

Planning and Land Use Committee Report

For the Meeting of February 19, 2014

To: Planning and Land Use Committee **Date:** February 5, 2014
From: Charlotte Wain, Senior Planner – Urban Design, Development Services Division
Subject: **Development Permit with Variances Application #000391 for 960-962 Yates Street**

RECOMMENDATION

Staff recommend that Committee forward this report to Council and that Council refer the Application to the Advisory Design Panel, with a request that the Panel pay particular attention to:

- the surface parking courtyard
- the residential entrance
- the design of the primary tower and the perceived massing of the upper storeys.

Following this referral, and after giving notice and allowing an opportunity for public comment and after the Public Hearing for Rezoning Application #00463, if it is approved, that Council consider the following motion:

“That Council authorize the issuance of Development Permit with Variances Application #000391 for 960-962 Yates Street, in accordance with:

1. Plans date stamped December 3, 2014.
2. Development meeting all *Zoning Regulation Bylaw* requirements, except for the following variances:
 - i. Schedule C, 7.2(g) – Required residential visitor parking is reduced from 10 spaces to five;
 - ii. Schedule C, 16.A.12(c) – Required residential parking is reduced from 1.4 spaces per dwelling unit to 1 space per dwelling unit;
 - iii. Schedule C, 16.C.4 – Required commercial parking is reduced from four spaces to two.
3. Final plans to be generally in accordance with the plans identified above to the satisfaction of the Assistant Director, Development Services Division, Sustainable Planning and Community Development Department.”

LEGISLATIVE AUTHORITY

In accordance with Section 920(2) of the *Local Government Act*, Council may issue a Development Permit in accordance with the applicable guidelines specified in the *Official Community Plan*. A Development Permit may vary or supplement the *Zoning Regulation Bylaw* but may not vary the use or density of the land from that specified in the Bylaw.

Pursuant to Section 920(8) of the *Local Government Act*, where the purpose of designation is the revitalization of an area in which a commercial use is permitted, a Development Permit may include requirements respecting the character of the development, including landscaping, the siting, form, exterior design and finish of buildings and other structures.

EXECUTIVE SUMMARY

The purpose of this report is to present Council with information, analysis and recommendations for a Development Permit Application for the property located at 960-962 Yates Street. The proposal is to construct an 18-storey mixed-use building containing 88 residential units and two ground-floor commercial units. A parking variance is related to this Application.

The following points were considered in assessing this Application:

- The proposed building is subject to regulation under Development Permit Area 3 (HC) and is generally consistent with the applicable Design Guidelines in the *Official Community Plan* (OCP) and the *Downtown Core Area Plan* (DCAP).
- Although the proposal is largely consistent with the relevant design guidelines, it would benefit from a review by the Advisory Design Panel with specific reference to the surface parking courtyard, the recessed residential entrance and the design of the primary tower.
- A parking reduction for both residential and visitor spaces is being proposed. The results from the accompanying parking study conclude that the impacts on the surrounding neighbourhood would be minimal.

BACKGROUND

Description of Proposal

The proposal is for an 18-storey mixed-use building with 88 residential units and two ground-floor commercial units. The proposed site plan, architecture and landscape design include the following details:

- high-rise building form with a primary street wall along the Yates Street and all parking provided underground
- a mixture of siding, including rough and honed limestone veneer, painted concrete in cream and grey colours
- aluminum windows with accent metal and panels of insulated spandrel glazing
- replacement of the two existing street trees with two new trees consistent with City standards
- sidewalk improvements adjacent to the building, consistent with the Harris Green standards
- a landscaped podium and recreational amenity space on the roof of the second floor, with deep planters that include bio-swales for rainwater management as well as various tree and shrub species
- 96 vehicle parking spaces and five visitor parking spaces located within three levels of underground parking
- 88 class one bike storage spaces located underground
- six class two bike racks located at street level adjacent to the main residential entrance.

A parking variance is associated with the proposal and is discussed in more detail later in this report.

Sustainability Features

As indicated in the applicant's letter dated November 27, 2014, the sustainability features associated with this Application include an electric vehicle charging station, secure bike storage for each residential unit and bioswales on the roof of the second floor podium for rainwater management.

Existing Site Development and Development Potential

The site is presently occupied by a single storey commercial building and surface parking.

Under the current S-1 Zone (Limited Service District), the property could be developed up to 1.5:1 FSR with a maximum height of 15m. The range of uses permitted in the Zone includes but is not limited to restaurants, clubs, garages, automobile rental and retail sales.

Data Table

The following data table compares the proposal with the relevant guidelines from the Downtown Core Area Plan (DCAP). An asterisk is used to identify where the proposal is less stringent than the policy.

