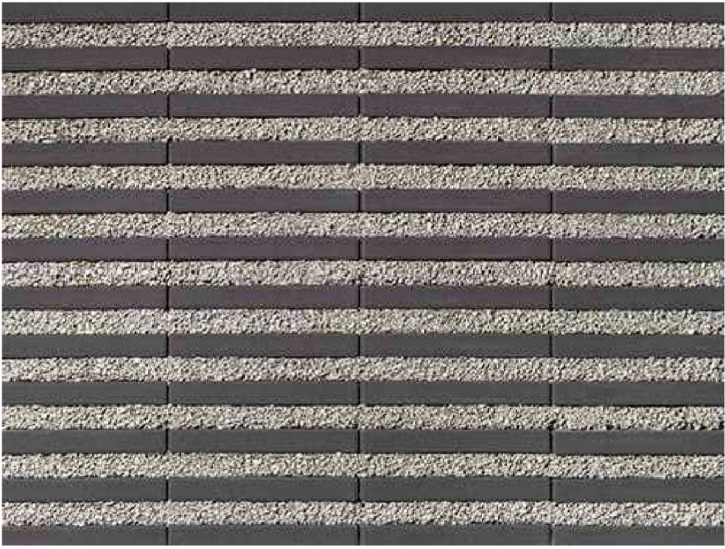
SITE PAVER SELECTION

FRONT PATIOS ON FAIRFIELD



ARISTOKRAT™ SERIES
CASCADE GRANITE

PARKING STALLS



NEWSTONE DORADO DRAIN
CHARCOAL

NOTES:

- BOULEVARD TREE PLANTING TO CITY OF VICTORIA STANDARDS, AS PER THE VICTORIA SUBDIVISION AND DEVELOPMENT SERVICES BYLAW NO 12-043, AND CITY OF VICTORIA SUPPLEMENTARY STANDARD DETAIL DRAWING S- P4
- REHABILITATE EXISTING BOULEVARD TO CITY OF VICTORIA STANDARDS, AS PER THE VICTORIA SUBDIVISION AND DEVELOPMENT SERVICES BYLAW NO 12-042 AND SUPPLEMENTARY STANDARD DETAIL DRAWING SD-P3
- SEPARATE IRRIGATION SERVICE AND AUTOMATIC IRRIGATION TO BE PROVIDED TO MUNICIPAL BOULEVARD TREES IF TREE LOCATIONS ARE APPROVED BY CITY ENGINEERING AND BC HYDRO

PLEASE NOTE: THERE IS INSUFFICIENT SETBACK FROM PROPOSED UTILITIES. IF WE USE SMALL CALIPER TREES (20mm) WITH A ROOT BALL OF 400 mm DIAMETER MAXIMUM, WE BELIEVE WE CAN ACCOMMODATE THE REQUIRED 1.0m OFFSET FROM THE ROOT BALL TO THE BC HYDRO UTILITY BUT NOT THE REQUIRED OFFSET TO THE PROPOSED STREET LIGHT CONDUIT.



1608 Camosun Street, Victoria BC V8T 3E6
Info@biophilialcollective.ca 250 590 1156

OWNER/CLIENT:

ARYZE DEVELOPMENTS

PROJECT ADDRESS:

1733 FAIRFIELD,
VICTORIA, BC

DESIGNED BY: BIANCA BODLEY
DRAWN BY: KIM TANG

NO.	ISSUE	YYMM/DD
1	ISSUED FOR DEVELOPMENT TRACKER	22/03/04
2	ISSUED FOR RZ/DP	22/05/02
3	ISSUED FOR COORDINATION	22/07/18
4	ISSUED FOR RZ/DP	22/07/20
5	ISSUED FOR RZ/DP RESUBMISSION	23/01/24
6	ISSUED FOR RZ/DP RESUBMISSION	2023/08/18

SEAL



NORTH ARROW



DRAWING TITLE:

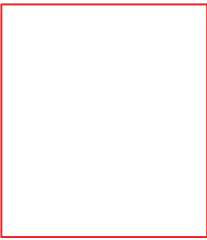
LANDSCAPE SITE PLAN

DWG NO:

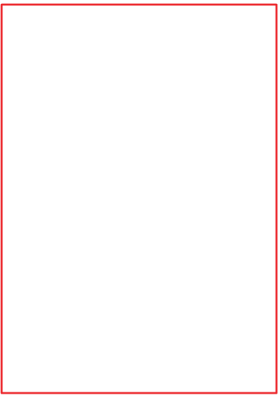
SCALE: 1:100

L1

PARKING STALL



PATIOS ON FAIRFIELD + BALCONIES ON UPPER LEVELS



1608 Camosun Street, Victoria BC V8T 3E6
Info@biophiliacollective.ca 250 590 1156

OWNER/CLIENT:
ARYZE DEVELOPMENTS

PROJECT ADDRESS:
**1733 FAIRFIELD,
VICTORIA, BC**

DESIGNED BY: **BIANCA BODLEY**
DRAWN BY: **KIM TANG**

NO.	ISSUE	YYMMDD
1	ISSUED FOR DEVELOPMENT TRACKER	220304
2	ISSUED FOR RZ/DP	220502
3	ISSUED FOR COORDINATION	220718
4	ISSUED FOR RZ/ DP	220720
5	ISSUED FOR RZ/ DP RESUBMISSION	230124
6	ISSUED FOR RZ/ DP RESUBMISSION	20230818

SEAL

NORTH ARROW



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**PAVEMENT
SPECIFICATIONS**

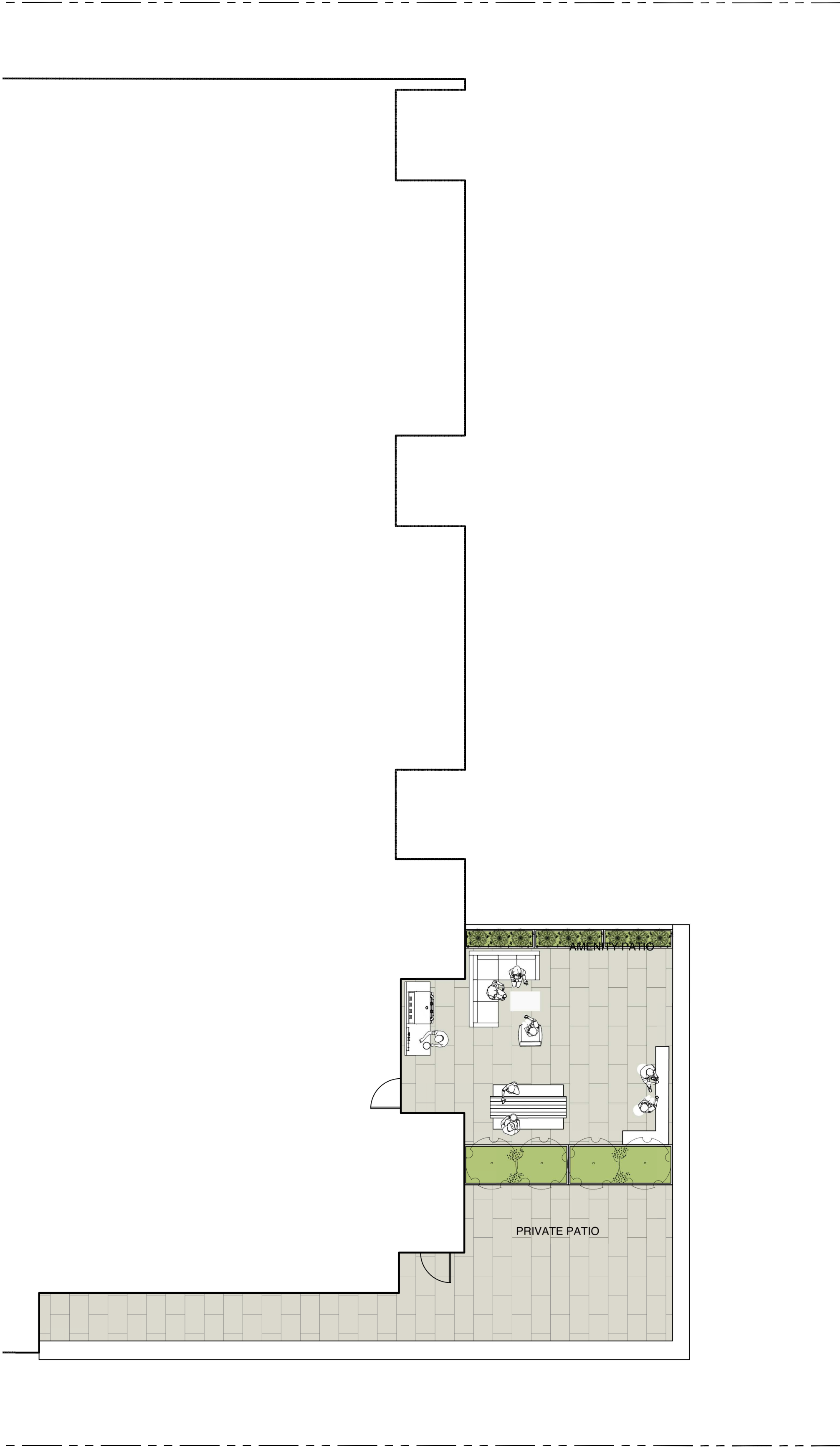
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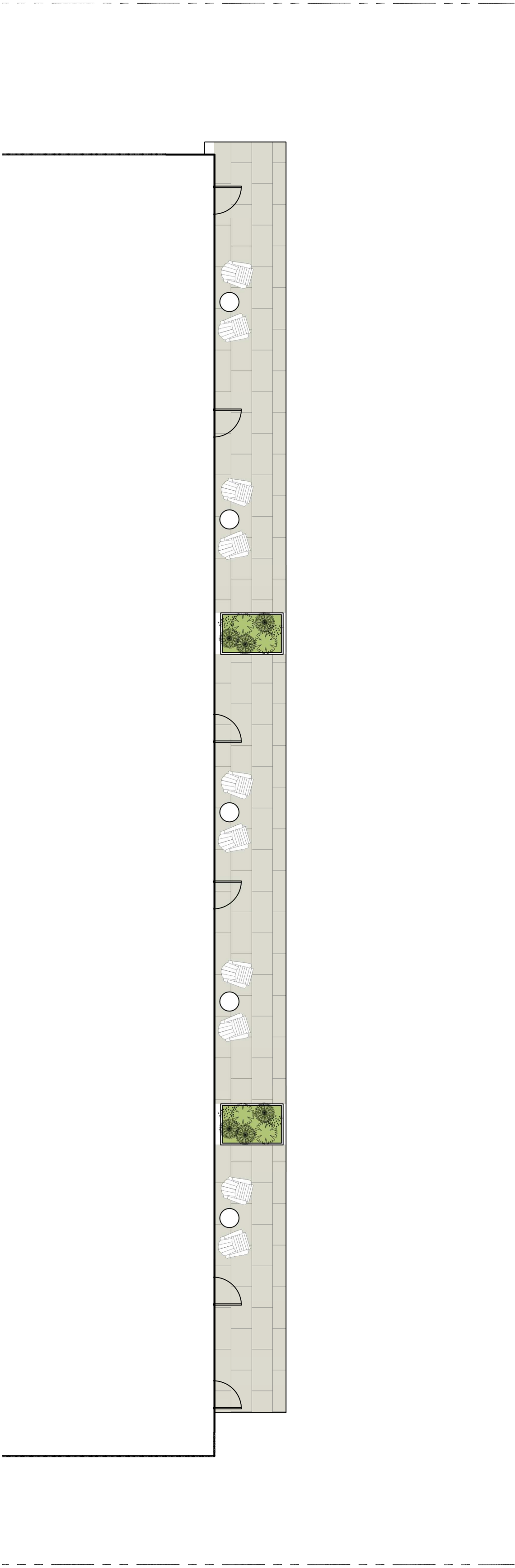
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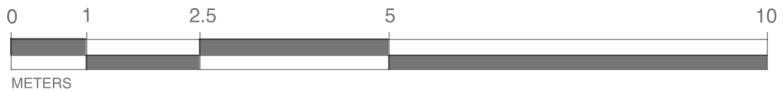
1
L2
LEVEL 2 BALCONY & PLANTER
1:100



2
L2
LEVEL 3 BALCONY & PLANTER
1:100



3
L2
LEVEL 4 BALCONY & PLANTER
1:100



1608 Camosun Street, Victoria BC V8T 3E6
Info@biophiliacollective.ca 250 590 1156

OWNER/CLIENT:
ARYZE DEVELOPMENTS

PROJECT ADDRESS:
**1733 FAIRFIELD,
VICTORIA, BC**

DESIGNED BY: **BIANCA BODLEY**
DRAWN BY: **KIM TANG**

NO.	ISSUE	YYMM/DD
1	ISSUED FOR DEVELOPMENT TRACKER	22/03/04
2	ISSUED FOR RZ/DP	22/05/02
3	ISSUED FOR COORDINATION	22/07/18
4	ISSUED FOR RZ/ DP	22/07/20
5	ISSUED FOR RZ/ DP RESUBMISSION	23/01/24
6	ISSUED FOR RZ/ DP RESUBMISSION	2023/08/18

SEAL



NORTH ARROW



DRAWING TITLE:
**UPPER LEVEL
LANDSCAPE PLANS**

DWG NO:

SCALE: **AS NOTED**

L2

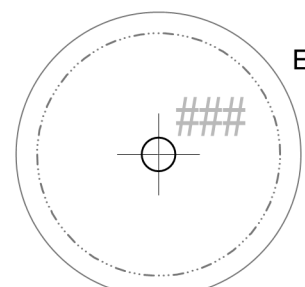
Table 1. Tree Inventory

Previous tag #	Tag or ID #	Surveyed ? (Yes / No)	Location (On, Off, Shared, City)	Bylaw protected ? (Yes / No / City)	Name		dbh (cm)	Ht (m)	Critical root zone radius (m)	Drip-line radius (m)	Condition		Retention Suitability (on-site trees)	Relative tolerance	General field observations/remarks	Tree retention comments	Retention status
					Common	Botanical					Health	Structural					
	M1	Yes	City	City	Purple leaf cherry plum	<i>Prunus cerasifera</i>	47	10	5.9	5	Good	Fair/good		Moderate	Multiple stems form at 3m above grade - included bark at main stem union with active response growth, historic pruning wounds with associated surface decay.	Project arborist to supervise all excavation required within the critical root zone. Will be impacted by the installation of the proposed water line.	Retain
	M2	Yes	City	City	Pacific sunset maple	<i>Acer truncatum 'Pacific sunset'</i>	5	6	0.6	1	Good	Good		Moderate	Recently planted - still staked, included bark at stem unions. Multiple stems form at 4m above grade - no major weaknesses visible at stem unions, historic pruning wounds with associated surface decay.	Project arborist to supervise all excavation required within the critical root zone. Possible for transplant.	Remove*
	M3	Yes	City	City	Purple leaf cherry plum	<i>Prunus cerasifera</i>	26	6	3.3	4	Good	Fair/poor		Moderate	Codominant stems form at 3m above grade - included bark - not active, historic pruning wounds with associated decay.	Project arborist to supervise all excavation required within the footprint of the proposed building. Located within the footprint of the proposed building.	Retain
313	3383	Yes	On	No	Fig	<i>Ficus sp.</i>	11.8, 6	5	2	2	Good	Fair	Unsuitable	Good	Growing within close proximity to existing building. Multiple stems form at 1m above grade - narrow angles of attachment, stems topped and decayed with sloughing bark. Topped historically at 7m above grade for overhead utilities clearance, stem decay with sloughing bark.	Located within the footprint of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
316	3384	Yes	On	No	Japanese maple	<i>Acer palmatum</i>	3, 3, 6	5	1.2	1.5	Good	Fair	Unsuitable	Moderate	Multiple stems form at 1m above grade - narrow angles of attachment, stems topped and decayed with sloughing bark. Topped historically at 7m above grade for overhead utilities clearance, stem decay with sloughing bark.	Located within the footprint of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
317	3385	Yes	On	Yes	Pacific dogwood	<i>Cornut nuttallii</i>	28, 23, 34, 32	8	8.8	4	Fair	Fair/poor	Unsuitable	Moderate	Topped historically at 5m above grade - multiple regrowth leaders form at topping locations.	Located within the footprint of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
322	3386	Yes	On	Yes	Golden chain tree	<i>Laburnum sp.</i>	21, 28	7	6.3	4	Fair	Poor	Unsuitable	Poor	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Located within the footprint of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
321	3387	Yes	On	Yes	Apple	<i>Malus sp.</i>	14, 23	10	3.9	3	Fair	Poor	Unsuitable	Moderate	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Located within the footprint of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
334	3388	Yes	On	Yes	Cherry	<i>Prunus sp.</i>	55	5	6.9	4	Fair	Fair/poor	Unsuitable	Moderate	Flowering cherry, multiple stems form at 2m above grade - narrow angles of attachment, main stem topped historically at 4m above grade - large topping wound.	Located within the footprint of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
298	3389	Yes	On	Yes	Cherry	<i>Prunus sp.</i>	19, 25	5	4.6	4	Fair	Fair/poor	Unsuitable	Moderate	Flowering cherry, codominant stems form at base - crossing at 5m above grade, large heading cuts.	Will be heavily impacted by excavation required to construct the foundation of the proposed building. Will also be impacted by excavation for the proposed parking stall.	Remove
3390	Yes	On	No		English yew	<i>Taxus baccata</i>	10.8, 8, 6	5	2.5	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 5 yew trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3391	Yes	On	No		English yew	<i>Taxus baccata</i>	5, 5, 5	5	1.4	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 5 yew trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3392	Yes	On	No		English yew	<i>Taxus baccata</i>	10, 8, 9	5	2.5	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 5 yew trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3393	Yes	On	No		English yew	<i>Taxus baccata</i>	8, 8, 4, 3	5	1.9	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 5 yew trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3394	Yes	On	No		English yew	<i>Taxus baccata</i>	10, 9, 9, 4	5	2.6	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 5 yew trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3395	Yes	On	No		English yew	<i>Taxus baccata</i>	11, 16	5	2.8	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 13 yew trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3396	Yes	On	No		English yew	<i>Taxus baccata</i>	18	5	2.3	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 13 yew trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3397	Yes	On	No		English yew	<i>Taxus baccata</i>	12, 8	5	2.1	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 13 yew trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3398	Yes	On	No		English yew	<i>Taxus baccata</i>	10, 10, 8, 5	5	2.6	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 13 yew trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3399	Yes	On	No		English yew	<i>Taxus baccata</i>	14, 8	5	2.4	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 13 yew trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3400	Yes	On	No		English yew	<i>Taxus baccata</i>	12, 5	5	1.9	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 13 yew trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3401	Yes	On	No		English yew	<i>Taxus baccata</i>	8, 8, 5, 5	5	2	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 13 yew trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3402	Yes	On	No		English yew	<i>Taxus baccata</i>	8, 8, 5, 4	5	2	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 13 yew trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3403	Yes	On	No		English yew	<i>Taxus baccata</i>	8, 6, 5, 4	5	1.8	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 13 yew trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3404	Yes	On	No		English yew	<i>Taxus baccata</i>	10, 8, 5, 4	5	2.2	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 13 yew trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3405	Yes	On	No		English yew	<i>Taxus baccata</i>	14, 10, 10	5	3	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 13 yew trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3406	Yes	On	No		English yew	<i>Taxus baccata</i>	16	5	2	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 13 yew trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3407	Yes	On	No		English yew	<i>Taxus baccata</i>	10, 10, 5, 5, 5	5	2.4	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 13 yew trees spaced less than 1m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3408	Yes	On	No		English yew	<i>Taxus baccata</i>	12, 5, 5, 5	5	2.3	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 7 yew trees spaced less than 1.25m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3409	Yes	On	No		English yew	<i>Taxus baccata</i>	8, 4	5	1.3	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 7 yew trees spaced less than 1.25m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3410	Yes	On	No		English yew	<i>Taxus baccata</i>	5, 5, 4, 4, 4	5	1.3	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 7 yew trees spaced less than 1.25m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3411	Yes	On	No		English yew	<i>Taxus baccata</i>	8, 8, 5, 5	5	2.2	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 7 yew trees spaced less than 1.25m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3412	Yes	On	No		English yew	<i>Taxus baccata</i>	8, 8, 8	5	2.2	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 7 yew trees spaced less than 1.25m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3413	Yes	On	No		English yew	<i>Taxus baccata</i>	12, 5	5	1.9	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 7 yew trees spaced less than 1.25m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3414	Yes	On	No		English yew	<i>Taxus baccata</i>	10, 12, 5, 5, 5	5	2.6	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 7 yew trees spaced less than 1.25m apart.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3415	Yes	On	Yes*		English yew	<i>Taxus baccata</i>	11, 33	5	5	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 5 yew trees spaced less than 1m apart. *1 Bylaw size stem within hedge.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3416	Yes	On	Yes*		English yew	<i>Taxus baccata</i>	22	5	2.8	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 5 yew trees spaced less than 1m apart. *1 Bylaw size stem within hedge.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3417	Yes	On	Yes*		English yew	<i>Taxus baccata</i>	5, 5, 5, 6, 6	5	1.5	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 5 yew trees spaced less than 1m apart. *1 Bylaw size stem within hedge.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3418	Yes	On	Yes*		English yew	<i>Taxus baccata</i>	13	5	1.6	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 5 yew trees spaced less than 1m apart. *1 Bylaw size stem within hedge.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
3419	Yes	On	Yes*		English yew	<i>Taxus baccata</i>	12, 13, 6, 5, 4	5	3.1	1	Fair	Fair/poor	Unsuitable	Moderate	Growing within a hedge row of 5 yew trees spaced less than 1m apart. *1 Bylaw size stem within hedge.	Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
Hedge 1	Yes	On	No		Emerald cedar	<i>Thuja occidentalis 'smaragd'</i>	5 - 10cm	5	0.5	0.5	Fair	Fair	Unsuitable	Moderate	Hedge row consisting of approximately 30 individual stems - no bylaw stems. Codominant stems form at 3m above grade - no major weaknesses visible at stem union, ivy covered trunk.	Located within the footprint of the proposed building. Will be heavily impacted by excavation required to construct the foundation of the proposed building.	Remove
314	3420	Yes	On	Yes	Apple	<i>Malus sp.</i>	21, 25*	5	4.7	4	Fair	Fair/poor	Unsuitable	Moderate	Topped historically at 5m above grade - multiple regrowth leaders form at topping locations.	Located within the footprint of the proposed parking stall. Will be heavily impacted by excavation required to construct the foundation of the proposed parking stall.	Remove
315	3421	Yes	On	No	Apple	<i>Malus sp.</i>	28*	5	3.5	4	Fair	Fair/poor	Unsuitable	Moderate	Topped historically at 5m above grade - multiple regrowth leaders form at topping locations.	Located within the footprint of the proposed parking stall. Will be heavily impacted by excavation required to construct the foundation of the proposed parking stall.	Remove
OS1	Yes	Off	No		Emerald cedar	<i>Thuja occidentalis 'smaragd'</i>	10 - 4	1.3	0.5	0.5	Fair	Fair		Moderate	Juvenile hedge tree growing within narrow planting strip.	Project arborist to supervise all excavation required within the crz. Possible for retention or transplanted elsewhere. Project arborist to supervise all excavation required within the crz.	Retain*

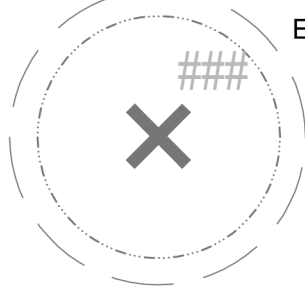


WINDMILL PALM REQUESTED TO BE TRANSPLANTED / REMOVED BY LANDSCAPE TO ALLOW FOR PLANTING OF LARGE CANOPY TREE

TREE STATUS LEGEND



EXISTING TREE TO REMAIN



EXISTING TREE TO BE REMOVED

- TREE PROTECTION FENCING
- TO OUTSIDE OF DRIFLINE WHERE POSSIBLE (AS SHOWN)
 - TO BE MAINTAINED UNTIL COMPLETION OF CONSTRUCTION
 - TO MUNICIPAL STANDARD

PROTECTED TREE AND SHRUB FOR REMOVAL

MUNICIPAL TREE FOR REMOVAL

BOULEVARD TREE M2 TO BE REMOVED AND RELOCATED BY CITY OF VICTORIA FOR INSTALLATION OF PROPOSED WATER CONNECTION (SEE CIVIL). REMOVAL TO BE PAID FOR BY DEVELOPER.

Previous tag #	Tag or ID #	Surveyed ? (Yes / No)	Location (On, Off, Shared, City)	Bylaw protected ? (Yes / No / City)	Name		dbh (cm)	Ht (m)	Critical root zone radius (m)	Condition		Retention Suitability (on-site trees)	Relative tolerance	General field observations/remarks	Tree retention comments	Retention status	
					Common	Botanical				Health	Structural						
	OS2	Yes	Off	No	Emerald cedar	<i>Thuja occidentalis 'smaragd'</i>	10 - 4	1.3	0.5	Fair	Fair		Moderate	Juvenile hedge tree growing within narrow planting strip.	Project arborist to supervise all excavation required within the crz.	Retain*	
	OS3	Yes	Off	No	Honey locust	<i>Gleditsia triacanthos</i>	10 - 5	1.3	2	Good	Good		Moderate	Juvenile hedge tree growing within narrow planting strip.	Project arborist to supervise all excavation required within the crz.	Retain	
	OS4	Yes	Off	No	Emerald cedar	<i>Thuja occidentalis 'smaragd'</i>	5, 5, 5 - 1.4	1		Good	Fair		Moderate	Multiple stems hedge tree growing within narrow planting strip.	Project arborist to supervise all excavation required within the crz.	Retain	
	OS5	Yes	Off	No	Emerald cedar	<i>Thuja occidentalis 'smaragd'</i>	8, 10, 4 - 5	2.5	1	Good	Fair		Moderate	Multiple stems hedge tree growing within narrow planting strip.	Project arborist to supervise all excavation required within the crz.	Retain*	
	OS6	Yes	Off	No	Cherry	<i>Prunus</i> sp.	8, 6, 6 - 5	1.9	1	Good	Fair		Moderate	Flowering cherry, growing within narrow planting strip.	Project arborist to supervise all excavation required within the crz. Will be heavily impacted by excavation required to construct the proposed parking stalls	Retain*	
	Hedge 2	Yes	On	No	Emerald cedar	<i>Thuja occidentalis 'smaragd'</i>	5 - 10cm	5	0.5	0.5	Good	Fair	Moderate	Hedge row consisting of 14 individual stems. No bylaw stems.	Project arborist to supervise all excavation required within the crz.	Remove	
	OS7	Yes	Off	No	Japanese maple	<i>Acer palmatum</i>	5, 5, 5, 4, 4	5	1.4	2	Good	Fair		Moderate	Multiple stems form at 5m above grade.	Project arborist to supervise all excavation required within the crz. Possible for retention or transplanted elsewhere. Project arborist to supervise all excavation required within the crz.	Retain
	3422	No	On	No	Windmill palm	<i>Trachycarpus fortunei</i>	29	4	3.0	2	Good	Fair	Unsuitable	Good	Crown raised approx. 50% CRZ + 0.5' d.b.h. (drawn from the center of the stem)	Will be heavily impacted by excavation required to construct the proposed parking stalls	Retain*

NOTE: SEE ORIGINAL TREE INVENTORY AND TABLES IN REPORT PREPARED BY TALBOT & MACKENZIE



1608 Camosun Street, Victoria BC V8T 3E6
Info@biophilicollective.ca 250 590 1156

OWNER/CLIENT:

ARYZE DEVELOPMENTS

PROJECT ADDRESS:

1733 FAIRFIELD,
VICTORIA, BC

DESIGNED BY: BIANCA BODLEY

DRAWN BY: KIM TANG

NO.	ISSUE	YYMMDD
1	ISSUED FOR DEVELOPMENT TRACKER	22/03/04
2	ISSUED FOR RZ/DP	22/05/02
3	ISSUED FOR COORDINATION	22/07/18
4	ISSUED FOR RZ/DP	22/07/20
5	ISSUED FOR RZ/DP RESUBMISSION	23/01/24
6	ISSUED FOR RZ/DP RESUBMISSION	2023/08/18

SEAL



NORTH ARROW



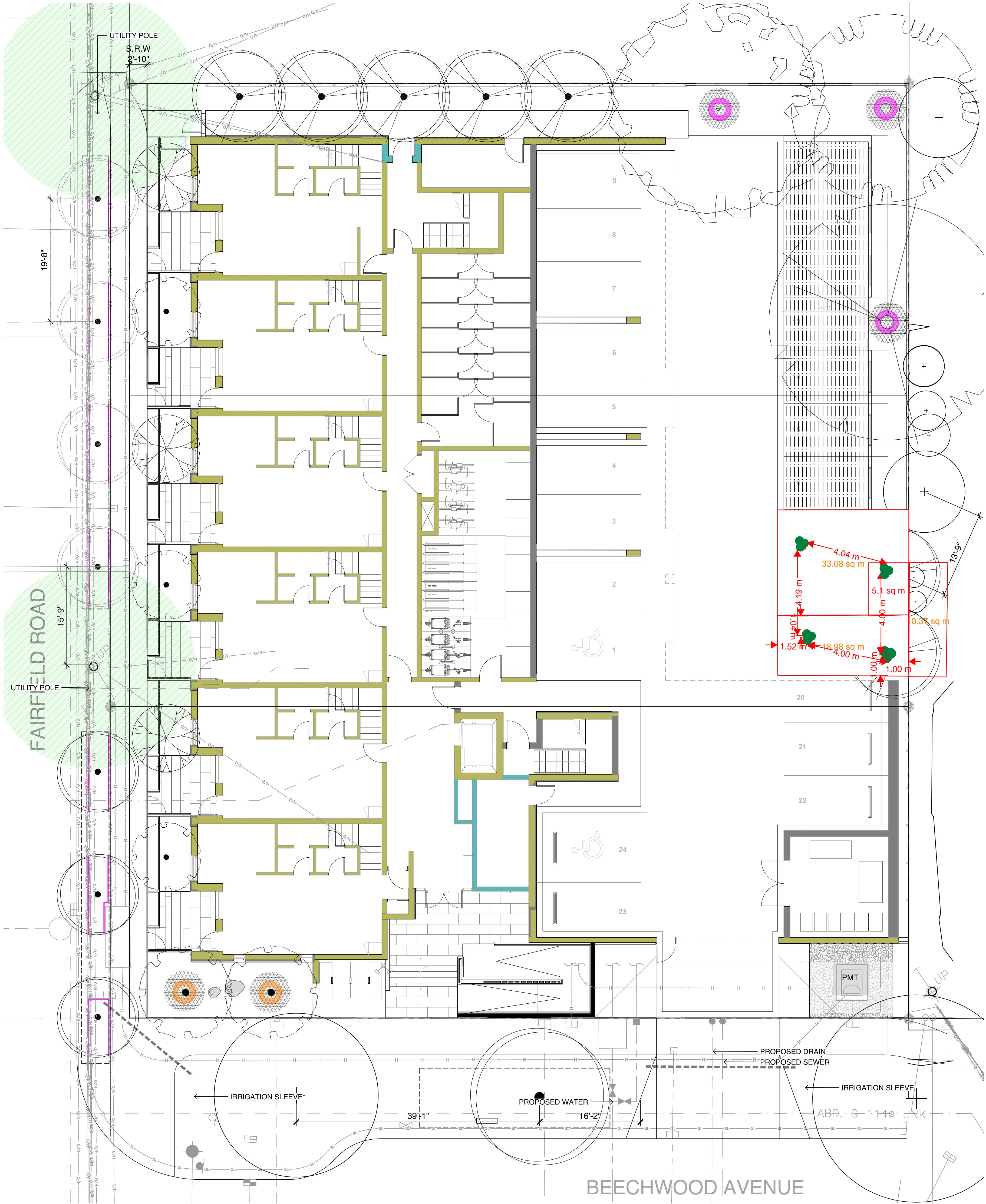
DRAWING TITLE:

TREE REMOVAL &
PROTECTION PLAN

DWG NO:

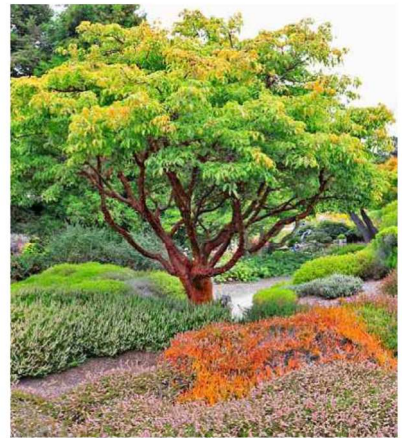
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L3



LEGEND

- OVERHEAD POWER LINE
- ROOT BARRIER
- 1:1 DESIGNATED REPLACEMENT TREE
- 2:1 DESIGNATED REPLACEMENT TREE
- TREE PIT WITH 1.0 m DEPTH GROWING MEDIUM FOR BOULEVARD TREES
- TREE PLANTING RESTRICTIONS
 - 5.0 m OFFSET FROM HYDRO OR LIGHT POLE
 - 1.0 m OFFSET AREA



PAPERBARK MAPLE



AUTUMN APPLAUSE WHITE ASH



PERSIAN PARROTIA 'VERONICA'



JAPANESE SNOWBELL



PIN OAK

TREE SCHEDULE										
Quantity	Symbol	Latin Name	Common Name	Container	Caliper	Size	Native	Remarks	Height at Maturity (m)	Spread at Maturity (m)
5		Acer griseum	Paperbark Maple	B&B	6 cm	>2 m		full/part sun	6-9	5-7
1		Fraxinus americana 'Autumn Applause'	Autumn Applause white ash	B&B	6 cm	>2 m		Medium	12-15	7-9
1		Liriodendron tulipifera	Tulip tree	B&B	6 cm	>2 m		full sun	18-24	9-12
7		Parrotia persica 'Ruby Vase'	Ruby Vase Parrotia	B&B	6 cm	>2 m		full sun	7-9	4-6
3		Styrax japonicus	Japanese snowbell	B&B	6 cm	>2 m		full sun - part shade	7.0	7.0
1		Quercus palustris	Pin Oak	B&B	6 cm	>2 m		full sun	10	10
8			BOULEVARD TREE TO BE DETERMINED BY PARKS AT BP	B&B	TBD					

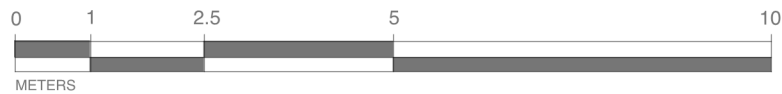
NOTES:
1. PLANTS IN PLANT LISTS ARE SPECIFIED ACCORDING TO THE CANADIAN NURSERY LANDSCAPE ASSOCIATION CANADIAN STANDARDS FOR NURSERY STOCK AND SECTION 12: CONTAINER GROWN PLANTS FROM THE SC LANDSCAPE STANDARDS, CURRENT EDITION.

TREE IMPACT SUMMARY TABLE						
TREE STATUS	TOTAL	TO BE RETAINED	TO BE REMOVED	REPLACEMENTS REQUIRED	MINIMUM REQUIRED FOR LOT SIZE	PROPOSED
ON-SITE TREES AND SHRUBS BYLAW PROTECTED	7	0	7	7	9	4
MUNICIPAL TREES	3	2	1	1		8
NEIGHBORING TREES, BYLAW PROTECTED	0	0	0	0		0

ACTUAL NUMBER : 5
2 x LARGE TREE (1:1)
1 x MEDIUM TREE (1:1)
2 x SMALL TREE (2:1)

REPLACEMENT TREES TO BE PLANTED ON SITE	4
REPLACEMENT TREE SHORTFALL	3
TOTAL PROPOSED TREES ON SITE (REPLACEMENT (ACTUAL) + OTHER)	18

- NOTES:
- IRRIGATION SYSTEM ON CITY PROPERTY SHALL COMPLY TO CITY OF VICTORIA SUPPLEMENTARY SPECIFICATIONS FOR STREET TREES AND IRRIGATION SCHEDULE C, BYLAW 12-042, SUBDIVISION BYLAW. IRRIGATION DRAWINGS MUST BE SUBMITTED TO PARKS DIVISION FOR REVIEW AND APPROVAL 30 DAYS PRIOR TO INSTALLATION WORK. THE FOLLOWING IRRIGATION AND SLEEVING INSPECTIONS BY PARKS STAFF ARE REQUIRED BY SCHEDULE C.
 - IRRIGATION INSPECTION REQUIREMENTS:
 - a. THE IRRIGATION SYSTEM AND SLEEVING INSPECTION REQUIREMENTS CAN BE FOUND IN SCHEDULE C OF THE VICTORIA SUBDIVISION AND DEVELOPMENT SERVICING BYLAW NO. 12-042.
 - b. IRRIGATION SLEEVING PRIOR TO BACKFILLING*
 - c. OPEN TRENCH MAIN LINE AND PRESSURE TEST
 - d. OPEN TRENCH LATERAL LINE
 - e. IRRIGATION SYSTEM, CONTROLLER, COVERAGE TEST, BACKFLOW PREVENTER ASSEMBLY TEST REPORT REQUIRED, BACKFLOW ASSEMBLY IS TO HAVE AN INSPECTION TAG COMPLETED AND ATTACHED.
 - 100MM SDR 28 PIPE WILL BE USED FOR IRRIGATION SLEEVING UNDER HARD SURFACES. INSTALLATIONS WHERE A 90-DEGREE BEND IS REQUIRED SHOULD BE INSTALLED USING 100MM SDR 28 GSX (22.5 DEGREE) LONG SWEEPS. INSTALL WILL BE AT 400MM DEPTH.



1608 Camosun Street, Victoria BC V8T 3E6
Info@biophiliacollective.ca 250 590 1156

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VICTORIA, BC**

DESIGNED BY: BIANCA BODLEY
DRAWN BY: KIM TANG

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6	ISSUED FOR RZ/DP RESUBMISSION	2023/08/18

SEAL



NORTH ARROW



DRAWING TITLE:
TREE PLANTING PLAN

DWG NO:

L4

SCALE: 1:100



1 TREE PLANTING SOIL VOLUME PLAN
L4.1 1:100



1608 Camosun Street, Victoria BC V8T 3E6
Info@biophilialcollective.ca 250 590 1156

OWNER/CLIENT:
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VICTORIA, BC

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5	ISSUED FOR RZ/DP RESUBMISSION	23/01/24
6	ISSUED FOR RZ/DP RESUBMISSION	2023/08/18

SEAL



NORTH ARROW



DRAWING TITLE:
SOIL VOLUME ANALYSIS

DWG NO:

SCALE: AS NOTED

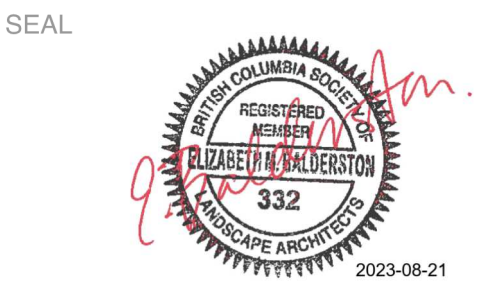
L4.1

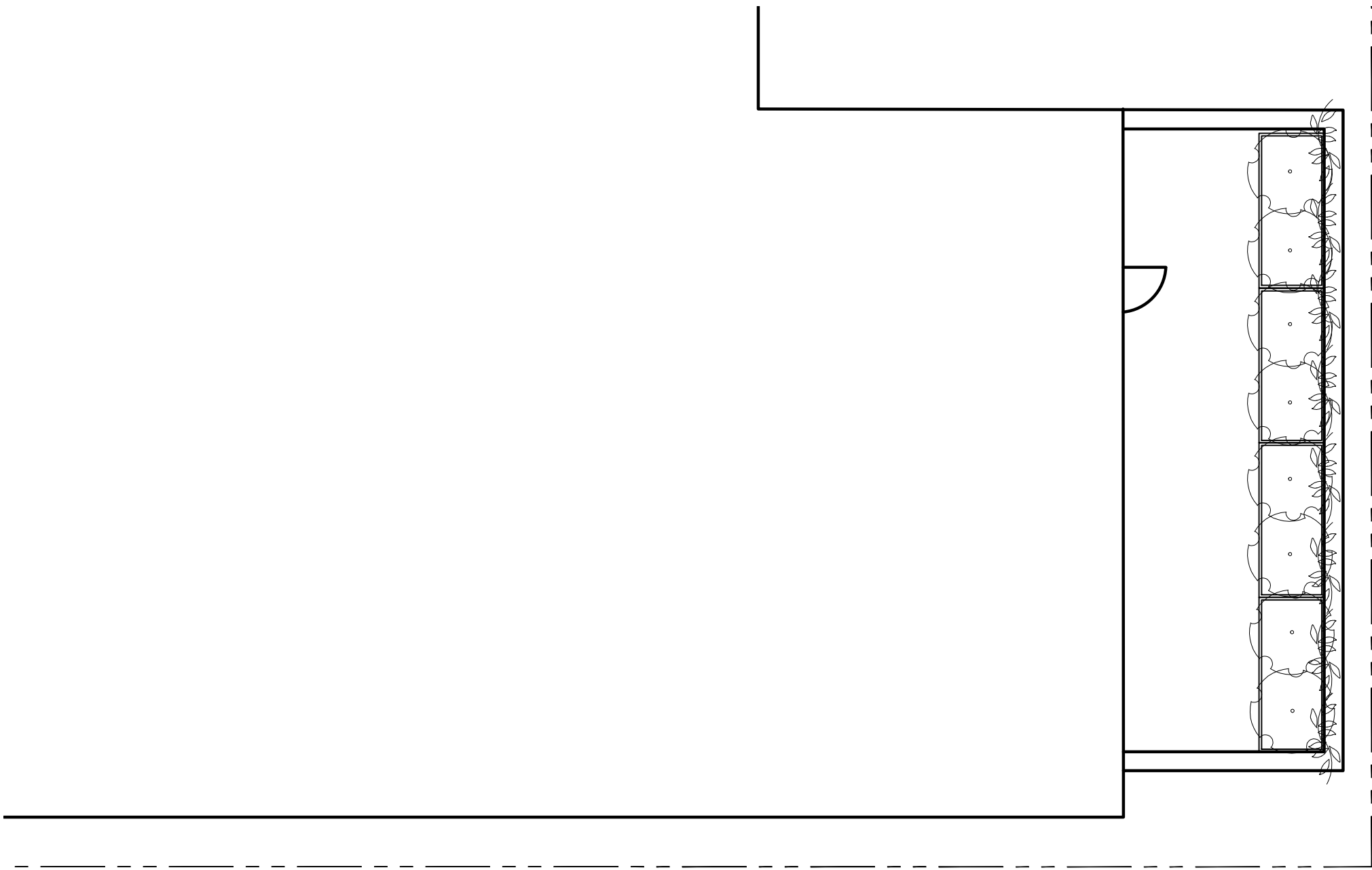


PLANT SCHEDULE						
Quantity	Symbol	Latin Name	Common Name	Container	Native	Pollinator
12		Armeria maritima 'Alba'	White-flowered Thrift	10cm		y
60		Arctostaphylos Uva-Ursi 'Massachusetts'	Kinnikinnick	10cm	y	y
30		Carex testacea 'Prairie Fire'	New Zealand hair sedge	#1		
10		Cephalotaxus harringtonia 'Prostrata'	Spreading Japanese plum yew	#2		
16		Choisya ternata	Mexican mock orange	#3		y
50		Hebe pinguiolia 'Sutherlandii'	Hebe Sutherlandii	#1		
30		Miscanthus sinensis 'Huron Light'	Huron Light maiden grass	#1		
25		Miscanthus sinensis 'Morning Light'	Chinese silver grass Morning Light	#1		
5		Verbena bonariensis 'Lollipop'	Dwarf purpletop	#1		Y
NOTES: 1. PLANTS IN PLANT LISTS ARE SPECIFIED ACCORDING TO THE CANADIAN NURSERY LANDSCAPE ASSOCIATION CANADIAN STANDARDS FOR NURSERY STOCK AND SECTION 12, CONTAINER GROWN PLANTS FROM THE BC LANDSCAPE STANDARD, CURRENT EDITION.						