Zoning Criteria	Proposal	DCAP Guidelines
Density (Floor Space Ratio) – maximum	5.50:1	3:1 (base) 5.50:1 (bonus)
Total floor area (m ²) – maximum	7570.00	N/A
Residential	7375.00	
Retail	145.00	
Floor Plate (m ²) – maximum		
Portion of building between 20-30m	475	930
Portion of building above 30m	460 Levels 15-16 330 Level 17	650
Height (m) - maximum	52.20*	50
Storeys - maximum	18* (mechanical unit only)	17
Setbacks (m) - minimum		
Front (Yates Street)	Levels 1-3: Nil Levels 4-10: 4.00 Levels 11-16: 4* Levels 17: 5.35*	Levels 1-3: Nil Levels 4-10: 3.00 Levels 11-17: 6.00
Rear	Levels 1-3: Nil Levels 4-10: 5.50 Levels 11-16: 5.50* Levels 17: 8.40	Levels 1-3: Nil Levels 4-10: 3.00 Levels 11-17: 6.00

Zoning Criteria	Proposal	DCAP Guidelines
Side (east)	Levels 1-3: Nil Levels 4-10: 5.50 Levels 11-17: 5.50*	Levels 1-3: Nil Levels 4-10: 3.00 Levels 11-17: 6.00
Side (west)	Levels 1-3: Nil Levels 4-10: 5.50 Levels 11-17: 5.50*	Levels 1-3: Nil Levels 4-10: 3.00 Levels 11-17: 6.00
Parking - minimum	96	N/A
Visitor parking - minimum Included in the overall units	5	N/A
Bicycle storage - minimum	88	N/A
Bicycle rack - minimum	6	N/A

Community Consultation

Consistent with the *Community Association Land Use Committee (CALUC) Procedures for Processing Rezoning and Variances Applications*, the applicant has consulted the Downtown Residents Association CALUC at a Community Meeting held on August 14, 2014. A letter dated October 15, 2014 is attached to this report.

ANALYSIS

Development Permit Area and Design Guidelines

The *Official Community Plan 2012* (OCP) Urban Place Designation for the subject property is Core Residential, which supports multi-unit residential, commercial and mixed-use buildings from three storeys up to approximately 20 storeys. The OCP also identifies this property in Development Permit Area 3 (HC) Core Mixed-Use Residential. The objectives of this DPA are to transform the form and character of the area through mid-to-high-rise residential mixed use and commercial buildings, with the greatest heights along Yates Street. Ensuring high quality architecture, landscape and urban design is also an important objective of this DPA. The proposed development at 960-962 Yates Street is consistent with DPA 3 (HC) objectives for redevelopment and overall high-quality design.

Downtown Core Area Plan

With respect to local area plans, the *Downtown Core Area Plan, 2011* (DCAP) applies to the subject site. Within the DCAP, the Residential Mixed District (RMD) includes the 900-block of Yates Street, noted as a transitional zone from high-to-medium density, in the "Cross Town Concept" and where height transitions from high-rise to mid-rise buildings in the "Urban Amphitheatre Concept" with the concentration of tall buildings along Yates Street, east of Douglas Street.

Multi-unit residential development is encouraged in the RMD with higher density focussed along Yates Street. The RMD encourages multi-residential development appropriate to the context, respecting the allowable building heights in the neighbourhood. Active commercial street-level uses are encouraged along Yates Street to help increase pedestrian activity. The current proposal is generally consistent with these objectives as it contributes new street-level commercial space in the RMD, which is further supported by residential uses above. However the proposal is for a 52.5m high building, which is 2.5m above the recommended 50m maximum height in the guidelines. Staff recommend for Council's consideration that this is supportable as it does meet the recommended

number of storeys (17) and the additional height relates only to the mechanical penthouse on the roof, which is also intended to be used for a transmission facility for the Victoria Fire Department.

The DCAP provides both broad urban design objectives for the Downtown Core and more detailed design guidelines for specific districts. The DCAP also includes policies related to the design of buildings. Overall, the proposal is consistent with these policies, however, some inconsistencies with the design policies are discussed below.

Building Setbacks and Street Walls

DCAP contains policies for street wall heights and setbacks that are appropriate for the context of each street. The intent of these guidelines is to:

- minimize the effects of shading and wind
- maintain views to the open sky
- avoid the visual presence of bulky upper building mass.

Yates Street fits the category “wide street” in the DCAP, where the front setback should be less than 3m in order to create a sense of animation. The proposed site plan for the residential mixed-use development is consistent with this guideline, although minor deviations exist for other aspects of the building design. The design guidelines recommend a side and rear setback of 6m for portions of buildings above 30m in height (in this case, for level 11 and above). The proposal does not comply with this for levels 11 to 16, which are setback 5.5m (half a meter under the recommended setback). This is considered an acceptable minor deviation from the design guidelines.

The primary street wall is 11.5m in height, which is slightly shorter (by 3.5m) than the recommended 15m. The applicant has indicated in the supporting letter that the intention of this design response is to ensure an appropriate transition from the adjacent building to the east and that the provision of a trellis helps to increase the apparent height at the street. In addition, there is no secondary street wall for the building as recommended in the guidelines. Instead the entire building is stepped back 4m at the fourth storey, with vertical articulation being emphasized from levels four to 14, and horizontal articulation at the top three storeys. This setback is 2m below the recommended guidelines for portions of buildings above 30m in height. Although the reduced floor plate of the tower and setbacks on the rear and sides, do help to reduce the apparent mass of the main building, staff suggest the Application would benefit from a review by the Advisory Design Panel (ADP) on this matter.