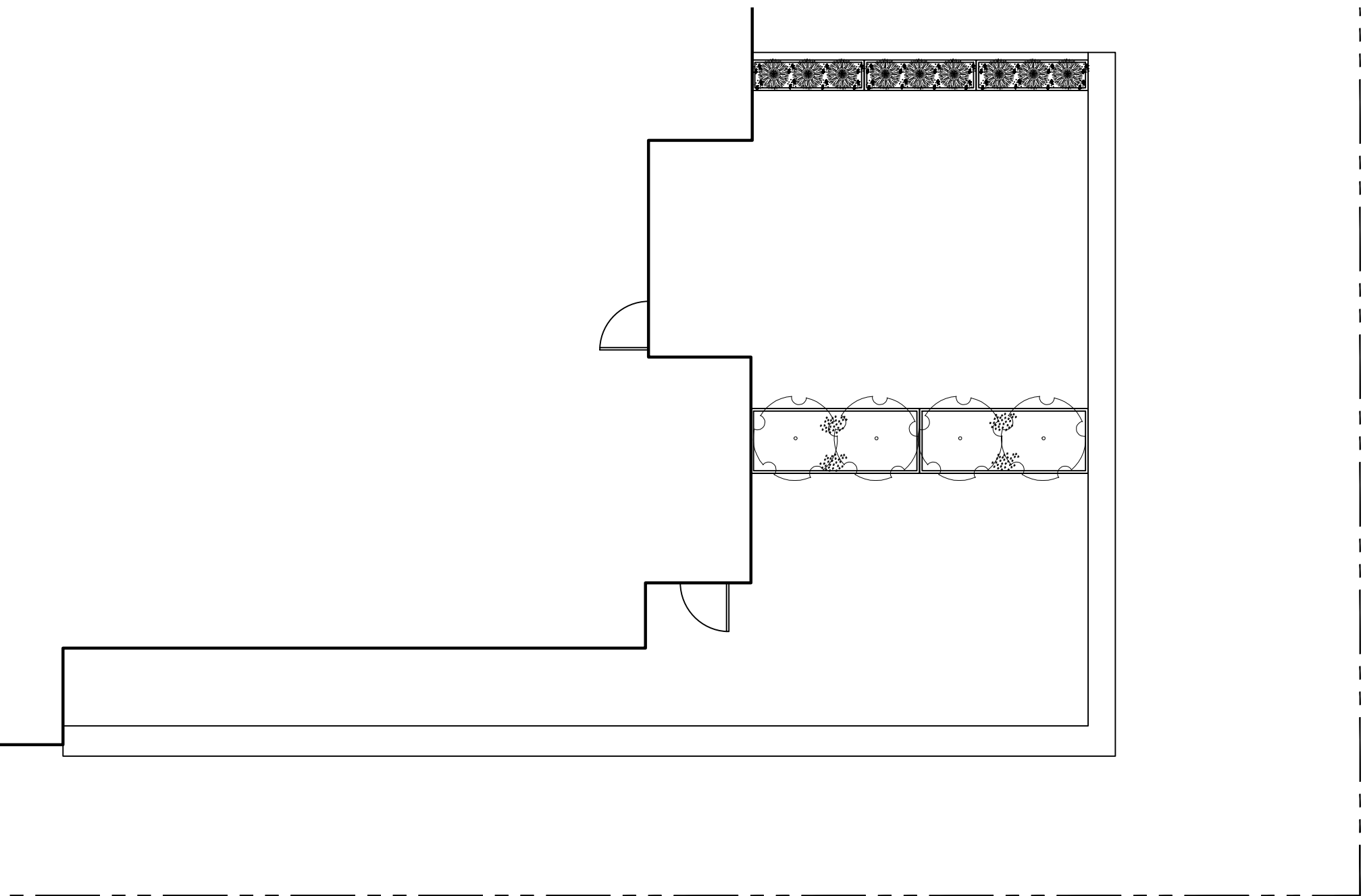


NO.	ISSUE	YYMM/DD
1	ISSUED FOR DEVELOPMENT TRACKER	22/03/04
2	ISSUED FOR RZ/DP	22/05/02
3	ISSUED FOR COORDINATION	22/07/18
4	ISSUED FOR RZ/ DP	22/07/20
5	ISSUED FOR RZ/ DP RESUBMISSION	23/01/24
6	ISSUED FOR RZ/ DP RESUBMISSION	2023/08/18

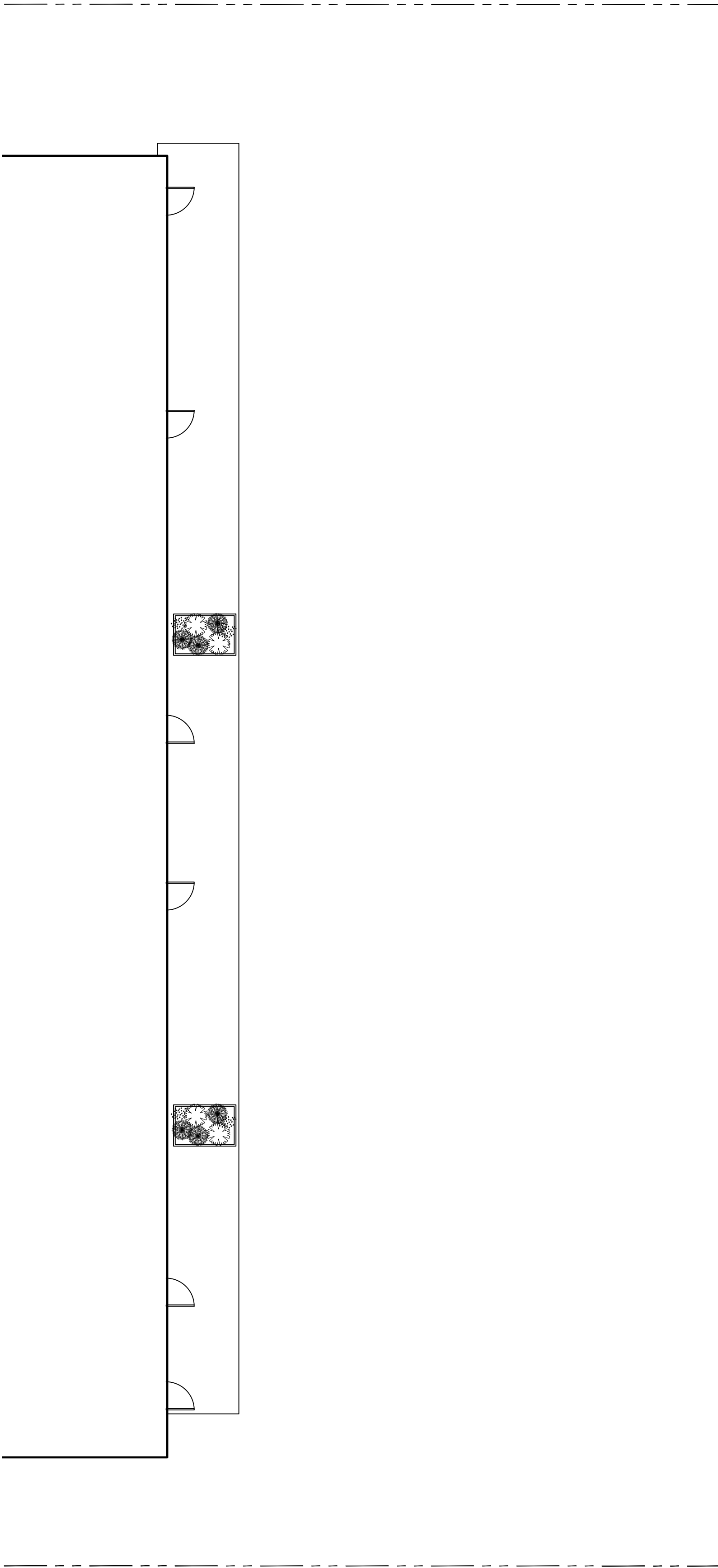




1 LEVEL TWO PLANTING PLAN
L6 1:100



2 LEVEL THREE PLANTING PLAN
L6 1:100



3 LEVEL FOUR PLAN
L6 1:100

PLANT SCHEDULE						
Quantity	Symbol	Latin Name	Common Name	Container	Native	Pollinator
6		Armeria maritima 'Alba'	White-flowered Thrift	10cm		y
10		Arctostaphylos Uva-Ursi 'Massachusetts'	Kinnikinnick	10cm	y	y
12		Choisya ternata	Mexican Orange Blossom	#3		y
15		Pinus strobus 'Horsford Dwarf'	Horsford Dwarf Eastern white pine	#2		
4		Rhododendron x Bloom-A-Thon® White Reblooming Azalea		#1		y
8		Parthenocussus quinquefolia	Virginia Creeper	#2		
NOTES: 1. PLANTS IN PLANT LISTS ARE SPECIFIED ACCORDING TO THE CANADIAN NURSERY LANDSCAPE ASSOCIATION CANADIAN STANDARDS FOR NURSERY STOCK AND SECTION 12, CONTAINER GROWN PLANTS FROM THE BC LANDSCAPE STANDARD, CURRENT EDITION.						



WHITE FLOWERED THRIFT



KINNIKINNICK



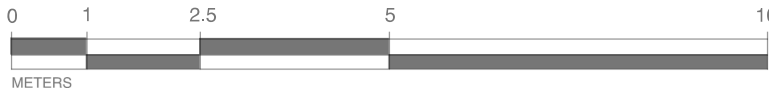
MEXICAN ORANGE BLOSSOM



HORSFORD DWARF EASTERN WHITE PINE



WHITE REBLOOMING AZALEA



1608 Camosun Street, Victoria BC V8T 3E6
Info@biophiliacollective.ca 250 590 1156

OWNER/CLIENT:
ARYZE DEVELOPMENTS

PROJECT ADDRESS:
1733 FAIRFIELD,
VICTORIA, BC

DESIGNED BY: BIANCA BODLEY
DRAWN BY: KIM TANG

NO.	ISSUE	YYMM/DD
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SEAL



NORTH ARROW

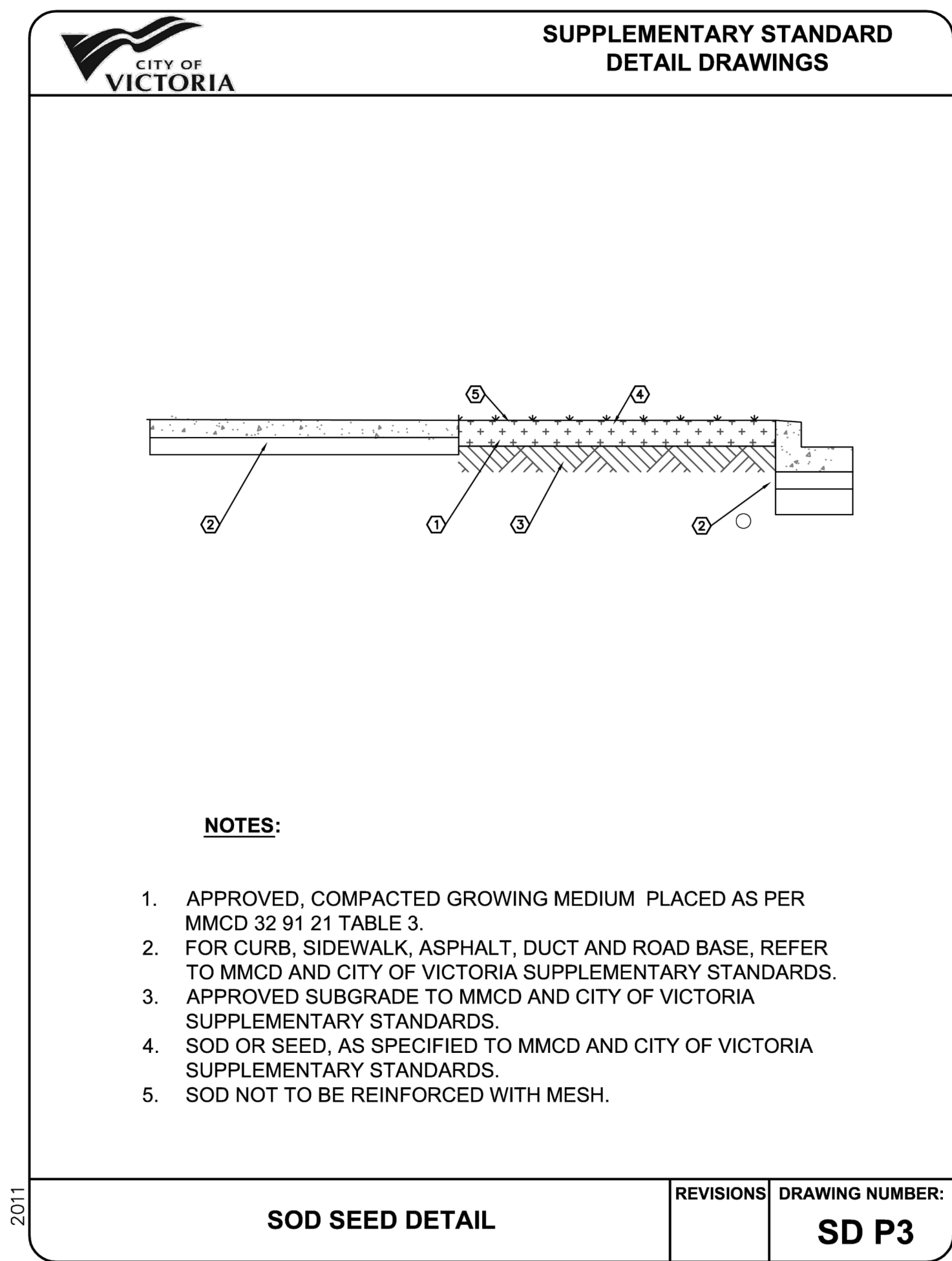


DRAWING TITLE:
UPPER LEVEL
PLANTING PLANS

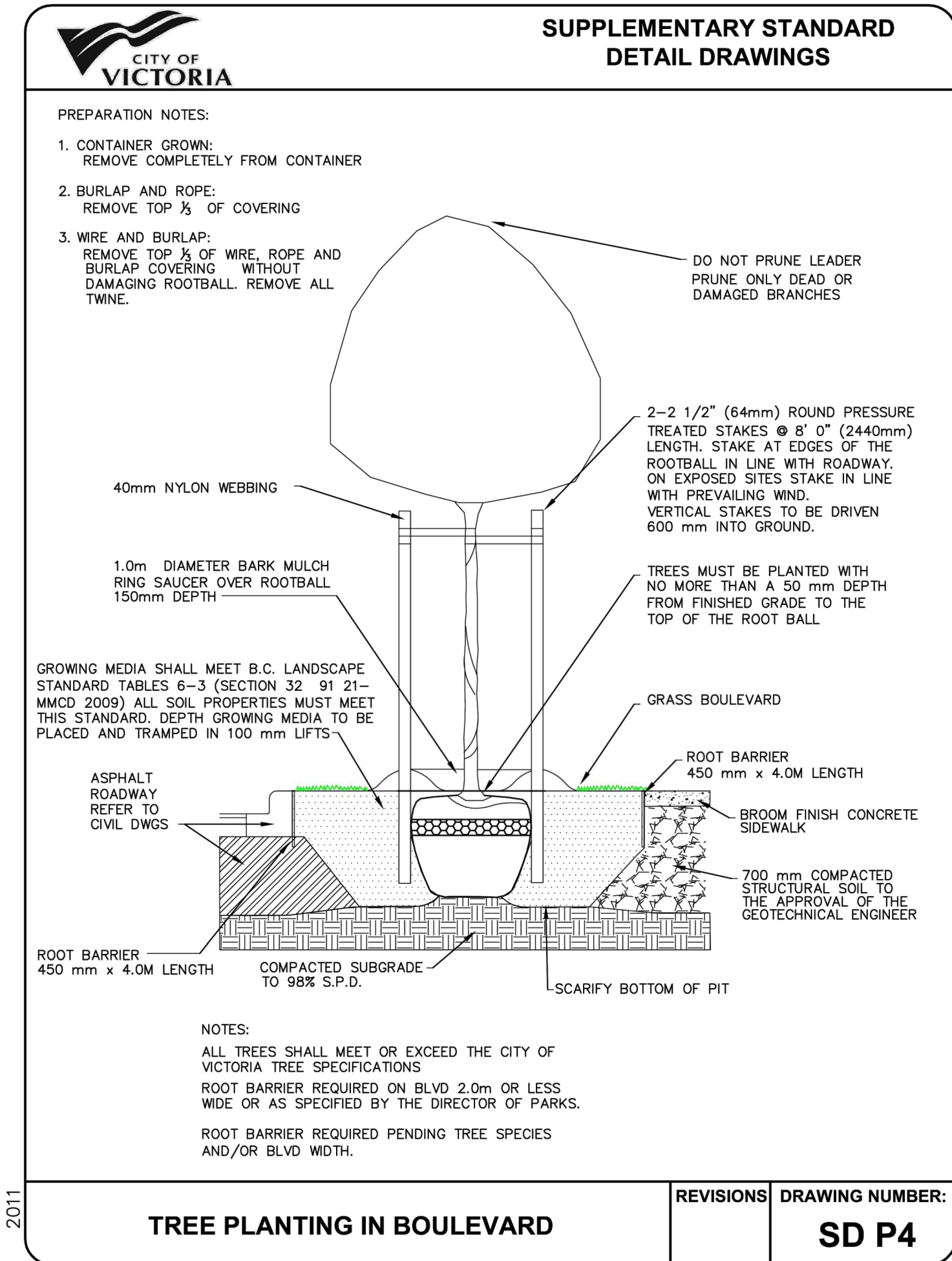
DWG NO:

SCALE: AS NOTED

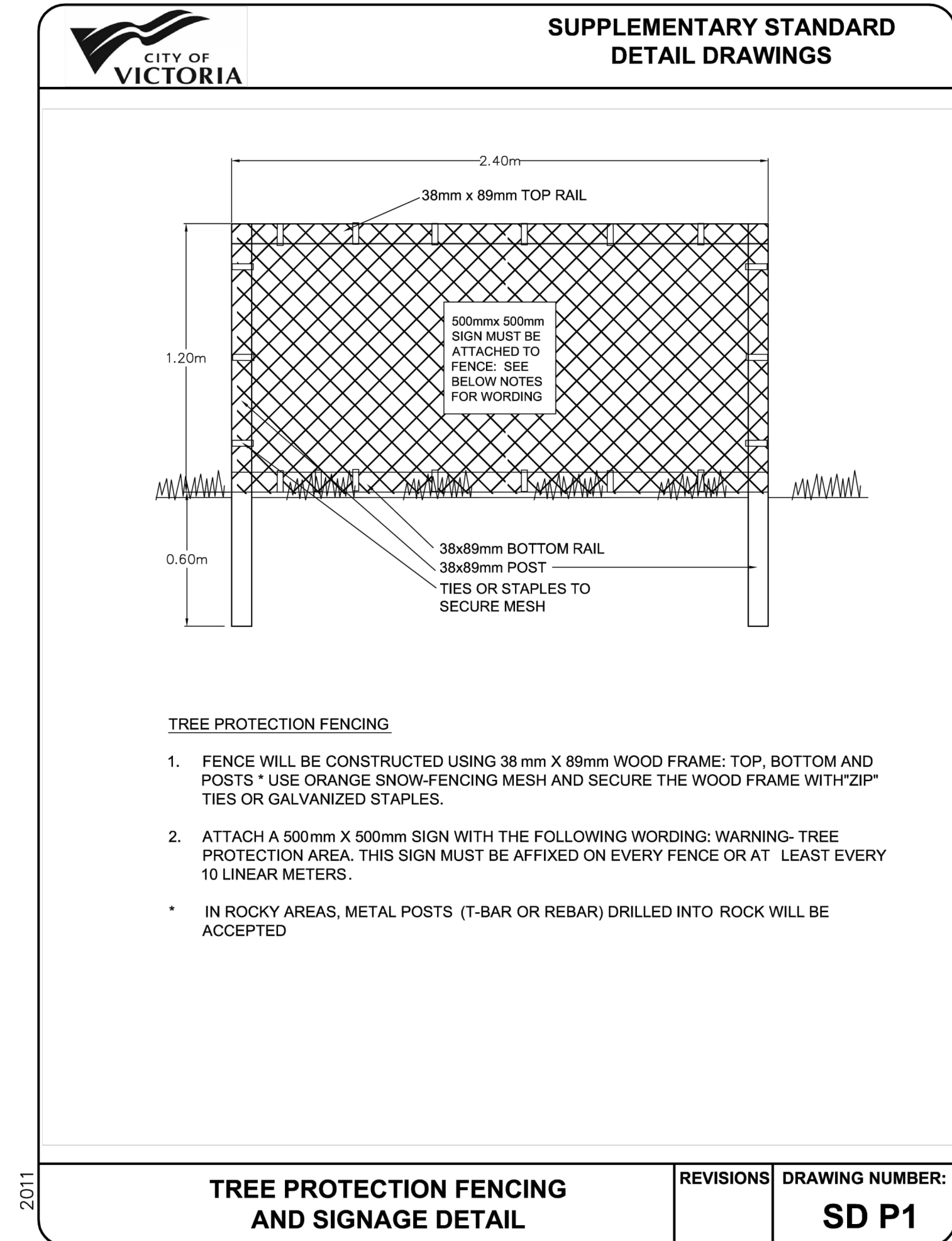
L6



1
L5 SOD SEED DETAIL
NTS



2
L5 TREE PLANTING IN BOULEVARD
NTS



3
L5 TREE PROTECTION FENCING AND SIGNAGE DETAIL
NTS

NO.	ISSUE	YYMM/DD
1	ISSUED FOR DEVELOPMENT TRACKER	22/03/04
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SEAL



NORTH ARROW



DRAWING TITLE:

DETAILS


DWG NO:


SCALE: AS NOTED

L7



SHEET NOTES	
No.	DESCRIPTION
1	EXISTING HYDRO SERVICE TO BE CAPPED AND ABANDONED BY CoV FORCES AT DEVELOPER'S EXPENSE. HYDRO POLE TO BE SUPPORTED DURING CONSTRUCTION.
2	EXISTING SANITARY SEWER SERVICE TO BE CAPPED AND ABANDONED AT PROPERTY LINE.
3	EXISTING STORM SEWER SERVICE TO BE CAPPED AND ABANDONED AT PROPERTY LINE.
4	NEW 150mm STORM SERVICE CONNECTION c/w INSPECTION CHAMBER TO PROPERTY LINE BY CoV FORCES AT DEVELOPER'S EXPENSE.
5	NEW 150mm FIRE SERVICE c/w METER CHAMBER AS PER CoV STD. DWG. SD W2G AND 50mm DOMESTIC WATER SERVICE BY CoV FORCES AT DEVELOPER'S EXPENSE.
6	NEW 150mm SANITARY SERVICE c/w INSPECTION CHAMBER TO PROPERTY LINE BY CoV FORCES AT DEVELOPER'S EXPENSE.
7	EXISTING GAS SERVICE TO BE CAPPED AND ABANDONED BY FORTISBC FORCES.
8	NEW CATCH BASIN PER CoV STD. DWG. SD S11a BY DEVELOPER'S CONTRACTOR. ENSURE OUTLET ELEVATION IS SET TO ACHIEVE MIN. 2% GRADE TO MAIN. 150mm PVC CATCH BASIN LEAD ADJUSTMENT AND CONNECTION BY CoV FORCES AT DEVELOPER'S EXPENSE.
9	NEW FIRE HYDRANT BY CoV FORCES AT DEVELOPER'S EXPENSE. LOCATION TO BE CONFIRMED DURING DETAILED DESIGN.
10	EXISTING WATER SERVICE TO BE RE-USED FOR IRRIGATION. BACKFLOW PREVENTOR TO BE INSTALLED.
11	NEW DUAL HEAD ELECTRIC VEHICLE CHARGER (LOCATION TO BE CONFIRMED DURING DETAILED DESIGN).

 EXISTING TREE TO BE REMOVED.

 EXISTING TREE TO REMAIN.

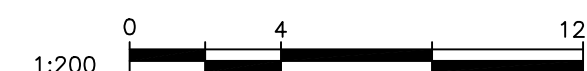
NOTES:
1. FOR BUILDING INFORMATION, SEE DRAWINGS BY CASCADIA ARCHITECTS INC.
2. FOR LANDSCAPING, SEE DRAWINGS BY BIOPHILIA DESIGN COLLECTIVE.
3. FOR LEGAL INFORMATION, SEE DRAWINGS BY J.E. ANDERSON & ASSOCIATES.
4. UTILITY SIZES AND LOCATIONS TO BE CONFIRMED DURING DETAILED DESIGN.
5. ALL EXISTING ON-SITE BUILDINGS, DRIVEWAYS, FENCES, HEDGES, RETAINING WALLS, ETC. TO BE REMOVED AND DISPOSED OFF-SITE (NOT SHOWN FOR CLARITY).
6. BASE INFORMATION SHOWN IS PRODUCED FROM A COMBINATION OF FIELD SURVEY AND GIS INFORMATION PROVIDED BY THE CITY OF VICTORIA (VICTORIA). ALL INFORMATION TO BE CONFIRMED IN THE FIELD PRIOR TO CONSTRUCTION (INCLUDING COMPLETING A BOONECALL).
7. ONSITE BIKE PARKING, ETC. NOT SHOWN FOR CLARITY (SEE ARCHITECT AND LANDSCAPE DRAWINGS).

PERMIT TO PRACTICE
McElhanney Ltd.
PERMIT NUMBER: 1003299
Engineers and Geoscientists of BC

SEE ARCHITECTURAL AND LANDSCAPE
DRAWINGS FOR ADDITIONAL INFORMATION

LEGAL PLAN & TOPOGRAPHIC
SURVEY PROVIDED BY J.E.
ANDERSON & ASSOCIATES

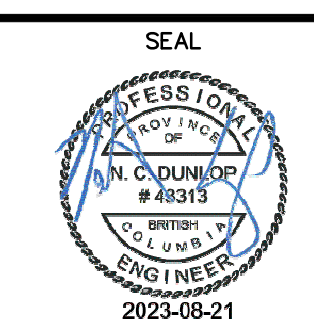
AUGUST 21, 2023
**ISSUED FOR
REZONING**



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NO.	DATE	BY	ISSUED	NO.	DATE	BY	REVISIONS
3	2023-08-21	ND	ISSUED FOR REZONING				
2	2023-01-25	ND	ISSUED FOR REZONING				
1	2022-04-27	ND	ISSUED FOR DEVELOPMENT PERMIT				

**McElhanney**
500 - 3960 QUADRA STREET
VICTORIA, BC V8X 4A3
PH (250) 370-9221



PROJECT: 1733 FAIRFIELD ROAD, VICTORIA, BC		SCALE HORIZ: 1:200	VERT:
TITLE: PRELIMINARY CIVIL PLAN		PROJECT NO. 21-187	ISSUED/REVISION 2
APPROVING AUTHORITY FILE NO.		DRAWING NO. 21-187-CSP	

1733-37 Fairfield Road

A Proposed Development by Aryze

1733-37 Fairfield Road

We acknowledge with respect the Ləkʷəŋən and WSÁNEĆ peoples on whose traditional territory the property stands and the Songhees and Esquimalt nations whose historical relationships with the land continue to this day.

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RE: 1733 Fairfield Rezoning and DP Submission

We are pleased to present this letter outlining the core content of our application to rezone and develop the lands at 1733, 1735 and 1737 Fairfield Road. Enclosed is a summary of a four storey, thirty (30) unit residential strata development, comprising six (6) ground-oriented townhomes and twenty-four (24) single-level residences—ranging from studio to one, two and three bedroom units on the upper floors. The building will include EV-ready surface parking, thoughtfully landscaped grounds, as well as a range of building amenities for owners. We will be seeking to amend these properties from the current R1-G zoning with an OCP amendment to a new site specific (TBD) zoning that could accommodate the development of a low rise multi-residential building.

Since first hosting a preliminary information session on February 28th, 2022—followed by our formal Community Association Land Use Committee meeting on March 28th, 2022—we have received valuable feedback from the community, along with comments from City of Victoria planning staff, including the Advisory Design Panel. This feedback has culminated in some revisions to the proposal, which you’ll find detailed throughout this document.

The biggest evolution of the proposal is in the form of the building design. Feedback from the community, staff and Advisory Design Panel was that the ‘modern mansard’ roof and fishscale metal shingle materials were too dominant and not in keeping with the neighbourhood’s form and expression. In response to these concerns, our approach to the project’s architectural design has been revised at the upper levels of the building.. This, in turn, has enabled us to provide more housing diversity in the proposal, which now includes a broader range of home types.

While some elements of the proposal have evolved, our original project pillars continue to be emphasized in the design—and in many ways—now exceed what we were able to deliver with the previous proposal. Combined, we believe this proposal focuses on the important tenets of city building: placing the right homes in the right places, thoughtful design that acknowledges the neighbouring context and future growth potential, all while preserving urban greenspaces. We look forward to discussing this proposal with you in more detail as we progress throughout the approvals process.



Chris Quigley
Director of Development
Aryze



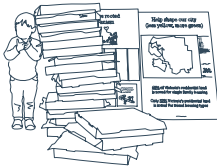
Aryze Philosophy

We're an integrated home building, development and urban planning team committed to increasing the quality of homes and communities in Victoria, BC.



Diversity

A healthy city needs quality housing options across the spectrum, including varying forms of tenure. We build a range of home types, in order to allow new households to form, young families to grow and downsizers to stay in their community.



Engagement

Community discussions are a valued benefit to our process. Members of the community are an excellent repository of the aspirations, needs and challenges of the neighbourhoods we work within and engagement is critical to the success of our projects.



Design

We are a close, cohesive team that is aiming to make a big impact on our built environment. Creative architecture and intelligent design add value to the urban fabric that makes our city more interesting, diverse and fun.



Analytics

Building something just for the sake of building isn't good enough. Proprietary data sets inform our decisions, and help us understand how people in urban areas interact with the built environment. Every Aryze home is created with its neighbourhood in mind.

Project Team

Rotunda, Victoria BC



Aryze Developments

We believe that every neighbourhood should have diverse housing types and tenures for all incomes and demographics. With a focus on innovation across a full spectrum of housing types, Aryze's projects include custom homes constructed in partnership with some of Canada's most acclaimed architects, along with creative urban infill developments built in established neighbourhoods you already know and love.

With nearly two decades of home building experience Aryze combines traditional building methods, innovative construction technologies and intelligent design to deliver architecturally-significant multi-family developments which are attainable to more people in their journey along the housing continuum.

aryze.ca

Bowker, Victoria BC



Cascadia Architects

Cascadia Architects is a 14 person architecture studio based in Victoria, British Columbia. The firm's identity is rooted in their combined 40 years of experience with respected and award-winning firms in Victoria and beyond. Cascadia's areas of practice are broad, and include commercial, institutional, mixed use and custom residential, with a focus on urban and infill projects. Attention to good urban design principles and sustainable construction systems inform their methodology.

The spirit of the firm grows from three simple ideas: creative collaboration is fun; beautiful surroundings enhance our daily lives; and it is the role of the architect to weave together the needs and resources of the client, community, and the natural environment to create spaces that are functional and uplifting.

cascadiaarchitects.ca

1326 Pandora, Victoria BC



Biophilia Design Collective

Biophilia Design Collective is a landscape architecture firm with 13 years experience in landscape design, urban planning and project management. Through their experience with residential, commercial and civic projects, they understand the value of achieving goals through innovative design, conservative planning and cost effective implementation measures.

Biophilia believes that exterior landscape and interior green space should respect, enhance and work symbiotically with architecture and be accessible to people of all abilities to help improve health and well-being. Their focus is to create synergies between the soft plantings and the hardscape aspects of the built environments they create that both relate to and enhance the architecture while ensuring accessibility.

biophiliacollective.ca

Area Context

Room to Grow




The neighbourhoods of Gonzales (and adjacent Fairfield) contain a range of housing types, from heritage homes and bungalows to large estate homes and low-rise multi-family residential buildings. That being said, as a highly desirable neighbourhood with room to grow, there is a historic undersupply of housing choice. Additionally the demand for housing has continued to significantly outpace supply forcing many families to look to other neighborhoods for a place to call home. For example when we look at adjacent neighborhoods with comparable statistics, Gonzales is far behind in both population growth and diversity in housing types being built. By providing the right homes in the right places, we keep our neighborhoods vibrant and healthy by encouraging people in all stages of their lives to live in their neighborhood of choice.

Due to the Gonzales neighbourhood being within walking distance of shops, schools, parks/beaches and health care facilities, it is a highly desirable place to live. The proximity to Hollywood Park, beaches, Fairfield Shopping Center, and health care facilities—as well as Fairfield Road being a transportation corridor—means that this is an ideal place for increased housing choice.

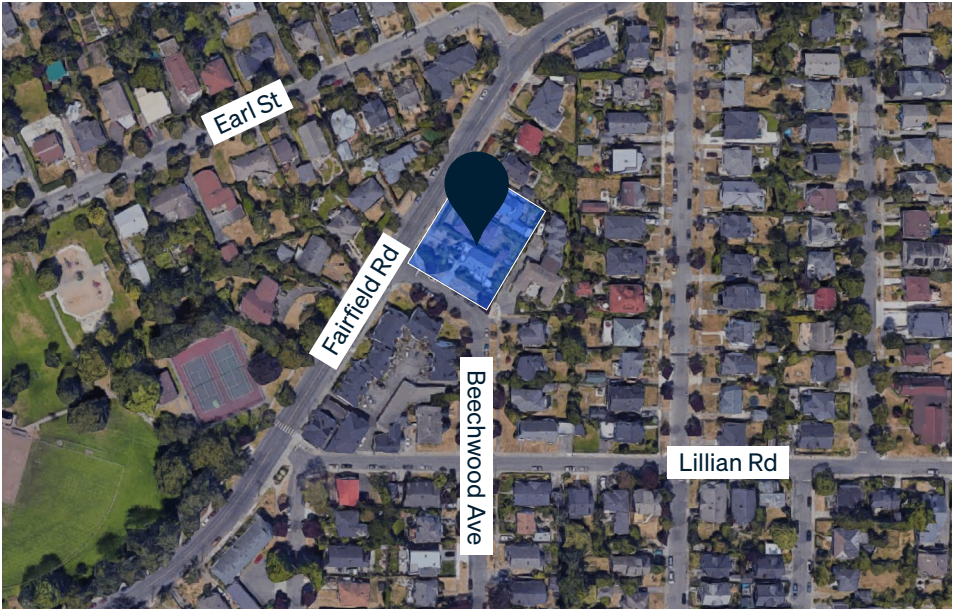
Although the area is predominantly zoned for ground-oriented single family homes, the neighbourhood directives outlined in the Official Community Plan envision the exploration of residential intensification opportunities in the areas in and around the Fairfield Shopping Centre Small Urban Village. Areas along the Fairfield Road corridor are particularly suitable for residential intensification since these sites are well connected to key transit routes while also being highly walkable and bike-friendly. The neighbourhoods are known for their access to numerous parks, schools, retail and other service offerings which makes them great places for increased housing choice for our city’s growing population.



Legend

-  Subject Site
-  Subject Lots 1733-37
-  Neighbourhood Retail
- 1. Thrifty Foods
- 2. Hollywood Park
- 3. Christ Church Cathedral Pre-School
- 4. St. Matthias Anglican Church
- 5. Margaret Jenkins Elementary
- 6. Hide + Seek Coffee
- 7. Abkhazi Garden
- 8. Carousel Childcare
- 9. Gonzales Park

Site Context



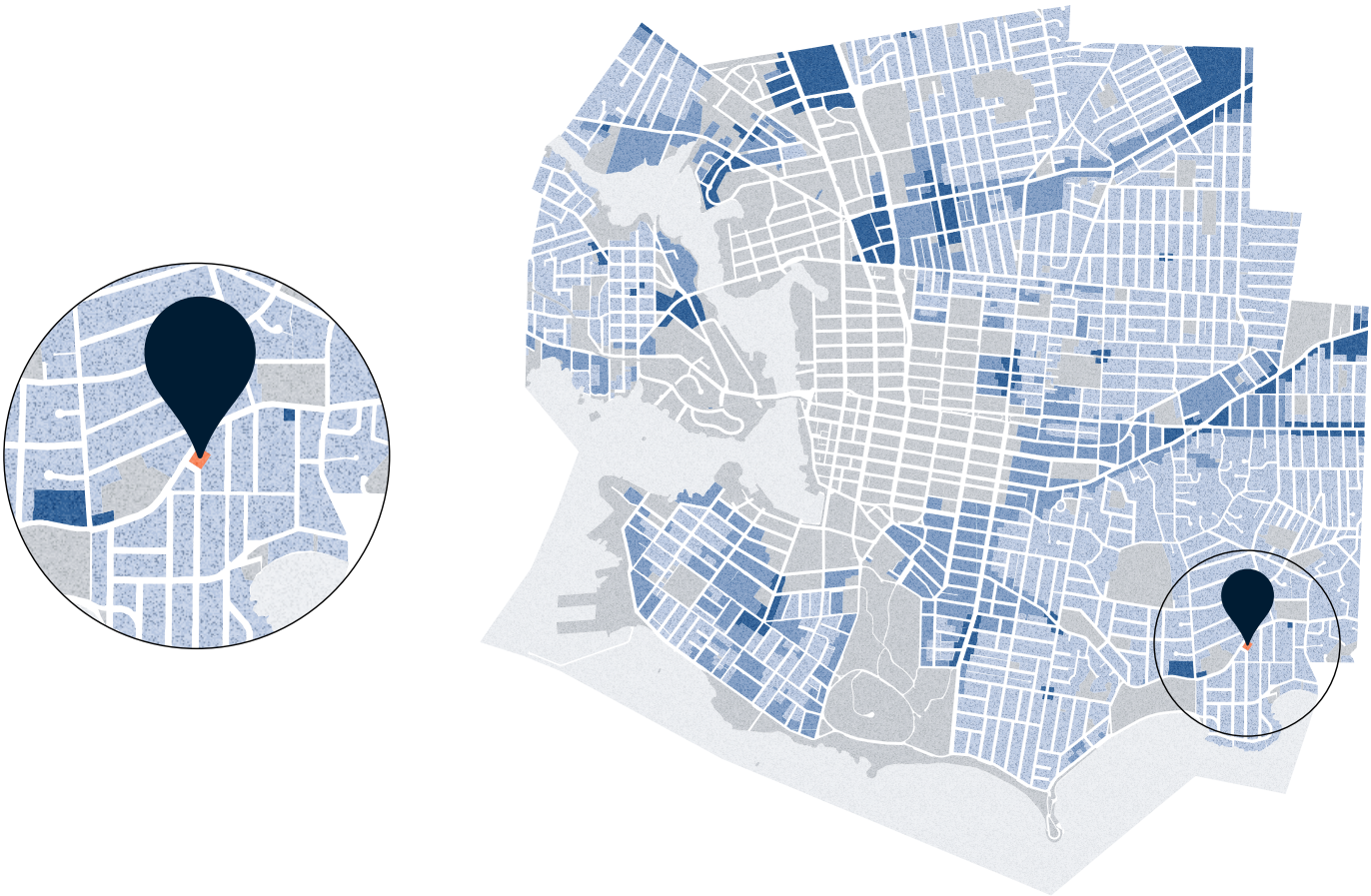
Legend

- Subject Site
- Subject Lots 1733-37

The subject site, located at 1733-1737 Fairfield Road, is a consolidation of three single-family residential properties on the south side of Fairfield, meeting at the corner of Fairfield and Beechwood in the Gonzales neighbourhood of Victoria. Situated along a key transit corridor, the subject site is well positioned to support a growing population and offers many of the day-to-day amenities to promote a ‘15 Minute City’ lifestyle. The site’s direct neighbors are primarily single family homes and local businesses. It is across the street from Hollywood Park, which is a favorite amongst recreators and

Rhodo, a townhome development by Aryze that was completed in the summer of 2022. Within walking distance—or short bike ride—you’ll find schools, beaches, healthcare facilities, numerous parks and the Fairfield Shopping Centre Small Urban Village.

Policy Context



Legend

- Subject Site
- Urban Residential
- Traditional Residential
- Urban Villages & Town Centres
- Others

From a policy context, the subject site holds a Traditional Residential Urban Place Designation and each of the three land parcels that make up this site are currently zoned R1-G for single-family dwellings. At 1.73 FSR, the proposal does exceed the 1.0 FSR density allowance in the Official Community Plan for Traditional Residential. However, for reasons laid out in detail throughout this document, this is a suitable location for an increase in density outside of the Traditional Residential range, and the introduction of a zone allowing for a low-rise multi residential housing type. This

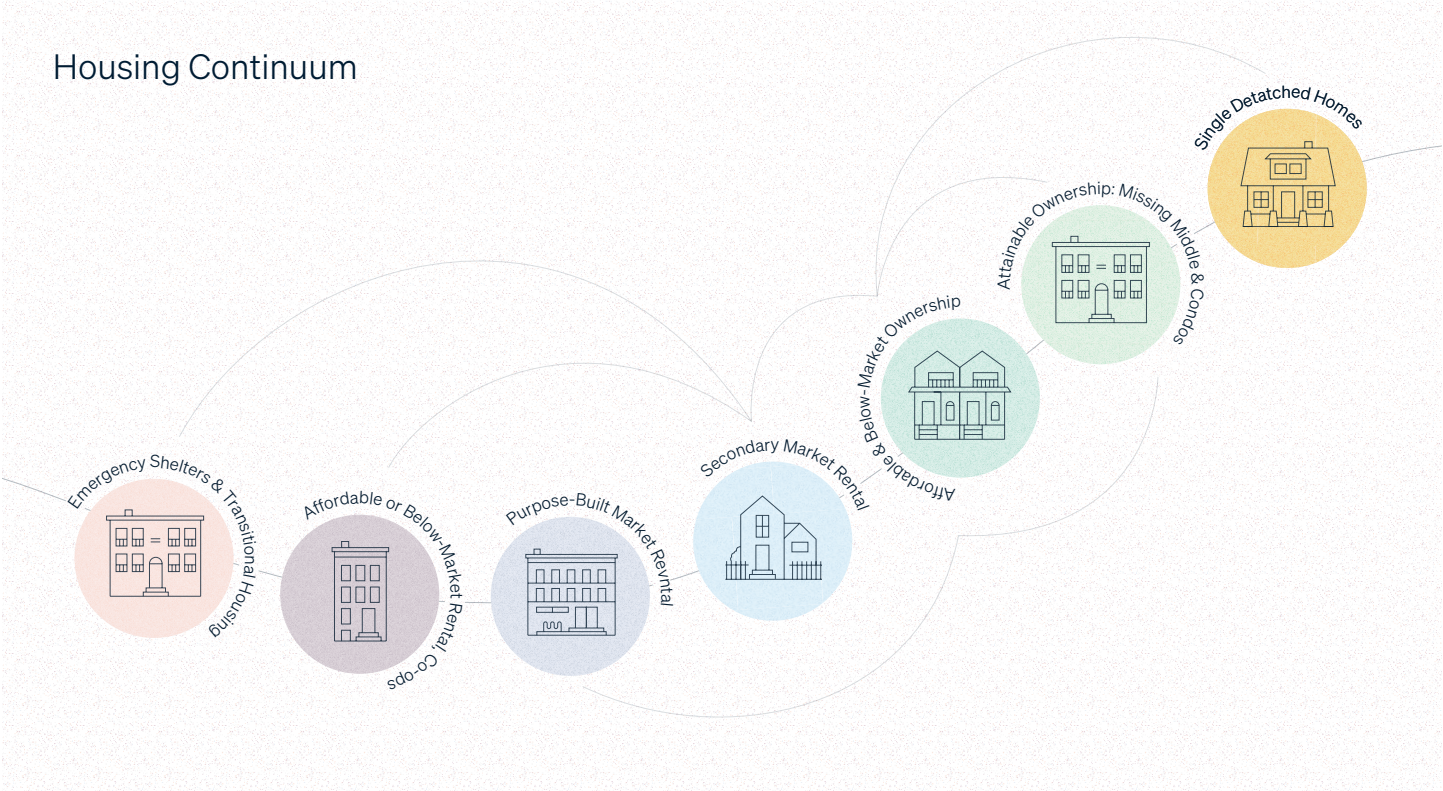
increase in density enables our proposal to include a mix of housing types ranging from studios to three bedrooms plus den which are intended to meet the needs of a greater variety of people at different stages of their life and welcome new families to the neighborhood. This is an important tenet outlined in the Gonzales Neighbourhood Community Plan and one that is central to this proposal.

Project Pillars

Our 1733-37 Fairfield Road proposal has been developed with the consideration of three key project pillars—defining elements responded to through design.