Building Design

Overall, the proposed form and massing is organized to demarcate the building base, body and top. Commercial units provide multiple entrances off the street, with canopies giving weather protection for pedestrians. Staff have raised concerns regarding the recessed residential entrance and visitor parking on the ground level of the building, which could become a Crime Prevention Through Environmental Design (CPTED) challenge. The applicant has responded to this by stating that the area will be well-lit and finished with high quality materials, although no specific details have been provided. Staff recommend that the Application be forwarded to the Advisory Design Panel for consideration of this issue.

Building and Street Interface

In the DCAP Appendix 5 “Building and Street Interface Guidelines”, this block of Yates Street is identified as a “Primary Commercial Street”. In these locations, public realm improvements should be designed to relate to adjacent land uses and to provide comfortable, safe and animated

pedestrian environments. The proposal includes commercial uses at grade and this would contribute to street animation through multiple entrances, extensive glazing and awnings for weather protection for pedestrians. Also, two new street trees are proposed along with sidewalk improvements consistent with the Harris Green standards for the Yates Street frontage (south.)

Advisory Design Guidelines for Buildings Signs and Awnings (1981)

These Guidelines state that an acceptable application will include consideration of an attractive streetscape and that the architecture and landscaping of the immediate area be identified and acknowledged. In evaluating a design, particular emphasis will be placed on the solution to these general aspects: comprehensive design approach, relevancy of expression, context, pedestrian access, massing, scale, roofline, detailing, street relationship, vistas, landscaping plan, colours and textures. The Application is consistent with these Guidelines.

Guidelines for Fences, Gates and Shutters (2010)

The objective of these Guidelines is to ensure that where fences, gates and shutters are required, they are designed well and complement their surroundings. The Application is consistent with these Guidelines and proposes to introduce high quality screening between the sidewalk and the surface level 'courtyard' parking.

Proposed Parking Variances

The proposed parking supply is for a total of 96 stalls, which is 41 fewer than the requirements under Schedule C. However, it is worth noting that the R-48 Zone (Harris Green District) is a common multi-unit residential zone that is applied to a number of properties in the area, which does not contain a parking requirement. Instead, it allows the market and developer to dictate the number of parking stalls that are provided. The requirements of Schedule C of the *Zoning Regulation Bylaw* are summarized below:

Parking Stall Type	Schedule C Requirement (minimum)	Proposed
Residential	123 (ratio of 1.4:1)	88 (ratio of 1:1)
Residential visitor	10	5
Commercial	4	2
Electric Charging Station	0	1
Total	137	96

The applicant has submitted a parking study to review the proposed parking supply and assess if the reduced parking provision will meet the demand for the subject site. The parking study has compared similar multi-unit sites in the Harris Green neighbourhood as well as vehicle ownership data from ICBC and concluded that the proposed resident and visitor parking supply will meet the parking demand. In addition, commercial visitor parking will be exceeded only in peak periods resulting in up to two vehicles seeking parking on the street. As the impacts to the surrounding neighbourhood will be minimal and on-street parking is available within one block of the subject site, staff recommend that Council support the proposed parking variance.

CONCLUSIONS

The proposed high-rise mixed-use development at 960-962 Yates Street would support the planning objectives for the Downtown found in the OCP and the DCAP. The proposal is generally consistent with the design guidelines contained within the DCAP and includes high quality building materials and landscape finishes. The proposed parking variance is considered supportable based on the vehicle ownership and demand within the surrounding area. However, the Application would benefit from a review by the ADP in relation to the potential CPTED concerns for the recessed residential entrance, surface parking courtyard and the and the perceived massing of the upper storeys.

ALTERNATE MOTION

That Council decline Development Permit Application #000391 for the property located at 960-962 Yates Street.

Respectfully submitted,



Charlotte Wain
Senior Planner – Urban Design
Development Services Division



Alison Meyer, Assistant Director
Development Services Division
Sustainable Planning and
Community Development Department

Report accepted and recommended by the City Manager:



Jason Johnson

Date:

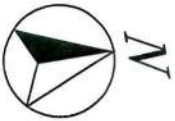
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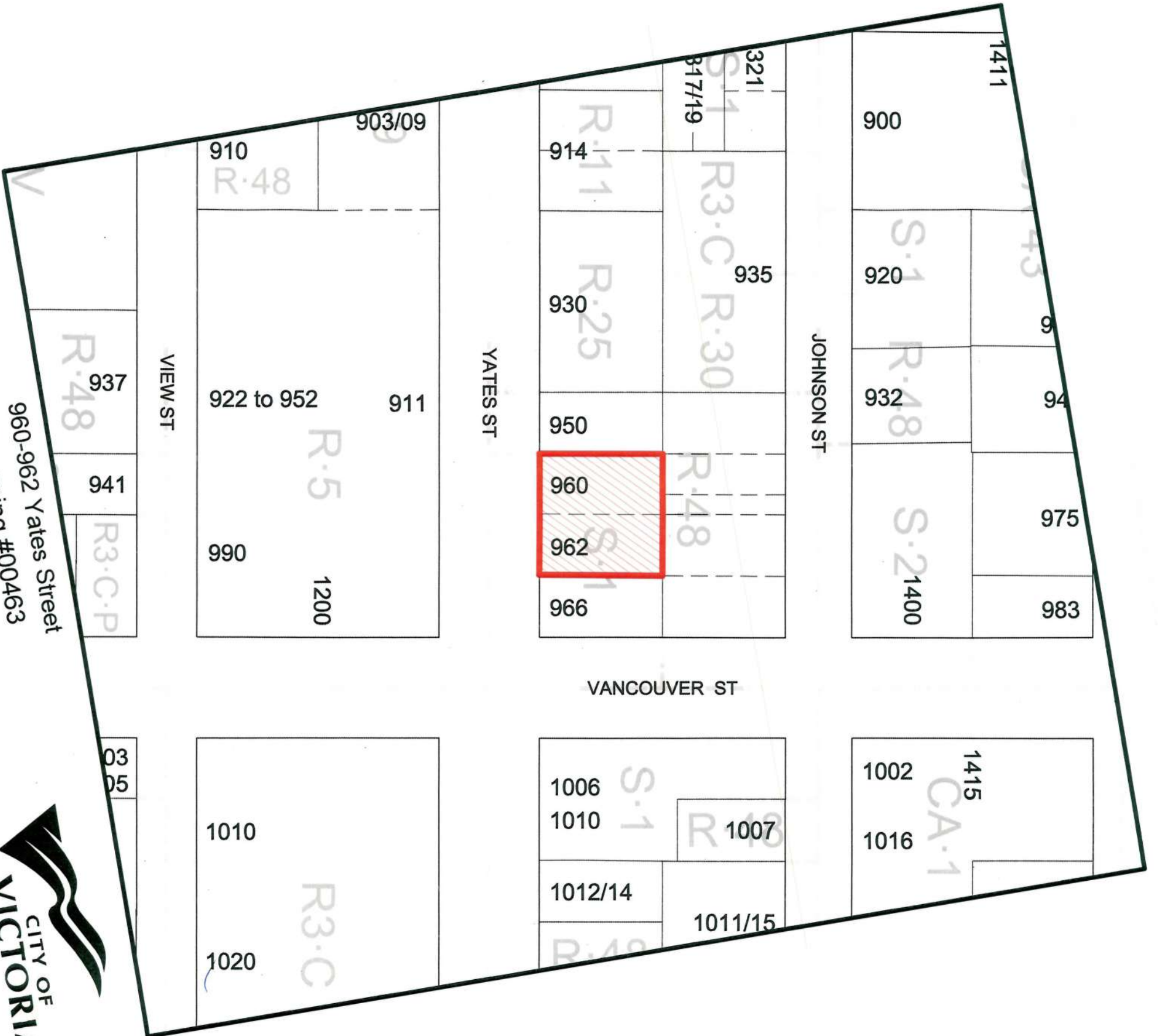
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List of Attachments

- Zoning map
- Aerial photo
- Letter from de Hoog & Kierulf architects, dated November 27, 2014
- Letter from Downtown Residents Association, dated October 15, 2014
- Plans for Rezoning Application #00463 and Development Permit Application #000391.



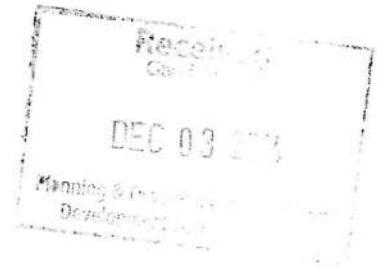
960-962 Yates Street
Rezoning #00463
Bylaw #





960-962 Yates Street
 Rezoning #00463
 Bylaw #





27 November 2014

Mayor and Council
City of Victoria
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RE: 960/962 Yates Street - Application for Re-zoning/Development Permit

Dear Mayor & Members of Council:

On behalf of our client, Alpha Project Developments Ltd., we are pleased to submit this application for re-zoning and concurrent development permit for the above named property.

The proposed development is located on the north side of Yates Street near the intersection of Vancouver and Yates in the Harris Green Neighborhood of Victoria. The site is currently zoned S-1 - LIMITED SERVICE DISTRICT.

We are requesting a site specific zone that is generally in compliance with the City of Victoria Downtown Core Area Plan (DCAP) and the Official Community Plan (OCP). The site is located within Development Permit Area 3 (HC), and is currently used as a single storey retail store with surface parking.

The proposed development is consistent with the development potential identified in the OCP and DCAP documents. The site is located in the Residential Mixed-Use District, anticipating predominantly residential buildings with street level commercial uses. The proposed mixed-use building is 17 storeys with three levels of underground parking and a mechanical penthouse. Two commercial/retail units are located on the main floor with 16 floors of residential suites above. The DCAP identifies base density for this site as 3.0:1 with a maximum density of 5.5:1. The proposed project achieves this maximum density. The DCAP identifies the maximum height for this site as 17 stories for residential use. The proposed project achieves this maximum height. An additional storey is provided as a mechanical penthouse to enclose mechanical equipment and the elevator controller closet, rather than expose mechanical equipment on the roof. It also provides space for a Crest Communications transmission facility for the Victoria Fire Department.

The residential lobby and two commercial/retail units are located at street level on the east half of the property and directly face the bus stop. Surface parking for visitors, and the underground parking drive aisle is located on the west half. A decorative screen expressive of the building's theme separates the surface parking from the sidewalk. Behind this screen, space is provided for the temporary storage of garbage/recycling containers on pick-up days. A 6-stall bicycle rack near the lobby entrance and convenient pedestrian access from the secure underground parking are also within the covered area. The entire area is directly observable from the street and is designed to be well-lit, finished with high quality, light coloured materials on walls and soffits, and feature a decorative drive surface creating a courtyard effect.

The existing street trees will be replaced with species consistent with City of Victoria requirements in locations that do not conflict with underground services. The curved grassed boulevards will be maintained. Sidewalk improvements adjacent to the building will be consistent with the Harris Green standard and tie into the proposed column locations. Existing decorative paving elements along the street will be maintained, protected, and reinstated, where necessary.

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The first two floors of apartments are located directly over the first floor with no set backs. This creates a 3 storey street wall of living rooms and terraces facing directly onto the street, enhancing the residential quality of the street frontage. Behind these street-front units, the second and third floors are dedicated to the building core and double height amenity spaces. The remainder of the second floor area is a landscaped podium with deep planters that include bio-swales for rainwater management and appropriately scaled trees and other plantings, patios, an outdoor kitchen, and exercise area for residents.

The upper fourteen residential floors of the building are set back from the main floor podium creating expansive terraces along Yates Street at the 4th level. All suites on the fourth to fourteenth levels are provided with balconies that overlook the landscaped podium below. There is a total of 88 residential suites consisting of 8 junior 1-bedroom suites, 4 junior 2-bedroom suites, 22 one bedroom/one bath suites, 44 two bedroom/two bath suites, 8 sub penthouse suites and 2 penthouse suites. Suites range in size from 47m² to 143m² and have been designed to provide a good mix for downtown living within an affordable price range. A covenant will be placed on title to allow rental use into perpetuity.

Proposed setbacks generally meet all requirements of the DCAP. The first three storeys are built to the front property line with no setback along the sidewalk in conformance with primary commercial street (wide) interface guidelines. This podium is approximately 11.5 meters high and the apparent height is increased with the addition of a trellis structure at the fourth floor. The tower then sets back 6m from all sides to meet the requirements in the DCAP for buildings at 45m high. The 15th and 16th floors step back an additional 3m at sides and back. The 17th floor generally steps back an additional 4m at sides and back. Due to planning considerations and to provide articulation on the 17th floor, the northwest and northeast corners infringe on this setback requirement. The result is a slender, elegant building with an efficient floor plan creating high quality and affordable residences.

The primary street-wall is proposed to be lower than recommended in the DCAP to prevent a stark contrast to existing buildings beside and across from the proposed development and provide a gentle transition to future buildings that would either need to conform to lower scale Harris Green zoning requirements or larger scale DCAP requirements. A secondary street wall, a strategy to reduce the bulk and mass of larger developments, is not introduced due to the small size of the site. The podium has been divided into six bays to provide a pedestrian scaled rhythm along the street frontage. Three bays are retail units, one bay is the residential lobby entrance, one bay is the parking entrance, and the last bay forms a screen between the surface parking and the sidewalk. The six bays create an appropriately scaled rhythm to the sidewalk with canopies and a common signage band weaving them into a cohesive urban fabric. Further articulation that may be required of a larger scale development to reduce its mass would result in a broken expression of the street frontage at this smaller scaled site.

The building facade has been articulated vertically to emphasize its presence at a significant site in the Harris Green neighborhood. Its location on the mid block and north side of the street minimizes its shadowing effect on the street. The shadowing effect on neighboring properties is mitigated by the generous setback from the north, east and west property lines.

Commercial/retail use is provided at ground level along street frontage to provide activity and interest to the pedestrian environment. There are no ground level suites along the street frontage. Upper level residential windows and balconies have been placed to allow overlook to the street below. Ground floor walls for retail/commercial spaces have maximized the amount of glazing to make activities and merchandise visible from the sidewalk to increase interaction between pedestrians and businesses. Metal and glass canopies are provided along the street frontage with a curved feature canopy at the residential entrance, giving the residential entrance a unique identity. Entrances to the commercial/retail units are recessed into glazed alcoves to provide further weather protection and

articulation of the building at the sidewalk. High quality, durable building materials are proposed including stone tile and painted concrete. Balcony guards are treated as ornamentation and feature horizontal railing details and glass panel accents.