Key Pillars

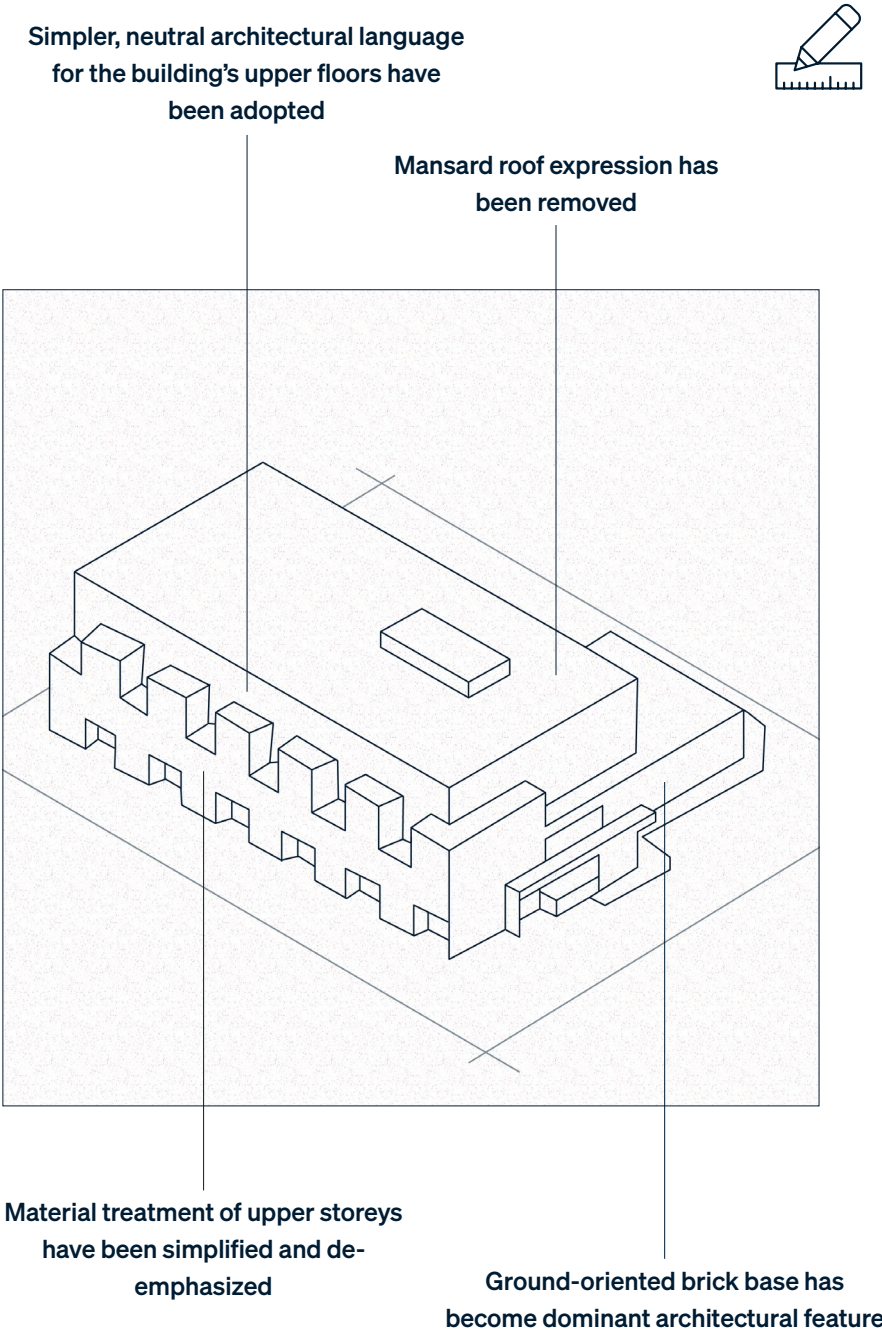
The Right Homes in the Right Places →
Thoughtful Design →
Preserving Green Space →



The Right Homes in the Right Places

The major challenge all growing cities and neighbourhoods are facing is: where should we locate housing to respond to population growth and the changing demographics of established neighbourhoods? It is widely understood that homes should be located near existing services, amenities and other destinations (in areas such as this Fairfield corridor). Locations like these are great candidates for more housing as they demonstrate an efficient use of urban infill land that avoids clearcutting forests on greenfield sites in order to provide the homes people need. The City’s policies speak to these trends with a whole host of strategies that aim to reduce car dependency, showcase climate leadership and promote

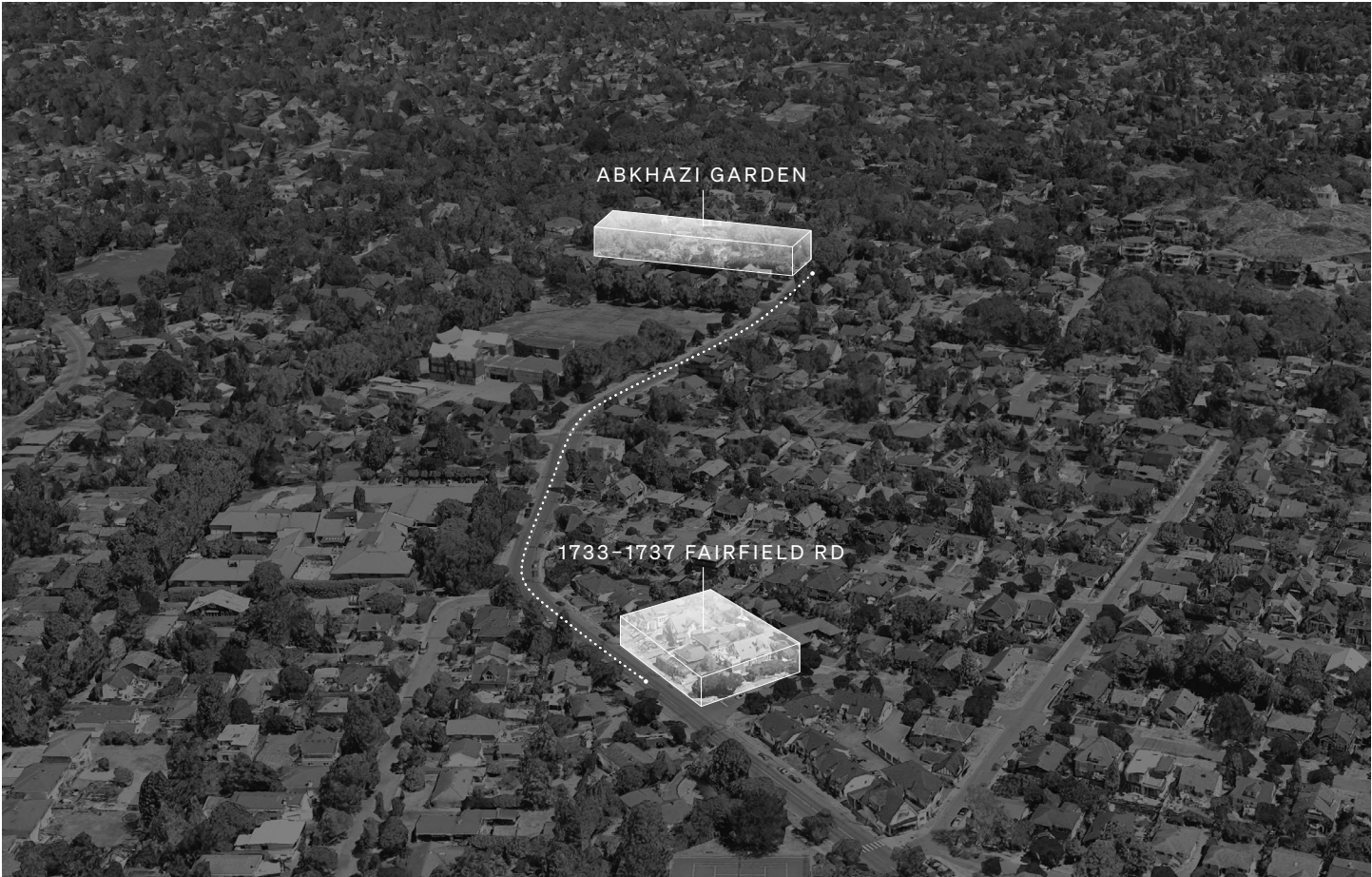
diverse housing choice in existing neighbourhoods. At Aryze we seek to provide a diversity of housing options in our city that will support Victoria’s development as an inclusive, diverse and resilient community. In the short time we have been a business we have worked on an array of duplex schemes, mid-rise apartments—and most recently—the nearby Rhodo townhouse development, located across the street from 1733-37 Fairfield Road. The evolution of this proposal further emphasizes our dedication to this concept, as we have revised the unit mix to include a broader demographic of home buyers beyond the downsizer audience which was originally the main focus for this development.



Thoughtful Design

The concept of ‘thoughtful design’ speaks to our design drivers from a variety of perspectives. First, it speaks to the value of purpose-built and purpose-designed spaces, like these, which have been envisioned with a specific local community audience in mind. Identifying this audience at the outset of our proposal development allows us to acutely address their day-to-day lifestyle needs through design. In this case, not only are we ensuring there are housing options with single-level living, but also units that could be modified to become adaptable if required. Second, ‘thoughtful design’ speaks to the key moves that have been implemented when looking at the overarching site context and complementing the existing neighbourhood grain.

Throughout the evolution of this proposal, we have engaged with the neighbourhood, Council and staff to receive feedback related to the design and how to ensure serve the needs of the community. We have taken these comments into consideration on several aspects of the building such as height, roofline, materials and landscaping. By gathering this information and making the necessary adjustments we were able to create a space that better fits the needs and wants of the neighbourhood, while aligning with key policy objectives.



Preserving Green Space

An important component of this proposal is the long-term preservation of neighbourhood green space through the use of a density transfer. While the concept may seem new to Victoria, a density transfer (or density bonusing) is a common tool used in cities across North America, often where heritage buildings can sell their development rights in order to protect their heritage status. Those zoning rights can be realized by someone else and the density moved to a more appropriate location.

In this instance, Abkhazi Garden is acting as the donor site with the land at 1733-1737 Fairfield Road being the receiver site of the density rights. The density transfer will officially downzone the entire Abkhazi Garden lands to a zone that preserves the site's current activities while also ensuring that the site receives its official heritage designation. Of the available 35,000 sqft of density available from the Abkhazi Garden site, our proposal uses 39% of the density with the balance being released and secured as a financial donation for The Land Conservancy of BC.

What We Heard

Throughout our engagement process, we received valuable feedback from residents. At times this means balancing competing priorities to arrive at evidence-based planning and design solutions that will respond to the planning objectives and embrace the future potential and overall housing needs of our City.

Here we have summarized what we have heard from our dialogue with the community to provide considered responses from the project team. Where we are changing our plans based on community feedback, we have identified these changes. The following sections outline the key themes of the stakeholder feedback we’ve received to date, along with the project team’s response to each theme or area of interest.



Housing Diversity

We have heard concerns about the lack of diversity in housing options within the Fairfield/Gonzales neighborhood, which consists primarily of single family homes. As our population and demographics change, we need to build housing that suits the current needs of our community. By redesigning the building to provide a broader variety of unit types and sizes at different price points, the homes will inherently suit a broader range of lifestyles and demographics including individuals, couples, families and seniors.

This is a positive evolution to the proposal, as we believe that every neighbourhood should have diverse housing types and tenures for all incomes and demographics. A densified, compact, walkable lifestyle is critical to solving our climate and housing crisis; all while creating more livable and healthier communities.



Roof Form/Design

Our feedback from the community, staff and Advisory Design Panel was that the ‘modern mansard’ roof and fishscale metal shingle materials were too dominant and not in keeping with the neighborhood’s scale and expression. In response to these concerns, our approach to the project’s architectural design has been substantially revised. The mansard roof has been removed and replaced with a conventional flat roof which de-emphasizes the building’s upper floors. The material palette has been revised to a more aesthetically familiar finish with different points of interest such as the brick treatments, metal cladding and wood finishing. In making these modifications to the top floor we have the added benefit of changing the floor plans to increase housing diversity within the building and layer in attainable housing options.



Attainable Housing

Changing the design of the building to include more housing diversity allows us to assist more people in their homeownership journey, thus providing them the opportunity to enter into a housing market—and neighbourhood—that may traditionally be out of reach. Currently, many first-time home buyers and young families who want to live in the Gonzales neighbourhood are not able to find a home that is within their budget range, forcing them to look elsewhere for housing. By offering a range of housing choice in an established neighbourhood with room to grow, we can help build a community with a diversity of incomes, lifestyles and demographics.



Parking Capacity

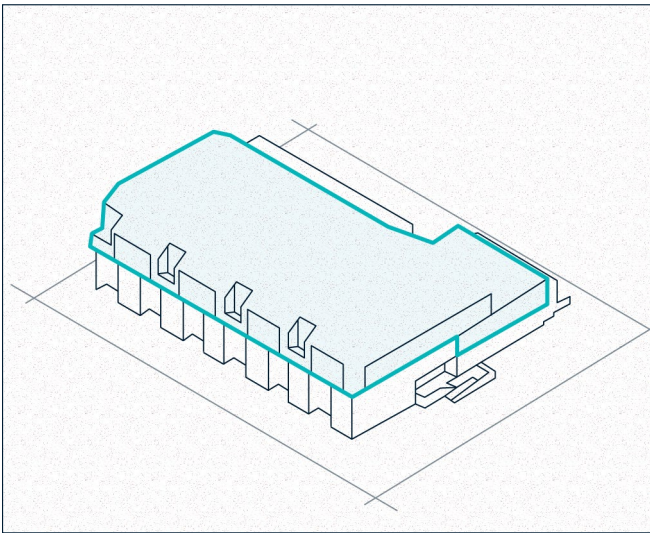
We understand that parking capacity is a concern for neighbours, so we executed a Parking Capacity Study to examine the availability of parking in the nearby area. Bunt & Associates’ On-Street Vehicle Parking Supply and Demand Study concluded that the observed area has a total of 81 parking spaces with a peak observed demand of 40 spaces (49%). There are 41 resident only parking spaces within an approximate one-block area from the site and peak demand periods only reduced availability by 16 spots, leaving 25 vacant spaces available. There are 31 publicly available parking spaces within an approximate one-block area from the site and these spaces had a maximum observed demand of 19 spaces which equates to 61% occupancy—approximately 12 vacant vehicle spaces.

Site Layout
& Massing

Step 2

Step 1

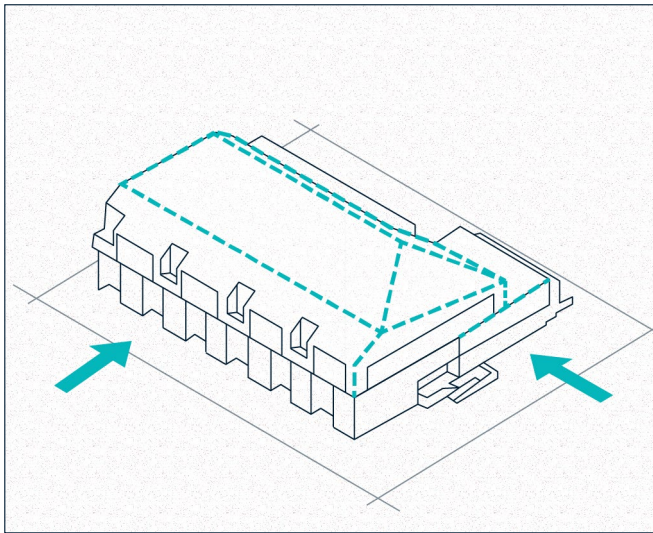
What We Heard



- The projecting walls at level 3 are too dominant and increase the building's perceived height and massing.
- The complex geometry of the proposal's roof creates an aggressive look and feel.
- The fishscale materiality is out of keeping with the surrounding residential neighbourhood.

Step 2

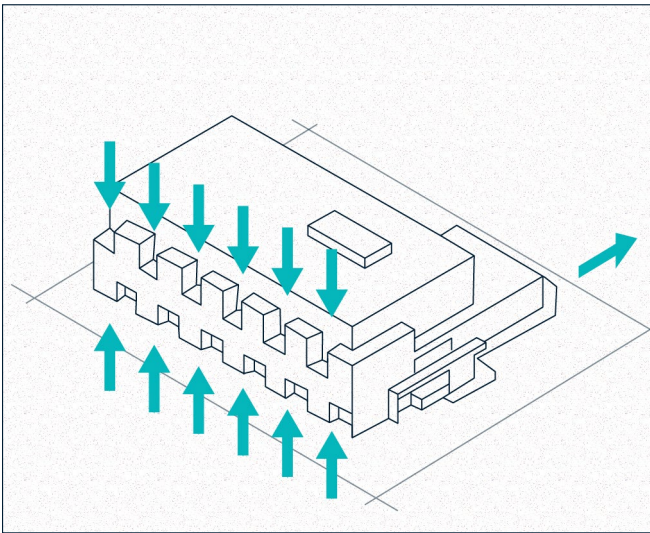
Simplify the Roof



- Reduce the visual complexity of the proposal's roofline, creating a calmer and less dominant expression to its upper floors.
- Step the proposal's upper floors back at the 3rd and 4th levels.

Step 3

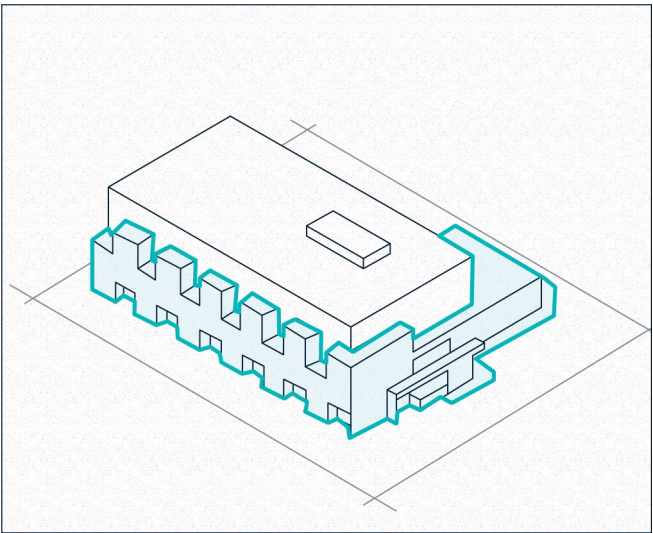
Articulate the Base



- Create a distinctive 2 and 3 storey language of alternating brick bays along Fairfield Road.
- Extend the brick material palette along the full extent of the Beechwood Avenue frontage.

Step 4

The Results



- The mansard and gable end wall roof expression have been removed and a simpler, neutral architectural language for the building's upper floors have been adopted.
- The material treatment of upper storeys have been simplified and de-emphasized.
- The upper storeys step back from the building face.
- The building's ground oriented brick base becomes its dominant architectural feature.

Building Form & Design

The proposed architectural expression is understated elegance that sensitively responds to site context and meaningfully contributing to the overall neighborhood aesthetic.

The previous projecting mansard expression with gable end wall entirely was redesigned to a conventional flat roof with parapet construction approach. The four storey volume therefore steps back considerably from the building face, in alignment with the Design Guidelines for Multi-Unit Residential Buildings, de-emphasizing the prominence of the Proposal's upper floors from its primary aspects. The Project's proposed brick treatment has become its new primary means of articulating human scale. A rhythmic A-B pattern of two and three

storey brick volumes with picket guards along Fairfield Road provide visual interest along this frontage in lieu of the variegated dormers and mansard roof formerly dominating this elevation. At the corner of Beechwood Avenue and Fairfield Road, the three storey brick base transitions downwards to a two storey volume, appropriately weighting the brick's mass at this prominent corner and transitioning to a smaller scale towards the lower density residences behind. Variations in brick coursing, transitions in brick depth, and a unique

brick screen detail at the project's primary residential entrance provide further emphasis on this high quality, durable material and visual interest to the project as a whole.

Mechanical units have been relocated to the roof adjacent to the elevator overrun. These units are screened in metal matching the Project's exterior finish, and located away from the building's edges in order to avoid significant sightlines from adjacent properties and the public realm.

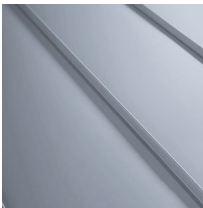


Materiality

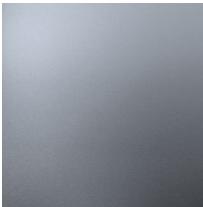
Materials



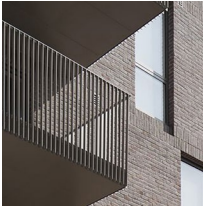
Light Brick



Metal Standing Seam



Prefinished Metal



Metal Picket Guard



Glazing



T&G Wood

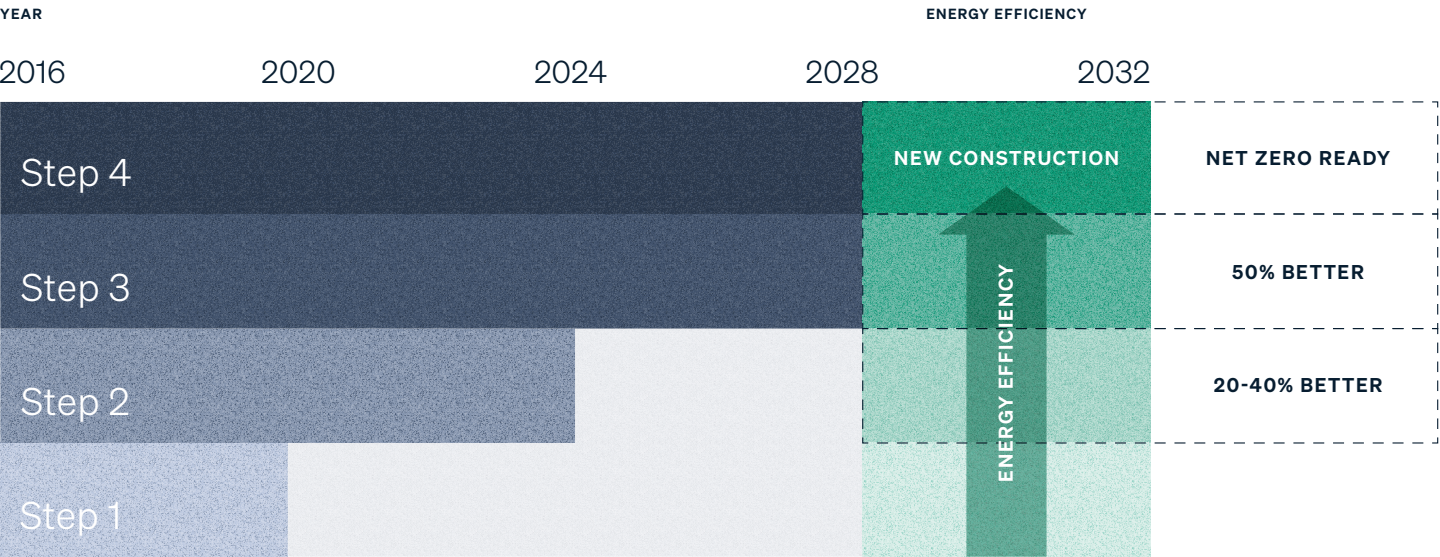
The material palette has been revised to a more aesthetically familiar and contextually sensitive standing seam and flat panel metal finish from the previous fishscale metal material palette. Instead the emphasis is weighed on the two and three storey brick base and de-emphasizing the impact of the third and fourth storey metal clad volume behind and above.

The primary residential entrance includes a vertical wood grain finish and is protected by a projecting brick clad canopy. The wood finish creates a warm, rich quality to the residential entrance while the projecting brick canopy balances the Beechwood Avenue elevation with appropriate weight and emphasis on the pedestrian realm in relation to the stepped back metal clad volume above.

The material treatment of Levels 1 to 3 has been partially revised at the NE and SE facing elevations. It is now clad in a metal finish in keeping with the proposed treatment of Levels 3 and 4 at all elevations, more suitably tying to this newly proposed material concept and allowing for the concentrated application of brick to the Project’s highly prominent aspects at Fairfield Road and Beechwood Avenue. Continuous planters are proposed at the southernmost balcony and outdoor common area located on Level 3. These planters buffer the balconies from Beechwood Avenue, the adjacent residential property, and the parking area, providing a softened edge and visual interest in this location.

Green Building Strategies

Transit-Oriented Infill Development



Transit-Oriented Infill Development

With the growing body of research indicating that densification holds the key for cities’ fight against climate change, reducing our dependence on cars is the most significant component of reducing greenhouse gas emissions. The central location of 1733-1737 encourages a car-lite lifestyle, offering alternative transportation modes like walking, cycling and transit as primary options for future residents. In order to achieve our long term climate goals we must make decisions now that will create a more sustainable future.

High-Performance Building Envelope

1733-1737 Fairfield Road is designed and constructed to BC Step Code 3, in accordance with the City of Victoria’s phased Step Code guidelines which were updated as of January 1st, 2020. Step Code 3 represents an incredible 50% increase in efficiency. This includes designing the building systems in a way that will reach high levels of performance in Thermal Energy Demand Intensity (TEDI), Total Energy Use Intensity (TEUI), and airtightness. Overall, this works to successfully improve energy efficiency and reduce energy costs.

100% Electric Building

The single biggest sustainability measure is our decision to proceed with a comprehensive renewable energy supply. This will result in a major reduction of GHG emissions produced during the building’s operational lifetime—a meaningful contribution to the City’s efforts to respond to the causes and impacts of climate change. 1733-37 Fairfield Rd will set a strong example of a low carbon, high performance condominium building in alignment with the principles and targets outlined in the City of Victoria Climate Action Plan.

Renderings

Beechwood Elevation Perspective

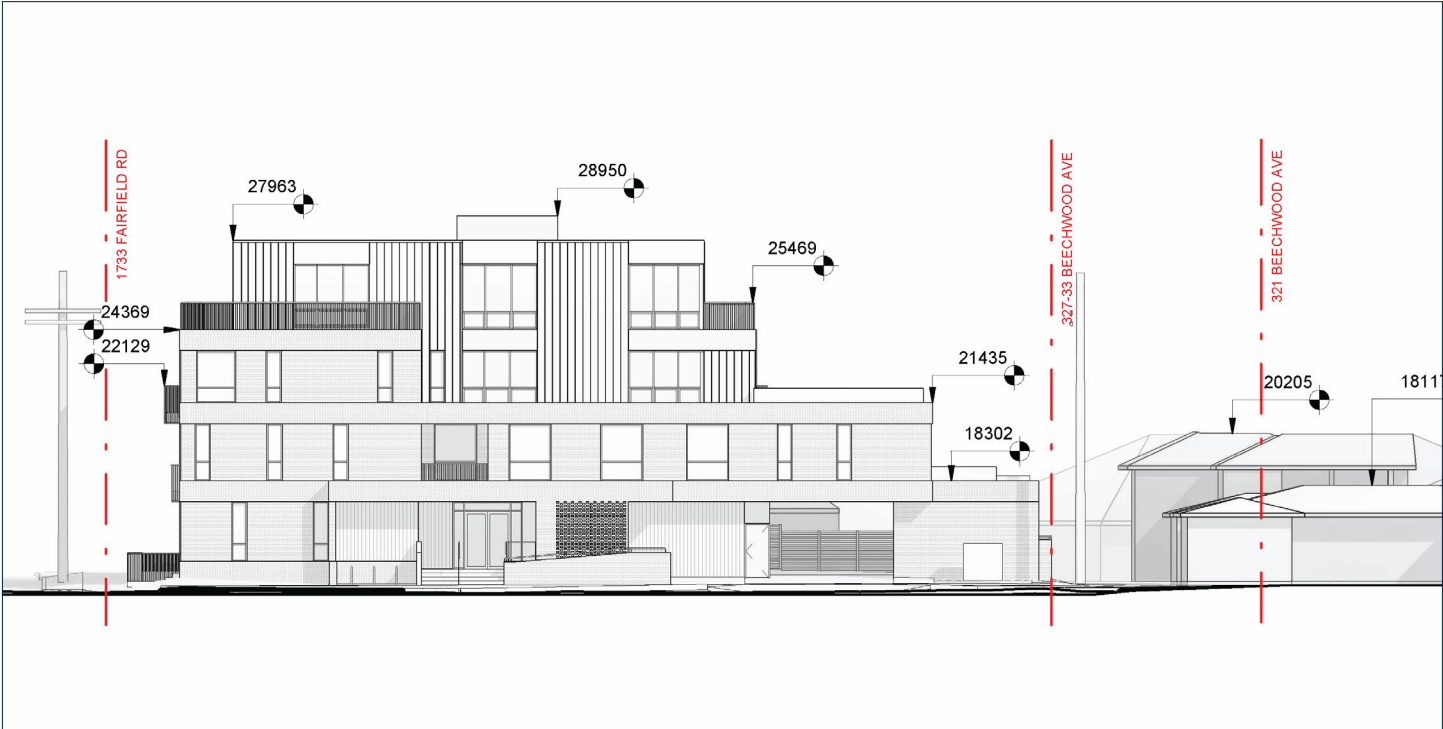


Fairfield Pedestrian View



Elevations

Beechwood Avenue



North Elevation



Project at a Glance



Height
4 storeys



North Setback
2.33m



Number of Homes
30 Homes



Short-term Bike Stalls
6 Stalls



- Unique Features
- Oversized storage lockers for residents
 - Zero-Carbon/100% Electric HVAC and hot water delivery systems
 - The building will target (and in some areas exceed) BC Step Code 3 energy efficiency standard

Site Area
18,751 sqft

East Setback
2.62m

Townhomes
6 Townhomes

Long-term Bike Stalls
54 Stalls

Floor Area
32,132 sqft

South Setback
0.50m-4.78m

Studio
2 Homes

Parking Stalls
23 Stalls
(2 Van Accessible)

FSR
1.71 FSR

West Setback
2.73m

1 Bedroom
13 Homes

Car Share
Modo Car Share

Site Coverage
65%

2 Bedroom
3 Homes

2 Bedroom + Den
4 Homes

3 Bedroom
2 Homes

Residential Tenure
Strata Ownership

Floor Plates



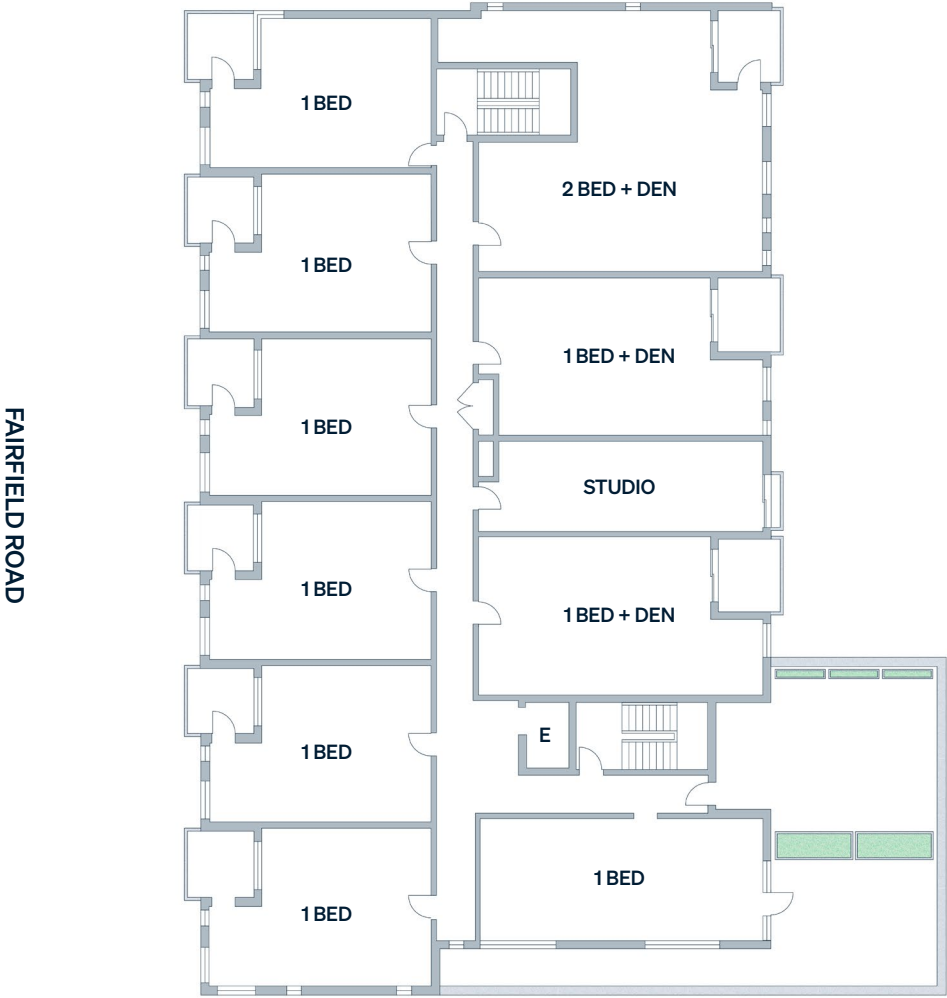
Ground Level

Level 2



Floor Plates

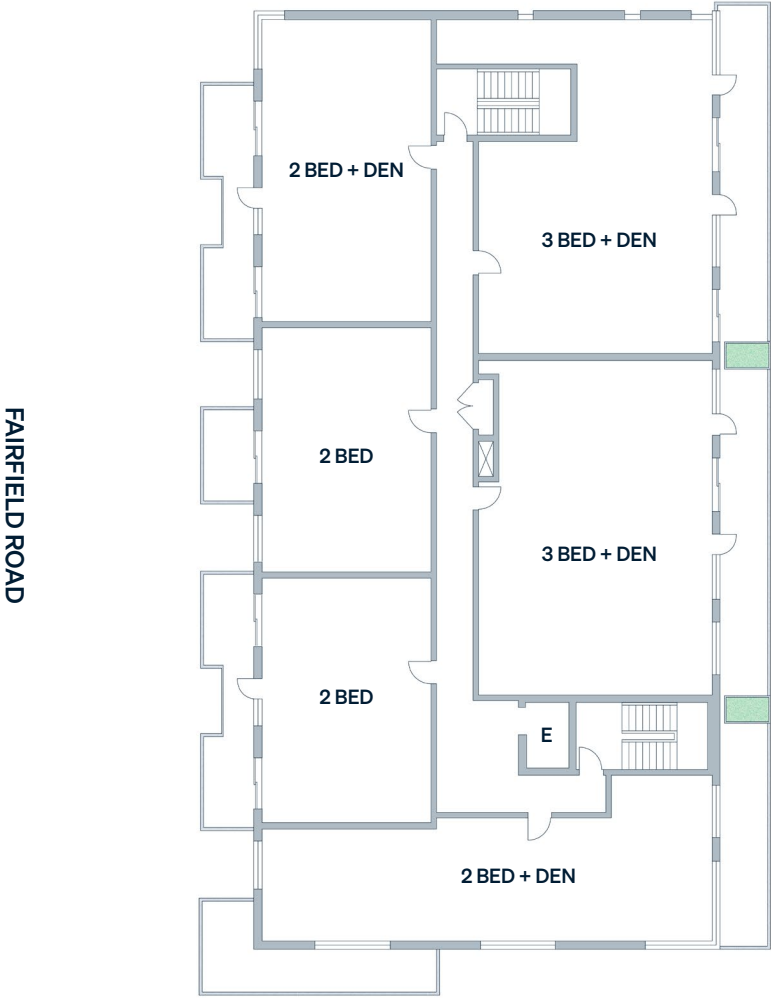
Level 3



BEECHWOOD AVENUE



Level 4



BEECHWOOD AVENUE



Street Views

Fairfield Road



Beechwood Avenue



Mobility Context

The site is located centrally within the Gonzales neighbourhood and is well connected to Oak Bay village centre to the north-east and approximately 3km to downtown Victoria to the west.



Road Network

The area is well served by local, collector and secondary arterial road networks. The immediate neighbourhood is made up of short neighbourhood blocks, which is indicative of a fine grain road pattern. This road design allows for ease of movement through the area as there are multiple connectivity options in all directions. From the subject site, east/west movements are captured by Fairfield Road, which connects to Cook Street and Downtown, while Foul Bay Road connects to the north/south. These corridors are mere steps away from the proposed development at 1733-1737 Fairfield Road and serve as the main corridors for vehicles, buses, pedestrians and cyclists alike.



Car-Lite Lifestyle

Overall, the central location of this subject site lends itself well to a car-lite lifestyle. Being in close proximity to numerous amenities as well as being located on a central transportation corridor with ample cycling and pedestrian pathways—makes alternate modes of transportation not only attractive, but preferable. As such, the proposal has been designed to encourage walking, cycling and transit as the primary transportation options for future residents. In addition to providing 24 EV ready parking stalls, we have included 52 secure bike stalls (including both standard and oversize/cargo bike stalls). The project will be providing a new car share vehicle which will benefit the new residents of the project and the wider community. Looking ahead, we anticipate future residents of 1733-1737 Fairfield Road to be making an increasing number of trips on foot to meet their daily needs as the local services and retail amenity options expand or use the provided car share rental program when longer trips are required. Curiously, when we surveyed residents of our Rhodo development we found that 77% of residents only own 1 car, 61% live and work in the same neighborhood, and 33% bike or walk as primary mode of transportation.



Cycling Networks

The site is well connected to both walking and cycling networks. All streets surrounding the development have side walks on both sides. Fairfield Street and Crescent Road are signed bike routes. Downtown can be accessed in 15 minutes via Fairfield Street or via the nearby Richardson Street to the north. Continuous bike lanes on Foul Bay Road and Henderson Road provide residents with a direct cycling route to Camosun College Lansdowne Campus and the University of Victoria. Plans call for the existing signed bike routes to be upgraded and protected creating a continuous, AAA cycling route that connects the site's neighbourhood to Victoria's downtown area. With its substantial volume of bike parking, the proposed development at 1733-1737 Fairfield Road is well positioned to support the anticipated cycling demand.

Transportation

Transit Network

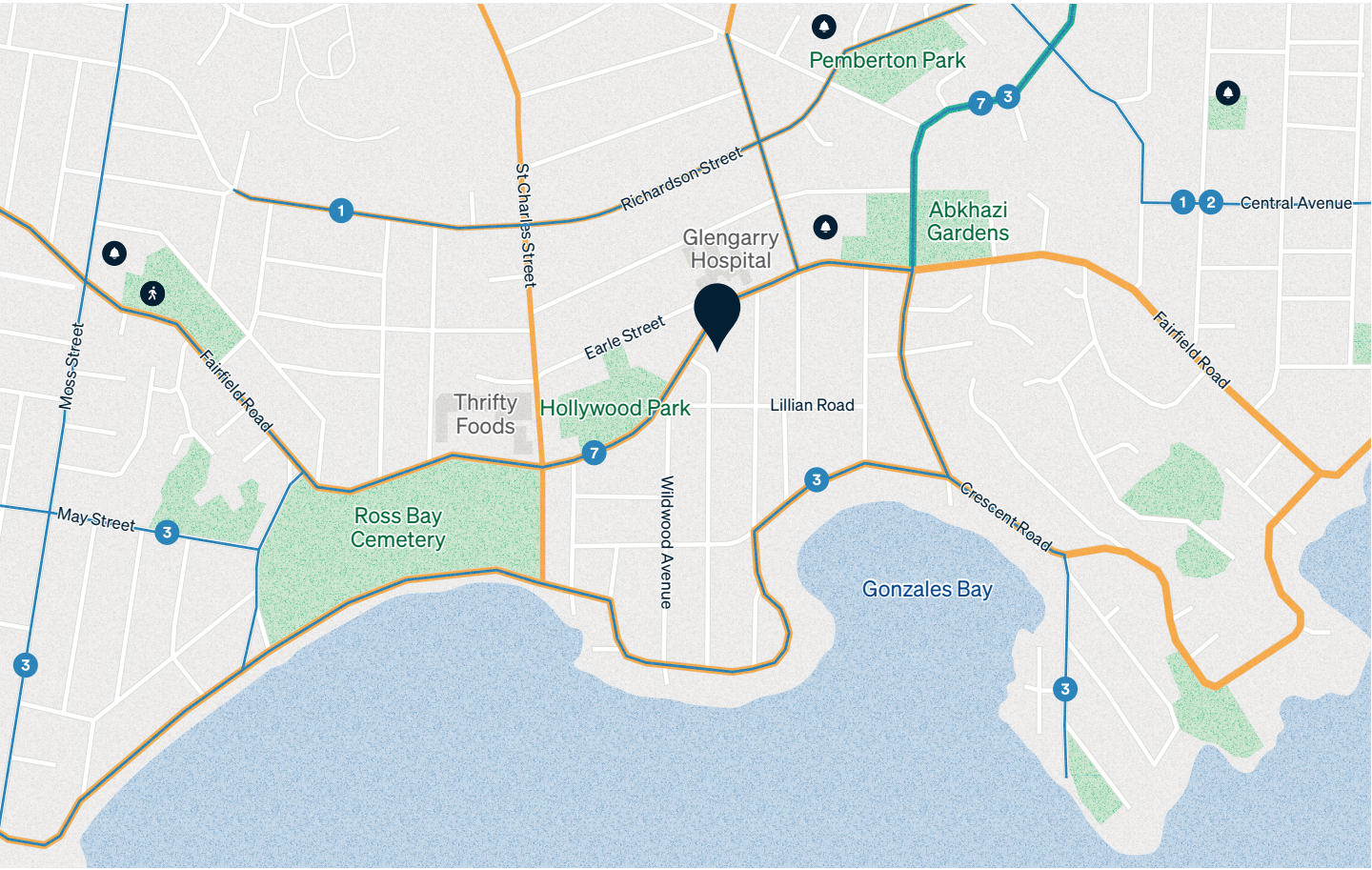
1733-1737 Fairfield Road is well served by public transit, with three transit routes within 800 metres of the site (an approximate 10 minute walk). The site is located less than 200m from stops on regional route #7, and 600-800 m from stops on local routes #1 and #3. Downtown or the University of Victoria is approximately a 16-minute trip on route #7 from the site, and Camosun College’s Lansdowne Campus is an approximate 14-minute trip. The table above shows the transit routes and bus frequencies (in minutes) that service this site.

Anticipated Traffic and Parking Impacts







The proposed 30 unit, 4-storey, building would have a trip rate of 0.4 vehicle trips per unit per weekday PM peak hour (Institute of Transportation Engineer Trip Generation Manual). This means that approximately 12 vehicle trips will be entering or exiting the site per peak hour. This quantity of vehicle traffic results in a negligible impact on the local road network and is not anticipated to have a significant impact on adjacent road operations. Additionally, we collected data as to the parking availability in close proximity to the site. During peak periods less than half of the available parking spaces were being utilized thus over 40 spaces remained open for residents and guests.

Route Frequency

#	Bus Route Name	AM	Mid-Day	PM	Evening	Weekend
1	South Oak Bay / Downtown	45	-	45	-	-
3	James Bay / Royal Jubilee	30	65	30	-	60
7	UVic / Downtown	15	20	15	30	30



Legend

-  Subject Site
-  School
-  Recreation Centre
-  Bus Routes
-  Shared Streets
-  Bike Lanes/Road Shoulders

Community Engagement



Our Commitment

We are committed to being good neighbours and having honest, open dialogues within the communities where we do our work. We are available to discuss project details with stakeholders through a variety of channels to build trust and shared vision for the project all while maintaining a respectful and open conversation. Our goal is to create an atmosphere where people feel comfortable to share their ideas, hopes and aspirations for the community.

Timeline	
11 February 2022 →	Neighbourhood letter distributed to neighbours
14 February 2022 →	Preliminary CALUC event postcards delivered
28 February 2022 →	Digital CALUC invitation sent to online registrants
28 February 2022 →	Preliminary CALUC and presentation
01 March 2022 →	Density transfer proposal press release
01 March 2022 →	Abkhazi Garden Community Information Session (CIS)
10 March 2022 →	Formal CALUC event postcards delivered
14 March 2022 →	Digital CALUC invitation sent to online registrants
28 March 2022 →	Digital CALUC invitation reminder sent to online registrants
28 March 2022 →	Formal CALUC and presentation
08 April 2022 →	Digital CIS invitation: Density Transfer Partnership
12 April 2022 →	Density Transfer CIS event hosted by The Land Conservancy
12 December 2022 →	Letter distributed detailing project additions and revisions
03 February 2023 →	City of Victoria resubmission notification letter distributed to neighbours
13 March 2023 →	CALUC #2 Event Postcards delivered
13 March 2023 →	Digital CALUC invitation sent to online registrants
27 March 2023 →	Digital CALUC invitation reminder sent to online registrants
27 March 2023 →	CALUC #2 and Presentation

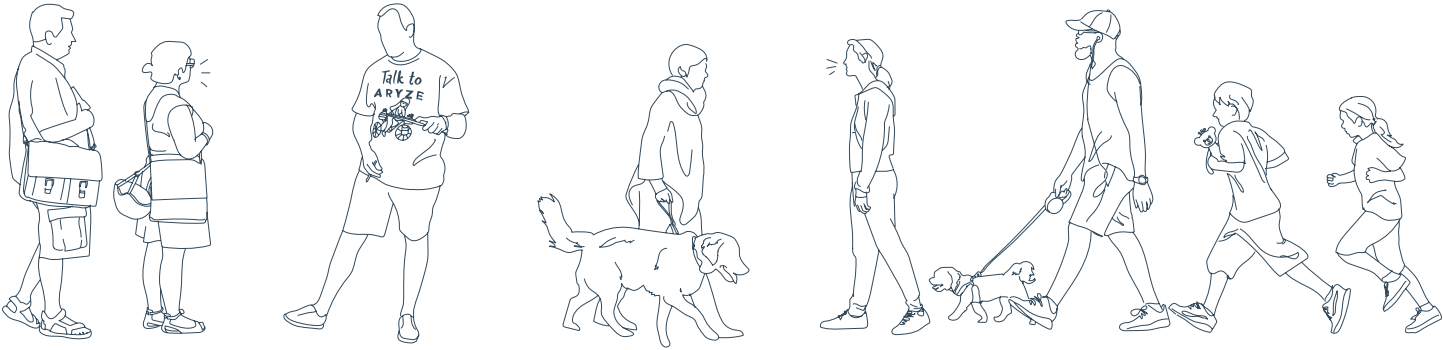
Tenant Assistance

The Aryze policy is to exceed the TAP program requirements in both offers of financial assistance (for rent and moving expenses), but also by offering numerous opportunities for relocation throughout our many purpose-built rentals (PBRs) across Victoria.

We're working closely with the City and their Tenant Assistance Policy on a number of projects, allowing us to take a proactive approach for those who require relocation support.

The official TAPs support has commenced alongside the submission of our Development & Rezoning Permit Applications and we continue to

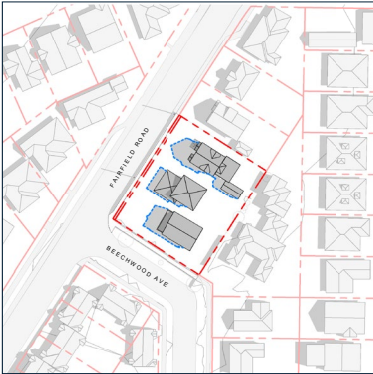
maintain ongoing communications with the tenants so they feel informed and supported throughout the relocation process.



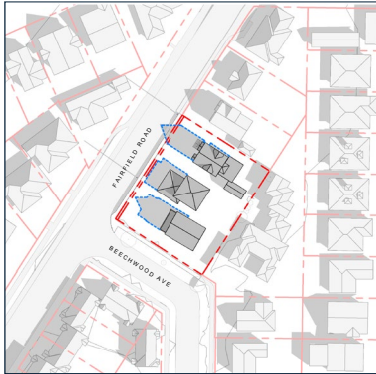
Shadow Studies

Existing

Summer Solstice 9:00am



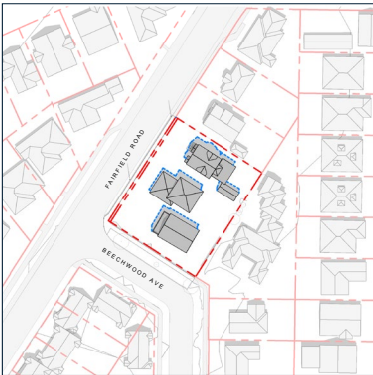
Equinox 9:00am



Winter Solstice 9:00am



Summer Solstice 12:00pm



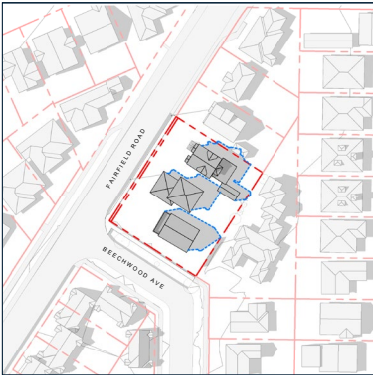
Equinox 12:00pm



Winter Solstice 12:00pm



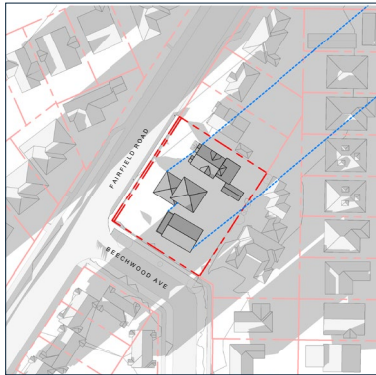
Summer Solstice 4:00pm



Equinox 4:00pm

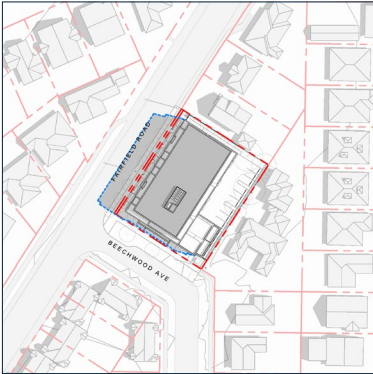


Winter Solstice 4:00pm

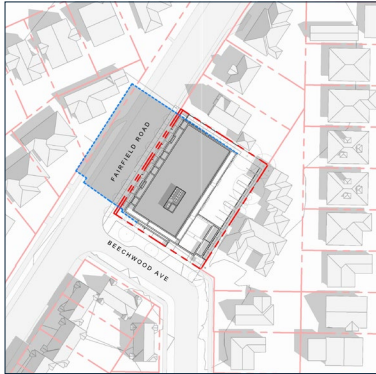


Proposal

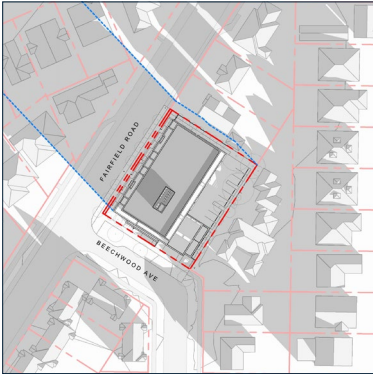
Summer Solstice 9:00am



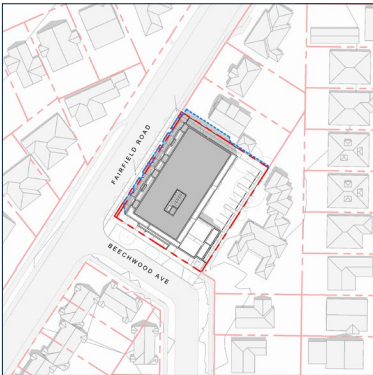
Equinox 9:00am



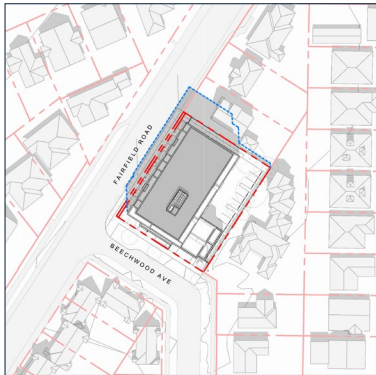
Winter Solstice 9:00am



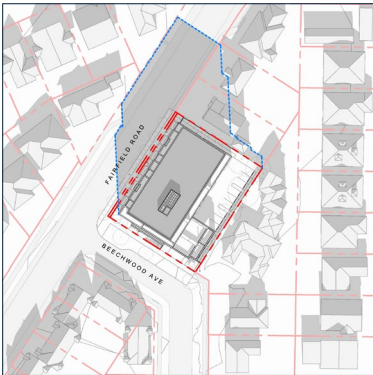
Summer Solstice 12:00pm



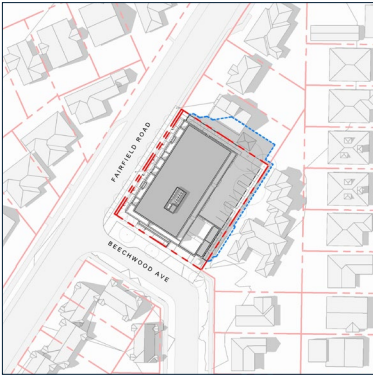
Equinox 12:00pm



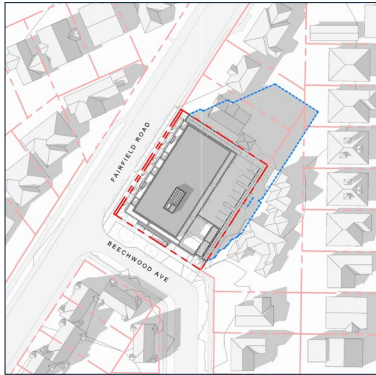
Winter Solstice 12:00pm



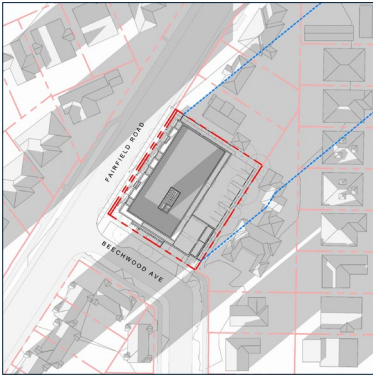
Summer Solstice 4:00pm



Equinox 4:00pm



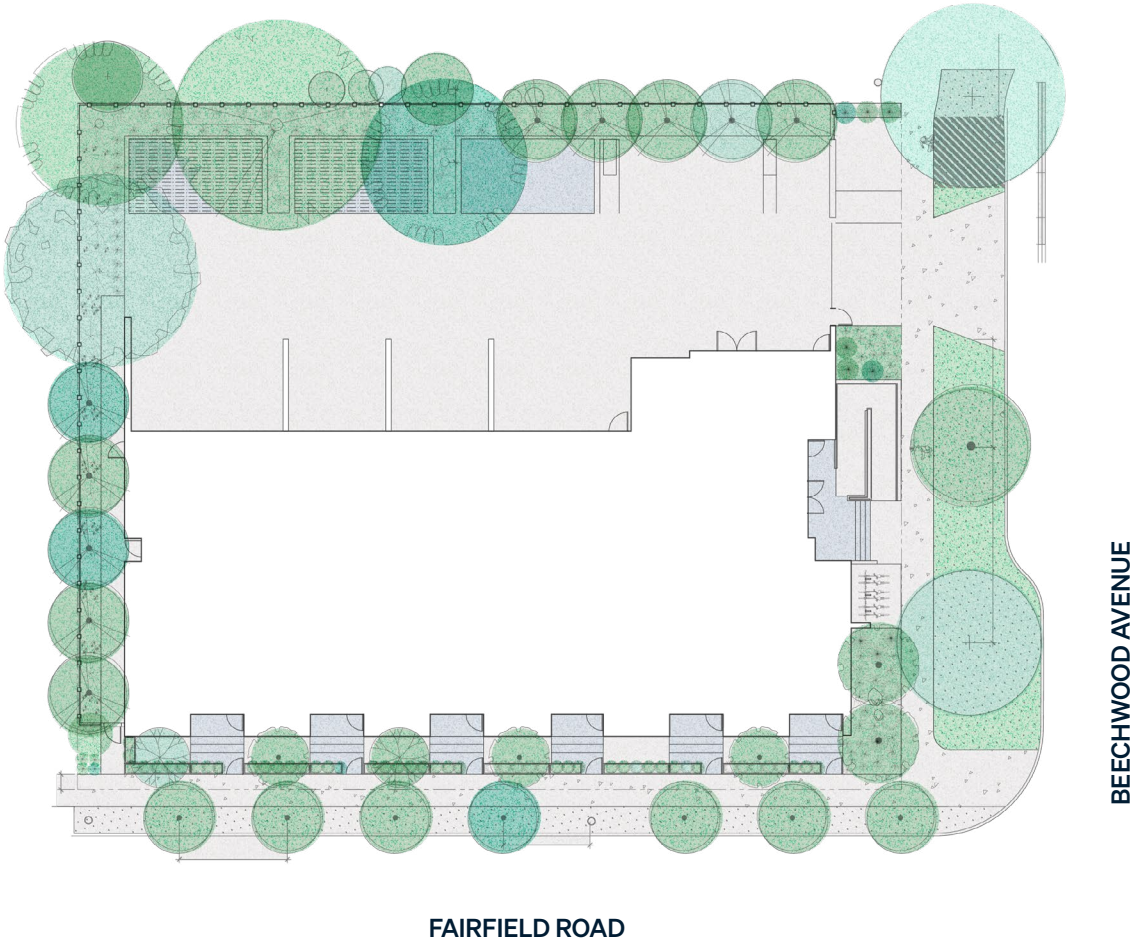
Winter Solstice 4:00pm



Landscape Plan

The landscape design proposed seeks to create a public streetscape that enhances the pedestrian experience while providing an enjoyable green space in private residential spaces.

Ground Floor Plan



Trees



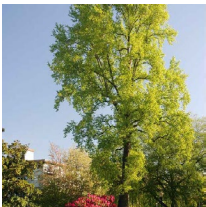
Paperbark Maple



Autumn Applause White Ash



Ruby Vase Persian



Tulip Tree

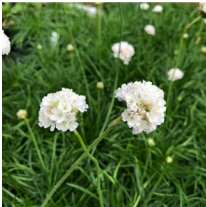


Japanese Maple



Pin Oak

Plantings



White Flowered Thrift



Kinnikinnick



New Zealand Hair Sedge



Spreading Plum Yew



Mexican Orange Blossom

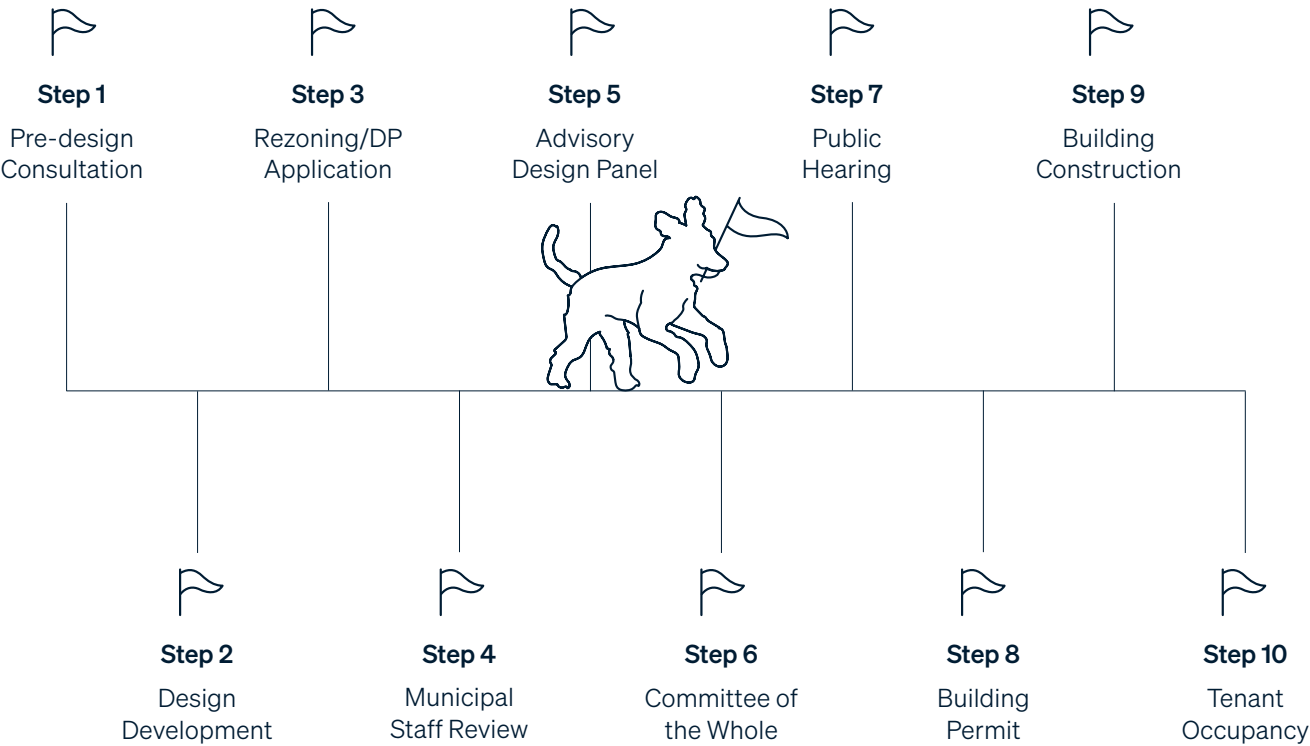


Chinese Silver Grass



Dwarf Purpletop

Application Process



Contact

Aryze Developments
1839 Fairfield Road
Victoria, BC V8S 1G9

(250) 391-2038

Register Online
aryze.ca/1733fairfield ↗

1733-37 Fairfield Road

A Proposed Development by Aryze

12 September 2023

Re: 1733-37 Fairfield Rd - Design Changes

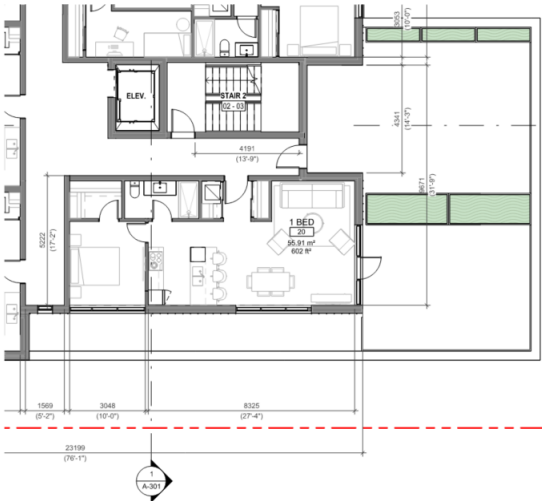
Dear Patrick,

Following on from our recent resubmission we have continued to study all aspects of the project to ensure it is financially viable at the same time as aligning with the design and planning principles consistently expressed since the proposal was first developed. In the short period from the first application submission to now the financial picture has changed in the country with interest rates moving from 1.5% to 5%. This presents a significant challenge for delivering new housing and we have therefore made the decision to amend the project in a small way to help set the project up for success, if approved.

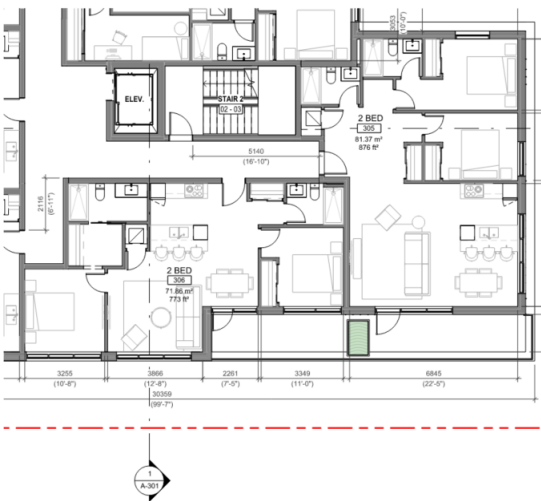
We are proposing to amend the current DP drawings in the following way:

- Convert a 1-bedroom home on L3 to a 2-bedroom home.
- Add an additional 2-bedroom home on L3 in lieu of the shared outdoor amenity space.
- We can achieve this without significantly affecting the building layout by re-sizing unit #20, shrinking the exterior patio from 63.2 m² to 20.6 m², and adding 12.6m² into the existing unit.
- Remove the shared amenity space with the understanding that all homes are provided with private outdoor space and the site is in close proximity to shared public spaces such as Hollywood Park and Gonzales Beach.
- Taken together this change will provide an increased number of larger homes in the project which are in significant demand in the city. All of this is achieved with keeping the same design intent of a stepped down building on Beechwood Avenue.

Existing Proposal



Revised Proposal



Please see changes to specific metrics in the chart below:

	Existing Proposal	Revised Proposal	Change
FLOOR SPACE RATIO	1.73:1	1.77:1	+ 0.04 FSR (2.3%)
SITE COVERAGE (%)	65%	65%	No change
OPEN SITE SPACE (%)	23%	23%	No change
TOTAL NUMBER OF UNITS	30	31	+1 home
UNIT TYPE	TOWNHOMES (6), STUDIO (2), 1 BEDS (13), 2 BEDS (7), 3 BEDS (2)	TOWNHOMES (6), STUDIO (2), 1 BEDS (12), 2 BEDS (9), 3 BEDS (2)	Additional 2-bedroom homes.

Adding one home to the project will have a minimal impact on transportation. The number of trips anticipated by car will increase marginally with no noticeable impact on the road network. The number of parking spaces remains at 23 and the Transportation Demand Management package being proposed can easily accommodate this small increase in demand. In summary:

- The proposed MODO car share will be provided to offset the parking variance, along with extra provision of bike parking.
- Bike parking
 - Long term - Schedule C will require 39 and we are providing 54
 - Short term - Schedule C will require 6 and we are providing a 6 bike rack

Finally, the conclusion of the Land Lift analysis was that the agreed upon payment for the density transfer is slightly higher than the estimated supportable CAC value. We are therefore confident that the increase in gross floorspace (97sqm or 3.2%) will not alter the conclusion of the original analysis. This is further reinforced by the changing interest rate situation since the original analysis was undertaken. This has increased the financing fees and therefore reduces the residual land value and thus reduces the supportable CAC value.

We trust this letter provides sufficient information on the proposed change. We propose making a complete drawing submission following Committee of the Whole so that we can also incorporate any further changes that come to light in the Council discussion. If you require further information please do not hesitate to contact us.

Thank you,



Chris Quigley
Director of Development
Aryze Developments



November 23, 2023

Mayor and Council
City of Victoria
1 Centennial Square
Victoria, BC V8W 1P6

Re: Abkhazi Garden rezoning application

Dear Mayor Alto, Council and Staff:

This letter summarizes the intent of our application to amend the zoning of Abkhazi Garden in order to protect this important community asset for the people of Victoria and further afield.

Primarily following direction outlined by the City Planning Staff (in the appended May 24, 2014 letter from City Staff), our aim is to:

- Downzone the lands at 1964 Fairfield Rd (Lot 1) and 507 Foul Bay Road (Lot 3) from the current 'RK-11 Townhouse District' and 'R1-G Gonzales Single Family Dwelling District' zoning to a site specific zone, and amend the OCP to redesignate the lands from 'Traditional Residential' to a 'Public Facilities, Parks and Open Space' designation.
- Encourage the City to enable the concurrent rezoning application process for 1733-37 Fairfield Road to enable a financial contribution from developers to TLC.
- Strengthen the protection of Abkhazi Garden by expanding the heritage protection to include 507 Foul Bay Road (Lot 3).

Site History & Context

In the year 2000, TLC acquired Abkhazi Garden from the developer who was planning to construct townhouses on the site. The local community supported TLC to raise the necessary funds, and have been actively involved since that time volunteering in support of the Garden as docents, gate greeters, and gardeners.

In 2013 TLC entered a restructuring process to address its debts. At this time Abkhazi Garden was subject to a mortgage, and TLC's creditors were pressing for repayment. These pressures put Abkhazi Garden at risk. TLC's volunteer Board and staff worked tirelessly for four years to extinguish all debt and secure TLC's future, and hence the future of the Garden. TLC's secured creditors were repaid in full. TLC's unsecured creditors were paid in full for debts of \$5,000.00 or less, and the remaining unsecured creditors were repaid 22.5% with a commitment to continue seeking revenue from the sale of densities from Abkhazi Garden for pro rata distribution to these 35 remaining creditors. The financial contribution from the neighbouring development site would be used toward this purpose.



In 2017 TLC, with the support of generous donors, established an endowment held at the Victoria Foundation to support Abkhazi Garden in perpetuity. Over 50 volunteers support the Garden currently.

As the Garden is technically zoned to permit Townhouse development, in 2014 TLC worked with the City of Victoria to establish a process whereby the excess density could be transferred to another site and protect the site against redevelopment for residential use [correspondence with the City is included with the supplementary documents in our application package]. We therefore propose to downzone the Abkhazi Garden site Lot 1 (1964 Fairfield Road) and Lot 3 (507 Foul Bay Road) to reflect the existing use.

Heritage Preservation

In addition to and concurrent with the downzoning, TLC has worked with City staff to register the former home of Nicholas and Peggy Abkhazi as a Heritage designated building, and look to expand the heritage designation to include 507 Foul Bay Road (Lot 3).

Section 3.4 of Victoria's Official Community Plan (OCP) cites *Inclusivity and Accessibility* as core values for the future of our city. Specifically, our ability to "Respect and respond to the perspectives, values and needs of Victoria's many individuals, groups and communities" speaks to these values. In addition, Section 3.8 emphasizes our need to build and maintain *Strong Local Communities*, which "Support and enhance the sense of place and community, and the uniqueness of Victoria's neighbourhoods". As such, Abkhazi Garden exemplifies these core values and is a neighbourhood treasure, nestled within a supportive community, and enjoyed by both locals as well as tourists from around the world who come to Victoria because of its gardens.

In addition, the OCP outlines *Placemaking* as a strategic directive for future growth and vibrancy in our city. As noted in the OCP, "*Victoria is vibrant and attractive with a unique character and sense of place. Victoria's cultural and natural heritage resources are protected and celebrated.*" Through this lens, the Abkhazi Garden is much more than a garden, as it holds a unique and powerful heritage as the former residence of Peggy and Nicholas Abkhazi. Abkhazi Garden has established a strong connection to the Country of Georgia, via the Georgian Ambassador to Canada. These ties resonate ever stronger today as we witness the invasion of Ukraine, or with the Ambassador's recent award of the Medal of Excellence to the TLC for preserving the culture and heritage so unique to Abkhazi Garden.

Further, Section 8 of the Gonzales Neighbourhood Community Plan speaks to *Heritage, Environment and Neighbourhood Features*, and seeks to outline opportunities in which to "*Identify and preserve heritage buildings, landscapes and streetscapes.*" As such, the Abkhazi Garden not only falls within the Proposed Heritage Conservation Area, the former residence of Peggy and Nicholas Abkhazi - a home designed by locally renowned architect John Wade -

retains tremendous heritage significance, therefore it makes sense to apply a Heritage Designation to the property as this move aligns strongly with neighbourhood goals.

Section 8.3.5 of the Gonzales Neighbourhood Plan highlights the importance of exploring ways in which to “*Encourage owners and builders to preserve and maintain, to the extent possible, neighbourhood features such as trees, fences, gardens and rock outcrops.*” Abkhazi Garden encompasses all of these features, in particular highlighting the rocky outcrops that inspired Peggy to purchase the land, and the central feature in the landscaping of the garden, and including a majestic grouping of Garry Oaks.

A comprehensive Maintenance Plan that was recently prepared by Don Luxton and Associates (June 2020) has been submitted to City staff and clearly demonstrates the heritage values and heritage preservation activities planned for the infrastructure. Recent works have included refinishing the hardwood floors and repainting the exteriors with the original heritage colours, supported by a City of Victoria Heritage grant.

With all of the above under consideration, a heritage designation has been registered to Lot 1. We seek to expand the designation to include Lot 3. Lot 3 includes the greenhouse and works area where volunteers spend hours weekly propagating the exotic and native plants featured in the garden.

Parking

Abkhazi Garden has been in operation for 22 years with minimal disruption to the neighbours, and during this time the handful of instances of minor conflict between visitors parking off-site and neighbours have been dealt with and resolved immediately or have been avoided altogether.

Specifically, volunteers to the garden successfully manage to avoid parking related issues with the neighbours as they are well informed of appropriate street parking and actively pass this knowledge on to visitors. Volunteers and the Teahouse operator also successfully avoid obstruction and noise related issues or concerns regarding idling tour buses, as they actively engage with both the bus companies and tour guides and inform them of appropriate places to park or load guests. To date we are proud to say we have always been able to respond promptly and respectfully to neighbour concerns.

Furthermore, on June 3rd and 4th of this year the Abkhazi Garden Site Manager completed an informal survey of the surrounding neighbourhood in an effort to seek input from neighbouring properties in regards to visitor parking and identify any concerns with how the Abkhazi Garden manages this.

Parking Survey

Site Manager and Volunteer Coordinator, Cherie Miltimore went door to door with the Abkhazi Garden parking survey along Fairfield Road from Foul Bay Road to Queen Anne, Shotbolt Road and Foul Bay Road from Shotbolt to Fairfield Road. In total, Cherie visited 34 homes along Fairfield Road (16), Shotbolt Rd (13) and Foul Bay Road (5), where she collected 24 direct responses at the door or by email, and left surveys in 10 remaining mailboxes from which we are still awaiting a response.

Details of this survey, including neighbour comments have been included with the supplementary documentation accompanying this application. However, a summary of the survey results are as follows:

- Out of 24 responses received, 15 (over 62%) residents had no issue with the current on-street parking for Abkhazi Garden.
- Of the 9 respondents who had any concerns with the parking situation at Abkhazi Garden, none of the residents had issues with Abkhazi Garden providing no on-site car parking. Instead, these residents were primarily concerned with traffic from vehicles and buses loading/unloading and temporarily waiting to transport visitors. These concerns could be easily resolved through a handful of traffic calming measures, as well as designating a temporary loading zone(s) along Fairfield Road.

Parking Demand & Alternative Transportation Initiatives

Due to land and space constraints, there is no ability to provide on site car parking to the general public. However, this has never posed an issue to the Abkhazi Garden operations because the vast majority of our staff, volunteers, and visitors, bike, bus, or use alternative means to access the garden. Therefore, the demand for on-site vehicle parking has, and is expected to remain, extremely low.

This makes sense as the site is located on a key transit route connecting the Downtown, Fairfield and Oak Bay Avenue commercial districts via Fairfield Rd and Foul Bay Rd, and these transportation corridors are very cycling and pedestrian friendly.

In addition, we have noticed that the number of visitors arriving at the Garden by car-free modes of transport has steadily increased in recent years, as our patrons are becoming increasingly concerned with climate change (especially since experiencing local weather anomalies such as the recent heat dome or uptick in frequency and severity of regional forest fires). Therefore, we would expect demand for car-free transportation alternatives to increase even further with the addition of on and off-site infrastructure improvements that better cater to the needs and concerns of the people visiting the Garden.

Though the TLC has already installed a bike stand on site to accommodate bicycle parking for up to 6 volunteers and guests, it is regularly utilized to full capacity, so we would like to expand this bicycle parking infrastructure to 14 (some hanging). In addition, to encourage green



transportation, TLC has committed to instituting a new program to provide free bus passes for our staff and volunteers.

Considering all of the above, we feel that the shifting demands towards car-free transportation options, as well as the long history of the operation of Abkhazi Garden clearly demonstrates that parking needs have been adequately addressed. Therefore there is no plan for future additions of car parking as this move would not increase visitation to the garden or teahouse, and is not aligned with the climate leadership initiatives of the TLC or many of our visitors.

Green Building Features

Considering that the Abkhazi Garden Teahouse is a small restaurant/retail space with low energy requirements, the majority of our sustainability initiatives stem from our landscape management practices. As such, our Head Gardener has worked hard to cultivate drought resistant plantings which help minimize our water needs.

One prime example of this was our initiative to re-seed all grass lawn areas with native, drought tolerant, grasses to reduce irrigation requirements. In addition, a water collection system has been installed to supplement the water needs of the plant propagation program [side note: this program also provides significant revenues via plant sales to support the maintenance costs of the garden]. The irrigation system also incorporates state of the art heads to control water output and minimize wastage.

Other sustainable landscaping practices include: the introduction of more permeable surfaces for pathways and driveway to improve water retention and reduce run off, as well as an extensive on-site organic waste and composting system which recycles valuable minerals and nutrients back into the garden.

Collectively, our minimal water and resource requirements, as well as a plethora of sustainable landscaping practices, ensure that the Abkhazi Garden operates with a low carbon footprint.

Conclusion

In summary, the Abkhazi Garden is so much more than a beautiful greenspace. It is a treasured neighbourhood amenity with a rich back story and historic significance, and deserves to be protected from future development so that it can be preserved and enjoyed for generations to come.

“Downzoning” the Garden lands to remove the maximum available buildable density, as well as redesignating them in the OCP from ‘Traditional Residential’ to a Public Facilities, Parks and Open Space’, would not only establish a designation more congruent with the Garden’s use, it would also create a necessary layer of protection in perpetuity.



That being said, the unused density from these lands does have value and we feel that rather than losing this value entirely through the rezoning process, we encourage the City to consider allowing a portion of this unused density to effectively be sold to another more appropriate site. Despite density transfers being a common practice in other BC municipalities and cities around the world, we acknowledge that the City of Victoria does not have a system in place to support this. However, the City may be able to accomplish this through a parallel rezoning application which is permitted under Section 904 of the Local Government Act.

In this case we are proposing to accomplish the above, and are submitting this application to be considered in tandem with Aryze Development's rezoning application for their proposed development of 1733, 1735, and 1737 Fairfield Rd.

Lastly, we understand that rezoning the Abkhazi Garden lands may trigger requests from City Staff to provide additional on-site parking, per the Zoning Regulation Bylaws. However, for the reasons outlined above, demand for on-site car parking has remained extremely low throughout our 22+ years of operations, and our zero on-site parking practices have had little to no impact on our neighbours during this period. At the same time, we are noticing a steadily increasing demand to accommodate car-free transportation alternatives such as on-site bicycle parking. For these reasons, we are seeking to continue with our existing parking availability to enable us to preserve existing green space and divert resources away from car-parking infrastructure towards the car-free transportation initiatives outlined above.

In closing, we would like to thank you for your time and consideration of this application.

Kind regards,

Dianna Stenberg
Executive Director
TLC The Land Conservancy of BC

We acknowledge and respect the lək'wəḡən peoples on whose traditional territory we live and work and the Songhees, Esquimalt and W̱SÁNEĆ peoples whose historical relationships with the land continue to this day.

5.2 Development Permit with Variance(s) Application No. 000204 for 1733-1737 Fairfield Road (concurrent with Rezoning Application No. 000821)

The proposal is to demolish three existing homes (1733, 1735 and 1737 Fairfield Road), consolidate three lots into one, and construct a 4-storey, 19-unit multiple dwelling development.

Applicant meeting attendees:

Greg Damant - Cascadia Architects
 Sara Huynh – Cascadia Architects
 Kim Tang– Biophilia Collective
 Elizabeth Balderson – Biophilia Collective
 Rob Starkey – Aryze
 Chris Quigley - Aryze

Patrick Carroll provided the Panel with a brief introduction of the application and the areas that Council is seeking advice on, including the following:

- surface parking impacts on greenspace, streetscape and buffering
- transition in scale
- sensitivity of design to neighbourhood context
- any other aspects of the proposal on which the ADP chooses to comment.

Chris Quigley & Sara Huynh provided the Panel with a detailed presentation of the site and context of the proposal, and Kim Tang provided a brief description of the landscape plan.

The Panel asked the following questions of clarification:

- What were the primary reasons for no below grade parking?
 - Our experience across the road with the soil quality and clay for a project of this size would be a massive expense.
 - There is a market driver for preference for future buyers wanting parking at grade.
 - The landscape architects have developed a softened gentle approach to the at grade parking experience.
- What is the ratio for storage lockers to the number of residential homes?
 - The parking area does include some private garages with some storage.
 - In the main building it's 1 per unit.
 - We would like to find some more space if we can.
- There was comment of possibly extending the top floor to make a full floor, what would that do to the proposed roofline?
 - I think for overall height it wouldn't change. I think there is still some ability to have it push back from Fairfield Road but keep within the current height. Working on the Beechwood side a bit more to make it more like a three storey as it tapers down.
- Is there a reason why the street trees don't space out along Fairfield Road?
 - There are 7 trees, we made changes after the plans were distributed to the panel.
- A question for the Planner: Is this the first of similar visions to come in this neighbourhood or is this the only of its kind?

- There is nothing to my knowledge and the existing documents that have been done to date that would indicate and envisioned increased beyond traditional residential densities and heights at this time.
 - There was a previous process around a new local area plan for the Gonzales neighbourhood, although I'm not aware of a completion timeline.
- Have you looked at the issues and comments by the City in regards to building type and character as well as the roof structure?
 - Yes, I think we will look at it again. The roof was a balancing game. We wanted it to be calm and controlled but liked that it broke down the massing of the building. Depending on our priorities, we will consider it and continue to do so.
- With regards to the level 4 floor plate, is it a requirement in your performa to make it viable and home some below market housing included in the project?
 - Yes. Currently the top floor is the penthouse type with a sizable unit. There could be a different path that would break it up and bring some efficiency to the building and have those affordable house spread throughout the building.
- This building is kitty corner to the building Aryze just completed on Fairfield Road correct?
 - Yes.
- Is there also an existing three-story rental building just down the street?
 - Yes.

Panel members discussed:

- Share staff concerns
- Mindful that we plan a City it based on policy and planning
- Significant ask and what are the aspects of the plan
- Form, massing and manipulation
- Mansard roof sits proud and prominent and dominant
- Need more landscaping for lushness
- Enhancing the public and private realm
- No consideration to the Abkhazi garden
- Neighbourhood context matters
- Roof peak, aggressive look
- Similar to the Rhodo in terms of roofline, not out of place
- Underground parking in costly
- Building doesn't fit well into its context
- Not fitting with the character

Motion:

It was moved by Colin Harper, seconded by David Berry that Development Permit with Variance(s) Application No. 000204 for 1733-1737 Fairfield Road be approved with the following changes

- Consideration to simplifying the roof form to be more sympathetic to the neighbouring context.

- **MINORITY REPORT:** Those that voted against believe the building is not consistent with the density, height and use envisioned for traditional areas in the OCP

Carried 4:2

For: Ben Smith, David Berry, Sean Partlow, Colin Harper

Opposed: Pamela Madoff, Will King

ADJOURNMENT

Motion to adjourn: Moved by Sean Partlow, Seconded by Ben Smith

The Advisory Design Panel meeting of October 26, 2022 was adjourned at 2:53 pm.

Will King, Chair (acting)

Statement of Significance: Abkhazi Garden

Description of Historic Place

Abkhazi Garden is located at 1964 Fairfield Road, in the City of Victoria, British Columbia. The site is 1.598 acres. The property was purchased in 1946 by Peggy Pemberton Carter who later wed Nicholas Abkhazi. The defining features of the Garden — glaciated rock outcroppings and mature Garry oak trees — strongly present the characteristics that are unique to the southern tip of Vancouver Island. The Garden is surrounded by residential properties on all three sides. The original residence has been repurposed as a teahouse. Other structures include the Summerhouse, storage shed, and greenhouse.

Heritage Value

One of the first things Peggy did was to hire John Wade to design her summerhouse. Wade was a young architect who had worked in the offices of Richard Neutra, the brilliant modernist architect in Los Angeles. Neutra had a strong sense of the flow between interior design and the exterior landscape that became a hallmark of the California West Coast style. The California landscape architects of the time were also expressing new ideas about a garden being “more than a collection of plants, more than an imitation of historical styles and that it could be, once again, an art form, expressive of its place, time and people.” These ideas strongly influenced Wade in his design of Abkhazi Garden’s structures and terraces.

Peggy Pemberton Carter probably needed little convincing about the soundness of these new ideas that embraced the familiar aesthetic of gardens near her former Shanghai home. Chinese gardens, essentially places of meditation and escape, must have been very appealing for both Peggy and Nicholas Abkhazi after their experiences in World War II POW (prisoner of war) camps. Nothing in a Chinese garden is hurried or blatant. Paths are not just a means of access; they are a way of exploring slowly changing views while journeying through the garden.

Abkhazi Garden is a dynamic work of art within a discipline imposed by the site. A unity of execution is evident in the layout of buildings, paths and plant material. Forms and materials were selected to express one overruling idea, the rhythm of the natural landscape. The house, summerhouse and garden shed, modest in size and construction, complement this landscape. The intimate paths show a human scale appropriate for the private world the Abkhazis wanted to create for themselves.

Character-defining Elements

The sculpturally strong Garry oaks predominate the site and provide a unifying sense of stability and serenity. Other significant plants on the property are especially notable for their maturity and precise placement. Some rhododendrons are over 100 years old, their gnarled trunks as attractive as their flowers. Trained mature conifers cascade down the rock faces, and carefully pruned azaleas provide living sculptures. Each season, naturalized bulbs carpet the garden in sheets of colour. Choice alpine plants are sited carefully in natural rock crevices.

Abkhazi Garden is more than the sum of its plants. As their tastes changed through their lifetime, the Abkhazis made modifications. Many plants have been lost over the years, as happens in all gardens, but others are being added; some have historical precedence, and others are new to the trade. With the best nurserymen of their day as their mentors, the Abkhazis chose plants to enhance the natural landscape, not detract from it. The plants that have thrived over the last fifty years are those that have proven to be best adapted to the site. This evolution to a more drought-tolerant planting is an ongoing process.

Abkhazi Garden is significant because, for over forty years, the same two people watched and managed its development, pruned and shaped its trees with a constant and shared vision that they lived to see come to maturity. In our mobile society this is a rare occurrence.

The dramatic story of the Abkhazis, with its tragedy, romance and reunion, is well documented and gives another level of interest and significance to the Garden. Peggy and Nicholas were both very private people, yet from as early as 1949, they welcomed groups of visitors to enjoy their beautiful property. The Abkhazis were cultured people who came to Victoria because Peggy was told that in Victoria one could be left to be as eccentric as one wanted to be. They pursued their eccentric lives here and left a unique and priceless garden legacy that has now been preserved for our eternal enjoyment and education.

- *Judith Brand (May 2008)*



Tenant Assistance Plan

The Tenant Assistance Plan and appendices must be submitted at the time of your rezoning application, and should be submitted directly to housing@victoria.ca. Please contact your Development Services Planner with questions or concerns.

Date of submission of Tenant Assistance Plan to Housing Policy staff:

Current Site Information

Site Address:	1733, 1735, 1737 Fairfield Rd
Owner Name:	
Applicant Name and Contact Info:	Aryze Developments Inc. 1839 Fairfield Road, Victoria BC V8S 1G9 email: robert@aryze.ca phone: 250 940 3568 (office)
Tenant Relocation Coordinator (Name, Position, Organization and Contact Info):	Robert Starkey, Development Coordinator Aryze Developments Inc. 1839 Fairfield Road, Victoria BC V8S 1G9 email: robert@aryze.ca phone: 250 940 3568 ext# 364 (office)

Existing Rental Units

Unit Type	# of Units	Average Rents (\$/Mo.)
Bachelor	1	\$750/Month
1 BR	1	\$1,250/Month
2 BR	3	\$1,965/Month
3 BR	2	\$3,000/Month
3 BR+		
Total	7	

Current Building Type (check all that apply):

- ☐ Purpose-built rental building
☐ Non-market rental housing
☐ Condominium building
☒ Single family home(s), with or without secondary suites
☐ Other, please specify:

Rights and Responsibilities of Landlords and Tenants

The rights and responsibilities of landlords and tenants are regulated by the Province and is set out in the [Residential Tenancy Act](#).

The City of Victoria's [Tenant Assistance Policy](#) is intended to supplement the Residential Tenancy Act and offer additional support for tenants in buildings that are being considered for redevelopment. To review the full Tenant Assistance Policy and supporting documents, please refer to the City of Victoria's [website](#).

POLICY APPLICATION

If your plans to redevelop this property will result in a loss of residential rental units AND will require tenants to relocate out of the existing building(s), please submit a Tenant Assistance Plan with your application.

Do you have tenant(s) who have been residing in the building for more than one year, at the time when application is submitted?

☒ Yes ☐ No

If yes, tenants are eligible for support. Please complete the full form.

If no, please skip to and complete Appendix A: Occupant Information and Rent Roll.

When completing this form, please refer to the Tenant Assistance Policy guidelines for Market Rental and Non-Market Rental Housing Development. Please note that the form includes the required 'OIPPA section 27(2) privacy notification which should be communicated to tenants.

APPLICANT: Please complete the following sections to confirm the details of the Tenant Assistance Plan:		CITY STAFF: Did applicant meet policy?
Compensation Please indicate how you will be compensating the tenant(s). Please specify whether option 1 or 2 will be provided, and whether at existing rents or C.)C average rates. (4ee Policy 4ection 4.1 or 5.1)	We will be compensating TAPs eligible tenants following Option (1) outlined in Section 4.1 of Tenant Assistance Policy. Compensation will be based on the higher of either the tenant's existing rental rate or the current CMHC average market rates.	Yes <input type="checkbox"/> No <input type="checkbox"/>
Moving Expenses Please indicate how the tenant(s) will receive moving expenses and assistance. Please specify whether option 1 or 2 will be offered. (4ee Policy 4ection 4.2)	We will be compensating TAPs eligible tenants following Option (2) outlined in Section 4.2 of Tenant Assistance Policy. We will also provide an extra \$250.00 compensation for moving expenses in addition to the required flat rate compensation amounts outlined in the Policy (as per Aryze internal policy).	Yes <input type="checkbox"/> No <input type="checkbox"/>
Relocation Assistance Please indicate how the tenant(s) will receive relocation assistance, including the staff responsible or whether a third-party will be involved. (4ee Policy 4ection 4.3 or 5.3)	Aryze will coordinate with tenants and provide relocation assistance directly (see details of Aryze appointed Tenant Relocation Coordinator on page 1). In addition, Aryze's Tenant Relocation Coordinator will receive support, as needed, from our contacts at various third-party property management companies (ex: Devon Properties, Proline Property Management, Cornerstone Property Management, etc.) to aid in searches for suitable alternative rental options for displaced tenants.	Yes <input type="checkbox"/> No <input type="checkbox"/>
Right of First Refusal Please indicate whether the applicant is offering right of first refusal to the tenant(s). Please indicate your reasoning. (4ee Policy 4ection 4.4 or 5.5).	Not applicable for this application.	Yes <input type="checkbox"/> No <input type="checkbox"/> N/A <input type="checkbox"/>
Tenants Requesting Additional Assistance Please indicate whether tenant(s) have requested additional assistance above policy expectations, and specify what additional assistance will be provided. (4ee Policy 4ection 6.0)	Yes, some tenants have requested additional assistance with finding alternative housing solutions that meet specific needs (ex: pet friendly, access to a community garden, etc.). The Tenant Relocation Coordinator will work to provide a range of housing alternatives that aim to meet these specific tenant requirements. In addition, Aryze will attempt to relocate interested tenants to other new rental buildings we have under development.	Yes <input type="checkbox"/> No <input type="checkbox"/>

APPLICANT:

Please complete the following sections to confirm the details of the Tenant Assistance Plan:

How and when did you inform tenants of the rezoning or development application? (Please refer to Policy 4ection 3.4)	<p>Tenants were first notified of our intent to redevelop these properties by direct mail letters issued on February 7, 2022. These letters also included the necessary TAPs forms and information resources, as well as additional postcard invitations to two (2) CALUC Meeting/Community Information Sessions held on February 28th and March 28th.</p> <p>In addition, we have connected with each tenant individually through both calls and in-person meetings to share details about our development proposal, discuss the municipal approval process and establish expectations about potential rezoning timelines, and review the Tenant Assistance Policy in detail.</p>
How will you be communicating to tenants throughout the rezoning or development application (including decisions made by Council)? (Please refer to Policy 4ection 3.4)	<p>We will continue to keep tenants informed about the status of our application through the following core methods:</p> <ol style="list-style-type: none">1) Direct mail notices delivered at the building2) Direct phone calls from our Tenant Relocation Coordinator (noted previously)3) Email notices issued to tenants who opt into our mailing list4) Encouraging all tenants to follow this project online via the City's Development Tracker, the project page on our website, or via our Instagram social media channel (where we post regularly about this and our other projects)
What kind of resources will you be communicating to your tenants and how will you facilitate tenants in accessing these resources (Please see the City's website for a list of resources)	<p>Tenants have received (and will continue to receive) copies of important TAP Forms and Policy documents, helpful website links, and regular project updates via email and direct mail.</p> <p>We have also provided the contact information of our Tenant Relocation Coordinator who is ready to provide direct support and answer questions at any time. Both our Tenant Relocation Coordinator and Engagement Coordinators may follow-up with tenants from time to time via phone as requested by some tenants.</p>

Other comments (if needed):

FINAL Tenant Assistance Plan Review - [For City Staff to complete]

Application reviewed by _____ (City Staff) on _____ (Date)

Did the applicant meet TAP policy? Yes ☐ No ☐ N/A ☐

Staff comments on
final plan:

Please complete all three Appendices as part of your Tenant Assistance Plan. Exception: If there are no eligible tenants, only complete Appendix A (see Policy Application on p. 1). To protect tenant information, the appendices are only submitted with the Tenant Assistance Plan to housing@victoria.ca. Please contact your Development Services Planner with questions.

The Tenant Assistance Plan includes the collection, use and disclosure of tenants' personal information for the purpose of achieving the goals contained in the Tenant Assistance Policy and guidelines. The collection, use and disclosure of tenants' personal information must comply with the Freedom of Information and Protection of Privacy Act (FOIPPA). Privacy requirements for compliance with FOIPPA have been adopted and expressed in privacy language in Tenant Assistance Plan documentation to ensure continued compliance.

For privacy compliance, please have:

- Tenants sign the Tenant Request for Assistance form to return to applicant (to be included in Appendix #)
- Applicants review and sign the Tenant Assistance Policy Compliance with FOIPPA form (Appendix C)

APPENDIX A: Current Occupant Information and Rent Rolls

Please attach the current tenant information and rent rolls as Appendix A. Note: Appendix A will be kept confidential.

All Units (existing and former tenants within the past year, at time of application)

Apartment Unit Number	Bedroom Type (Bachelor, 1 BR, 2 BR etc.)	Tenant Name (if none, list as vacant with reason for end of previous tenancy)	Does the Tenant Require Additional Assistance (Y/N)? If yes, what additional support?	Start Date of Tenancy	Current Monthly Rent Amount
APPENDIX A - See Attached					

APPENDIX A: Current Occupant Information and Rent Rolls, continued:

Apartment Unit Number	Bedroom Type (Bachelor, 1 BR, 2 BR etc.)	Tenant Name (if none, list as vacant with reason for end of previous tenancy)	Does the Tenant Require Additional Assistance (Y/N)? If yes, what additional support?	Start Date of Tenancy	Current Monthly Rent Amount
APPENDIX A - See Attached					

Note: Appendix A will be kept confidential. Please ask City staff for additional Current Tenant Information and Rent Rolls if needed.

APPENDIX B:

Tenant Correspondence & Requests for Assistance

The applicant is responsible for submitting the Tenant Request for Assistance forms signed by tenants, as well as copies of all written correspondence and notification to tenants to City staff as Appendix #.

For non-profit organizations that may have their own forms to use, please contact City staff to determine if those can be used in lieu of the Tenant Request for Assistance form.

Note: Appendix # will be kept confidential.

APPENDIX B - See Attached

APPENDIX C:

Tenant Assistance Policy Compliance with the Freedom of Information and Protection of Privacy Act



Please ensure this form is signed by both the Applicant and the Tenant Relocation Coordinator, if applicable.

The City of Victoria's Tenant Assistance Plan (TAP) collects tenant personal information to assist them to find new, comparable, accommodations. Collecting tenant personal information requires the City and developers' Tenant Relocation Coordinators to collect in compliance with the *Freedom of Information and Protection of Privacy Act* (FOIPPA). Following these privacy guidelines will maintain the required compliance.