The proposed development provides 96 parking stalls, exceeding a ratio of 1:1 stalls per suite. Secure, underground parking includes 88 parking stalls for residents and one stall specifically designated as an electric car high-speed charging station. Surface parking includes 5 stalls for visitors consistent with recommendations by the parking study (included with the rezoning/development permit package). One visitor stall is designated as an accessible stall. There are also two designated commercial stalls, one for each commercial unit. Bicycle parking includes one Class 1 bicycle stall for each suite located in common bicycle rooms sized and arranged according to recommended City standards. A 6-space bicycle rack is also provided at the front of the building adjacent the residential lobby entrance.

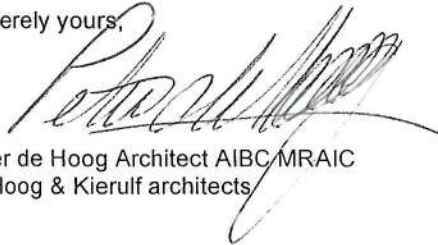
All outdoor spaces will be designed in accordance with CPTED guidelines to ensure that safety and security requirements will be addressed for all users. The covered surface parking will be screened from the street. This area will also be finished with high quality materials, well lit, and protected with security cameras. Glass block and glazing at the residential entrance and other lighting will maintain a glowing atmosphere within the covered area that is inviting and safe for residents with no blind corners. Visibility and security will also be addressed in the parkade by eliminating blind corners and providing glass enclosed elevator lobbies, painted walls, and appropriate lighting levels.

Conclusion

The proposed development is a high quality mixed use building that conforms to the new OCP requirements, and with only a few minor variances, is also consistent with the Downtown Core Area Plan. All recommended guidelines are followed with a few minor exceptions for architectural expression.

This project develops a significant urban site in Harris Green, enlivens the street-front along Yates Street, and expands the available housing options in the downtown area by providing a good mix of suite sizes accessible to all income levels. The project is of an appropriate scale to its location and has been designed to minimize the impact of the building on the street and adjacent building sites, while providing a handsome and affordable addition to the Harris Green neighborhood.

Sincerely yours,

A handwritten signature in black ink, appearing to read 'Peter de Hoog', with a stylized, sweeping flourish extending from the end of the name.

Peter de Hoog Architect AIBC/MRAIC
de Hoog & Kierulf architects

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October 15, 2014

Re: 960 Yates Street – CALUC Meeting Review

Dear Mayor Fortin and Council,

The DRA LUC has reviewed the drawings for the proposed building and hosted a CALUC meeting on August 14, 2014 for the above-mentioned application. Thirty-two people registered their attendance at the door. The presentation was conducted by Peter De Hoog of Kierulf de Hoog Architects.

Based on the information presented by the applicant, the purpose of the rezoning is to create a 17 storey, 88 unit residential condominium tower with ground floor commercial space fronting Yates Street. The building would have an underground parking structure for 88 cars. The proposal appears to comply with OCP guidelines and no variances are requested.

Comments and concerns raised at the CALUC meeting by the public are as follows;

- Concerns were expressed about the excavation and the possibility of neighbouring properties subsiding. The architect mentioned much of the excavation will be primarily in rock.
- Questions were asked about the future land use for the property between the proposal and the Manhattan building. The Architect responded that they had no control over that property as the applicant did not own it.
- A resident of the 12th floor of the Manhattan building at 930 Yates expressed the height should be reduced as his view would be obstructed.
- An attendee commented that when they purchased their units downtown they expected to lose their view eventually as they did not own the adjacent properties.
- A resident of the Manhattan building asked if a traffic study would be conducted and recommended that the number of parking stalls be increased as their experience was that there was a need for additional stalls in more upmarket buildings.
- An attendee suggested that a carshare spot be dedicated for residents and a carshare program be set up by the strata.

- Two attendees expressed concerns regarding the proposed height and density of the proposal.
- Concerns were expressed that the exterior be clad in the highest quality materials possible. The Architect described the exterior cladding as a combination of rough cut limestone aluminum and metal spandrels and painted concrete.
- Concerns were expressed about the colour scheme and that any proposed colour scheme should avoid the gray pallet if possible.
- Complements were made regarding the design and colour scheme of the developers last project "The Mondrian".

DRA Land Use Committee review of this proposal finds it in keeping with the objectives of the OCP and of a high build quality and design. It is also commendable that stormwater management has been included within the proposal to treat and detain roof runoff.

The DRA supports this proposal will bring additional vitality to the Harris Green precinct and provide additional high quality residences Downtown.

Sincerely,

A handwritten signature in black ink, appearing to read 'Ian Sutherland', with a stylized, flowing script.

Ian Sutherland
Chair Land Use Committee
Downtown Residents Association

cc COV Planning



Received
City of Waterloo
DEC 03 2014
Planning & Development Department
Development Services Division

960 YATES STREET
Issued for Rezoning/DP



1 Location Plan
A001 1500

Received
City of Victoria

DEC 03 2014

Planning & Development Department
Development Services Bureau

BUILDING ZONING BYLAW SUMMARY

BUILDING DESCRIPTION:
17 STOREY RESIDENTIAL BUILDING + MECHANICAL PENTHOUSE

USES:
PRIMARYLY RETAIL, MAIN FLOOR, RESIDENTIAL ABOVE

ZONE:
EXISTING: S-1, LIMITED SERVICE DISTRICT
PROPOSED: NEW ZONE

DEVELOPMENT PERMIT AREA:
DPA3 (H)

SITE AREA: 1377 m² (14,822 s.f.)