Collection: Appendix A of the Tenant Assistance Plan (TAP) collects this personal information, tenant name, length of tenancy, dependents and needs and vulnerabilities (e.g. fixed income, affordable housing, disabilities). Section 26 of FOIPPA lists all the purposes in which personal information may be collected. Helping tenants find new, comparable, accommodations is the only purpose for collecting their personal information. This purpose complies with section 26(c) that states: "*the information relates directly to and is necessary for a program or activity of the public body*". Tenants' personal information cannot be used for any other purposes.

Use: Tenant's personal information must comply with section 32(a) of FOIPPA that states, "*it must be for the purpose for which that information was obtained or compiled, or for a use consistent with that purpose (see section 34)*". The purpose is the same as that in which it was collected under section 26(c). There are no consistent purposes under the TAP program. Tenant's personal information can only be used to provide the assistance that the TAP program provides.

Disclosure: FOIPPA list only those reasons in which personal information may be disclosed and it can only be disclosed to individuals inside Canada. The tenants' personal information can only be disclosed in accordance with section 32.2(a) that states, "*for the purpose for which it was obtained or compiled*". In other words, disclosure is only to those who require it in order to perform work that "*relates directly to and is necessary for*" delivering the assistance available under TAP (e.g. on a "Need to Know" basis).

Accuracy: FOIPPA requires that "*every reasonable effort*" be employed to collect personal information. When tenants complete a tenant letter, they need to review the personal information they provide to confirm it is correct. Also, double for accuracy when transcribing from the letters to Appendix A.


Correction: Tenants can request to review and correct their personal information at any time including a year after the decision is implemented regarding the assistance they received under TAP. The City will provide the access, therefore, developers do not need to retain their tenant records for a year.

Protection: Every reasonable effort must be made to protect tenant information from unauthorized collection, use, disclosure, access or premature destruction. This includes password protecting tenant information, keeping it separate from other information, keeping it in one location, limiting access (need to know) and not sharing it unencrypted are all reasonable security efforts.

Storage and Access: FOIPPA requires that the tenant personal information be stored and accessed only from within Canada. Storing it on a cloud service provider, even one in Canada, is still likely to allow access from the US. Keeping it in a secure electronic folder with only one person with access is the most FOIPPA compliant.

Retention: Personal information is only kept for as long as it is operational required. Under TAP it can only be kept for one year after a decision has been made and implemented regarding the assistance a tenant is eligible for under TAP. After that, tenant letters must be destroyed so they cannot be reconstituted and the personal information in Appendix A must be aggregated so that specific individuals cannot be identified.

Applicant: I have read and understand my responsibilities with regard to compliance with FOIPPA as explained above

Signature:  Print Name: Robert Starkey -- Date: May 4, 2022

Relocation Coordinator (if applicable): I have read and understand my responsibilities with regard to compliance with FOIPPA as explained above

Signature: ----- Print Name: ----- Date: -----

**VIA EMAIL**

25 June 2023

Patrick Carroll
Senior Planner
City of Victoria
1 Centennial Square
Victoria, BC
V8W 1P6
email: pcarroll@victoria.ca

Dear Patrick:

Re: Summary of CAC Analysis for Proposed Rezoning of 1733 to 1737 Fairfield Road

Aryze Developments has submitted a rezoning application for the three properties at 1733 to 1737 Fairfield Road. These properties are currently designated Traditional Residential in the OCP and zoned R1-G Gonzales Single Family District. The proposal is to rezone from R1-G to a site specific zoning district that would permit a 4-storey 30 unit strata apartment development. An OCP amendment is also required.

The City of Victoria's Inclusionary Housing and Community Amenity Policy requires residential rezonings to provide amenity contributions or affordable housing (or both), depending on the specifics of the rezoning. The Policy identifies some types of rezonings (atypical rezonings) where the amenity contribution and/or inclusionary housing contribution is determined based on an economic analysis. For these types of rezonings, the target for the CAC contribution is 75% of the increase in land value created by the rezoning (beyond existing zoning). Atypical rezonings include applications where an OCP amendment is required (as well as other situations).

The proposed application is considered an atypical rezoning. So, an economic analysis is required to determine if the rezoning creates an increase in land value that can be used to support amenity contributions and/or inclusionary affordable rental units.

Therefore, the City commissioned Coriolis Consulting Corp. to complete the economic analysis to determine if the rezoning application creates an increase in land value that can be used to fund amenity contributions.

As part of the analysis, we estimated:

- The land value under existing zoning.
- The land value supported by the proposed rezoning concept, which includes strata residential units.

Based on these estimates, we determined whether or not there will be an increase in land value created by the proposed rezoning and the implications for the project's ability to provide an amenity contribution.

This letter summarizes the concept analyzed, the approach to the analysis and the findings. The revenue and cost assumptions used in the analysis are based on market conditions as of April 2023.

Rezoning Concept

The three lots at 1733, 1735 and 1737 Fairfield Road are proposed to be rezoned from R1-G Zone Gonzales Single Family District to a site-specific zoning district that would permit a 4-storey 30 unit strata residential development, with a combination of surface and tuck-under parking stalls.

The proposed rezoning concept includes 6 townhouse units and 24 apartment units with 22 parking stalls. The proposed density is 1.73 FSR (plus exclusions).

Approach

To complete the analysis, we:

1. Estimated the current value of the three single family lots at 1733-1737 Fairfield Road under existing zoning, based on:
 - Existing assessed values.
 - Sales of similarly zoned single family properties in nearby areas.
2. Analyzed the likely financial performance of the proposed rezoning concept using a proforma/land residual analysis. This includes the following steps:
 - Estimate the potential sales revenue from the completed project.
 - Deduct all project costs other than the land cost.
 - Deduct a profit margin (15% of total costs including estimated land value).
 - Calculate the remaining value which is the land residual. This represents the amount that a developer could afford to pay for the property, complete the project and earn the target profit margin.
3. Compared the estimated land value under the proposed rezoning concept with the estimated land value under existing zoning.
4. Determined whether there is additional land value created by the rezoning proposal.
5. Calculated the supportable amenity contribution based on 75% of the estimated extra land value created by the rezoning proposal.
6. Compared this estimated supportable CAC value with the contribution being proposed by the applicant.

Estimated Value Under Existing Zoning

The site is currently zoned R1-G which allows single family dwellings. To estimate the market value of the three lots under current zoning, we reviewed the existing assessed values of each property and recent sales of comparable single family properties.

Based on our analysis of market evidence, we estimate that the current market value of the three lots is about \$4,435,000¹.

Estimated Land Value Supported by the Proposed Rezoning

We used a land residual proforma to analyze the expected financial performance of the proposed project and to estimate the value of the land supported by the proposed rezoning. The analysis is based (in part) on confidential information provided by the applicant and its consultants so we have not included the detailed financial assumptions and analysis in this summary.

¹ In order for a developer to acquire multiple adjacent single family homes simultaneously to create an assembled site, a developer needs to pay a premium above market value in order to create an incentive for all of the adjacent owners to sell simultaneously for redevelopment. Our estimated existing value includes a reasonable allowance for assembly costs to acquire all three lots simultaneously.

Based on our land residual analysis, we estimate that the land value supported by the proposed project is about \$4,870,000. This is equivalent to a land value of about \$150 per square foot buildable (on FSR) which is at the upper end of development site sales in Victoria. So this land value estimate may be optimistic. However, given the location of the site we think it is reasonable.

Estimated Increase in Value and Implications for CAC Value

Exhibit 1 summarizes our property value estimates and identifies the implications for the supportable CAC value.

Exhibit 1 – Estimated Increase in Land Value due to Rezoning and Implications for CAC

	Estimated Values (rounded)
Total Estimated Value Under Existing Zoning (including assembly allowance)	\$4,435,000
Estimated Land Value Supported by Rezoning	\$4,870,000
Increase in Land Value due to Rezoning	\$435,000
CAC at 75% of Estimated Increase in Value	\$326,000

As shown in the exhibit, we estimate that the rezoning will create an additional \$435,000 in land value. The calculated supportable CAC (at 75% of the increased value) is \$326,000.

Conclusions

The estimated increase in land value created by the proposed rezoning of 1733-1737 Fairfield Road is about \$435,000, which (under existing City policy) supports a CAC value of about \$326,000.

Our understanding is that the applicant has agreed to contribute \$350,000 to the owner of the Abkhazi Garden site to transfer a portion of the existing permitted residential density from the Abkhazi Garden site to the proposed project at 1733-1737 Fairfield Road.

The City is considering an application to rezone (downzone) the Abkhazi Garden site to a new site-specific zone that would limit development to the existing use plus some additional floor area for a future accessory building. This rezoning would help ensure retention of the Abkhazi Garden for the long term but lower its market value as a potential urban development site.

The proposed \$350,000 contribution from Aryze to the owner of the Abkhazi Garden site is intended to help compensate the property owner for any reduction in value associated with the protection of the Abkhazi site. The proposed payment for the density transfer is higher than the estimated supportable CAC value, so there is no financial room for additional amenity contributions from the proposed project at 1733-1737 Fairfield Road.

This analysis is based on the most recent project information that the applicant and the City have provided to us. The analysis should be updated if there are any changes to the proposed rezoning concept or requirements from the City.

Please let me know if you have any questions about our findings.

Yours truly,

COROLIS CONSULTING CORP.



Blair Erb



1733-1737 Fairfield Road Parking Variance

Final Report

Prepared for
Aryze Developments

Date
April 26, 2022

Project No.
04-22-0057

April 26, 2022
04-22-0057

Robert Starkey
Development Coordinator
Aryze Developments
1839 Fairfield Road
Victoria, BC
V8S 1G9

Dear Robert:

**Re: 1733 - 1737 Fairfield Road, Parking Variance
Final Report**

Bunt & Associates Engineering Ltd. (Bunt) has completed our parking variance study for the proposed residential development at 1733-1737 Fairfield Road, Victoria, BC. Our Report is provided herewith, it addresses the potential transportation impacts related to the proposed development.

We trust that our input with this report will be of assistance. Please do not hesitate to contact us should you have any questions.

Best regards,
Bunt & Associates



Jason Potter, M.Sc. PTP
Senior Transportation Planner / Associate

CORPORATE AUTHORIZATION

Prepared By: Jason Potter, M.Sc. PTP
Professional Transportation
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Date: April 26, 2022
Project No. 04-22-0057
Status: Final

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EXECUTIVE SUMMARY

Aryze proposes the development of a 4 storey, 19-unit strata residential building at 1733 - 1737 Fairfield Road, Victoria, BC. The site is currently occupied with three single family-homes.

The proposed supply of 22 parking spaces is 8 spaces below the City of Victoria Zoning Bylaw requirements for 30 spaces, therefore a parking variance is required.

The proposed variance will be supported by:

- Providing electric charging abilities to all of the vehicle spaces;
- Exceeding Victoria Bylaw bicycle parking requirements with enhanced accessed 32 Long-term bicycle spaces and six weather protected Short-term spaces;
- Providing the Long-Term bicycle spaces with 110 volt charging ability;
- Providing a bicycle repair station and bike wash station; and
- Improving site fronting sidewalks and boulevards to enhance the area's walkability, and
- Providing a Transportation Option Information Package or Brochure for new residents.

1. INTRODUCTION

1.1 Study Purpose & Objectives

Aryze is proposing the development of a 4-storey residential strata building at 1733-1737 Fairfield Road. The site is currently zoned as R1-G (Gonzales Single Family Dwelling District).

The project will feature 17 residential units. They consist of two 3-bedroom units, 12 2-bedroom units and five 1-bedroom units.

Bunt & Associates were retained by Aryze to conduct a parking review for the proposed parking variance and propose Transportation Demand Management (TDM) strategies to help reduce the development's demand for vehicle parking. This parking variance study will accompany Aryze's rezoning application.

The location of the proposed development site is illustrated in **Exhibit 1.1**.

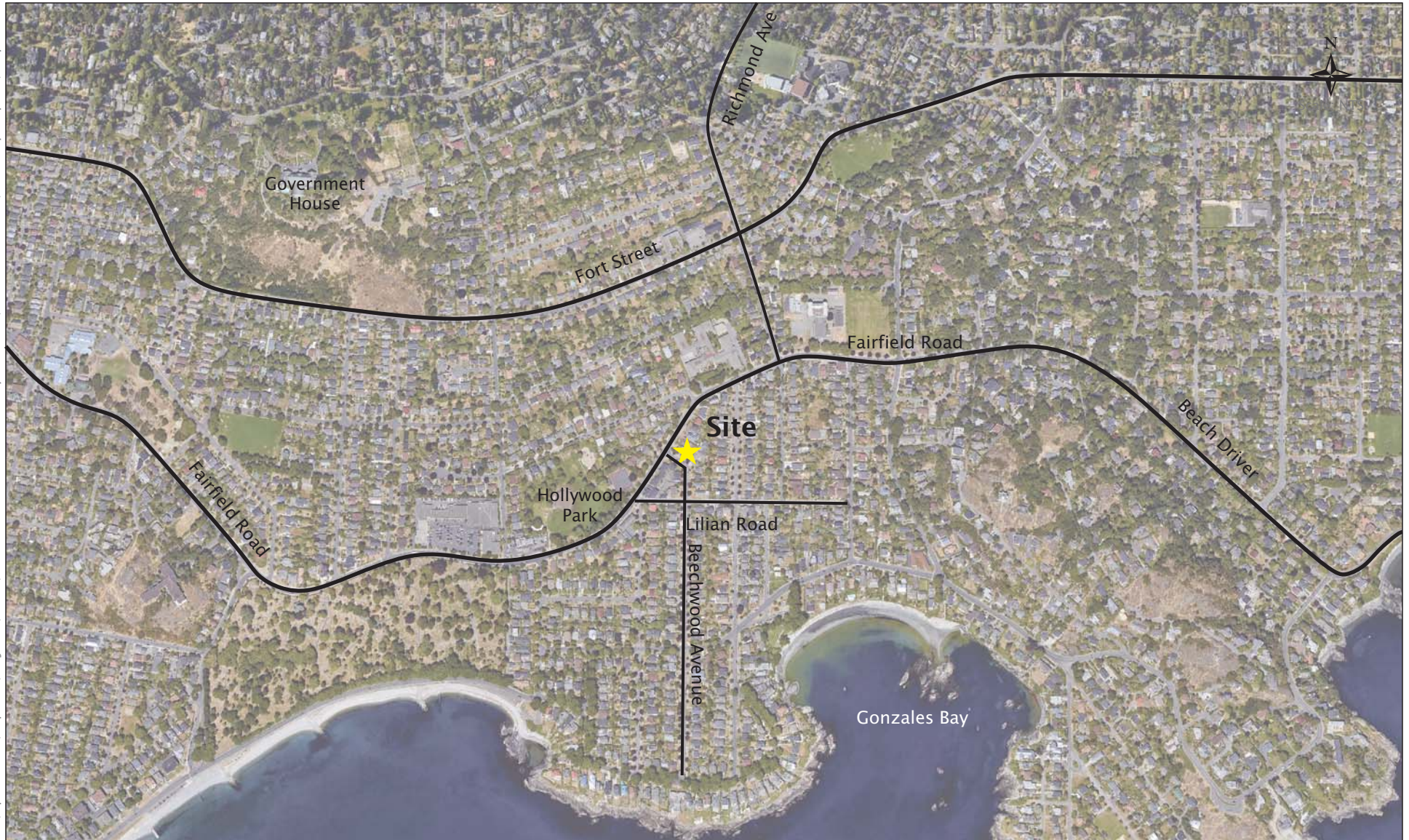


Exhibit 1.1 Site Location

04-22-0057

1733-1737 Fairfield Road
February 2022

1.2 Proposed Development

The proposed development is summarized in **Table 1.1**.

Table 1.1: Proposed Land Uses and Unit Breakdown

LAND USE	UNITS	APPROXIMATE SIZE (M ²)
Townhome – 2 bedrooms	6	106-111
Apartment – 1 bedroom or 1 with den	5	73-97
Apartment – 2 bedrooms or 2 with den	6	97-125
Apartment – 3 bedrooms	2	146-254
RESIDENTIAL TOTAL	19	-

All residential units will be condo strata units.

The development proposes including 22 parking spaces on the ground level. One of these spaces is designated for visitor parking, the remaining 21 are for residents. The two 3-bedroom units will have two parking spaces each in private garage style areas and the remaining 17 units will have one vehicle parking space.

Vehicle access to the parking is on Beechwood Avenue on the south edge of the site.

Bicycle parking will exceed bylaw requirements with 32 Long-term and 6 Short-term bicycle spaces.

The proposed site plan (ground level) is shown in **Exhibit 1.2**.



CADD Revision Control		DATE
NO.	DESCRIPTION	DATE

CASCADIA ARCHITECTS INC.

1733-1737 Fairfield Rd

APR 22

LEVEL 1 PROPOSED PLAN

DATE: APRIL 22, 2022

SCALE: 1:100

PROJECT: 2123

REVISION: 2

DATE: APRIL 22, 2022

BY: A-101

Exhibit 1.2 Site Plan



2. EXISTING CONDITIONS

2.1 Land Use

1733-1737 Fairfield Road is currently occupied by three single-family homes, two are accessed from Fairfield Road and one from Beechwood Avenue. Land use adjacent to the site is primarily composed of single-family residential dwellings, with commercial land uses and a cemetery to the west of the site on Fairfield Road.

The site is located approximately 2.7 km southeast of Victoria's downtown area in the Gonzales neighbourhood. It is also located approximately 2.1 km south of Royal Jubilee Hospital and 3.7 km south of Camosun College Lansdowne Campus.

2.2 Existing Transportation Network

2.2.1 Road Network

Fairfield Street adjacent to the site is a two-way street that connects to Victoria's Downtown. Beechwood Avenue adjacent to the site operates similar to a local road. Nearby intersections are all stop-controlled.

The adjacent street network is illustrated in **Exhibit 2.1**.

There is currently curb side parking along both sides of Beechwood Avenue and on Fairfield Road.

2.2.2 Transit Network

The site is well served by public transit, with three transit routes within 800 metres of the site (an approximate ten-minute walk). These routes and local area bus stops are presented in **Exhibit 2.3**.

The site is located less than 200 m from stops on regional route #7, and 600-800 m from stops on local routes #1 and #3. Downtown or the University of Victoria is approximately a 16-minute trip on route #7 from the site, and Camosun College's Lansdowne Campus is an approximate 14-minute trip. **Table 2.1** shows the frequencies of the transit routes near the site.

Table 2.1: Existing Transit Service Frequency

ROUTE		APPROXIMATE HEADWAY (MIN.)				
#	BUS ROUTE NAME	AM	MID-DAY	PM	EVENING	WEEKEND
1	South Oak Bay / Downtown	45	-	45	-	-
3	James Bay / Royal Jubilee	30	65	30	-	60
7	UVic/Downtown	15	20	15	30	30

2.2.3 Cycling & Pedestrian Networks

The site is well connected to both walking and cycling networks. Fairfield Street and Crescent Road are signed bike routes. Downtown can be accessed in 15 minutes via Fairfield Street or via the nearby Richardson Street to the north. Continuous bike lanes on Foul Bay Road and Henderson Road provide residents with a direct cycling route to Camosun College Lansdowne Campus and the University of Victoria. The existing cycling network surrounding the site is shown in **Exhibit 2.4**.

All streets surrounding the development site have sidewalks on both sides.

The City of Victoria is rapidly upgrading its network of All Ages and Abilities (AAA) cycling infrastructure. Plans call for the existing signed bike route on Richardson Street to be upgraded to protected cycling lanes, creating a continuous, AAA cycling route that connects the site's neighbourhood to Victoria's downtown area. It is anticipated that these protected AAA cycling facilities will increase the attractiveness and viability of cycling to and from the development. With its substantial volume of bike parking, the proposed development at 1733-1737 Fairfield Street is well-positioned to support the anticipated cycling demand.

The location is within a walking distance of most everyday amenities and services, and all daily errands can be accomplished either on foot or on a bike. The Fairfield Plaza with food and services can be accessed in 6 minutes on foot or 2 minutes by bike. Walk Score is an on-line tool that assesses the walkability and bikeability of a location based on distances to a wide variety of amenities and services. The site scores a 65 for walkability which it defines as "Somewhat".

The location receives a Bike Score of 90 out of 100, placing it in Walk Score's "biker's paradise" category. This already high score is expected to improve with the cycling upgrades performed over the next few years.

2.2.4 Car-Share

Modo is a two-way carsharing service; registered members can pick up the vehicle from a parking spot and must return it to the same spot when they are done. The site has two Modo carshare vehicles located within 1 km of the site, the closest of which is located approximately 800 m away at Rockland Avenue and St Charles Street. Approximately 20 other Modo vehicles are located in downtown Victoria, which is easily accessible by bike or transit. Vehicles range from compact cars and sedans to SUVs and minivans, and sedan and compact are present within 1 km of the site. **Exhibit 2.5** shows the locations of nearby Modo carsharing vehicles.

Evo car share launched in Victoria in the summer of 2021 with 80 vehicles. Evo vehicles can be used and dropped off anywhere within the designated City of Victoria area.

Another potential carsharing option for residents of the proposed development is Turo. Turo allows individuals to rent out their private vehicles when not in use. As of February 2022, one vehicle is listed on

Turo within walking distance of 1733-1737 Fairfield Road, and more than 25 vehicles are listed in the greater Victoria area.

Other new car-sharing opportunities are anticipated in the years ahead as these types of businesses become more viable with app based and autonomous vehicle technologies.



Exhibit 2.1 Existing Road Network and Laning

04-22-0057
1733-1737 Fairfield Road
February 2022



Exhibit 2.2 Existing Transit Routes and Stops

04-22-0057 1733-1737 Fairfield Road
February 2022



Exhibit 2.3 Cycling Network

04-22-0057 1733-1737 Fairfield Road
February 2022



Exhibit 2.4 Carshare Vehicles

04-22-0057 1733-1737 Fairfield Road
February 2022

3. SITE PLAN DESIGN REVIEW

3.1 Site Access Design

The proposed development will have one vehicle access point from Beechwood Avenue. Pedestrian access to the site's apartment units will be accessed from Beechwood Avenue. The development will also have six townhomes that will front and will have pedestrian access fronting Fairfield Road.

3.2 Parking Supply

3.2.1 Vehicle Parking Bylaw Requirements

The City of Victoria's parking bylaw (Schedule C, Zoning Bylaw) specifies four the off-street parking sub-areas with different requirements. The proposed development is located in the "Other Areas" category. The residential component of the development must provide 0.85 to 1.45 resident parking spaces per residential unit depending on unit size. These rates consider the development's location outside of the Core or Village areas and the units being condo units rather than rental units.

In addition, Bylaw requires the development provide 0.1 residential visitor parking space per unit.

Bylaw requirements are summarized in **Table 3.1**.

Table 3.1: Vehicle Parking Supply Requirement & Provision

LAND USE	DENSITY	BYLAW RATE	BYLAW SUPPLY REQUIREMENT	PROVIDED	DIFFERENCE
Townhomes or Condominiums	0 units	0.85 spaces per unit that is less than 45m ²	28	21	-7
	0 units	1.00 spaces per unit that is equal to 45m ² and up to 70m ²			
	19 units	1.45 spaces per unit that is more than 70m ²			
	19 units total	0.10 visitor parking spaces per unit	2	1	-1
-	-	-	30	22	-8

As shown in Table 3.1, the proposed total parking supply of 22 spaces is 8 spaces below Bylaw requirements. The shortfall from Bylaw is comprised of seven resident spaces and 1 visitor space.

All parking spaces are accessible from Beechwood Avenue.

3.2.2 Bicycle Parking Bylaw Requirements

Well managed, secure, accessible and covered bicycle parking will be provided as part of the development plan. The site plan indicates a total of 32 long-term bicycle spaces in a dedicated bicycle parking room including one space for an oversized cargo bicycle. In addition, 6 short-term bicycle spaces will be provided at ground level in a well lit, weather protected and highly visible area.

Current City of Victoria Bylaw requirements are provided in **Table 3.2**.

Table 3.2: Bicycle Parking Supply Requirement & Provision

LAND USE	DENSITY	BYLAW RATE	BYLAW SUPPLY REQUIREMENT	PROVIDED	DIFFERENCE
Townhomes or Condominiums	19 units (all greater than 70m ²)	Long-term: 1.25 spaces per unit that is 70m ² or greater	24 Long-term 6 Short-term	32 Long-term 6 Short-term	+8 Long-term 0 Short-term
		Short-term: The greater of 6 spaces per building or 0.1 spaces per unit			

The proposed development plan offers 32 long-term bicycle spaces and 6 short-term spaces for a total of 38 bicycle parking spaces. This proposed supply exceeds Bylaw requirements for Long-Term spaces. The development's prioritization of long term, secure parking space is preferred over additional short-term spaces.

4. PARKING SUPPLY ANALYSIS

4.1 Resident Parking

The proposed vehicle parking ratio of one vehicle space per unit (plus two spaces for the 3-bedroom units) is consistent with City objectives to right size vehicle parking. The site is well located in regard to nearby viable non-private vehicle transportation options as well as nearby amenities. The one space per unit ratio allows for simple and clear marketing of the units.

4.2 Visitor Parking

Previous research conducted by Bunt has repeatedly suggested that a visitor parking rate of 0.05 spaces per unit for residential buildings is adequate to accommodate peak demands. This is supported by Metro

Vancouver's comprehensive "2012 Metro Vancouver Residential Apartment Parking Study"¹. The study found peak visitor parking demand rates in the range of 0.05 to 0.07 vehicles per unit for multi-family residential. This is consistent with Bunt's in-house database of peak visitor parking demand rates.

A visitor parking demand rate of 0.05 spaces per unit would translate to peak period demand of approximately one parking spaces for the proposed 19 units. This is consistent with the proposed one visitor space. While additional curbside spaces are available along both road frontages and are anticipated to be used by visitors and short-term delivery vehicles, they do not count towards the site's parking count which must all be on-site.

5. VEHICLE TRIP GENERATION ESTIMATES

Vehicle trip generation for a typical condo residential building of this type is in the range of 0.4 total wo-way vehicle trips per weekday PM peak hour (ITE Trip Generation Manual, Edition 10) which equates to approximately eight vehicle trips either entering or exiting the site per peak hour. This quantity of vehicle traffic is considered negligible and is not anticipated to have a significant impact on adjacent road operations.

6. SERVICE VEHICLE OPERATIONS

The City of Victoria Zoning Bylaw does not stipulate a requirement for off-street loading for residential land use. Loading activity for the proposed 19 residential units would likely involve vehicles no larger than a garbage/ recycling vehicle. Garbage and recycling bins will be accessible from the parking area.

Small sized delivery vehicles are anticipated to use the on-site visitor parking spaces or the available curbside parking along the site's Fairfield Road and Beechwood Avenue site frontages.

¹ 2012 Metro Vancouver Apartment Parking Study available at:
https://www.esquimalt.ca/sites/default/files/docs/municipal-hall/EVP/schedule_m_parking_study.pdf

7. TRANSPORTATION DEMAND MANAGEMENT (TDM)

7.1 Transportation Demand Management

Transportation Demand Management (TDM) is defined as the “application of strategies and policies to reduce travel demand (specifically that of single-occupant private vehicles), or to redistribute this demand in space or in time”². A successful TDM program can influence travel behaviour away from Single Occupant Vehicle (SOV) travel during peak periods towards more sustainable modes such as High Occupancy Vehicle (HOV) travel, transit, cycling or walking. The responsibility for implementation of TDM measures can range across many groups, including regional and municipal governments, transit agencies, private developers, residents/resident associations or employers.

7.2 TDM Initiatives

7.2.1 Bicycle Repair Station and Bike Wash

Aryze will provide an on-site bike repair station and a bike wash station within the bike room. This removes one of the barriers to cycling for residents who may otherwise have to purchase the tools required to perform basic maintenance on their bicycles. With the significant supply of bicycle parking and strong access to nearby cycling routes, a bike repair station would likely be well used in this development.



Figure 4.1. Bicycle Repair Station

² <http://ops.fhwa.dot.gov/tdm/index.htm> FHWA Travel Demand Management home page

7.2.2 Parking Management

The proposed parking supply of 22 spaces allows for simplified space allotment. The two 3-bedroom units will each have two vehicle spaces within enclosed garages. The remaining 17 units will each have one vehicle parking space. The development will also have one visitor parking spaces within the parkade structure.

7.2.3 Specialized Parking

Current Bylaw required energized electric vehicle outlets for each resident vehicle space. Aryze will meet this Bylaw requirement.

7.2.4 Bicycle Parking

The development will be providing 32 long-term bicycle parking spaces and 6 short-terms spaces. This supply exceeds bylaw requirements by over 27%. By doing this the developer will make bicycles a part of the buildings architecture and character. This will help to promote cycling.

The developer will provide electric charging abilities for the long-term bicycle parking spaces.

7.2.5 Transportation Options Information Package

New residents are considered a pliable demographic for transportation mode change as they have yet to establish travel patterns from their new address. Clear and simple messages along with practical information about local transit services and walking and cycling routes to and from the site can help encourage residents to use more sustainable transportation modes. Information should be distributed to residents upon their move-in or made available through a website or webpage. The information provided in print or on-line should include:

- Map showing local transit routes (can be obtained from BC Transit - Victoria website);
- Map showing local area cycling routes (can be obtained from City website – Victoria Bike Routes);
- Map showing amenities within a typical walking catchment of 800 metres (can be obtained from Walk Score website: www.walkscore.com)

8. SUMMARY

1. The proposed development at 1733 - 1737 Fairfield Road consists of 19 condo and townhome residential units.
2. Vehicle access to the building's parking spaces will be from Beechwood Avenue on the site's east frontage.
3. The development proposes 22 vehicle parking spaces located within a parkade structure.
4. The proposed vehicle parking ratio of one vehicle space per unit, plus two spaces for the two 3-bedroom units.
5. This proposed vehicle parking supply is 8 spaces short of the Bylaw requirement of 30 spaces. The shortfall from Bylaw is comprised of seven resident spaces and 1 visitor space.
6. The vehicle parking variance will be supported by the proposed TDM plan which includes the following initiatives:
 - a. Bicycle parking will exceed bylaw requirements.
 - b. Bicycle rooms will have enhanced access with at minimum 41-inch door widths, accommodate a cargo bike and have ground orientated spaces above bylaw requirements.
 - c. A bike repair station and a bike wash station will be provided within the bike room.
 - d. Adjacent sidewalk and public realm improvements.
 - e. Transportation Options Information Package

It is Bunt's view that the proposed vehicle parking supply at 1733 - 1737 Fairfield Road is appropriate for this development and this location. Saved costs from not building additional parking spaces can be passed onto residents through reduced development costs.

CALUC Meeting Report:
DATE: March 28, 2022

Address: 1733, 1735, 1737 Fairfield Road

Developer: Aryze
 Presenter: Chris Quigley, Aryze
 Representative from The Land Conservancy (Cathy Armstrong?)
 Architect: Greg Damant, Cascadia Architects



Attendance: 75 people on Zoom, including the applicants and members of the FGCA CALUC

Rezoning Requested	Current	Proposed
	R1-G Single Family Dwellings	Site-specific Zone Low-rise multi-residential units
Variances		
OCP Amendment required?	OCP describes ‘small village up to 3 storeys’	4 storeys
Number of Units	3 houses	6 townhouses 13 condos
	Current Zone	Proposed
Site Coverage		64%
Number of parking stalls		22 cars 32 long-term bike stalls 6 short-term bike stalls
Set Back East	1.5m	2.34m
Set Back West	3.5	2.40m
Set Back South	9.1m	2.00m

Set Back North	7.5m (2.5m to front steps)	2.36m
	Actual Building	Proposed Building
FSR (Floor Space Ratio)	0.5 to 1	1.61
Height	7.6 m = 2 storeys	13.8 m – 4 storeys

The Community Land Use Committee (CALUC) facilitates dialogue between land use applicants and the community to identify concerns regarding land use applications which may influence the proposal and result in changes more appropriate to the neighbourhood. The CALUC encourages a respectful meeting environment allowing everyone the opportunity to speak and be heard. The meeting is about the proposal not about the applicant or others involved in the project. There is no decision by the CALUC to support or oppose an application made at, or after, community meetings. Community members are encouraged to share their views with City Council via email (mayorandcouncil@victoria.ca). If an application is submitted to the City, information can be obtained through the Development Tracker feature of the City's website. (<https://www.victoria.ca/EN/main/residents/planning-development/development-tracker.html>)

Themes

Density Transfer

In February 2000, Abkhazi Garden was purchased by The Land Conservancy (TLC). According to the TLC, the zoning for the Gardens is not accurate and the buildings don't have heritage designation (explained that heritage designation is currently in the works, working to correct status being given to the wrong building). A partnership between Aryze Developments and TLC will officially remove any future possibility of developing the Abkhazi Garden greenspace through a density purchase agreement between both parties.

A density transfer is a first for Victoria, although it is believed to be a common tool used across North America, often where heritage buildings can sell their development rights in order to protect their heritage status. In this instance, Abkhazi Garden is acting as the donor site with the land at 1733-1735-1737 Fairfield Road being the receiver site of the density rights.

According to Aryze, of the available 35,000 sqft of density available from the Abkhazi Garden site, the proposal uses less than 30% of the density with the balance being released and secured as a financial donation for TLC . The density transfer will officially downzone the entire Abkhazi Garden lands to a zone that maintains the site's current activities.

Design

Three single-family character homes are currently on the site. There is an agreement with the landowner regarding the future of these houses, subject to the rezoning application. The proposed application is for a multi-unit building housing six 2bdr/2bth townhouses and 13 condos (1, 1+den, 2, 2+den options). Parking is at grade and there are 22 EV-ready parking stalls and 32 long term and 6 short-term bike stalls. Applicant says the application addresses the missing middle housing options. Plans include stairs and a ramp to the front entrance, 1.5m wide to address accessibility. 32% of the units are adaptable.

Community Consultation

A preliminary meeting and community meeting were held, one month apart. There were 75 people in attendance at the community meeting, held virtually on ZOOM. The CALUC received many inquiries about the process of applying for rezoning, about being heard as long-time residents of the neighbourhood and many emails expressing support or opposition to the application. (emails are included below).

Neighbourhood Comments/Feedback on Development Proposal:

Parking

- Considerations should be for underground parking to allow green space.
- What would be the EV charging provisions? (Response - not been confirmed yet)
- Will there be electric scooter parking and charging (Response: will look into including)

Accessibility

- Is there also a ramp on the Beachwood entrance, how is the town house consistent with suggested single level living.

Greenspace

- No mention of garden plots being included? (Response - possibility exists)

Neighbourhood Compatibility

- OCP states it should be up to 3 storeys and not 4 storeys
- Concern around the scale of the building. How to make it not a 4 storey
- Lengthy discussion regarding the Gonzales Neighbourhood Plan; applicant stating that the plan was 20 years old; community members stating that there is a new plan, it just hasn't been ratified by the City.

Light & Noise

- Is there mitigation around construction? Is there any noise or safety mitigation applicant would put in place? (Response: yes that will be considered. Also confirmed there will be no blasting due to no underground parking like with the Rhodo project across the street).
- Many comments concerning the height of the building and how it will block out light to gardens and sightlines in general

Density Transfer

- Is the density transfer agreement contemplated with a covenant? (Response: no)

- The land transfer seems to be used as a lever to disrupt the whole neighbourhood plan. It does not comply with the OCP, neighbourhood plan, FSR is 1 and half when it should be one; the mass is greater. This is a design that is not embraced by the neighbourhood.

Missing Middle

- Lengthy discussion regarding definition of 'missing middle' and the fact that this application uses the term, but it's not an accurate use of the term (more discussion in accompanying letters)
- Missing middle has already been constructed near the tennis court.

Further Comments (emailed to the CALUC)

Emails shared with mayorandcouncil@victoria.ca, and comments from the development tracker are not included here as the City receives these:

Please consider another alternative to the box-like construction that is almost at completion across the road. How about retaining the character of the removed houses by designing townhouses with an apartment in the basement to help with the mortgage.

The houses that occupy the 3 addresses now must have some valuable wood==I hope it would be recycled instead of producing landfill

I know the city needs to create more accommodation-but well-designed structures would add value to the neighbourhood instead of devaluing it.

Waine Ryzak 322 Robertson St.

As a community, we are seeing more loss of affordable housing stock so that developers can maximize profits. A couple of 450 square foot units, registered with CRD as below-market purchase options (thrown in for good measure to ensure approval of the bigger project), will not replace the full-size suites in the single-family homes that have been lost.

Currently, development in Fairfield/Gonzales appears to be focused on maximizing profits and not providing housing for those who live in, have grown up in, and/or work in Victoria. Building in Fairfield/Gonzales instead of other areas results in developers being able to charge, perhaps, 50% more for a unit even though the actual cost of land is not 50% more. Consequently, there is less focus on building in more affordable areas, and the affordable housing options being lost in Fairfield/Gonzales are not being replaced.

An additional impact of the status quo is that displaced tenants are having to move further away, and it is almost certain that they will not be using a bike to commute back and forth to Sooke, Langford or Shawnigan Lake, which defeats the whole purpose of the push for cycling networks.

For the 1733 Fairfield project, we would like to see something more innovative in the way the neighbourhood is being developed that could provide a combination of affordable homeownership and affordable rental, possibly in collaboration with the CRD? Based on the size of the lot, it could potentially accommodate 10 3-storey townhouses with suites. If a townhouse with a suite sold for \$1.2 Million, the suites could be registered with CRD so that the maximum rent would be tied to 30% of Victoria's median income (\$1,750 per month, with tenants being income-tested in some way). Likewise, buyers should be

income tested in some way which would ensure that those with the means do not take the opportunity away from those who would otherwise not be able to live in the neighbourhood. CRD and/or BC housing could, perhaps, provide some amount of guarantee to lenders, because there will be guaranteed income coming from the suites. Buyers in lower-income ranges would not, otherwise, qualify for a mortgage without some type of indemnified arrangement.

This would achieve 4 objectives

§ It would limit the amount that someone would be prepared to pay for one of the townhouses in the future as a buyer would be limited as to how much they could charge for rent – this would slow down the future appreciation of the townhouse. Currently, homeowners can charge whatever they want for rent (market) and so seem more inclined to bid up the price of property.

§ It would bring back some amount of both affordable homeownership and affordable rentals to the neighbourhood (20 units, which the developer is proposing; however, 10 of them would be rentals).

§ It would be more appropriate construction for the location and would be less obtrusive to neighbours.

Joanna Betts, on behalf of another community member

My name is Matt Hansen and I live at 351 Robertson Street. I'm writing to express my concern about the proposed Aryze development at 1733, 1735, and 1737 Fairfield Rd.

I am all for development and densifying housing but I, and many others in our neighborhood, feel that this proposed development is going to be too high. This project is going to set a precedent for future developments in the Fairfield / Gonzales community and if this project gets passed at the proposed height all of the future developments will push to be the same.

I strongly feel that 3 stories should be the most allowed along Fairfield Rd east of Moss st. Our community doesn't suite buildings any taller, it's just too much. Plus more stories means more people and with more people come more vehicles. With our street parking already at close to capacity that needs to be strongly considered as well.

On behalf of myself and my neighbors thank you for your consideration.

Kind regards,

Matt Hansen

City of Victoria website outlines the "missing middle." What is being proposed does not align, even though the developer keeps claiming they are there to help address the missing middle

<https://can01.safelinks.protection.outlook.com/?url=https%3A%2F%2Fengage.victoria.ca%2Fmissing-middle-housing&data=04%7C01%7Cplanandzone%40fairfieldcommunity.ca%7C242f91d953c440475e4f08da1a8bb487%7C1952fb077b0141879c3e2538855b9738%7C0%7C0%7C637851484578754863%7CUnknown%7CTWFpbGZsb3d8eyJWljojMC4wLjAwMDAiLCJQIjoiV2luMzliLCJBTil6lk1haWwiLCJXVCI6Mn0%3D%7C3000&data=IRf8Pltt6W6w1teYqocJfZlqQdfiobewDxfD6TsS6rs%3D&reserved=0>

This is going to council on April 14th - they recommend "applications must be consistent with OCP" which is how many in our neighbourhood feel about the proposed development at 1733-1737 Fairfield.

The building needs to be no more than 3 floors, as per our OCP. Perhaps the developer needs to also consider having their plans start below grade to help meet the height issue, which is of utmost concern to many of us.

The developer also claimed they wanted it to start at ground level to "minimize disruption" to the neighbourhood but I would argue it saves them money, while ruining our privacy and light. They need to go back to the drawing board and design something that follows our OCP.

https://cdn.discordapp.com/attachments/833778297116688386/962046262050455552/2022-04-08_HFL_-_Rapid_Deployment_of_Affordable_Housing_-_Support_Letter.pdf

Sincerely,
Joanna Betts

I reside at 330 Robertson Street directly behind this proposed development site. I am writing to express my strong opposition to the development as proposed by Aryze. Ours is one of several properties that will be impacted by the height of the proposed building as it will look directly into our backyard and home, impacting both the light we can expect during the day and our privacy. As avid gardeners this is of concern to us.

I am also concerned about the proposed density transfer from the TLC's Abkhazi property to this site currently zoned for single family dwellings. I believe that the developer is overreaching in terms of the site specific rezoning requested. They are using the density transfer to overbuild on the Fairfield site and greenwashing the whole process with the Abkhazi Garden downzoning. I hope Council will see through this manipulative proposal. While I value the continued presence of Abkhazi Garden in our community I do not think their goals to secure cash for unused density merits the added density and overbuilding proposed.

I support the goals of providing additional housing where it can be done in a way that complements the existing community rather than imposing this level of intrusion and disruption.

Let's stick with the OCP guidelines and build in a manner consistent with its aims. It is after all an expression of the community's aspirations and wishes. Please don't keep disregarding the concerns of local residents in a rush to meet developers and investor's wishes. The developers will still be here and we do have an opportunity to take our time and come up with a much better solution that will meet our needs for more housing and balance the increased density in a way that will better serve residents.

I'm not a NIMBY I'm a TBYA (Think Before You Act) Let's get this right before we set a bad precedent that will be pointed to by developers in the years to come. Let's send this back to the drawing board please.

Shawn and April Robins
330 Robertson Street

For 2 years, I have owned the house at 311 Robertson St., located behind this proposed development. I am opposed to this development going forward in its current form.

The Density and Height do not fit within the Community Plan that we the neighbours of Gonzales worked on.

The Land Conservancy needs money, so they are selling their Density to Aryze. This creates a precedent allowing developers to "buy" the ability to override bylaws and community plans. Abkhazi Gardens, the property and source of the density transfer, is located in Oak Bay. The proposed condo is in Victoria. How can this be allowed to go through? Buying density from another municipality is just wrong. The huge warehouse shaped building has no Green Space. Currently, the backyard neighbours look upon 3 houses which have trees, gardens, lawns, and space for families to enjoy the outdoors. The proposed condo complex has a setback from Fairfield Rd. and from the property back line that is less than 3 metres. There is no green space, only a parking lot. There are a few trees along Fairfield and at the property corners - no gardens or lawns.

We will lose property value in our homes. Prospective buyers will not want to look out on a 4 story building, 3 lots wide, where there once were 3 homes separated by trees, lawns, gardens and views of the sunset. Now there will only be a 4 story wall to look at. I would never purchase a home like that. Our homes are losing value because a developer wants to maximize their profit by forcing 19 condos into a space that once had 3 homes.

We are not NIMBYS. We live with 4 multi-family apartments or condo within 1 block of this proposal. All are only 3 stories tall, are within the density allowed for Victoria multi-family buildings and have green space. This huge building proposal contravenes height and density laws, has no green space, does not fit into the existing neighbourhood with its design, is not affordable to most people and negatively impacts a number of people living on Robertson Street and Fairfield Road. It does not meet the City of Victoria's Design Guidelines for Attached Residential Development. Townhouses would work/ a warehouse of a condo building does not. This proposal needs to be sent back to Aryze to redesign this complex to fit in our Neighbourhood.

Thank you -
Linda Maasch

I live near this proposed project at the corner of Robertson Street and Lillian. Our house is a designated heritage house and we have lived in it for 40 years in December. I can see the second storey of the present house at 1733 Fairfield through two back windows on our second storey; a bedroom window and a bathroom window. I cannot see the houses on the other two lots because of trees and an intervening house on Beechwood. From the main floor and our deck I can only see a bit of the roof of 1733 Fairfield.

I attended both the February and March CALUC meetings and reviewed the plans for the proposed project through development tracker prior to the March meeting.

I think that the proposed project is a good one for our neighbourhood. It offers both townhouses and condos of different sizes for various households who don't want or can't possibly afford a single family

house in this neighbourhood. It is on a main east west commuter road with frequent public transit. There seems to be extensive space for secure bicycle/stroller storage and provision for charging electric vehicles. It is a very short walking distance to Fairfield Plaza with Thrifty's, a pharmacy, a hardware store etc. and to Margaret Jenkins School.

I think that Aryze making a payment to Abkhazi Garden in exchange for a transfer of density to this property a short distance away on Fairfield Road is of benefit to everyone in Greater Victoria, as well as all those who visit the Garden during a holiday in Victoria. I had some involvement with the court proceedings around TLC's application for creditor protection in 2013-15 and it is gratifying that such a jewel of a property can never be under threat in the future once it is downzoned.

The Aryze project, Rhodo, that is beside Hollywood Park and kitty corner from the proposed project is almost completed, at least on the exterior. I can see the top storey of Rhodo from the same two back windows on our second storey. I do not find that Rhodo is too tall for a project on Fairfield Road though many people objected to its height before it was approved. Nor do I think it is too dense or too close to Fairfield when I drive or walk on Fairfield Road or visit the park. I find it a very attractive looking addition to our neighbourhood from all directions and think it serves as an excellent recommendation for this new proposal.

Most importantly I think the Fairfield Gonzales community should welcome new innovative projects providing a variety of household types, "the missing middle", at somewhat more affordable prices than most single family houses. It is possible that my husband and I might be interested in moving into a development such as this as we have talked about moving into a condo sometime in the next several years and we would really appreciate being able to stay in this neighbourhood.

Sharon Walls

Hi there,

I wasn't able to make the March 28th CALUC meeting and would like the following input to be added to the CALUC report for the 1733/1735/1737 Fairfield Road proposed development:

- I am a Gonzales resident, and I support the development
- I would like to see more 3 bedroom or 2 bed+den units, but I support the proposed approach as-is
- This neighbourhood desperately needs more housing supply. This project is a step towards helping to address this need for people. I think that the city council should demonstrate leadership in meeting the needs of the community now, and the needs of the future. As such, housing developments like this should be approved

Thank you for including this feedback in the report.

Brian Vatne

My wife and I attended both your meetings on the proposed development at 1733-37 Fairfield. Our house is at the corner of Lillian and Robertson – so not far from the site in question. We have lived there for almost 40 years.

The proposed development seems appropriate for the site and the neighbourhood. It is too bad that the existing houses cannot be saved, but, if they are carefully deconstructed, that is the next best thing. The proposed transfer of development rights from Abkhazi Gardens to this site is of great benefit to the community. Having attended the bankruptcy proceedings for TLC years ago, I well know that the TLC has few saleable assets, and that these are the most important ones. The deal with Aryze will offer the TLC some financial relief and ensure that future TLC creditors cannot force a sale of Abkhazi Gardens to settle the TLC's debts.

Fairfield Road is the most appropriate site in the neighbourhood for apartment/townhouse developments, for which there is a great need. Given the high price of land in the neighbourhood – the land our house sits on was assessed at over a million dollars, and this is for an ordinary-sized lot – it is unrealistic to expect a private developer to offer units of any sort for sale at truly affordable prices. In relative terms, apartments and townhouses are bound to be cheaper than single family houses on separate lots. So, in that sense, this development will make living in the neighbourhood more affordable, whatever the ultimate costs of the units in question.

Warren Magnusson
Professor Emeritus
Department of Political Science
University of Victoria

304 Robertson Street
Victoria BC Canada V8S 3X7
250-595-8735

I attended the community meeting and I was quite disturbed by many of the issues regarding this proposed development. I have a few questions I was hoping you could answer for me as I would like to look into this further. Firstly, I was very concerned that the developer wanted to disregard our official community plan. He referred to the older community plan being too old to count and the newer 2018 community plan not counting because it wasn't "ratified". Are people allowed to just discount the plans like this? And if so what is the point of even making them? Clearly, neither the new version or older version of the community plan allows for his gross overdevelopment plan that disrespects this family residential neighbourhood. So my question to you is doesn't the community plan need to be followed? Or can it just be ignored like this? And who has power over insuring the community plan is followed? Is there a name of someone at the planning department I could speak with? Thank you in advance for any direction you can give me.

Next, this property density transfer seems very sketchy. There is no process in place for this in Victoria. It seems like nothing other than a financial bribe to get to overdevelop by manipulating and purposely misinterpreting the rules. I question the legality of this. Would involving a lawyer and/or the press be a good idea in order to ensure unscrupulous behaviour isn't being encouraged by having unclear rules? Does this not set a precedent that could lead

to massive corruption of the rules into the future? I feel like everyone in Victoria should be aware and alarmed by this.

I plan to continue to pursue this until I can get to the bottom of it. Any contacts or advice you can give me would be greatly appreciated. I will be also contacting the Times Colonist regarding these issues as well as my lawyer. Hoping we can make sure that we can all make a plan together as a community to address this. Thank you so much for your help.

Sincerely,

Catherine McCartney

We would like to thank the Fairfield Gonzales CALUC for hosting the public meeting for the proposed development on 1733, 1735 and 1737 Fairfield Road. We have numerous concerns with the proposal and believe it will be highly disruptive to the neighbourhood. The whole notion of a Density Transfer with The Land Conservancy is highly questionable. It appears to manipulate the density transfer concept to usurp planning and zoning standards in order to increase site density well past what is reasonable. The proposed 4 story apartments are incompatible with the neighbourhood because of the density, design, mass, height and layout. The development should be addressing the needs of families rather than one and two bedroom apartments. We request Aryze to go back to the drawing board and resubmit a proposal that meets the following criteria:

- townhouses with individual ground entrances
- maximum height be limited to 3 stories
- proper setbacks be aligned with the neighbours
- greenspace and proper landscaping
- density of no greater than 1.0 FSR
- appropriate design that fits into the neighbourhood
- proper massing with the neighbourhood
- adequate required parking (ideally underground)

Janice Linton and Kevin Warren (356 Robertson Street)

Hello,

I am writing to express strong opposition to Aryze's building proposal at 1733-1737 Fairfield Rd. While I understand and support densification, development projects such as this need to be aligned with the Fairfield/Gonzales neighbourhood plan. The proposed 4-story structure is disrespectful in this regard, both to the plan and to the neighbours whose properties and privacy will be negatively impacted.

Moreover, the notion of using a 'density transfer' to justify the increased building height is inappropriate. Density transfers are typically used in core urban areas, not single family

residential neighbourhoods. In addition, while these transfers have been used to protect heritage sites, Abkhazia Gardens is not under threat and the property's zoning 'asset' should not be allowed to be purchased and moved. There is no rationale or need for this with either of the involved properties, other than maximizing profits for Aryze.

Finally, to the best of my knowledge, the City of Victoria does not currently have any governing policy or process in place to allow for a density transfer. It would be crucial to have this in place prior to allowing this, or any other 'density transfer' proposal to move forward.

Please help support me and my many concerned neighbours of the Fairfield/Gonzales community by ensuring this development does not move forward as proposed.

Sincerely,
Sean McCartney

Hello,

We would like to express our opposition to the current building proposal at 1733-1737 Fairfield Rd. This proposal is not respecting current zoning and the proposed 4 story height will destroy many surrounding neighbours' ability to enjoy their properties and privacy. Density needs to occur in a respectful way for the neighbourhood. Developers need to stick within the current zoning requirements and height limitations. This proposal of "density transfer" is inappropriate. The city of Victoria does not have a policy for this nor a process in place. Density transfers are used in downtown cores and city centres not residential areas, and only in cities that have a clear existing policy and governance for this process. How can we allow someone to just make up new rules? Density transfers are used to protect heritage sites, but Abkhazi Gardens is not under threat. This is simply manipulation in order to maximize profit at the expense of the neighbourhood. We want to be supportive of appropriate densification, as clearly outlined by the neighbourhood plan and the rules for height and setbacks that are currently in place. Please help support our neighbourhood and protect us from uncontrolled over-development.

Sincerely,
Catherine McCartney

CALUC Meeting Report:
DATE

Address: 1733, 1735, 1737 Fairfield Rd

Developer: Aryze Developments

Presenters: Chris Quigley (Aryze Developments); Greg Damant (Cascadia Architects); Peter Johannknecht (Cascadia Architects); Diana Stenberg (TLC)

Architect: Cascadia Architects

Attendance: 20



Rezoning Requested	Current	Proposed
Variances		
OCP Amendment required?	Yes	TBD
Number of Units	3	31
	Current Zone (R1-G)	Proposed Zone (TBD)
Site Coverage	30%	65%
Number of parking stalls	36 stalls	22 stalls
Rear Yard Setback (East)	1.5 m or 15% of lot width (whichever is greater)	2.62 m
Front Yard Setback (West)	3.5 m or 15% of lot width (whichever is greater)	2.73 m
Internal Side Yard Setback (South)	9.1 m or 30% of lot depth (whichever is greater)	0.5m-4.78m
Internal Side Yard Setback (North)	7.5 m	2.33 m
	Actual Building	Proposed Building
FSR (Floor Space Ratio)	0.5:1-1:1	1.79:1
Height	7.6m (1.5-2 storeys)	13.39 m (4 storeys)

Commented [OS1]: Might just be worth confirming the info in the table. Couldn't find the latest version of the plans but feel free to send them my way!

The Community Land Use Committee (CALUC) facilitates dialogue between land use applicants and the community to identify concerns regarding land use applications which may influence the proposal and result in changes more appropriate to the neighbourhood. The CALUC encourages a respectful meeting environment allowing everyone the opportunity to speak and be heard. The meeting is about the proposal not about the applicant or others involved in the project. There is no decision by the CALUC to support or oppose an application made at, or after, community meetings. Community members are encouraged to share their views with City Council via email (mayorandcouncil@victoria.ca). If an application is submitted to the City, information can be obtained through the Development Tracker feature of the City's website. (<https://www.victoria.ca/EN/main/residents/planning-development/development-tracker.html>)

Themes

Building Design

1. Would this development be considered to be part of the Missing Middle or a different initiative?
 - Not included as a missing middle housing form under the Missing Middle Initiative due to the location and zoning.
 - Would be a different form of housing.
 - Doesn't sound like the middle to the questioner.

Abkhazi Garden

2. What is happening at the Abkhazi Gardens? Can't the City just change the zoning?
 - Related to downzoning the gardens to protect it in perpetuity, which is connected to the upzoning of this site.
 - The City gave the TLC the ability to monetize its development rights. It is providing this density benefit to this application in exchange for purchasing those rights.
 - The application would still need to provide an amenity contribution under the CAC Policy. This is an example of a contribution.
 - No other amenities will be provided on this application based on the land lift analysis presented to staff and Council.
3. Is this a precedent for heritage preservation?
 - This has been done in other cities for the purposes of heritage preservation or other public benefits.
 - Abkhazi probably would persist regardless.
 - This is quite a bit bigger than what would normally be allowed under the OCP. Seems odd that there is a fixed density that must be met.
 - Ultimately Mayor and Council will make the decision on the merits of the development.
4. Is there a guarantee that the TLC will downzone?

- The downzone is directly tied to the upzoning of this project.
- The City will also be putting a covenant on the property to maintain a garden on the site.
- Surprised by the arrangement and ultimately that the garden space will be lost on these existing homes.

5. Did this project come up with the TLC Board of Directors?

- Yes, it did.
- It was previously expressed that the TLC's concern is provincial in scope.

CALUC Meeting Report: March 27, 2023

Address: 1733, 1735, 1737 Fairfield Rd and 1964
Fairfield/507 Foul Bay Road (Abkhazi Garden)



Applicant: Aryze Developments

Presenters: Melanie Ransome (Aryze Developments); Chris Quigley (Aryze Developments); Greg Damant (Cascadia Architects)

Architect: Cascadia Architects

Attendance: 50 in-person attendees

CALUC: Joanna Fox (Chair), Owen Sieffert, Carrie Fuzi (Don Monsour, Ashley Fernandes absent)

The first Community Meeting for this application was held on March 28, 2022. There have been considerable changes in the application, which required a second Community Meeting.

Rezoning Requested	Current	Proposed
Variances		Parking Variance: 30 units, 22 parking stalls
OCP Amendment required?	Required <ul style="list-style-type: none">- to amend 1.0 FSR density allowance for Traditional Residential- to amend the height of the 3-storey maximum allowed for Traditional Residential	1.73:1 FSR 4 storeys - 13.39 m
Number of Units	3	30
	Current Zone (R1-G)	Proposed Zone (TBD)
Site Coverage	30%	64%
Number of parking stalls	3 stalls	22 stalls
Set Back East	1.5 m or 15% of lot width (whichever is greater)	2.33 m

Set Back West	3.5 m or 15% of lot width (whichever is greater)	4.78 m
Set Back South	9.1 m or 30% of lot depth (whichever is greater)	2.62 m
Set Back North	7.5 m	2.73 m
	Actual Building	Proposed Building
FSR (Floor Space Ratio)	0.5:1-1:1	1.73:1
Height	7.6m (1.5-2 storeys)	13.39 m (4 storeys)

The Community Land Use Committee (CALUC) facilitates dialogue between land use applicants and the community to identify concerns regarding land use applications which may influence the proposal and result in changes more appropriate to the neighbourhood. The CALUC encourages a respectful meeting environment allowing everyone the opportunity to speak and be heard. The meeting is about the proposal not about the applicant or others involved in the project. There is no decision by the CALUC to support or oppose an application made at, or after, community meetings. Community members are encouraged to share their views with City Council via email (mayorandcouncil@victoria.ca). If an application is submitted to the City, information can be obtained through the Development Tracker feature of the City's website. (<https://www.victoria.ca/EN/main/residents/planning-development/development-tracker.html>)

Themes

Density and Growth Areas

1. Would love to live and play near their work in the neighbourhood and would hope that some future homes are found in Fairfield Gonzales that could include their family.
2. What does the OCP say as a designation?
 - a. Townhomes.

Design

3. What is height of the development in feet?
 - a. Each unit would have a 9-foot ceiling with 10-foot ceilings on the top floor.
 - b. Total height is 13.39 m or 43.9 feet) was previously 14.45 m or 47.4 ft.
4. Why can't the development be 3 storeys?
 - a. The approach is guided by trying to use land efficiently and not under build in this location. As such, the applicant is looking to build with future growth along Fairfield

Road in mind. If the applicant doesn't use this land efficiently this demand for housing will go to other communities.

5. What will the rear façade look like?
 - a. The storeys facing Fairfield Rd will be metal on the top level and brick below; at the rear, the levels are lower and will be brick.
6. How will the neighbouring homes be considered in the design and the shadowing? This is an important consideration for a 4-storey building in the middle of the residential neighbourhood.
 - a. Shadowing from the existing SFD is shown in the supporting documents. This shows that there may not be significantly more shadowing cast from the existing dwelling.
 - b. Applicant can see why there would be major challenges and impacts on neighbours and generally, is trying to find growth that is suitable for this site.
 - c. Moved mechanical away from immediate neighbour's property line and is planning extra landscaping.
7. Height is the issue, not the density and is likely the biggest concern for most people. With this design, neighbours are losing all their privacy.
8. Not comfortable with the way that the density transfer is being handled, not enough transparency.
 - a. The developer has heard a mix of feedback from the community.

Trees and Landscaping

1. There is a provision for a couple of trees in the corner and along the front of the building. There will be virtually no greenspace on-site. Also, feel it is disingenuous that Abkhazi Garden will be the contribution to greening the community; the merits of this development should be considered away from the partnership with Abkhazi.
 - a. Will have permeable pavement on the site to allow for infiltration.
 - b. Sufficient soil volume for healthy trees (both existing and future) will be retained, 10 trees are being removed and 21 will be replanted along with 7 boulevard trees.

Accessibility

1. Please provide clarity around adaptable/accessible units. The brief says "not only are we ensuring there are housing options with single-level living, but also units that could be modified to become adaptable if required. "[Generally, one builds adaptable and could modify to accessible. Is this statement what the applicant meant?]
 - a. How many adaptable units will be built if no one "orders them" during the sales process?

- We are exploring how the revised design could accommodate a percentage of units that can be easily converted to adaptable. Should a prospective purchaser request this, we will happily accommodate through the sales process.

b. Is the applicant willing to convert adaptable to accessible during the sales and finishing process?

- If a prospective purchaser requests this through the sales and finishing process, we will be willing to convert an adaptable unit to be fully accessible.

c. Which units would be adaptable, and would they be closest to the elevator?

- We are currently working on an analysis to determine the number of units that could be easily converted to adaptable units. This analysis will be completed prior to the application going to Council. Once confirmed, we will have a better idea of which units could achieve this and where they are located in relation to the elevator.

d. Is the applicant willing to list commit to a number of adaptable units in Residential Use Table as a Unit Type?

- As mentioned above, we are currently determining the number of units in the revised design that could be easily converted to be adaptable based on the 2018 BCBC.

2. How will the objectives of accessibility in landscaping be met? Please provide more clarity and specificity regarding this statement. “Biophilia believes that exterior landscape and interior green space should respect, enhance and work symbiotically with architecture and be accessible to people of all abilities to help improve health and well-being. Their focus is to create synergies between the soft plantings and the hardscape aspects of the built environments they create that both relate to and enhance the architecture while ensuring accessibility.”

a. Will the applicant take the OPALS rating into consideration?

- The proposed landscaping follows the City of Victoria requirements and provides a variety of deciduous trees, native plantings and species selected for their ornamental and aesthetic qualities. Further to this, the use of planting beds will be placed along with parkade drain tiles to offer efficient stormwater management absorption.

b. Are the proposed pavers accessible for someone with mobility disabilities?

- The proposed pavers are designed to be used in multiple settings such as on terraces, roof decks, courtyards, swimming pools or any traditional hardscape areas. As such, they are designed with an anti-slip surface.

Accessible parking

1. What class of parking is the one accessible stall: visitor or resident, van or regular accessible?

- Currently the drawings classify the accessible stall as a resident stall, however, we have been considering changing it to a visitor stall to provide better parking diversity and options for all future residents. We are open to hearing your recommendations on how this stall should be classified, what are your thoughts on this approach? The stall is a wider width to accommodate a van.

2. Is the applicant willing to label the stalls as such in the plans? (Currently, only the dimensions are shown on the plans)

- Yes, this can be shown in the next set of drawings.

3. Is the applicant willing to describe the accessible parking in the Project Details table? (All it says now is 20 provided, 2 visitors, no mention of accessible)

- Yes, this can be shown in the next set of drawings.

4. What is the width of the door to the building, and will it be equipped with an automatic door?

- We are required to have an automatic door, and the door width will meet accessibility requirements.

5. Does the proposed Dorado Drain Paver meet Schedule C accessible surface requirements?

- The Dorado Drain Pavers will provide a durable, slip-resistant surface per the Schedule C.

6. What provision is there for mobility scooter storage and charging?

- No specific allocated space has been provided for mobility scooter storage and charging at this time.

Accessible Amenities

1. Are all amenities such as waste disposal and storage rooms accessible?

- Yes, we are carrying the minimum accessible width.

2. Are solid waste and recycling accessible in terms of access doors, height, level of effort, etc.? i.e., if those large, tall private garbage skiffs are being use, how will residents in wheelchairs or limited physical strength and ability use them?

- Ample area has been allowed for the standard garbage and recycling totes utilized by most third-party waste management companies. If a prospective resident requested additional totes at a smaller size to be better accessed, we are open to working with the waste management company to facilitate this.

3. Is the applicant willing to commit to providing accessible outdoor furniture, such as a wheelchair accessible picnic table? (the one shown appears not to be)

- This will occur at the time of building finishing; however, it is something we will consider accommodating.

4. What is the width and treatment of the storage room access and interior doors? (automatic?)

- We will explore wiring all interior common area doors to be automatic, and the widths will meet the minimum standards.

5. Is there maneuvering space for a wheelchair user in the storage room?

- The minimum width is achieved in the storage room and common corridors.

Accessible Bicycle storage

1. Is there sufficient transfer space in the bike storage room for an adapted bike.

- Currently, this is not contemplated in the bike room, however, we are providing above the Schedule C bike parking requirements. We will explore the idea of removing some of the bike parking stalls to accommodate the space for this if the City of Victoria permits it.

2. What is the treatment on the door to the bike room? Is it accessible to all residents, as required by Schedule C?

- Yes, and we will explore hard wiring the door to be automatic through design development.

Accessible Miscellaneous

1. Is there isolation of air from one suite to another and within the suite from the common areas? - Suites will be individually ventilated and will meet all applicable standards and regulations for new buildings.

Neighbourhood Comments/Feedback on Development Proposal:

Parking

1. Where are all the cars and bicycles going?
 - a. Bikes and vehicles will all parked be on the ground floor.
 - b. Traffic engineer provided analysis of how local on-street parking is used. There are 41 public spaces, of which were approximately 61% occupied, so there is ample on-street parking.
 - c. There could be support given to having residential parking only on Beechwood or other neighbouring streets.
2. How will construction parking be managed? (Based on discussion of how construction parking for the Rhodo development was frustrating and often dangerous for neighbours)
 - a. Applicant will try their best with trades to manage behaviour during the construction period.
 - b. The developer is constantly working with crews and trades to educate on safety practices and have their own safety manager to ensure high standards on their job sites.
3. The site plans show a curb extension on the northwest side of Beechwood. Why has the curb been bumped out?
 - a. To prevent people from parking there to preserve sightlines onto Fairfield Rd.
 - b. This would result in the loss of a couple of parking spaces near the corner. However, these spaces should likely not be parked in due to safety concerns.
4. How many accessible parking stalls will be provided?

- a. There is 1 stall that adheres to the dimensions outlined in Schedule C of the Zoning Bylaw.
5. How are parking stalls being allocated (as there are less stalls than units)?
 - a. They will likely be assigned to a unit as part of a sale. It is rare for stalls to be decoupled and purchased separately and is not something that the applicant has done before.
6. Have you considered having some tenants be car-free?
 - a. Not currently considering legal agreements as they are difficult to enforce.
 - b. Location is everything about transport and encouraging fewer trips by automobile. Providing alternate transportation options is the best method.
 - c. There are noticeable downward trends in vehicle ownership and driver license registrations. The applicant has previously sold to people who want to live that lifestyle. It is unlikely that legal agreements requiring car-free residents would be implemented.
7. Would hope to see lots of bicycle parking, EV charging, and car share, as more new developments are integrating these elements to reduce vehicle use. How are these being addressed in the proposal?
 - a. 52 bicycle parking stalls that are heated, secured, indoors, and provide opportunities for charging.
 - b. All parking stalls will be electrified.
 - c. Commitment for all residents to have free memberships and there have been conversations with car share providers to increase vehicle supply in the area.

Neighbourhood Compatibility

1. As a renter across from Hollywood Park, everyone uses the park like their backyard and this development could be the same. They feel they are the people who can afford this bracket of housing.
2. In response to a comment that the units could be purchased by people who will only use them for part of the year, it was noted that this would not be unique to this property as there are already homes in Fairfield Gonzales that have been vacant for years. Residents are equally concerned about amenities, such as the grocery store, as other aspects of growth.

Traffic Safety

1. How will the building affect the safety of neighbouring homes, including sightlines for access and egress off Fairfield Rd? There have been near misses at this location with existing sight triangles and this development could worsen that.

- a. Comparing this project to Rhodo is difficult since the access to building will be off the Beechwood and therefore does not provide the same sightlines for those on Fairfield Rd.
- b. Developer can take some photos from the property to work with the traffic engineer to manage sightlines and support safe access.

Sustainability

1. Are the existing buildings being re-purposed or recycled?
 - a. Have had success with moving older buildings and reusing them. Have had early conversations on the viability of that move for this site. If it can't happen, the buildings will be deconstructed to reuse most of the materials.
 - b. Generally, not interested in building buildings that need to be replaced.
 - c. The building will also be fossil fuel free; however, there are no renewables planned for the site.
2. Is there shared greenspace in the development?
 - a. Could consider providing a pass to the residents to access Abkhazi Gardens such as those provided to the immediate neighbours of the gardens.

Affordability

1. Will there be below market units in the increase in units?
 - a. Looking into supports for first-time buyers.
2. How much is the 2-bedroom townhome going to be?
 - a. Difficult to say how much given that the market could change significantly in the next three years or so.
 - b. Going forward with price points is unfair.

Tenant Support

1. How will existing tenants be supported in the redevelopment?
 - a. The developer will provide tenant assistance and support (relocation, moving assistance) through their staff Tenant Assistance Program coordinator.

Abkhazi Garden

1. It is understood that Abkhazi is likely to be safe, irrespective of this development. Still, this is being used as a rationale to move this development forward. No one has listened when the fourth floor has been seen as too much.

- a. The garden remains zoned for townhomes, as when TLC purchased the property. TLC feels like the zoning needs to align with the use (park and tourism), and this has been echoed by the City.
- b. Bonus density from the City has been the alternative instead of the partnership of Abkhazi. The City provides similar provisions for other community amenities.
 - i. Instead of cutting a cheque for the City, the funds will be going directly to the preservation of a heritage asset.
 - ii. This is the first density transfer.
- c. Could downzoning for Abkhazi Gardens not be achieved for free?

Email comments:

From Carol Armstrong, Gonzales

I attended the CALUC meeting on 1733-1737 Fairfield Road. I am not opposed to the development in principle but have the following concerns:

- the 4 stories proposed/overall height of the building. This is NOT in character with the neighbourhood. Neither is the flat roof design
- the density has gone from 19 to 30 units. Originally capacity projected approx. 43 people, now it's potentially 96 people. It seems likely that more than 22 people will have vehicles
- parking - discussed at length tonight. Available street parking is already a competitive sport.
- equally concerning is the access/egress to the proposed development. Aryze did not make it clear to neighbours that they would be removing 3 existing street parking spaces for people currently living at Monterey Mews across Beechwood. Due to the already tight corner on this portion of street, they should have explained to the public that this is the only way access would work, by removing current residential parking.

Also, traffic concerns about traffic entering from Fairfield without causing congestion backing onto Fairfield.

Other concerns:

- the shading of neighbouring gardens/homes
- the privacy for immediate neighbours both within their homes and using their decks/gardens, especially with regards to the 4th floor.
- the reduced setbacks from the original proposal.

I live in the 300 block of Beechwood and anticipate much busier street traffic that is less child friendly.

From Susanne Rautio, Gonzales

The OCP designation for this part of Fairfield Road is Townhouses. I respectfully request that council honour what has been agreed to by the community and only allow this type of development. And

further, that any changes to this designation and the OCP be undertaken with community involvement.

The bonus density "purchased" by ARYZE from Abkhazi gardens would possibly account for the addition of the third level but not to a fourth one.

Overall, the building mass is too large; it extends almost to the property line on three sides. We know that the doors opening onto Fairfield Road by Rhodo are not friendly to the community and certainly not to the owners. The curtains are always drawn as the windows are too close to the road. There is no privacy. Please learn from this mistake - the building should be further back for the sake of the owners.

The fact that there are not enough parking spots will create more problems with Fairfield Road similar to what is happening with Rhodo. Again, we need to learn from previous decisions. This is the third building ARYZE will be building in our community. We have lost at least 50 trees because of their developments. We request that a community amenity be provided by ARYZE to the neighbourhood of Gonzales; specifically, the planting of 100 mature, native trees on those private properties that will take care of one each. This will be in line with the Tree bylaw whereby 2 trees are to be replanted to replace one that is lost. This can be administered by the FGCA or the GNA.

From Janice Linton, Robertson Street

I respectfully request Victoria City Council reject the revised proposal to develop 1733, 1735 and 1737 Fairfield Road. The revisions do not effectively address the main issues that were brought to the attention of the developer.

The height, mass and density far exceed what is reasonable in this area. The proposed density transfer provides no benefit to the city and is highly disruptive to our neighborhood. The four-story apartment building design is not compatible with principles of family-focussed attainable housing or green space enhancement.

I am supportive of densification efforts in Gonzales that provide quality housing for families. In Traditional Residential areas I would like to see ground-oriented dwellings that are limited to two and a half story house plexes, duplexes and townhouses with landscaped outdoor activity space.

From Kevin Warren, Robertson Street

The proposed development at 1733, 1735, and 1737 Fairfield Road is a looming four-story apartment building that is over 20 feet higher than the surrounding houses and would become the highest structure in all of Gonzales.

There are no front or rear setbacks, no useable ground level outdoor space, and outdoor areas are paved with minimal landscaping. The building is highly disruptive to the neighbourhood because of its height and mass, density, layout, appearance, number of units, parking, and impact on the neighbor's privacy.

Aryze has not demonstrated any added community amenities to merit the proposed density transfer, nor does it provide a convincing case that the receiver site is suitable. The developer has simply

bought density from a third party to maximize profit and usurp city planning bylaws well beyond what should be considered reasonable for the site.

The requested density is far beyond what the site and neighborhood can accommodate. OCP amendments will be needed to increase the height beyond the three stories maximum required in a Traditional Neighbourhood designation (Section 6.1.5 and Map 23) and increase Floor Space ratio (FSR) from 1.1 to 1.73, as well as front/back setback variances.

I believe densification efforts in Gonzales should provide quality housing options for families. Any new development needs to be compatible with neighbors, have respectful front and rear yard distances, usable rear yards, access to outdoor open green space, consistent massing, adequate underground parking, and consistent character. In other words, all infill buildings in Traditional Residential areas of Gonzales should be ground-oriented dwellings that are limited to two and a half story house plexes, duplexes and townhouses.

The revised proposal by Aryze did not address the main issues and will significantly impact our neighbourhood, setting a dangerous precedent. I respectfully request Victoria City Council reject the proposal.

From Kelly Galitzine, Fairfield

I fully support the proposed developments at 1733/1735/1737 Fairfield Rd plus 1964 Fairfield/507 Foul Bay. I live at Rhodo, 1720 Fairfield Rd, in unit 104.

Thank you for this opportunity to voice my support for these much-needed developments.

Survey Responses

1733, 1735 and 1737 Fairfield Road, 1964
Fairfield Road and 507 Foul Bay Road

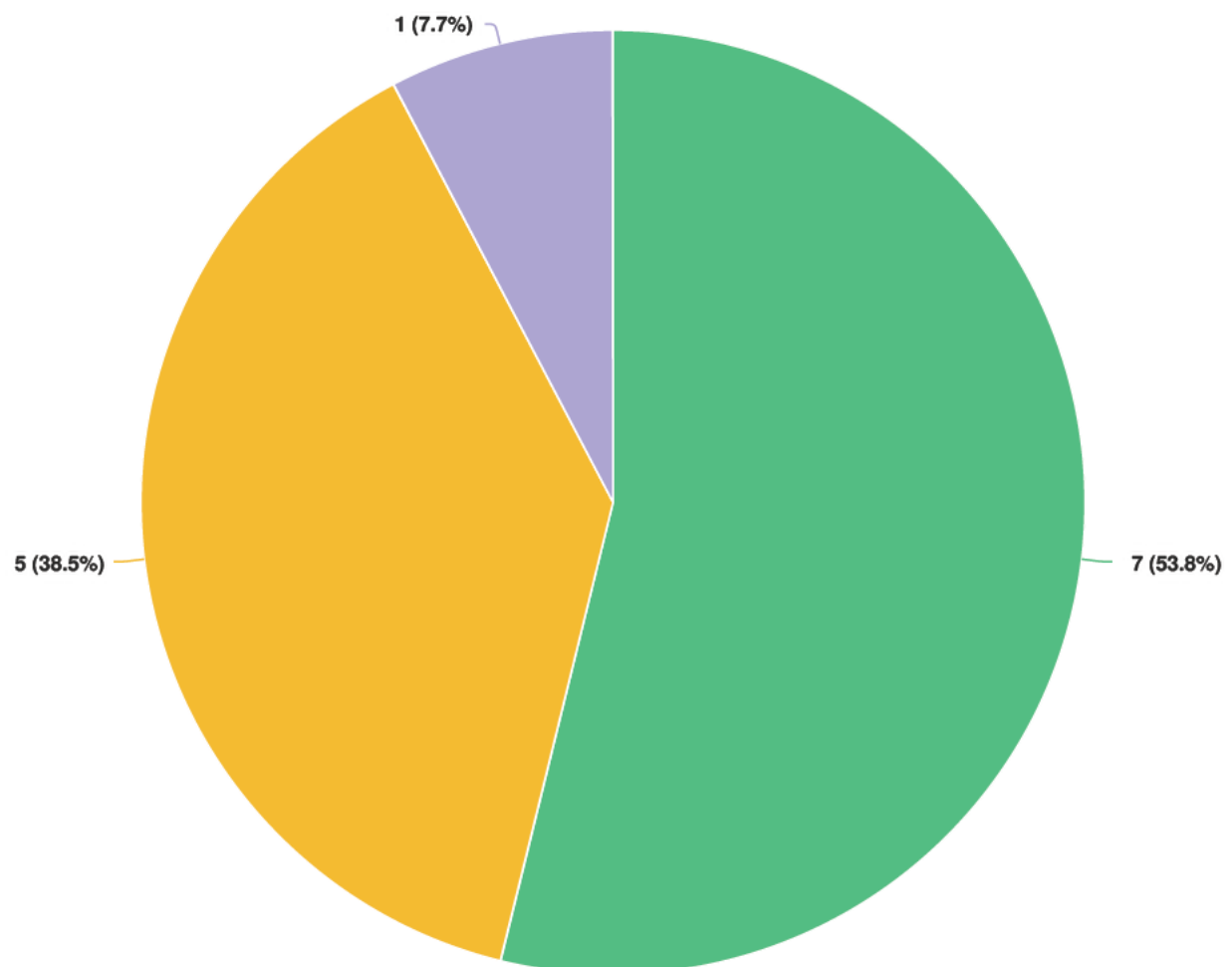
Have Your Say

Project: 1733, 1735 and 1737 Fairfield Road, 1964 Fairfield Road and 507 Foul Bay Road



VISITORS					
15					
CONTRIBUTORS			RESPONSES		
13			13		
1	0	12	1	0	12
Registered	Unverified	Anonymous	Registered	Unverified	Anonymous

Q1 What is your position on this proposal?



Question options

Support Oppose Other (please specify)

Mandatory Question (13 response(s))

Note: Participants may submit multiple responses. See detailed feedback in the following pages.



Respondent No: 1
Login: Anonymous

Responded At: Mar 14, 2023 18:54:01 pm
Last Seen: Mar 14, 2023 18:54:01 pm

Q1. What is your position on this proposal? Oppose

Q2. Comments (optional)

Frs . PARKING I ve across from the back of the three story apartment building at Fairfield and Lan S . where there are more apartment units than parking spaces. I have watched those who park in the Resden a Parking On y spaces in front of my house go in and out of the apartment building. At least once a week one of their cars blocks the driveway of my house making it impossible. If I see the drivers I ask them to please move but too often by law officers have to come and check the offenders or they keep parking illegally. If the 30 unit Aryze development were approved the residents who do not have one of the 20 parking spaces will be looking for street parking in the Resden a neighbourhood. Not only the 10 unit south parking but all the two bedroom units have the potential for second cars. Taxpayers in the existing homes will have compromised driveway access and the City Parking employees will be busier than ever. Many of us believe we need a car to go off Vancouver Is and, to grocery shop, have social lives and generally cope with the cold weather. I belonged to Modo for a year of car free living and was very frustrated. I understand why people in multifamily dwellings have cars. But they must have parking on site for every unit! Second. APPEARANCE The blocky modern design does not fit the aesthetics of our hundred year plus neighbourhood. Also, a four story building is not okay just because there are some asphalt roofs nearby. The diagrams showing the height comparisons are smoke and mirrors. Third WATER What provisions have been made for waste water and storm water runoff? The number of trees being increased by about 800%. Additionally, we have enough problems with drainage in this old neighbourhood. Substituting the existing lawns and gardens with impermeable surfaces seems very unwise. Fourth TDB I am opposed to a "transferable density bonus" schemes. Population density is based on real humans on real land and not an abstract concept. Thank you for considering my emphatic opposition.

Q3. Your Full Name Maureen Eey-Round

Q4. Your Street Address 267 Wood Ave

Q5. Your email address (optional)





Respondent No: 2
Login: Anonymous

Responded At: Mar 29, 2023 22:00:14 pm
Last Seen: Mar 29, 2023 22:00:14 pm

Q1. What is your position on this proposal? Oppose

Q2. Comments (optional)

#1. 4 storeys too tall. Consider stepping back top 2 floors on the NE side next to the Ray's house to give them more sun. Adding 10 more units from the original 19 is obviously a greedy move. Also step back the corner to allow better visibility to back out for the Rays. Or the other option is move the whole 4th floor closer to Beechwood. #2. Specify what the height in feet and meters of each floor. #3. Parking is minimal. I know there is a move to get rid of cars, but in the future if there are parking spots not used, that area could be used for amenities like children play area, bocce, outdoor patio space for the building. #4. Fear mongering regarding Abkhaz being turned into townhouses is ridiculous. They can get the rezoning changed without this particular density bonus. #5. I was opposed to 19 units before but am appeased with this new option.

Q3. Your Full Name Rob n Jones

Q4. Your Street Address 234 Beechwood Ave

Q5. Your email address (optional) no answered



Respondent No: 3

Login: Anonymous

Responded At: Mar 31, 2023 08:42:32 am

Last Seen: Mar 31, 2023 08:42:32 am

Q1. What is your position on this proposal? Support

Q2. Comments (optional)

I've very close to the site and love the design and how it impacts the neighbourhood! It's great to finally see more multifamily housing options being built in Gonzales, hopefully, we will see more and more on the Fairfield transit corridor.

Q3. Your Full Name David Berry

Q4. Your Street Address 1607 Chandler Ave

Q5. Your email address (optional)





Respondent No: 4
Login: Reg s ired

Responded At: Mar 31, 2023 10:07:12 am
Last Seen: Mar 31, 2023 16:54:04 pm

Q1. What is your position on this proposal? Support

Q2. Comments (optional)

I am not including my full personal info as I have been harmed by housing opponents, the municipality should do more to ensure public participation is safe and respectful. This project has my full support, this neighborhood would be wonderful for the 30 families which could be provided housing in this wakabe community. This project serves to provide much needed housing with the additional benefit of protecting Abkhaz gardens. Having more options of housing types benefits the community for the many who don't desire a sfh (possibly due to life stage, maintenance, cost) and provide more housing for residents than a sfh. Really excruciating zoning as a whole should be abolished, projects like this are part of a better community.

Q3. Your Full Name Luna

Q4. Your Street Address V c o r a

Q5. Your email address (optional) no answered



Respondent No: 5
Login: Anonymous

Responded At: Mar 31, 2023 21:06:09 pm
Last Seen: Mar 31, 2023 21:06:09 pm

Q1. What is your position on this proposal?

Other (please specify)

Needs ammendmen s

Q2. Comments (optional)

Th s deve opmen w affec he charac er of h s des rab e ne ghbourhood wh ch w be a se ng po n BUT he deve opmen as proposed w degrade he ne ghbourhood. Th s projec shou d no proceed as proposed. S gn fican changes are needed

1. Number of s oreys. Concern over he gh and dens y. Curren Zon ng 2 S oreys/7.6 me ers (25 fee) Proposed 4 s oreys no he gh prov ded W Y. Perhaps a comprom se s 3 s oreys ha s s s gn fican y above he curren zon ng of 2 bu o no s a e he he gh of he 4 s ory proposa s unaccep abe. 2. Lack of park ng w h n he proposa . The proponen does no wan o dg o prov de underground park ng for un s. I s ess expense o have owners park on he s ree aka no cos o he deve oper. Th s expor of he park ng o he pub c causes commun y hardsh p as he park ng n adjacen areas s a ready a capac y. The approva of he projec needs o sh f respons b y on o Aryze o ns a park ng for he r deve opmen as was done for Rhodo across he s ree . No approva w hou underground or sufficien park ng aka one s a per un off s ree . 3. The rade w h TLC for he ncreased dens y sough s uncear. I s uncear wha TLC go for h s agreemen o g ve dens y o Ayrze and wha Ayrze go n erms of po en a mone ary ga n. Fu d sc osure and ransparency s needed on who go wha . I s expec ed ha Ayrze may have ga ned much more ha g ven. Transparency s needed as pub c sen men can probab y ge a be er dea for Abhkhaz han a back room dea TLC s ruck. The message s here s a need for fu d sc osure on wha was raded and a w ngness o rev ew he arrangemen and dec de f h s rade s ac ua y n he pub c good. I s sugges ed ha an ndependen rev ew of he rade be done o nform he dec s on or a er he erms of he curren apparen agreemen . To much of an ns der dea needs ransparency and ne her TLC nor Aryze s capab e of h s. Th rd par y needed w h a manda e o fac find from bo h TLC and Aryze and repor pub c y and ndependen y of CALUC.

Q3. Your Full Name

Mchae Fenger

Q4. Your Street Address

511 Fou Bay Road

Q5. Your email address (optional)





Respondent No: 6

Login: Anonymous

Responded At: Apr 02, 2023 13:49:27 pm

Last Seen: Apr 02, 2023 13:49:27 pm

Q1. What is your position on this proposal? Support

Q2. Comments (optional)

I emphatically support this proposal, as it is designed to transform three (3) single family homes into thirty (30) housing units. This represents a tenfold increase in the number of families who will be able to live in Fairfield Gonzales, and a gain of 27 housing units. Furthermore, this development is within easy walking distance of Fairfield Plaza, Margaret Jenkins School, and several parks. It is a wonderful area for households of any kind, but especially for those with families. As a Fairfield Gonzales homeowner, parent of two young children, and an active member of the Fairfield Gonzales Community Association, I applaud this development and the chance it represents to welcome more families to our neighborhood.

Q3. Your Full Name Dan E. Powe

Q4. Your Street Address 1024 Fairfield Road, Victoria, BC V8V 3A5

Q5. Your email address (optional)





Respondent No: 7

Login: Anonymous

Responded At: Apr 02, 2023 14:26:23 pm

Last Seen: Apr 02, 2023 14:26:23 pm

Q1. What is your position on this proposal? Support

Q2. Comments (optional)

We don't have enough housing, and building this project will mean that 30 families will have a place to call home. Farfield doesn't do as good a job of building new housing and should build a lot more. Please approve this ASAP.

Q3. Your Full Name Mark Edwardson

Q4. Your Street Address 1566 Yates

Q5. Your email address (optional) no answer



Respondent No: 8

Login: Anonymous

Responded At: Apr 02, 2023 14:52:19 pm

Last Seen: Apr 02, 2023 14:52:19 pm

Q1. What is your position on this proposal? Support

Q2. Comments (optional)

This project provides badly needed renovation on Fairfield. Fairfield has systematically underbuilt housing, even for kids growing up here, for generations now due to homeowner opposition. Given that context submit this project as a conservative. Housing just like this should be automatically permitted on every lot in Fairfield, especially a block or two off noisy and polluted areas.

Q3. Your Full Name Robert J Berry

Q4. Your Street Address 1683 Richardson

Q5. Your email address (optional)





Respondent No: 9

Login: Anonymous

Responded At: Apr 02, 2023 18:04:28 pm

Last Seen: Apr 02, 2023 18:04:28 pm

Q1. What is your position on this proposal? Support

Q2. Comments (optional)

This project will provide much-needed family-oriented housing in an area that is almost exclusively reserved for those wealthy enough to buy single-family homes. If we want a wider range of families to live together, we need to provide more affordable options for families to move into neighborhoods like Fairfield. Yes, I understand that this is not a subsidized development, but as a multi-family development it will be more affordable than the current single-family homes that dominate this neighborhood. Please allow this development to go through, providing a wider range of housing options in the Fairfield-Gonzales neighborhood.

Q3. Your Full Name Sacey F. Simmons

Q4. Your Street Address 3022 Washington Ave. Victoria, BC

Q5. Your email address (optional)





Respondent No: 10

Login: Anonymous

Responded At: Apr 03, 2023 11:12:14 am

Last Seen: Apr 03, 2023 11:12:14 am

Q1. What is your position on this proposal? Support

Q2. Comments (optional)

I strongly support this development and commend the developer for trying to bring some of her husband's ideas to life and amenity to the neighborhood! Adding more housing supply is one of the keys to fixing the housing crisis and this development is a step in the right direction. This is exactly the type of development I will be looking to live in when I start a family.

Q3. Your Full Name Clifford Chads

Q4. Your Street Address 505 quadrasree

Q5. Your email address (optional)





Respondent No: 11

Login: Anonymous

Responded At: Apr 05, 2023 12:42:10 pm

Last Seen: Apr 05, 2023 12:42:10 pm

Q1. What is your position on this proposal?

Oppose

Q2. Comments (optional)

I am a ne ghbour of he proposed deve opmen . I ve n he 300 b ock Beechwood. I have ved happ y n my e home for 20 years. There s much o address n h s proposa by Aryze. We have jus bare y absorbed he r Rhodo deve opmen af er wo pa nfu , oud and d srup ve bu d ng years, dur ng he COVID pandem c. S op he madness w h dens fy ng our beau fu ne ghbourhood. The reason my fam y and I choose h s par of own was for he swee s ng e home que res den a o s. Th s proposed dens y swapp ng s a gu se. Abkhaz gardens s pro ec ed and has no bus ness ho s ng h s mess on o us. We are an nc us ve ne ghbourhood w h many secondary dwe ngs, basemen su es, sma ren a un s e c. We do no need upwards of 100 new humans com ng and go ng down an a ready rcky b ock for nav ga ng w h very few "ex ra" park ng spo s r gh now. The d rec on of raffic s usua y owards Fa rie d no he o her way and add ng a h s ex ra s ra n on he ex s ng hru fare s r d cu ous. The proposed en rance and ex ha Aryze has des gned has no been properly hough ou as w h n a few fee are wo o her busy dr veways ha ead o 4 d fferen homes a w h sma ch dren who enjoy ng pay ng ou s de. 40 spaces for park ng? ow far a reach d d he da a search go for ha number? Our 300 b ock s fu mos days and n gh s w h park ng. Where do he gues s or he poss b e upwards of 100 res den s park? Need ess o say s d sappo n ng o h nk of he dem se of 3 proper y o s wh ch used o house an assor men of peop e and n come eve s a for he greed of a deve oper o have he r way w h our swee ne ghbourhood. A sad day f h s proposa con nues as presen ed. Why shou d we s ar rezon ng away from s ng e homes w h mor gage he pers and m xed use w h some runn ng he r bus nesses ou of he r homes o a dense bu d ng wh ch w subsequen y affec our ves and ne ghbourhood cohes veness and gen e ways? Ano her po n s ha h s proposed deve opmen w nega ve y effec our hous ng prces. Aryze s very s ck n he r approach and commun y focus bu sad y my ne ghbours and I can see wha hey are do ng and ha s fo ow ng he r own m ss on o make money. Le hem concen ra e on he r b g Te us bu d ng and o her such deve opmen s and p ease eave our e funky ne ghbourhood a one. The push for address ng he m ss ng m dd e has been skewed w h h s proposa - 4 dwe ngs per s ng e fam y o s on he ab e NOT 30 over 3 o s.

Q3. Your Full Name

A son Tremba h

Q4. Your Street Address

315 Beechwood Avenue

Q5. Your email address (optional)

no answered



Respondent No: 12

Login: Anonymous

Responded At: Apr 09, 2023 14:27:34 pm

Last Seen: Apr 09, 2023 14:27:34 pm

Q1. What is your position on this proposal?