FLOOR AREA:
COMM RETAIL L1: 145 m² (1 560 s.f.)
RESIDENTIAL L1: 80 m² (870 s.f.) - includes exit mezzanine
RESIDENTIAL L2: 435 m² (4 680 s.f.)
RESIDENTIAL L3: 375 m² (4 040 s.f.)
RESIDENTIAL L4-L16: 475 m² (5 115 s.f.) X 11 = 5 225 sq m
RESIDENTIAL L15-L16: 460 m² (4 950 s.f.) X 2 = 920 sq m
RESIDENTIAL L17: 330 m² (3 550 s.f.)
MECHANICAL PH: 50 m² (540 s.f.)
TOTAL PROPOSED: 7 570 m² (81 465 s.f., excl. parking)

FLOOR SPACE RATIO:
PERMITTED (DCAP): 5.5 : 1 FSR 7 574 m² (81 523 s.f.)
PROPOSED: 5.5 : 1 FSR

SITE COVERAGE: 99%

OPEN SITE SPACE: 1% (L2 & L4 landscaped terrace = 55%)

GRADE OF BUILDING:
22.1 m (GEODETTIC) See Site Plan for Grade Calculation

HEIGHT OF BUILDING:
PERMITTED (DCAP): 50.0 m MAX.
PROPOSED: 50.0 m + 2.2m Mechanical Penthouse = 52.2m

NUMBER OF STOREYS:
17 STOREYS + Mechanical Penthouse = 18

RESIDENTIAL PARKING:
REQUIRED (Inclusive C): 140 stalls per suite = 123
PROVIDED: 107 stalls per suite = 94 stalls
(excl. 5 visitor w/1 HC)

COMMERCIAL PARKING:
REQUIRED (Inclusive C): 1 stall per 37.5 sq m = 4
PROVIDED: 2

BICYCLE PARKING:
RESIDENTIAL: 1 per suite = 88 (100% Class I) + 6-space rack
PROVIDED: 88 Class I + 6-space rack

MAIN FLOOR SETBACKS:
FRONT: 0.0 m provided (DCAP) 0.0 m (Yates Street)
SIDE: 0.0 m 0.0 m (East & West)
REAR: 0.0 m 0.0 m (North)

UPPER LEVEL SETBACKS:
See Building Plans & Sections

SUITE SUMMARY:
Junior 1 Bed: 8 suites @ 47 sq m = 376 sq m
Junior 2 Bed: 4 suites @ 54 sq m = 216 sq m
1 Bed / 1 Bath: 22 suites @ 54 sq m = 1 188 sq m
2 Bed / 2 Bath: 22 suites @ 83 sq m = 1 826 sq m
2 Bed / 2 Bath: 22 suites @ 86 sq m = 1 892 sq m
2 Bed + Den / 2 Bath: 8 suites @ 107 sq m = 856 sq m
2 Bed + Den / 2.5 Bath: 2 suites @ 143 sq m = 286 sq m
TOTAL: 66 SUITES @ 6 990 sq m

BUILDING CODE SUMMARY

REFERENCED DOCUMENT:
BRITISH COLUMBIA BUILDING CODE 2012 - PART 3

MAJOR OCCUPANCY CLASSIFICATION:
- GROUP C - RESIDENTIAL

BUILDING AREA:
- 1260 sq. m. (13 563 s.f.)

BUILDING HEIGHT:
- 18 STOREYS

NUMBER OF STREETS FACING:
- 1

ACCESSIBLE FACILITIES:
- ACCESSIBLE ENTRANCE
- ACCESSIBLE PARKING STALL

CONSTRUCTION REQUIREMENTS:
- 3.2.2.47 GROUP C, ANY HEIGHT, ANY AREA, SPRINKLERED
- NON-COMBUSTIBLE CONSTRUCTION WITH
3HR MIN FIRE RESISTANCE RATINGS TO FLOORS AND
LOADBEARING WALLS.

ADDITIONAL REQUIREMENTS FOR HIGH BUILDINGS
Ref. 3.2.6.1 (1)(d)

PROJECT DESCRIPTION

CIVIC ADDRESS:
960-962 YATES STREET,
VICTORIA, BC

LEGAL DESCRIPTION:
LOTS 803 and 814, VICTORIA CITY

REGISTERED OWNER:
Alpha Project Developments Ltd.
P.O. Box 548
Victoria B.C.
V8W 2R9

ARCHITECT:
de Hoog & Kieuff architects
977 Fort Street
Victoria, BC
V8V 3K3
P: 658-3367
F: 658-3337
info@dhk.ca