Oppose

Q2. Comments (optional)

I am opposed to this development as presented and am very supportive of densification in the neighbourhood along the arterial roads such as Farfield Road. I believe it is essential to follow the OCP recommendation of up to three stories of townhouses and condominiums as the best way to densify. The danger with developers presenting four stories as the only way to work (this was clearly stated at the recent meeting by the developers: "anything else spends the sky") is that this then becomes fact. It is not fact. I believe that the proposed four stories with 30 homes is over-densification. Yes, we must think long term. Yes, this does allow some missing middle families to live in and own in this neighbourhood. That is a good thing. I would strongly suggest this is a money making venture first and foremost and that is not the best mode for densification in Gonzales. I would like to see a shared rooftop space for residents to enjoy the view and to gather. I'd like to see solar collection on the majority of the roof in this excellent site for installation of renewables on a new build. I'd like to see two spaces on the street reserved for car share co-ops to acknowledge the reduction of car parking on the street while providing these residents with easy access from the location. I'd like to see permeable surfacing on the whole parking area rather than impermeable surfaces on a significant portion. When I listened and observed to the people at the recent meeting, it was very clear that the widely held opinions were that densification was supported, that up to three stories of mixed townhouses and condos was welcomed, and that four stories was too high and too much of a "wall" and was strongly opposed. Given that there were about 100 people here and given that each person has shown up to a meeting of enthusiasts for many others who couldn't attend, it seems that support for the OCP recommendations is solid and ought to be followed. Please do not let developers push the City into overdevelopment in residential neighbourhoods.

Q3. Your Full Name

D. Louisa Ekin

Q4. Your Street Address

Man 1907 Shoblo Road, Victoria, B.C. V8S2L1

Q5. Your email address (optional)





Respondent No: 13

Login: Anonymous

Responded At: Apr 09, 2023 18:55:56 pm

Last Seen: Apr 09, 2023 18:55:56 pm

Q1. What is your position on this proposal?

Oppose

Q2. Comments (optional)

I oppose the proposal, not in principle, but in detail. I live in the neighbourhood, in Gonzales for 4 years, and in Farfield for over 20 years. I attended the public meeting organized by CALUC. I am an urban planner by profession, very recently retired in December 2022. I am in favour of the principle of densification in our neighbourhoods, but the proposed developments are too dense. I agree that Farfield Road provides an ideal opportunity for densification as outlined in the OCP. Current zoning for townhouses, up to three stories, provides appropriate scale for the locale. Currently the locale is single-family homes (although many have been modified to include one or more secondary suites), and increasing the density to three-story townhouse and condos is appropriate. Moving to four-story developments is over-densification, with significant impact on existing local residents and the neighbourhood in general. The developer is proposing four-story developments based on a density bonus arrangement with the TLC. Density bonus arrangements work well in the downtown, and were originally designed for increased density in a downtown locale. The danger of bonus-density arrangements is single-family neighbourhoods over-densification and coffee-ke scale developments overshadow local housing, and the whole locale. My concern with bonus-density arrangements, such as the one being proposed, is that just because it provides an opportunity, it becomes a default reality. The developer discussed the proposal at the meeting as if four-story developments are the only reality for this site. I urge the council to resist this type of thinking. Densify our neighbourhoods by all means, using building height as a key indicator of appropriate scale, but please do not over-densify our neighbourhoods just because bonus-density arrangements provide an opportunity; or because a developer sees it as an opportunity to set their agenda.

Q3. Your Full Name

T McKen

Q4. Your Street Address

1907 Shobon Road

Q5. Your email address (optional)

[REDACTED]

Survey Responses

1733, 1735 and 1737 Fairfield Road

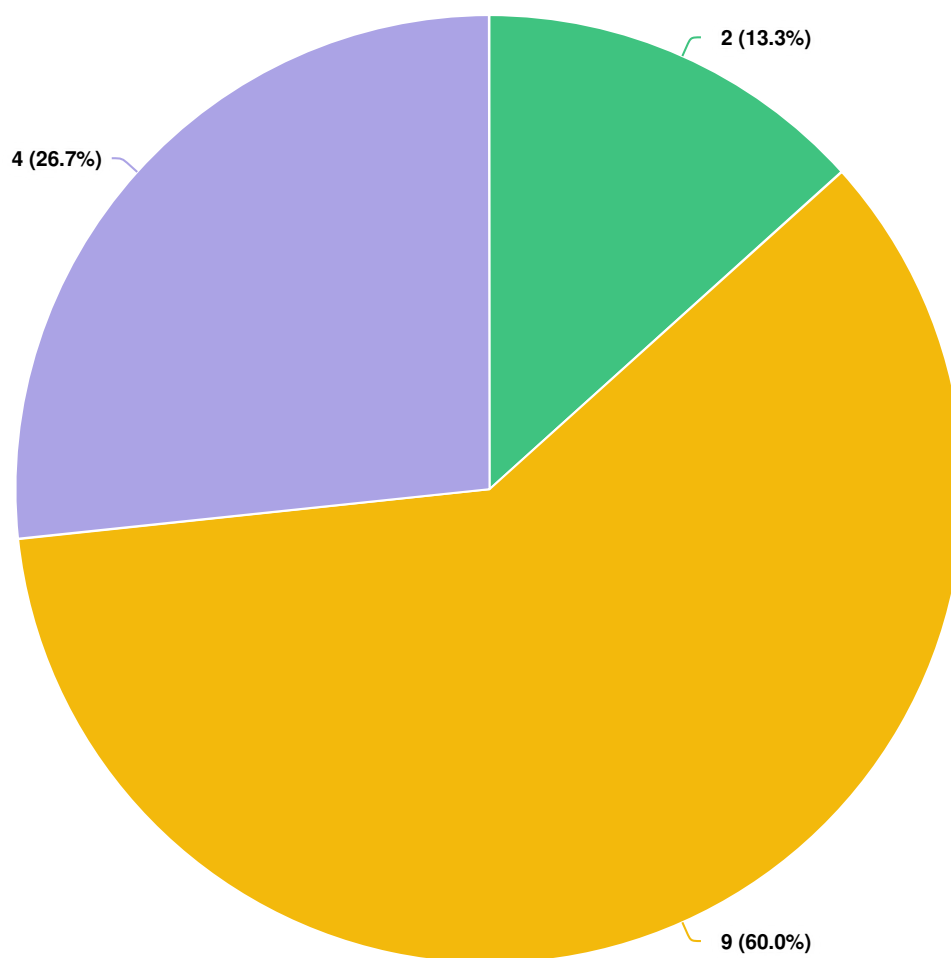
Have Your Say

Project: 1733, 1735 and 1737 Fairfield Road



VISITORS					
17					
CONTRIBUTORS			RESPONSES		
15			15		
0	0	15	0	0	15
Registered	Unverified	Anonymous	Registered	Unverified	Anonymous

Q1 What is your position on this proposal?



Question options

☒ Support ☒ Oppose ☒ Other (please specify)

Mandatory Question (15 response(s))

Note: Participants may submit multiple responses. See detailed feedback in the following pages.



Respondent No: 1

Login: Anonymous

Responded At: Mar 14, 2022 22:21:59 pm

Last Seen: Mar 14, 2022 22:21:59 pm

Q1. **What is your position on this proposal?**

Oppose

Q2. **Comments (optional)**

Our property will be directly affected by this development. We will end up with units staring down into our back yard. When we bought our house 22 years ago we specifically found an area with reasonable privacy and purposely not near or backing onto large developments or apartments buildings. I have spoken with a rep from the developer and made my concerns VERY clear prior to their plans being released, but our concerns have been 100% disregarded. There is NO need for a an increase from the current height of 7.6 meters to for the height to 13.8 meters. I EMPLORE the city to stay with the currently height as we are currently zoned. I am not against the "missing middle" being addressed but am tired of developers purchasing lots and then saying the only way they can make money is to get these incredulous variations. I am also concerned they are trying to rush this through under the current city leadership. My husband and I are opposed to the current plans. I would love to see neighbours concerns addressed. I believe the use of terminology of missing middle is being conveniently used to help the developer push through their plans. The height, on top of the increased density (deal they made with Abakazi Gardens) does not sit at all well with us. Sincerely, Joanna and Paul Betts 334 Robertson Street

Q3. **Your Full Name**

Joanna Betts

Q4. **Your Street Address**

334 Robertson Street

Q5. **Your email address (optional)**



Respondent No: 2

Login: Anonymous

Responded At: Mar 15, 2022 09:42:35 am

Last Seen: Mar 15, 2022 09:42:35 am

Q1. **What is your position on this proposal?** Support

Q2. **Comments (optional)**

I am in full support of the proposed development at 1733 Fairfield Rd. Being on a busier street with good access to transit, close to the new bike lanes on Richardson, and in walking distance to Fairfield Plaza it makes sense to develop more diverse housing options along this stretch of Fairfield Rd. I hope there are considerations to make this development car light, or include a modo/evo car share subscription for buyers (while also including adequate bike parking and cargo bike parking). Our stretch of Beechwood has very little street parking with many home owners with multiple vehicles, so I don't want to see increased demand for limited street parking. I appreciated at the first CALUC meeting that the developer indicated a desire for the building to be 100% electric.

Q3. **Your Full Name** Miranda Andrews

Q4. **Your Street Address** 321 Beechwood Ave

Q5. **Your email address (optional)**



Respondent No: 3

Login: Anonymous

Responded At: Mar 15, 2022 12:49:30 pm

Last Seen: Mar 15, 2022 12:49:30 pm

Q1. **What is your position on this proposal?** Support

Q2. **Comments (optional)**

Please ensure that the Fairfield rd crosswalk (at Lillian) is updated to one with blinking lights prior to any more large development in this area. This developer is already engaged in a development beside Hollywood Park and the extra trucks parked on Fairfield significantly obstruct the sight lines for the existing crosswalk. This has gone on for 2 years already. There are regularly close calls with children crossing and cars not stopping. Additional development in this same area and the associated parking on Fairfield Rd will continue to present a danger to the children using this crosswalk.

Q3. **Your Full Name** Stewart Cavers

Q4. **Your Street Address** 256 Wildwood Ave.

Q5. **Your email address (optional)** not answered



Respondent No: 4

Login: Anonymous

Responded At: Mar 17, 2022 13:05:33 pm

Last Seen: Mar 17, 2022 13:05:33 pm

Q1. **What is your position on this proposal?** Oppose

Q2. **Comments (optional)**

I don't think such a dense design of a several story condo building addresses the "missing middle" nor does it align with the character of the Fairfield neighbourhood. The missing middle homes are duplexes, carriage houses and townhomes. A double row of townhouses would be better suited along Fairfield Road. As a current single family homeowner planing on retiring in the next five years there are no missing middle options for me to downsize to and stay in my neighbourhood.

Q3. **Your Full Name** Katharine Geddes

Q4. **Your Street Address** 325 Robertson St

Q5. **Your email address (optional)**



Respondent No: 5

Login: Anonymous

Responded At: Mar 18, 2022 15:16:07 pm

Last Seen: Mar 18, 2022 15:16:07 pm

Q1. What is your position on this proposal?

Other (please specify)

I oppose the density of this project and the ability of the neighbourhood to support the extra traffic it will cause.

Q2. Comments (optional)

With the closure of Richardson St. Traffic has been deflected to Fairfield road. Being a resident of Earle St. My carbon footprint has increased significantly as I am idling at the top of Earle sometimes three times longer in order to make a left turn onto Fairfield and using St. Charles means pulling over and idling as in many spots two way traffic is not possible. I think that in all fairness the project "Rhodo" also on Fairfield should be completed and time given to determine the parking and traffic problems it will create. Also as a nearby resident it would be nice to consider a break from the disruption of construction. I have noticed my sunlight has been affected by the "Rhodo complex and hate the thought of four stories being erected so close to the very high three stories of "Rhodo. I don't understand when so many attractive townhomes are being built (the one at Chandler and foul bay comes to mind) that these out of character for the neighbourhood building are allowed. I'm not against more homes but density is only good if the area can carry the excess traffic. With Richardson closed this area is not able to support more traffic. I am already preparing my self for the residents of Rhodo to be parking on Earle making visibility around Hollywood Park a concern. Concentrating density in one area is a mistake. My family has lived in the area for over 80 years and with roads being narrowed (Memorial cr.) and given over to bikes as in Richardson and Vancouver Streets. Pedestrian safety has deteriorated.

Q3. Your Full Name

Lucinda Ferguson

Q4. Your Street Address

1667 Earle St.

Q5. Your email address (optional)



Respondent No: 6

Login: Anonymous

Responded At: Mar 19, 2022 17:01:54 pm

Last Seen: Mar 19, 2022 17:01:54 pm

Q1. **What is your position on this proposal?** Oppose

Q2. **Comments (optional)**

4 stories not in compliance with zoning or in line with neighbourhood plan. Density transfer should not be allowed - There is no process or precedent here. Developer should not be able to 'purchase' zoning from Abkhazi to apply to other properties. Disrespectful to processes already in place.

Q3. **Your Full Name** Sean McCartney

Q4. **Your Street Address** 350 Robertson St

Q5. **Your email address (optional)**



Respondent No: 7

Login: Anonymous

Responded At: Mar 20, 2022 13:02:13 pm

Last Seen: Mar 20, 2022 13:02:13 pm

Q1. What is your position on this proposal?

Other (please specify)

Support on condition.

Q2. Comments (optional)

Depending on whether or not the development is targeted and reserved for home affordability for residence that would like to access the housing market.

Q3. Your Full Name

Nicholas Fieger

Q4. Your Street Address

1738 Fairfield rd

Q5. Your email address (optional)



Respondent No: 8

Login: Anonymous

Responded At: Mar 20, 2022 15:08:40 pm

Last Seen: Mar 20, 2022 15:08:40 pm

Q1. **What is your position on this proposal?** Oppose

Q2. **Comments (optional)**

This building, at the proposed height and volume, will further destroy our single family/duplex neighborhood. Another thing- there are only 22 parking spaces for 25 suites. Each suite is bound to have at least one vehicle perhaps more, as many families do. That puts more vehicles parked on the street.

Q3. **Your Full Name** Edwin Adye

Q4. **Your Street Address** 1692 Earle Street

Q5. **Your email address (optional)**



Respondent No: 9

Login: Anonymous

Responded At: Mar 26, 2022 08:29:38 am

Last Seen: Mar 26, 2022 08:29:38 am

Q1. What is your position on this proposal?

Other (please specify)

Don't know enough yet about the project....

Q2. Comments (optional)

I'm concerned about what happens with the 3 structures on these properties. They look like beautiful homes - hardly 'tear-downs'. So, are they going to be moved, 'deconstructed'? Surely not bulldozed and carted away to the dump? Also, since Rhodo by ARYZE is across the street, I expect the new structure might look similar?

Q3. Your Full Name

Elaine Weidner

Q4. Your Street Address

1648 Earle Street

Q5. Your email address (optional)



Respondent No: 10

Login: Anonymous

Responded At: Mar 26, 2022 18:09:54 pm

Last Seen: Mar 26, 2022 18:09:54 pm

Q1. What is your position on this proposal?

Other (please specify)

I oppose density transfers for any reason. However, I support the application to amend the zoning on these three properties to permit a walk-up apartment (rental) building. All of Fairfield Road east of Moss Street should be rezoned for 3-4 story apartment buildings.

Q2. Comments (optional)

not answered

Q3. Your Full Name

Trip Kennedy

Q4. Your Street Address

1610 Pinewood Avenue

Q5. Your email address (optional)

not answered



Respondent No: 11

Login: Anonymous

Responded At: Mar 28, 2022 21:13:18 pm

Last Seen: Mar 28, 2022 21:13:18 pm

Q1. **What is your position on this proposal?** Oppose

Q2. **Comments (optional)**

not answered

Q3. **Your Full Name** Geoffrey Bird

Q4. **Your Street Address** 325 Robertson St

Q5. **Your email address (optional)** not answered



Respondent No: 12

Login: Anonymous

Responded At: Apr 07, 2022 10:25:10 am

Last Seen: Apr 07, 2022 10:25:10 am

Q1. **What is your position on this proposal?** Oppose

Q2. **Comments (optional)**

Here is an example of 3 houses that could be turned into affordable suites with some renovations. It would be disheartening, again, to see 3 perfectly good houses torn down for a development that will surely be unaffordable to most. We need affordable rentals in the city for so many people living on lower incomes that provide much needed services to all aspects of the city.

Q3. **Your Full Name** Heather Keenan

Q4. **Your Street Address** 1825 Lillian Road

Q5. **Your email address (optional)**



Respondent No: 13

Login: Anonymous

Responded At: Apr 08, 2022 13:52:38 pm

Last Seen: Apr 08, 2022 13:52:38 pm

Q1. **What is your position on this proposal?** Oppose

Q2. **Comments (optional)**

This development is too large for the site and not consistent with the Missing Middle intent.

Q3. **Your Full Name** Thomas Lacey

Q4. **Your Street Address** 1823 Fairfield Road

Q5. **Your email address (optional)**



Respondent No: 14

Login: Anonymous

Responded At: Apr 10, 2022 13:08:48 pm

Last Seen: Apr 10, 2022 13:08:48 pm

Q1. **What is your position on this proposal?**

Oppose

Q2. **Comments (optional)**

-I believe four storeys is too large for the site. -The Fairfield Neighbourhood Plan (City of Victoria - 2019) states several times that new buildings "should provide for sensitive transitions through massing and scale." This development does not do that. -Given the dire shortage of affordable rental housing, if any of the current residents of 1733-1737 Fairfield Road are renters they will face hardship finding other homes -we should have a comprehensive plan for the Fairfield corridor between St. Charles Street and Foul Bay Road, rather than haphazard spot zoning. -the proposed density transfer offers scant benefits to the neighbourhood or the city -homes in the proposed development will be expensive and will in no way address the affordable housing crisis facing the city -I fail to see how the proposed development is advantageous to the neighbourhood or the city. -Evan Stewart

Q3. **Your Full Name**

Evan Stewart

Q4. **Your Street Address**

343 Robertson St.

Q5. **Your email address (optional)**



Respondent No: 15

Login: Anonymous

Responded At: Apr 10, 2022 21:09:45 pm

Last Seen: Apr 10, 2022 21:09:45 pm

Q1. **What is your position on this proposal?**

Oppose

Q2. **Comments (optional)**

This proposal is terrible in so many ways. At the risk of sounding completely negative it is hideous, too large and ill fitting for this neighbourhood. Taking down 3 affordable houses to build what looks like an institution is insulting to the area the site and Gonzales residents. Losing affordable housing should never be an option. The Gonzales plan says "to preserve and protect old stock housing not preserve and protect developers wallets. As long as the city allows affordable rentals and houses to be torn down to create unaffordable housing we will not see a change in our affordable housing crisis. This massive ugly building is not serving this purpose. Along with a housing crisis we also are creating a work force crisis. It is no surprise that affordable housing has disappeared along with a work force for many businesses. Who can actually afford to work and live here if you allow the housing to be torn down!! I also believe if this development requires an amendment to the community plan the community (not the developers or their friends or people from Vancouver or anywhere else) should be asked if we actually want any amendments to our community plan. Regular home buyers should not have to compete with the deep pockets of developers for housing. Oh, yes I live in Gonzales. I appreciate the hard work that was done on our community plan and find ignoring the plan insulting and arrogant.

Q3. **Your Full Name**

Deborah Lowry

Q4. **Your Street Address**

1829 Lillian Rd

Q5. **Your email address (optional)**

not answered

Our property will be directly affected by this development. We will end up with units staring down into our back yard. When we bought our house 22 years ago we specifically found an area with reasonable privacy and purposely not near or backing onto large developments or apartments buildings.

I have spoken with a rep from the developer and made my concerns VERY clear prior to their plans being released, but our concerns have been 100% disregarded.

There is NO need for a an increase from the current height of 7.6 meters to for the height to 13.8 meters. I EMPLORE the city to stay with the currently height as we are currently zoned. I am not against the "missing middle" being addressed but am tired of developers purchasing lots and then saying the only way they can make money is to get these incredulous variances. I am also concerned that this is all happening so quickly?

My husband and I are opposed to the current plans. I would love to see neighbours concerns addressed. The height (huge impact into our property) on top of the increased density (deal they made with Abakazi Gardens) does not sit at all well with us.

Lastly, many of us were involved in helping with input and careful consideration into OUR community plan - to see the possibility of that being disregarded is extremely disheartening.

I am unable to attend the meeting scheduled on March 28 so this email is our "comments."

Sincerely, Joanna and Paul Betts
334 Robertson Street

What are we thinking? Developers are rushing local decision makers into rash and knee jerk reactions to attempt to solve a growing housing shortage. They are changing the very nature of our liveable neighbourhood by eliminating green space and privacy and disrupting the quiet enjoyment of our established community. To what end? To maximize the profitability of the properties they have purchased. As a local resident for 40 years I am directly impacted if this proposed 4 storey structure is approved. The structure will loom over my home and garden. Does it need to be this height to provide a reasonable number of housing units? Does it merit transferring density from Abkhazi Garden purportedly to protect the property from future development? Will Abkhazi fail without the cash from Aryze? This development needs a serious reconsideration. As proposed it does not provide more affordable housing, it does not contribute to the liveability of the community and its residents and it only offers more disruption, cars and traffic and a lot less green space. If we must develop let's do it in a way that is consistent with the OCP which expresses the views of most residents. Take a hard look. I have and I am adamantly opposed to the development as proposed.

Shawn Robins

The other item which is important to add - the proposed 4 storey development will not only cause us to lose privacy at the back of our house, it will also change the amount of sunlight and length of time we can enjoy the sun.

Lastly, for now, I do not see how this will not negatively impact the value of our property. Any future buyer will surely need to consider what they are willing to pay to have all those units looking at the back of our house - yet another reason we chose this property.

Why can't the developer be reasonable and build something that meets the current allowable height.

PLEASE think about those of us who currently live in this neighbourhood, and consider us - developments need to conform with our neighbourhood plan and be considerate and respectful of who is impacted (which by their proposal it is evident they are not at all) - this CAN be done while providing more housing.

Sincerely,
Joanna and Paul Betts
334 Robertson Street

Sent from my iPhone with my "iThumbs" so please excuse typos!

> On Mar 14, 2022, at 10:48 PM, Joanna Betts [REDACTED] wrote:

>

> Our property will be directly affected by this development. We will end up with units staring down into our back yard. When we bought our house 22 years ago we specifically found an area with reasonable privacy and purposely not near or backing onto large developments or apartments buildings.

>

> I have spoken with a rep from the developer and made my concerns VERY clear prior to their plans being released, but our concerns have been 100% disregarded.

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>

> My husband and I are opposed to the current plans. I would love to see neighbours concerns addressed. The height (huge impact into our property) on top of the increased density (deal they made with Abakazi Gardens) does not sit at all well with us.

>

> Lastly, many of us were involved in helping with input and careful consideration into OUR community plan - to see the possibility of that being disregarded is extremely disheartening.

>

> I am unable to attend the meeting scheduled on March 28 so this email is our "comments."

>

> Sincerely, Joanna and Paul Betts
> 334 Robertson Street

>

Greetings,

As a former resident of Robertson Steet and current resident of Brooke Street, I want to voice my significant concerns about the proposed height of the proposed development at 1733 - 1737 Fairfield Road.

At a height of four stories, the proposed development (which requires an amendment to the Official Community Plan or nearly **double** the current zoning of 7.6 metres) will not only destroy the aesthetic of Fairfield's small-town community feel, it will also infringe upon the privacy of nearby residents, not to mention the deleterious effect it will have on the amount and length of sunlight nearby houses receive.

Thank you for your consideration.

Regards,
Mike

Hello,

>

> We would like to express our opposition to the current building proposal at 1733-1737 Fairfield Rd. This proposal is not respecting current zoning and the proposed 4 story height will destroy many surrounding neighbours' ability to enjoy their properties and privacy. Densityfication needs to occur in a respectful way for the neighbourhood. Developers needs to stick within the current zoning requirements and height limitations. This proposal of "density transfer" is inappropriate. The city of Victoria does not have a policy for this nor a process in place. Density transfers are used in downtown cores and city centres not residential areas, and only in cities that have a clear existing policy and governance for this process. How can we allow someone to just make up new rules? Density transfers are used to protect heritage sites, but Abkhazi Gardens is not under threat. This is simply manipulation in order to maximize profit at the expense of the neighbourhood. We want to be supportive of appropriate densification, as clearly outlined by the neighbourhood plan and the rules for height and setbacks that are currently in place. Please help support our neighbourhood and protect us from uncontrolled over-development.

>

> Sincerely,

>

> Catherine McCartney

Hello,

I am writing to express strong opposition to Aryze's building proposal at 1733-1737 Fairfield Rd. While I understand and support densification, development projects such as this need to be aligned with the Fairfield/Gonzales neighbourhood plan. The proposed 4-story structure is disrespectful in this regard, both to the plan and to the neighbours whose properties and privacy will be negatively impacted.

Moreover, the notion of using a 'density transfer' to justify the increased building height is inappropriate. Density transfers are typically used in core urban areas, not single family residential neighbourhoods. In addition, while these transfers have been used to protect heritage sites, Abkhazia Gardens is not under threat and the property's zoning 'asset' should not be allowed to be purchased and moved. There is no rationale or need for this with either of the involved properties, other than maximizing profits for Aryze.

Finally, to the best of my knowledge, the City of Victoria does not currently have any governing policy or process in place to allow for a density transfer. It would be crucial to have this in place prior to allowing this, or any other 'density transfer' proposal to move forward.

Please help support me and my many concerned neighbours of the Fairfield/Gonzales community by ensuring this development does not move forward as proposed.

Sincerely,
Sean McCartney

Hi,

How do we go about getting city staff and council members to do a “site” visit to homes that will be negatively impacted by the proposed development? I feel strongly they need to visit our home to truly understand what is at stake for us.

I strongly feel the developments needs to fit with our community plan - which is not as old or as outdated as Aryze representatives have stated.

There IS a way to add housing into our neighbourhood which respects the housing needs (which I might add are SEVERELY impacted by the likes of Airbnb’s/short term rentals) and RESPECTS those of us who live and will be “neighbours” to a development.

Also, it would be worthwhile for city staff to visit the “comparable” buildings Aryze mentioned that they claimed set a precedence on Fairfield Road. I have photos of each. The one on Richmond and Fairfield I would argue is not comparable - it is a tasteful 3 story building. Next to it on Richmond is a large 3 story home. The home behind it was built within the last 7 years after a property was subdivided.

The other property is next to the Ross Bay Villa heritage site (which is not a home) and only really impacted one large home behind it.

It is very important that CALCU and city staff and councillors do their own deep dive and NOT rely on the information/comparisons provided by the developer.

If the developer really “cared” about the neighbourhood then they would have incorporated feedback they collected from us - but instead we were deceived and given incredibly vague information about a project they knew more about than they disclosed.

Lastly, I am concerned about how “personal” relationships with members involved in project have with the mayor and how this impacts decision making. Perhaps Mayor Helps should be clear and disclose.

Sadly I do not feel confident that the best interests of ALL residents are being considered when architects, and perhaps certain developers, have blurred relationships directly with the mayor.

Sincerely,
Joanna and Paul Betts
334 Robertson Street

I can't believe a big developer can manipulate the city council and the FCP in its favour and we neighbours have no say!!

I get it, money talks, but YOU should listen to us, those who live here too.

What about the 3 story height limit, the requirements for parking spaces- or lack thereof, and the need for green space, and trees!!!! All 3 of those "rules" are being broken for the sale of ANOTHER 2-bedroom multiple ultra rich development.

This developer is NOT building affordable family homes, they are not keeping those design is keeping with the heritage homes in the area AND they are making money hand over fist

STOP, look and listen to us and allow for input, honor the community plan- lower height (3story maximum) and stop stripping Fairfield of its character.

Sincerely,

Mully Jackson
266 Robertson St
Victoria
V8S 3X5

Hi- For the last 22 years, I have owned my "forever" home on Robertson Street, 300 block, behind this proposed development.

I am writing to ask that you send this proposal back to Aryze for changes.
I am totally opposed to this development going through as proposed.

- * The building height is over 3 stories. It is 4 stories. Our Community Plan sets the height limitation at 3 stories along the Fairfield Road Corridor. We must keep buildings under this height.

- * The Density is too high for the project: Aryze is proposing a density "transfer". They are buying increased density for \$300,000 from Abkhazi Garden. How can this be? Can you now buy increased density, so you can build overriding the Community Plan? There is inherently something wrong with this process.

- * . Green Space: the building is too close to the road. The three large lots will be almost totally filled by the building and parking lot. All the trees and plants will be removed, leaving only a couple of trees at the back property line. This does not meet the standards for our neighbourhood.

- * To my eye, because of its size, the building is not attractive and does not fit into our Community. It's much larger than any home or apartment building in the area. Unfortunately, because it houses 19 condos, is 4 stories tall, fills 3 large lots, it will be similar to looking at a warehouse. This does not fit into our Community Plan.

- * This project might reduce our property values: we currently look out on 3 large lots containing 3 homes, with space between the homes, many bushes, lawns, gardens, trees and views of the sunset. If I were looking to purchase in this neighbourhood with the proposed building of 19 condos/4 stories high just across the way...I wouldn't.

Please try to see this proposed building as we do, who will live behind or in front of it. It's just too large and too tall for our Community.

Please don't see me or my neighbours as NIMBYs. We have two other older, 3 story apartment complexes (not the Rhodo Condo) just next to the proposed development. They are not a visual or environmental issue: they are set back from the road, are 3 stories high and have lots of green space around them. They fit into our Community and into the lots they are built on.

I am not opposed to a multiple-housing option being built on the property. But, it needs to meet our Community Plan's guidelines and have more greenspace and trees, have more parking, not be this large or tall. It must ultimately add beauty to the surrounding community. This is what a Community Plan is all about.

Thank you for your time and service to our Community-
Linda Maasch
311 Robertson St.

I live in the 200 block of Beechwood Avenue and in general am in favour of the ongoing and proposed developments nearby (namely the 1 ongoing by Aryze, the LONG overdue one at Foul bay, and now the proposal at 1733-1737 Fairfield)

I used to live in James bay and later in Oak bay- before moving to Beechwood avenue

I loved the densification of James bay, in part made easy by loads of little shops, NOT JUST ONE PLAZA. Is there a hand-in-hand strategy the city is looking at to provide more convenience shops, small bakeries etc to this area. The Fairfield plaza is basically a nightmare from 8am til 8pm and I end up sadly driving up to red barn or to cook st or downtown- all a 40-60 minute walk from my house.

Is there anywhere I can be looking to see how the city is promoting things like Demitasse (oak bay), the Niagara grocer, Ambrosia in Fernwood, Tom Lees (oak bay), and countless others down in James bay)

Rebecca Lang

Dear Council,

Please don't approve this oversized apartment building project for its planned location.

- Its four story height is totally out of proportion to the surrounding neighbourhood
- 19 high-priced units is too dense
- A 22 space surface parking lot opening onto peaceful Beechwood is a dreadful idea: Beechwood has a high number of young children and pets that would be in danger of the increased traffic.
- The parking lot and outsized building would cover a lot of existing greenspace
- The donation to the Abkhazi Gardens can be recognized by swapping some more suitable location than the destructive Fairfield one
- There is no societal benefit included in the plan to assist low income residents
- The RHODO project has already damaged the neighbourhood with its elimination of green space and its visual encroachment on the neighbouring park and Fairfield Road

Too many values will be violated by this proposal. We worked on the Gonzales neighbourhood plan and it was approved by Council. What good is it if Council now just ignores it?

Thanks for your attention to this.

Victor Ivan Carlson
118 Beechwood Avenue

I'm writing to you in reference to the proposed Aryze development at 1733/35/37 Fairfield Road.

I believe that there's a failure to appreciate the density in this neighbourhood already. I'm not sure where the data is collected to then determine that there is a lack of density, other than by considering legal suites and apartments, thereby assuming that every 'single family home' is in fact what it appears to be: a single family. The reality is that almost all homes in this neighbourhood are multi-family: whether they be students, single professionals, single mothers with children, or traditional 'families'. To pinpoint this neighbourhood as being low density and therefore deserving of more ugly architecture, more traffic, with zero benefit to the neighbourhood, when the surrounding area from which it is transferred (abkhazi) is of incredibly low density seems to be simply a corporate decision to maximize profits and leverage existing infrastructure; How much money or infrastructure has Aryze contributed toward Hollywood park, given that Rhodo sales will leverage the proximity of the park given that they've basically used it as the green space for new owners of Rhodo and will likely do the same with this new proposal?

As a city council, incredibly poor city planning decisions have been made to date and this will be yet another one if it is allowed to proceed.

I find it odd that the city permits landowners to tear down single family homes to construct new single family homes that are massive and don't increase density but do increase real estate prices (buy for 1.3 million, knock it down and build and sell a new home for a minimum of 2.5 million). And on the other hand, the mayor and council decry the lack of density. Why not only provide permits to new single family homes developed on residential zoned properties if they include secondary suites at minimum?! Otherwise, no, you can't knock it down. That will help lower real estate prices, encourage densification with new stock and be more aligned with what you claim is your desire.

The proposed development, apart from not adhering to bylaws and community plans and which requests variances, does absolutely nothing to enhance this neighbourhood. Apart from being a blight on the landscape like Rhodo (let's face it, it is juvenile architecture that is categorically unpleasant to look at), any new densification from which Aryze - which claims to be community focused but in reality is essentially simply saying that as their tag line, since in reality, they're sole concern is to generate profit - hopes to benefit, should include benefits to the neighbourhood. That could come in the form of retail spaces (think coffee shop, bakery, restaurant etc.) and or significant contributions to the improvement of the appearance, function and capacity of the neighbourhood.

Why would Aryze contribute hundreds of thousands of dollars to Abkhazi, if not to attempt to solidify their claim, and pressure the city to permit their continued efforts to maximize their profits and rob the neighbourhood of its aesthetic, appeal and family-focused environment (which, by the way, is the opposite of 'building community')?

Frankly, I have to say shame on you all for the weakness you've shown in your civic duties to date: you seem incapable to me to see beyond what you hope will be a positive legacy and listening to singular voices and being swayed by corporate dollars and revenue from construction permits and future tax revenue.

Paul Crossley

Dear Council,

As the current resident and owner of 1757 Fairfield Rd, I am extremely concerned and disappointed about the proposed development at 1733/1735/1737 Fairfield Rd.

Council has many tools at its disposal to improve the availability of affordable housing and I do not accept that this developer or the proposed additional development achieves your objective or the desires of this community or neighborhood. The neighborhood, with much disappointment accepted the Rhodo development and it is my belief that we have yet to see the full impact of that development given it is not yet complete and is not currently occupied. Immediately approving another development essentially across the street does not provide Council or the community the time and space to fully understand the impact of the development in a way that allows it to inform future approvals for the development of the Fairfield corridor. If we are interested in evidence-based decision making, which I certainly am, we need to ensure that we have the evidence through data and information gathering on the existing project, once fully occupied, for a period of a year or two before any of us can formulate concrete conclusions, positive or negative.

My questions for you related to the Rhodo development are:

1. What is the socio-economic profile of the owners for each of the units?
2. What was the average sale price of each unit?
3. How has this data and information been used to inform future development considerations?
4. Has this development achieved your strategic plan objectives and if so how, in concrete terms informed by evidence?
5. What community amenities has the development contributed as part of the development that are a positing impact on the local community and specifically the neighborhood?
6. What is the developers profit margin on this development?

I have been paying attention to the housing availability and affordable issues in the City. I have read with interest some of the information released related to the “missing middle” as the City references. For context, we have 5 school age children and understand that the chance of them owning their own single-family dwelling is highly unlikely given current market conditions and escalation. We are not opposed to development or options, what we do expect is that each project considered has an impact that respects the local community or neighborhood and results in the achievement of shared goals, values and objectives. It is unclear to me if the ‘missing middle’ is a desire to have housing options that are in the “middle” of the current housing prices in Victoria or is a desire to have options that are affordable to middle income individuals and families. If the former, this development does nothing to improve accessibility to housing given that the average price is likely around \$1 million, if the later this development will not achieve the objective as median income in Victoria was \$64,600 in 2018 (<https://www.capitaldaily.ca/news/income-inequality-victoria-one-percent-canada>) Yes, that’s 4 years ago but it also doesn’t reflect the impact of the pandemic and I suspect it hasn’t moved materially higher for 2022. Even for two income households, I ask how this development will provide affordable options – unless of course you as Council willing to use the tools at your disposal to place a maximum sale price on each unit aligned with your definition of “missing missile”. Is Council willing to, by way of by-laws, title charges, or other tools at its disposal place conditions on the developer and the

development to achieve its “missing middle” objectives? If not, I suggest this development is not aligned with your own strategic plan or the principles and objectives which are espoused within it.

I have reviewed Council’s strategic plan and the goals and suspect that there is a desire for developments to meet a variety of your strategic objectives and am requesting that you provide an evaluation of this new proposal’s alignment with your strategic objectives and how its development will advance them.

Objective 1: Good Governance and Civic Engagement

Your objective	My Response
There is clear, open and transparent two-way communication between the City and the public, with the ability for public input to effect change.	I am providing input and am not feeling like my or the neighborhoods input is being used to effect change
There is broad engagement with a diversity of participants conducted in a respectful and inclusive way	Seems like standard engagement on rezoning – put up signs, ask for community input and ignore so the developer can do what they want.
The community feels heard	Not this community, what are you going to do about it?
The City demonstrates regional leadership in transparency and open government initiatives	Well signs are up on the properties in question, not sure
There are clear, relevant measurable outcomes for each objective that Council measures and reports on	Where? I do not see evidence of this for this process or project.

Objective 2: Reconciliation and Indigenous Relations

Your objective	My Response
Deeper and more engagement with Songhees and Esquimalt Nations including with both hereditary and elected chiefs	Have they been engaged on this project or redevelopment of the Fairfield corridor writ large?
Increased awareness of and support for reconciliation and recognition of Indigenous sovereignty	How has this been achieved on this project?
City Council, staff and residents are more aware of Indigenous history, treaties, and leadership structures	Has the project understood the ancestral use of this land. Is there required interpretation and signage?
Increased community collaboration and capacity to do the work of reconciliation by harnessing existing expertise in our community	N/A
More Indigenous involvement and inclusion in all aspects of civic life, including economic development	Are they receiving economic development opportunities from redevelopment?

Talent acquisition programs and initiatives are in place that support the diversification of the City's workforce, including programs and initiatives that focus on Indigenous Peoples	Are developers required to contribute back to support this work through required contributions, endowed positions or other initiatives?
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Objective 3: Affordable Housing

Your objective	My Response
Decrease in number of people spending more than 30 per cent of income on housing	Will this development achieve or help achieve the objective?
Decrease in homelessness (Point-In-Time Count numbers go down)	Will this development achieve or help achieve the objective?
Increase the number of 'Missing Middle' housing units	Will this development achieve or help achieve the objective? See earlier question on what the definition of 'missing middle' is
Increase number of co-op housing units	Will this development achieve or help achieve the objective?
Increase in rental apartment and housing vacancy rate	Will this development achieve or help achieve the objective?
Increase in percentage of Victoria residents who own their own homes	Will this development achieve or help achieve the objective? Are the propose units only for sale or will they be rentable?
Victoria is seen as development friendly	Will this development achieve or help achieve the objective?
Neighbourhoods are diverse, accessible and affordable across all ages, incomes and abilities	Will this development achieve or help achieve the objective?

Objective 4: Economic Inclusion

Your objective	My Response
Business owners feel that it's easy and rewarding to work with City Hall	N/A
Neighbourhood and village centres have thriving economies	Fairfield is already thriving, should this development be considered elsewhere in the city to achieve this objective for a neighborhood?
Low vacancy rate in downtown retail spaces is maintained	N/A
Employers can find enough qualified workers to fill available jobs	N/A
People who work in Victoria can afford to live in Victoria	Will this development achieve or help achieve the objective?
Increase in number of businesses actively engaged in reducing GHGs (See Objective #6)	Will this development achieve or help achieve the objective?
Increase in number of urban-agriculture related businesses	Will this development achieve or help achieve the objective?
Reduced use of food banks	Will this development achieve or help achieve the objective?

Objective 5: Health, Well-Being and a Welcoming City

Your objective	My Response
Increase sense of belonging and participation in civic life among all demographic groups	Will this development achieve or help achieve the objective?
Increase in number of people who feel safe and part of the community	Will this development achieve or help achieve the objective?
Increase in number of people who have a family doctor and overall increase in the number of people working in the health and well-being professions	Will this development achieve or help achieve the objective?
Increase in availability of free recreation options	Will this development achieve or help achieve the objective?
Increase in people accessing nature	Will this development achieve or help achieve the objective?
Increase in number of people who are more active, including increase in registrations in the City's recreational programs	Will this development achieve or help achieve the objective?
Clear improvement on mental health and addictions	Will this development achieve or help achieve the objective?

Increased local food security	Will this development achieve or help achieve the objective?
Fewer people are living below the poverty line and more people have access to a living wage	Will this development achieve or help achieve the objective?
Increase in cultural literacy, deepening understanding and welcoming of diversity	Will this development achieve or help achieve the objective?

Objective 6: Climate Leadership and Environmental Stewardship

Your objective	My Response
The City is making measurable advances reducing community GHG emissions by 50 per cent from 2007 levels by 2030, and cutting the City of Victoria's corporate emissions by 60 per cent by 2030	Are you setting targets for developers?
Citizens and businesses are empowered and inspired to take meaningful action to reduce carbon pollution	Will this development achieve or help achieve the objective?
Citizens and businesses are making measurable advances in community and business GHG reductions and a tangible movement to 100 per cent renewables to meet or exceed climate targets	Are you requiring developers to use energy efficiency targets with energy modelling and required sequestration strategies including embodied carbon targets, mass timber use and passive house envelopes?
There is a Zero Waste Strategy in place and we are making progress towards goals	Are you requiring developers to use deconstruction techniques and setting recycled building content use requirements?
There are optimized local compost solutions in place for both food and garden waste	N/A
Increase in tree canopy on public and private property	Will this development achieve or help achieve the objective?

Objective 7: Sustainable Transportation

Your objective	My Response
Increase in residents using public transit, walking and cycling	Then why is a large parking surface part of the project consideration and rezoning request?
Decrease in number of collisions and fatalities on City streets	Will this development achieve or help achieve the objective? Suggest that it will increase traffic on Fairfield with correlation to collision increase.
Decrease in transportation-related GHG emissions	Will this development achieve or help achieve the objective? How will you know?
Fewer cars on the road and decrease in vehicle kilometres travelled	Will this development achieve or help achieve the objective? How will you know?
Increase in car sharing	Will this development achieve or help achieve the objective?
Decrease in annual household spending on transportation	Will this development achieve or help achieve the objective?
Increase in public and private EV charging stations	Will this be required as part of the development. Will the Fairfield corridor get increased and improved EV charging options?
New transportation services are available, including rapid transit, to and from Victoria, supported by first-mile and last-mile solutions, mobility-as-a service (MaaS) and a single payment platform	Are you going to request/require an increase in bus service as part of the proposed development?
There is a positive shift in public attitude towards sustainable transportation	Will this development achieve or help achieve the objective?
There is a direct link between City transportation investments, policies, and services and the City's Climate Leadership goals	Will this development achieve or help achieve the objective?
Victoria is recognized as a global leader in multi-modal transportation	Well – its close to Richardson I guess.

Objective 8: Strong, Liveable Neighbourhoods

Your objective	My Response
Increase in number of opportunities for engagement with neighbourhoods	Will this development achieve or help achieve the objective?
People feel listened to and consulted about what makes a neighbourhood distinctive	Will this development achieve or help achieve the objective?
Increase in affordable housing in all neighbourhoods (See Objective #3)	Will this development achieve or help achieve the objective?
Increased access to social determinants of health in all neighbourhoods	Will this development achieve or help achieve the objective?
People feel that their neighbourhood is safe and walkable	Will this development achieve or help achieve the objective?
Increase number of people walking compared to other modes of getting around within neighbourhoods	Will this development achieve or help achieve the objective?
Increase in the amount of green space in neighbourhoods	Will this development achieve or help achieve the objective?
All neighbourhoods are thriving, distinctive, appealing, viable and have amenities	Will this development achieve or help achieve the objective?

> > > > > >

If this development is not

this is truly about providing affordable housing in a way that requires densification and multi-story and multi-unit developments, I would suggest that the Rhodo development is then evidence that the rationale for these developments is a false narrative and only used to prompt the interest of the developer and increase their profits.

Brian Jonker

From: [REDACTED] <[REDACTED]>
Sent: June 11, 2022 9:08 AM
To: Patrick Carroll <pcarroll@victoria.ca>
Cc: Aryze Engagement <[REDACTED]>; Cathy Ray <[REDACTED]>
Subject: 1733-1737 Fairfield Road

Good morning,

I am sitting at my kitchen table at 1745 Fairfield Road on a Saturday morning. We live next door to the proposed 4-story development from 1733-1737 Fairfield Road.

First off, the early engagement representative from Aryze, Julian has been friendly. I even met with Chris, the lead designer(?) the other day and he was pleasant.

The fact is, if approved, a 4-story building will go where a single family dwelling once was. I am not adverse to higher density, but a four story building is out of scale for this neighborhood. Row houses? Duplexes?

The building will block much of our south facing sun based on Aryze's own projections. We choose this neighborhood for a reason in 2012. We choose this house for a reason and have adapted it since then for increased mobility to support my Multiple Sclerosis (MS).

What is being proposed does not fit. I understand economics, but the building needs to be scaled down. If it is not economical, then maybe this is the wrong place.

Please forward this to where it needs to go.

Rahul Ray
1745 Fairfield Road

From: [Patrick Carroll](#)
To: [Katie Lauriston](#); [Ayla Conklin](#)
Subject: FW: 1733-1737 Fairfield Road
Date: June 15, 2022 4:36:53 PM

Good day Katie/Ayla,

Please add this correspondence to this rezoning file to be attached to the COTW folder. I do note it is only addressed to me but says " Please forward this to where it needs to go." I can hold on this and have the resident confirm they mean forward to council.

Thanks!
Patrick

-----Original Message-----

From: rahulray [REDACTED]
Sent: June 11, 2022 9:08 AM
To: Patrick Carroll <pcarroll@victoria.ca>
Cc: Aryze Engagement <community@aryze.ca>; Cathy Ray [REDACTED]
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Please forward this to where it needs to go.

Rahul Ray
1745 Fairfield Road

RE: Proposed development at 1733-1737 Fairfield Road
Attachment: Level 4 Proposed Plan - Drawing A-104

Please note my objection to this proposed development, for the following reasons:

1. The density is too high - I must say that I consider the Density Rights Transfer from Abkhazi Gardens to be a scam, and maintain any reasonable person would, too.
2. There is no setback from Fairfield Road. Why not dig a hole and put the parking underground? Just go across the street and down the block to the "Rhodo" to see what "too close to the road" looks like.
3. The layout of the gerrymandered 3-bedroom unit on Level 4 is bizarre, to say the least. Just follow the dots from the elevator to the master bedroom (image attached), and you will be well on the way to completing your steps for the day. On the plus side, it's not everyday that an apartment layout will support the installation of a standard-length bowling alley (60 feet) in the northwest corridor ...

Thanks for hearing me out.

John Kell
204 Memorial Crescent
Victoria, BC

I respectfully request Victoria City Council reject the revised proposal to develop 1733, 1735 and 1737 Fairfield Road. The revisions do not effectively address the main issues that were brought to the attention of the developer.

The height, mass and density far exceed what is reasonable in this area. The proposed density transfer provides no benefit to the city and is highly disruptive to our neighborhood. The four story apartment building design is not compatible with principles of family-focussed attainable housing or green space enhancement.

I am supportive of densification efforts in Gonzales that provide quality housing for families. In Traditional Residential areas I would like to see ground-oriented dwellings that are limited to two and a half story houseplexes, duplexes and townhouses with landscaped outdoor activity space.

Thank you,
Janice Linton
356 Robertson Street

I fully support the proposed developments at 1733/1735/1737 Fairfield Rd plus 1964 Fairfield/507 Foul Bay. I live at Rhodo, 1720 Fairfield Rd, in unit 104.

Thank you for this opportunity to voice my support for these much needed developments.

Kelly Galitzine

Mayor and Councillors,

I am a resident at 1689 Earle Street, and I was out of the country when the last public meeting was held to review a revised application by Aryze for a new development at 1733, 1735 and 1737 Fairfield Road. I apologize for the lateness of this feedback, but I hope you will take it into your consideration of this proposal.

I participated in the process of reviewing the Aryze proposal for the Rhodo development, which is adjacent to Hollywood Park, and which is about 5 metres from one corner of my back yard.

I have to say that I am very disappointed and frustrated by this latest proposal by Aryze. It appears to me to closely resemble the Rhodo approach, where multiple "approvals" had been given by the City, prior to the wider review process open to the public. That included bypassing the formal guidance on setbacks on all four sides, height, density, and the removal of all but one tree from the two combined properties. In addition, City Council had approved a suggestion from the City only a few weeks prior to the submission, to apply a "transition" to any proposal for a new development immediately adjacent to a City park.

All of these exceptions, including ignoring the transition to the park, were approved by the City and by City Council. I saw no corresponding "give-and-take" from the developer. The result, which you can see today, is a cramped series of buildings, hard up against all boundaries, including the park, adding nothing to the Fairfield Road streetscape, and setting a bad example for future development.

I am again very disappointed and frustrated by this latest proposal by Aryze, which seems again to be getting approval for any and all requested exceptions to the formal guidance in place. The three storey Rhodo building height is now superseded by a four storey monster, which would dominate and overwhelm adjacent properties. Again, setbacks on all sides are minute, which would overwhelm the surrounding residences, trees will disappear, and parking will be aggravated even further than it is today.

Of particular concern is the convoluted undertaking by Aryze to "buy" increased density from another property, thereby increasing their original proposal for 19 units to 31 units, by way of adding a fourth floor to the building.

I understand and support increasing population density in the City of Victoria, but I strongly feel that support and guidance from the community has been overwhelmed by commercial developers.

Mayor and Councillors, I urge you to establish a balance between these two entities, thereby allowing density increases to be accompanied by minimizing negative impacts on our neighbourhoods, and ensuring the development of a vibrant environment which residents can enjoy and be proud of. I did not vote for companies like Aryze to determine the future of my neighbourhood.

Thank you for your attention,

Graham Whitehead
1689 Earle Street,
Victoria, BC, V8S 1N4

From: andrea kober [REDACTED]
Sent: Tuesday, May 16, 2023 7:57 PM
To: Victoria Mayor and Council <mayorandcouncil@victoria.ca>
Subject: Re: 1733-37 Fairfield Rd

Hello,

I am writing about the above potential townhouse development, that will no doubt go through and construction will begin next spring/summer.

My concern is about parking. I live half a block away, in a rental townhouse, and the only parking I am able to access is street parking. We don't have driveways. When I moved here there was no problem with parking. There were enough spaces for the residents, their guests and even people who parked here because they were using Hollywood Park, up the street.

Since Rhodo was built last year, the parking has gotten out of hand. Often I come home and cannot find parking and end up circling till I find something further away. Those tenants in Rhodo are not supposed to park on our side of the street. They were given less parking spaces, with the hopes/intent that most people purchasing these townhomes would only have one car per family, or cycle everywhere. It is not the case. Most people have two cars. As I sit here, there are 10 cars from Rhode on the street, and I am sure their parking spots on the property are all filled up as well.

When 1733-37 will be built, there will be 20 odd parking spaces for 30 odd units. They will also be taking away around 6 street parking spaces near the vicinity. They claim there is enough parking for everyone. Well, they don't live here and experience what I do.

I am all for change. I am also a firm believer we need to use our cars less. I walk everywhere, and only use my car for work, when I have to carry heavy equipment to where I am going.

And at the same time I find this parking situation so frustrating. The developers don't really care, as most of them live in big homes with their own driveway and are oblivious to this situation.

If the new owners of these upcoming townhomes want to live there, they should be the ones committing to having only one car per unit, or have bicycles. If most come in with two cars, there will be no parking at all, and it will be a frustrating mess. This, will not make people give up their cars, btw. When Rhodo was built, I was told people know about the parking situation, and the people who will be attracted to living here will be cyclists and people who care about the environment etc. Ha! Most of the second cars I see, are huge trucks and SUV's that are taking up 1.5 spaces.

I don't know if there is anything that can be done. I am venting, and I am also extremely frustrated. I am a senior, and doing my best. And I feel like I am going to be pushed out of my parking spot in no time.

Thanks for listening.
Andrea

Dear Mayor and Council,

I continue to be amazed by the insatiable greed of Aryze Developments.

We have zoning for a reason. This council even changed the zoning of our neighbourhood (against the wishes of the people here) to make it easier for Aryze to make more money. And yet they keep coming for more concessions.

No more zoning "gifts" please. We all know each of the 31 proposed tiny units will be sold for over \$1M.

Thanks for your attention.

Michael Muret
1987 Fairfield Road

Mayor and Council Members,

I read with trepidation about the “progress” of this proposal, which has now been increased by the developer from 3 stories to 4 stories.

This despite this developer’s lauding of Fairfield Road as all-three-storey buildings, back in the Rhodo fiasco.

Not content with taking the maximum number of variances for the three-storey plan, this developer has manipulated a switch of zoning with the Abkhazi Gardens to add a FOURTH storey to the new development.

This process is out of control. It is being manipulated by the developer, and unfortunately with the support of the City. Like other local residents (I live within 100 yards of this proposal), I am angry and frustrated that this kind of manipulation is allowed by the City. I am NOT against densification. I am against densification at whatever the cost.

It appears that the employees of the City of Victoria who work in the area of redevelopment are now completely redundant: the rules mean nothing any more. The department may as well be disbanded.

Mayor and Councillors, I urge you to reflect on who it is that you represent: is it the citizens of Victoria, or a handful of wealthy property developers?

Your performance is being closely watched by the voters of this City.

Graham Whitehead
1689 Earle Street
Victoria

The amended proposal for the development at 1733, 1735, and 1737 Fairfield Road does not address the main issues and adds even more unwarranted density and provides less parking. I trust City Council will consider the impact to our neighbourhood; and in so doing so rejects the proposal.

Aryze is proposing a looming four-story apartment building that is over 20 feet higher than the surrounding houses and will become the highest structure in all of Gonzales. There are no front or rear setbacks, no useable ground level outdoor space and minimal landscaping. The building is highly disruptive to the neighbourhood because of its height and mass, density, layout, appearance, number of units, parking and impact on the neighbor's privacy.

Aryze has not demonstrated any added community amenities to merit the proposed density transfer nor does it provide a convincing case that the receiver site is suitable. The developer has simply bought density from a third party to maximize profit and usurp city planning bylaws well beyond what should be considered reasonable for the site.

The requested density is far beyond what the site and neighborhood can accommodate. OCP amendments will be needed to increase the height beyond the three stories maximum required in a Traditional Neighbourhood designation (Section 6.1.5 and Map 23) and Floor Space ratio (FSR) from 1.1 to 1.77, as well as front/back/side setback variances.

I believe densification efforts in Gonzales should provide quality housing options for families. Any new development needs to be compatible with neighbors, have respectful front and rear yard distances, usable rear yards, access to outdoor open green space, consistent massing, adequate underground parking and consistent character. In other words, all infill buildings in Traditional Residential areas of Gonzales should be ground-oriented dwellings that are limited to two and a half story houseplexes, duplexes and townhouses.

Kevin Warren

As a close neighbor to this site, I am writing to express my concern about the revised proposal to develop 1733, 1735 and 1737 Fairfield Road. The revisions do not address the main issues that were brought to the attention of the developer. The height, mass and density far exceed what is reasonable in this neighbourhood. It will be highly disruptive, both to close neighbours and the wider community, who use nearby parks, shopping and schools.

The proposed density transfer appears to benefit the developer with little benefit to the city, and a significant detriment to the Gonzales neighborhood. The developer has not demonstrated that the receiver site can accommodate this level of density, which is far beyond current OCP guidance and will require amendments. The four story apartment-style building design with no front or rear setbacks, no usable ground level outdoor space and paved outdoor areas with minimal landscaping, is not aligned with principles of family-focussed attainable housing or green space enhancement.

I am supportive of densification efforts in Gonzales that provide quality housing for families, are compatible with the character of our neighborhood, respectful of neighbors, and protect our greenspace. In Traditional Residential areas I would like to see ground-oriented dwellings that are limited to two and a half story houseplexes, duplexes and townhouses.

I respectfully request Victoria City Council reject the revised proposal.

Janice Linton
356 Robertson Street

Hello,

Abkhazi Gardens is such a treasure. I hope you can protect it from being developed.

Pam Verhagen

2023

CITY OF VICTORIA | Sustainable Planning & Community Development

Rezoning and Development Permit with Variances Application

1733, 1735 and 1737 Fairfield Road
1964 Fairfield Road and 507 Foul Bay Road

COMMITTEE OF THE WHOLE | December 7, 2023



1

2

Legislative Authority

LGA, Sect. 479- Council may regulate the permitted uses, density, siting, size and dimensions of land, buildings and other structures within a zone

Sect. 482- A zoning bylaw may establish different density regulations for a zone, one generally applicable for the zone and the others to apply if certain conditions are met.

Sect. 489- Council may issue a Development Permit in accordance with the applicable guidelines specified in the Community Plan.

Sect 611- Council may designate real property, in whole or in part, as protected property, which may apply to more than one property and may apply to landscape features.



2

Aerial Photo

(1733, 1735 and 1737 Fairfield)



Aerial Photo

(1964 Fairfield & 507 Foul Bay)



2



← 1733

Subject Properties




← 1737

↑ 1735




5



6

1745 Fairfield Road (northeast)



6

7



**327/333
Beechwood
Avenue
(southeast)**



7

8



**View across
Beechwood
Avenue**



8

9

**View across
Fairfield Road**



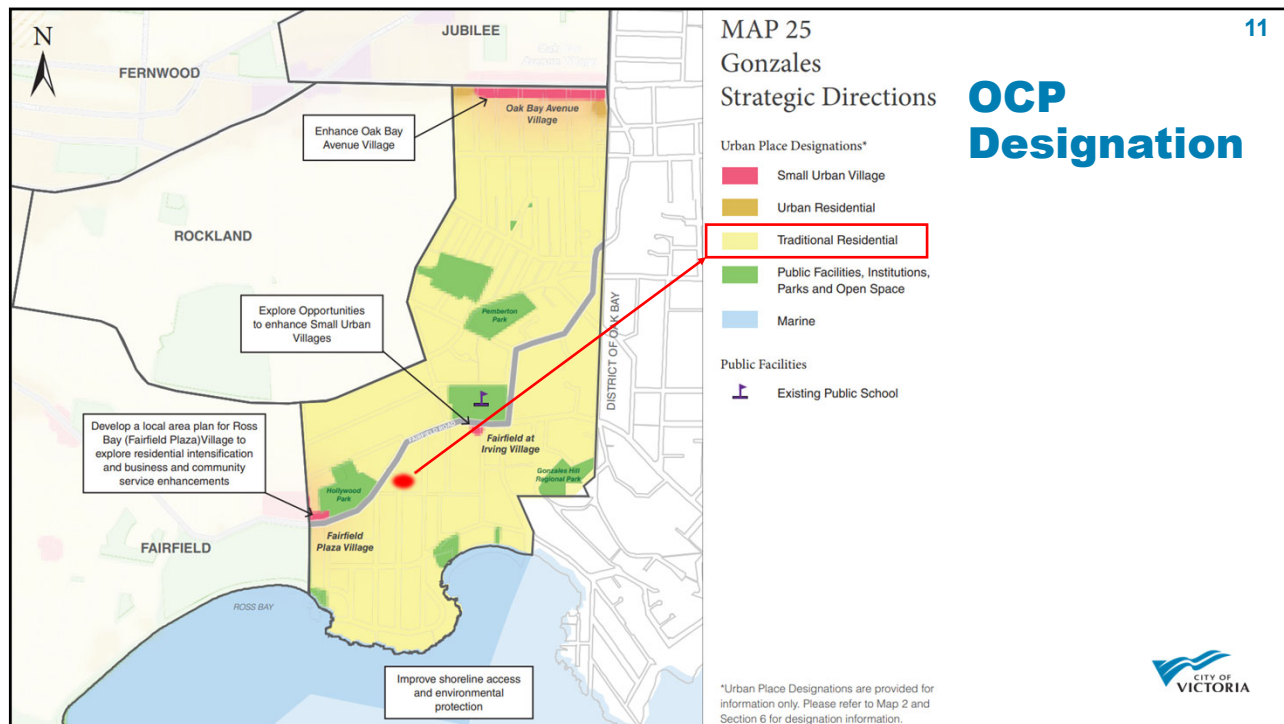
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3D Aerial



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11

Proposed OCP Amendments

1733, 1735 and 1737 Fairfield Road

- permit a four-storey multiple dwelling- 1.77:1 FSR (amending envisioned density, use and height)
- contributes to growth management, housing diversity, heritage protection objectives
- supported, contingent on greater constituency with design guidelines

1964 Fairfield Road and 507 Foul Bay Road

- designate as Public Facilities, Institutions, Parks and Open Space
- supported, aligns with objectives to preserve cultural assets and historic sites

12

13

Inclusionary Housing and Community Amenity Policy

OCP Amendment on a Traditional Residential property

Alternative proposed to inclusionary housing units or cash contributions to City reserves:

- concurrent OCP amendment, rezoning, secured public access, and heritage designation amendment

Monetary donation to TLC in the amount of \$350,000- not secured as part of the rezoning



13

14

Variation from URMD Zone, Urban Residential Multiple Dwelling

Density and Site Coverage:

- FSR from 2:1 to 1.77:1
- minimum lot area from 1840m² to 1740m²
- site coverage from 40% to 65%
- open site space from 50% to 23%

Siting:

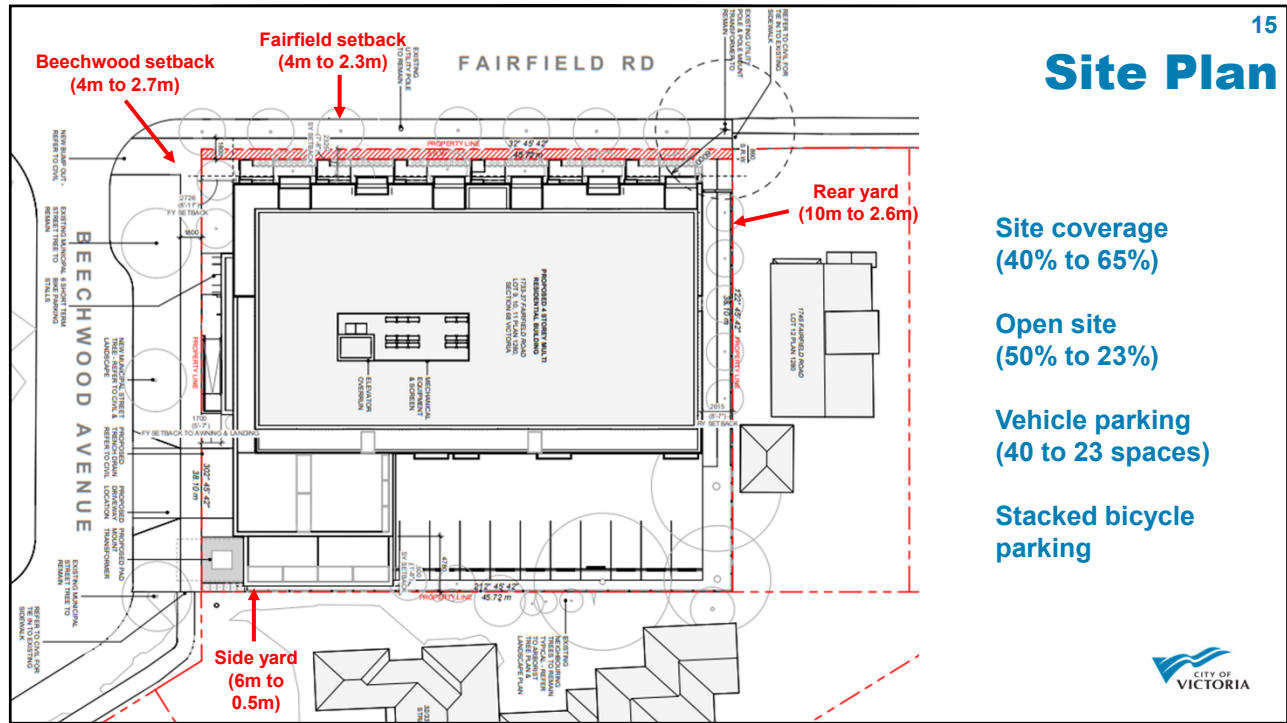
- Beechwood Avenue setback from 4.00m to 2.70m
- rear yard setback from 10.00m to 2.60m
- Fairfield Road setback from 4.00m to 2.30m
- side yard setback (southeast) from 6.00m to 0.50m

Parking:

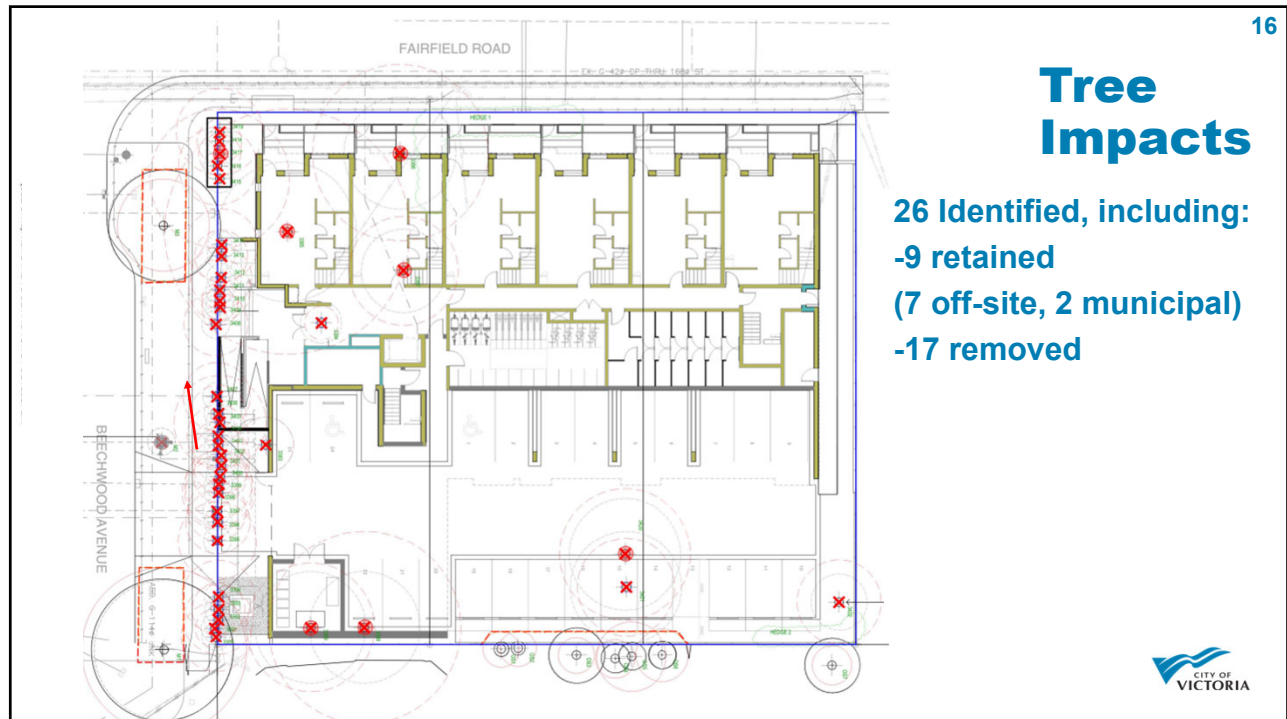
- vehicle parking from 40 spaces to 23 spaces
- permit stacked long-term bicycle parking



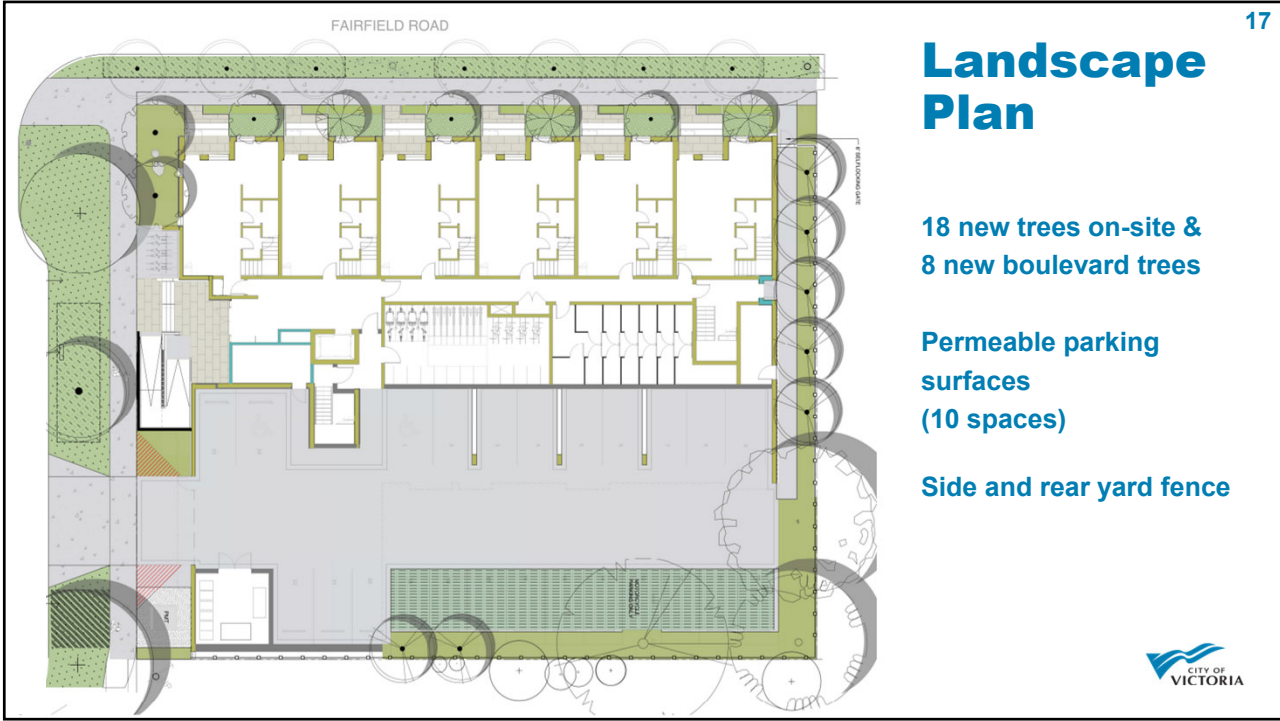
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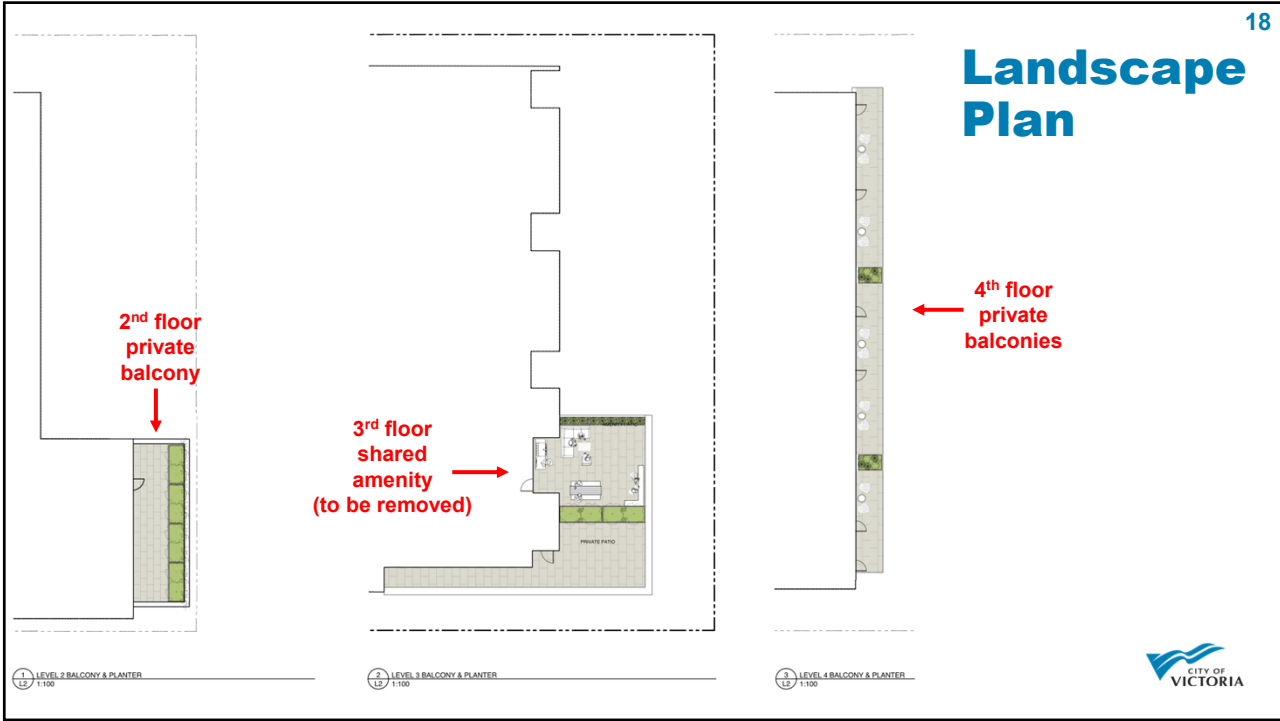
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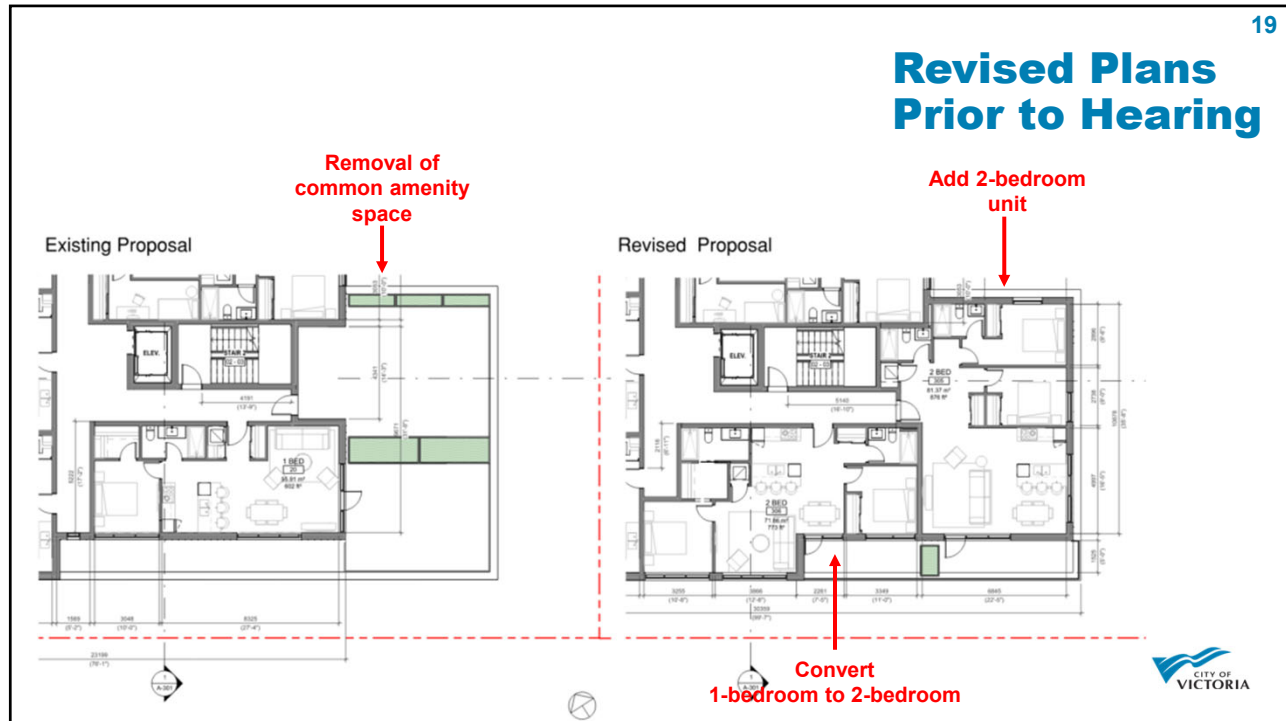
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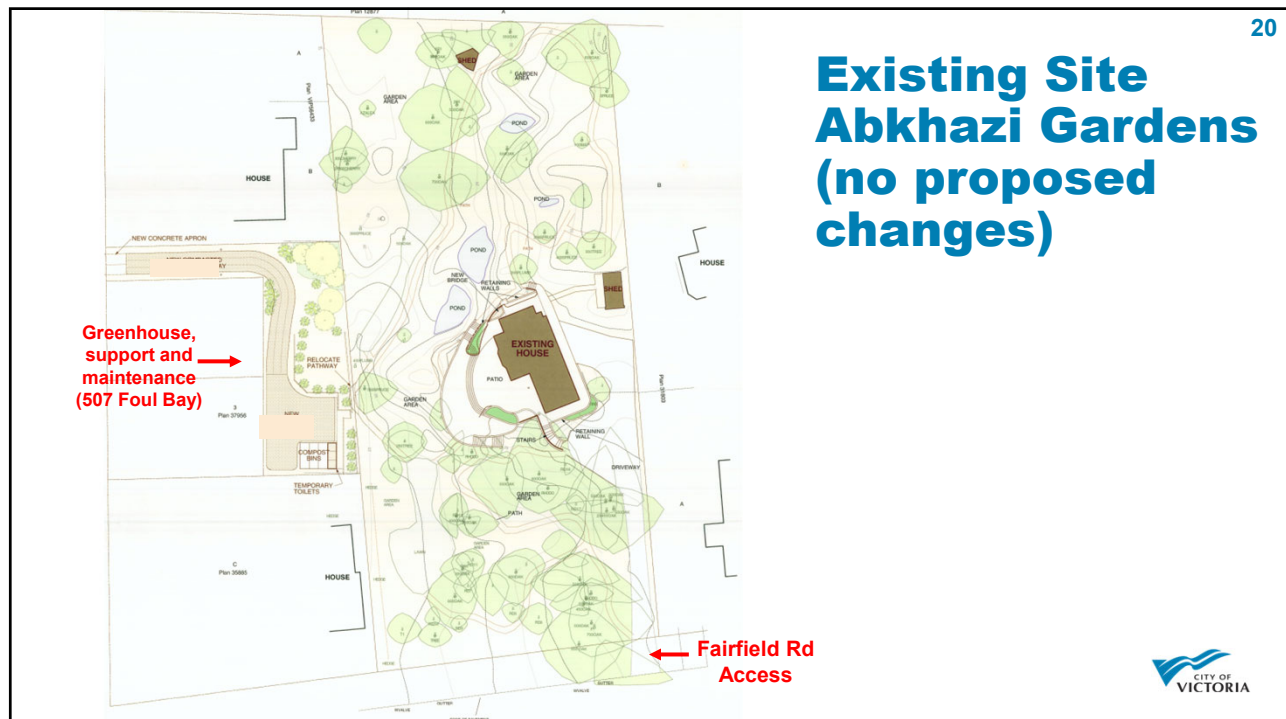
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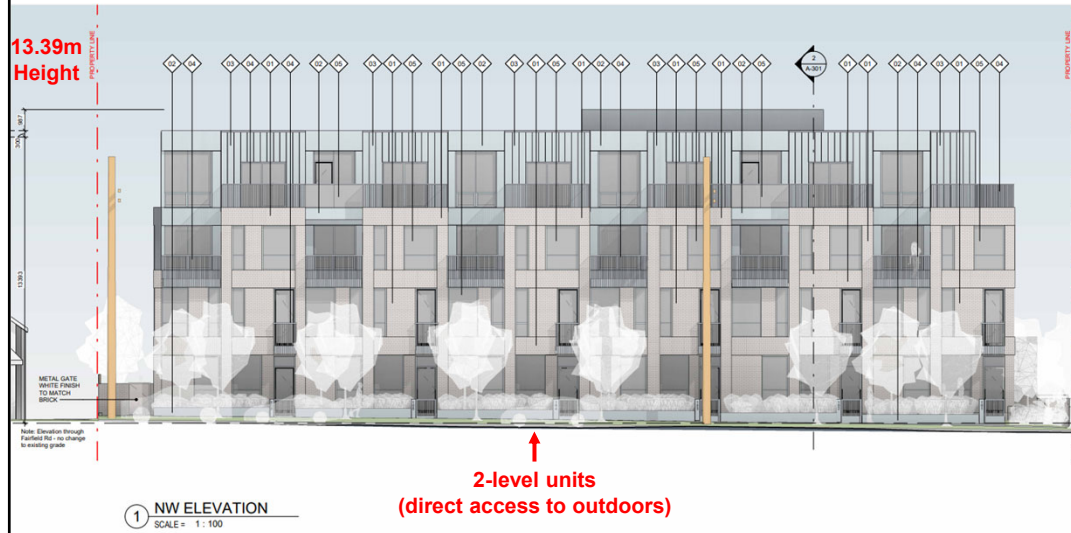
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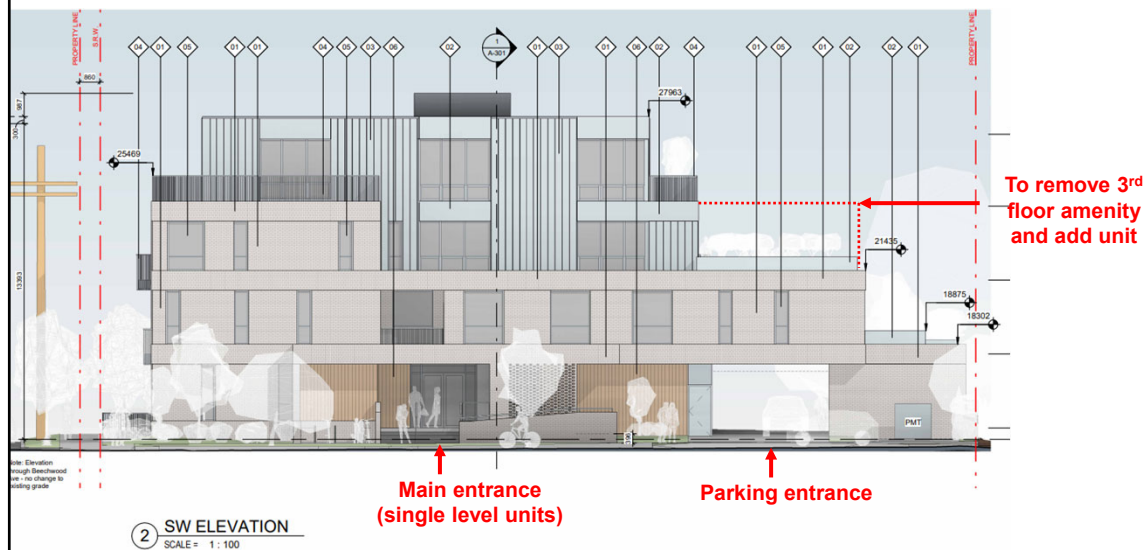
Fairfield Road Elevation



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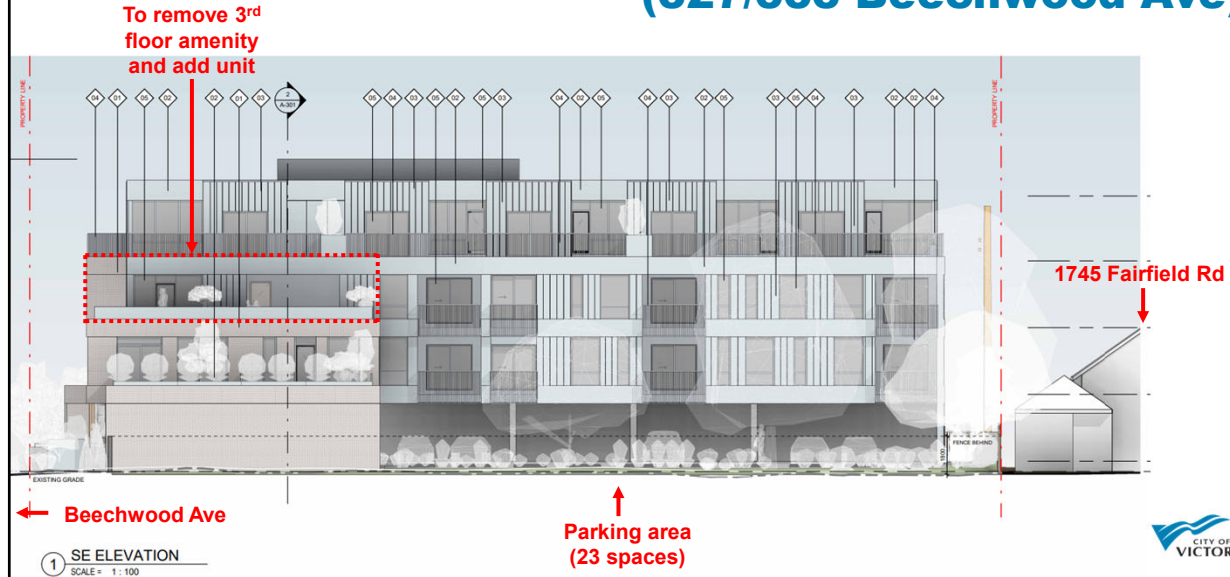
Beechwood Avenue Elevation



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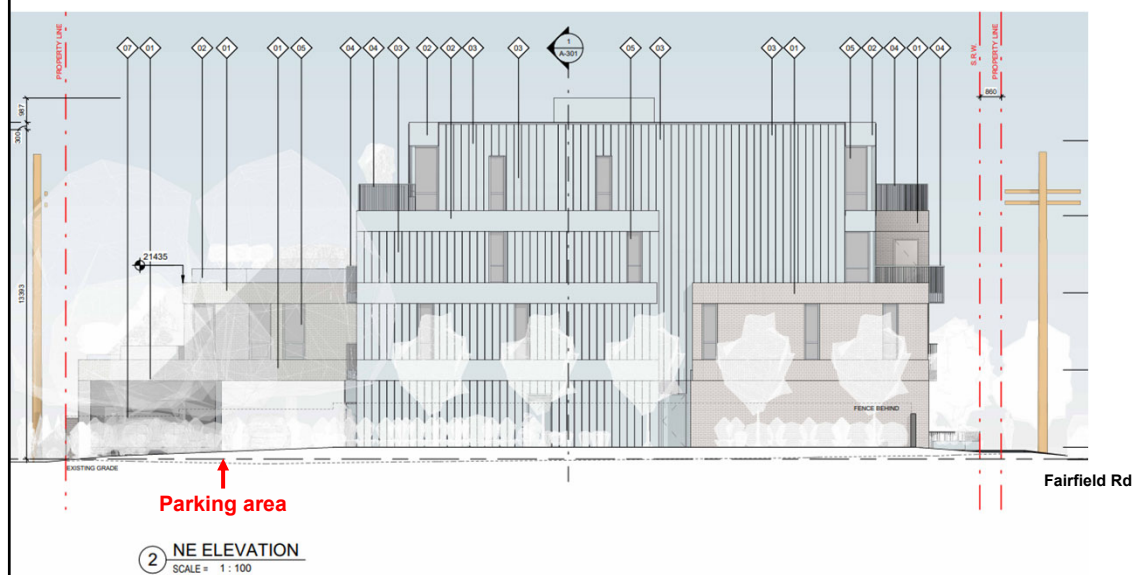
Southeast Elevation (327/333 Beechwood Ave)



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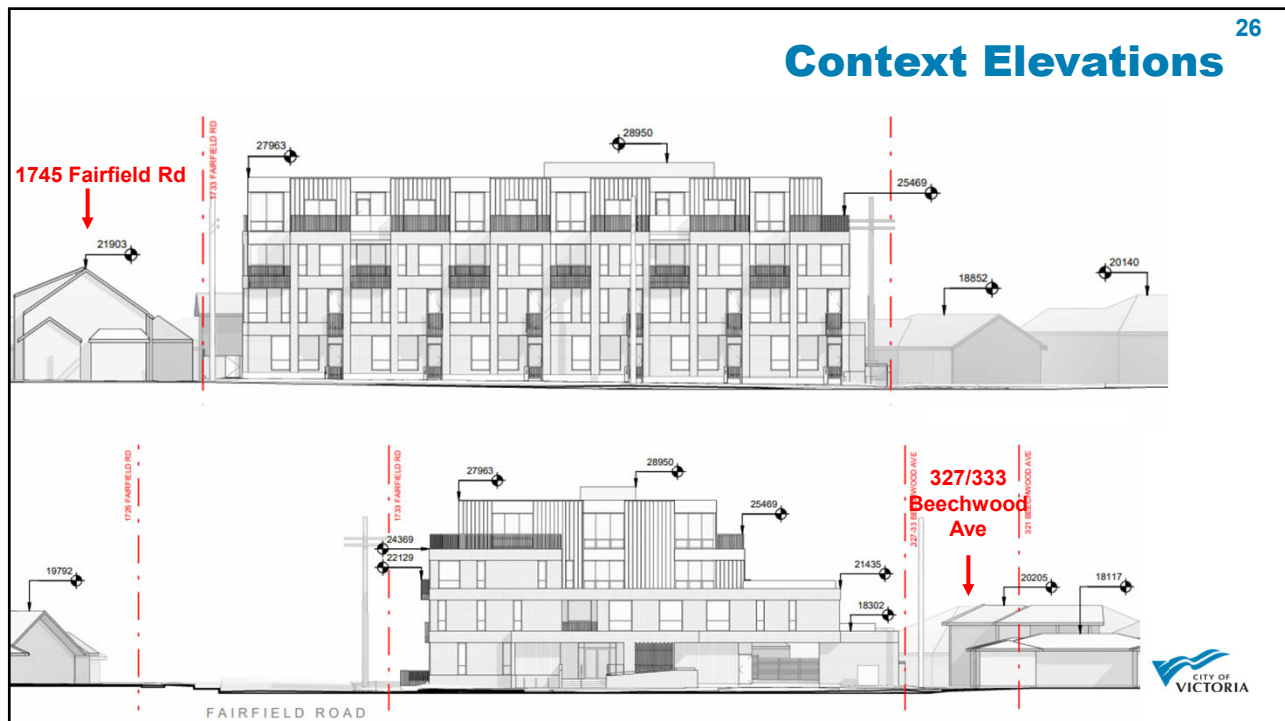
Northeast Elevation (1745 Fairfield Rd)



24



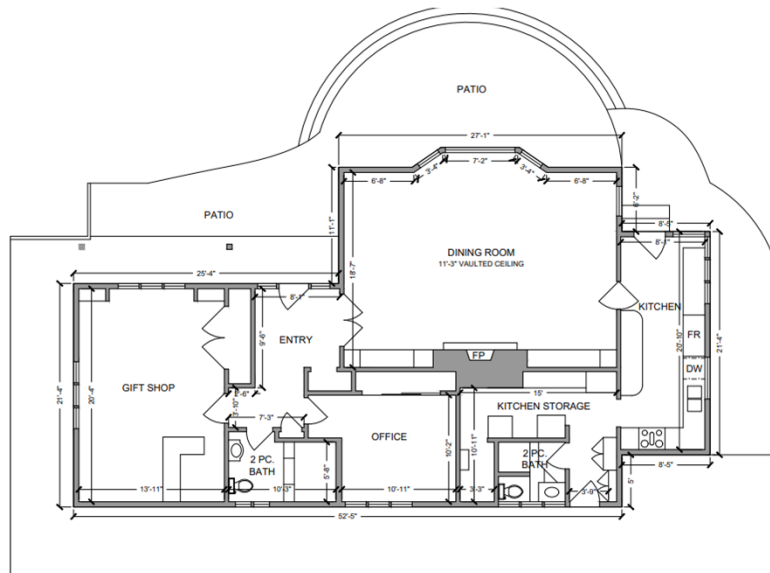
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Abkhazi Garden (no proposed changes)



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DPA 16 General Form and Character

Site Design:

Generally consistent, except revision recommended to:

- provide adequate usable amenity space
- minimize parking area, maximize landscaping
- may include reduced parking, underground parking or a landscaped amenity space be provided

Building Design:

Generally consistent, except revision recommended to:

- provide a transition in height where directly abutting lower density
- recommend revision such as step-in of 4th storey on northeast elevation



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Conclusion

OCP

- Exceeds the use, density and height envisioned for Traditional Residential
- advances growth management objectives, adds housing diversity, located on secondary arterial road, strengthens protection of Abkhazi Gardens

Design Guidelines

Generally consistent with DPA 16

- reduced parking, minimizes visual impact through design
- building design responds to context, contributes to streetscape

Recommended revision to building massing- northeast elevation

Recommend parking be underground or further reduced and with provision of outdoor amenity