STRUCTURAL CONSULTANT:
RJC
220 - 645 Tye Road
Victoria, BC
V8A 6X5
tel: 366-7794
fax: 361-7900
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SURVEYOR:
Brad Cusim
82 - 576 Hillside Avenue
Victoria, BC
V8T 1Y5
tel: 351-2257
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LANDSCAPE ARCHITECT:
Murdock de Groot Inc.
200-524 Columbia Rd.
Victoria, BC
V8Z 1G1
tel: 412-2819
fax: 412-2892
murdockdesign.ca

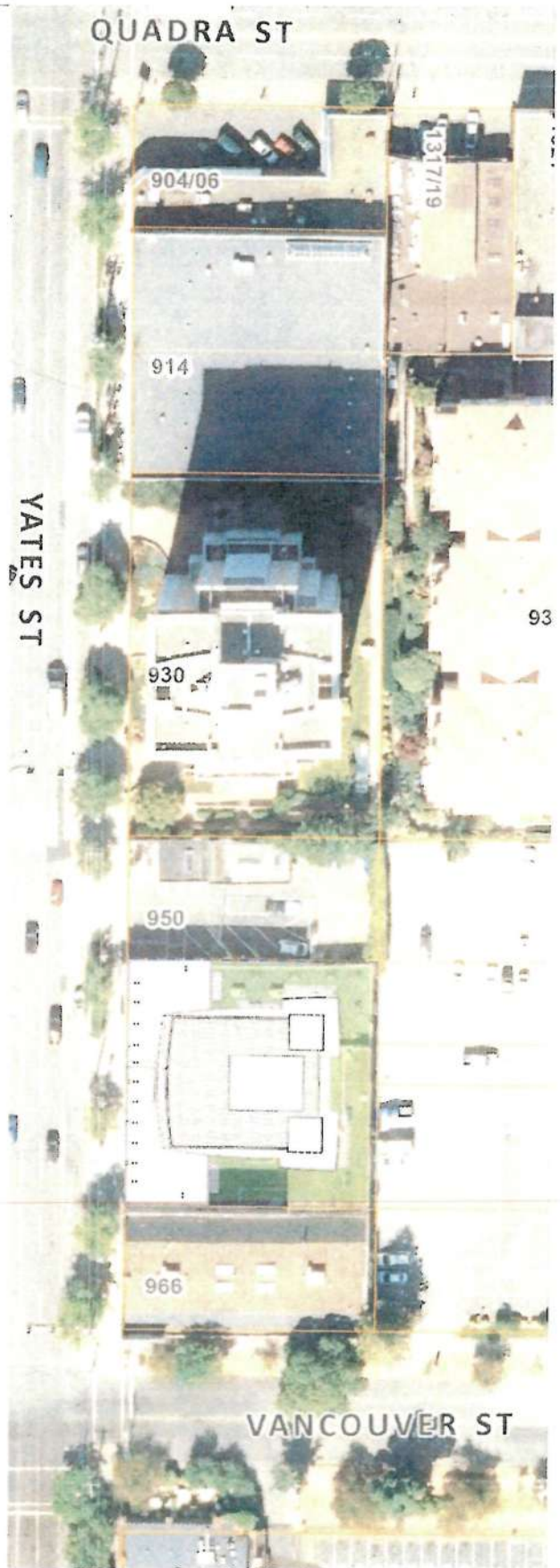
LIST OF DRAWINGS

Architectural	
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A002	Street Context
A003	Street Views
A004	Shadow Studies
A101	Existing Site Plan
A102	Site Plan
A201	P3 Level Parking Plan
A202	P2 Level Parking Plan
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A204	Level L1 Plan
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A208	Levels L15 & L16 Plan
A209	Level L17 Plan
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A401	Building Sections
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Landscape	
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L2.1	Planting Plan
L3.1	Stormwater Management Plan

NO.	DATE	DESCRIPTION	BY	CHECKED
1	2014.09.15	PRELIMINARY PROJECT DATA	de Hoog	Kieuff
2	2014.09.15	ARCHITECTURAL DRAWINGS	de Hoog	Kieuff
3	2014.09.15	LANDSCAPE DRAWINGS	de Hoog	Kieuff
4	2014.09.15	STORMWATER MANAGEMENT PLAN	de Hoog	Kieuff
5	2014.09.15	SHADOW STUDIES	de Hoog	Kieuff
6	2014.09.15	STREET VIEWS	de Hoog	Kieuff
7	2014.09.15	STREET CONTEXT	de Hoog	Kieuff
8	2014.09.15	EXISTING SITE PLAN	de Hoog	Kieuff
9	2014.09.15	SITE PLAN	de Hoog	Kieuff
10	2014.09.15	P3 LEVEL PARKING PLAN	de Hoog	Kieuff
11	2014.09.15	P2 LEVEL PARKING PLAN	de Hoog	Kieuff
12	2014.09.15	P1 LEVEL PARKING PLAN	de Hoog	Kieuff
13	2014.09.15	LEVEL L1 PLAN	de Hoog	Kieuff
14	2014.09.15	LEVEL L2 PLAN	de Hoog	Kieuff
15	2014.09.15	LEVEL L3 PLAN	de Hoog	Kieuff
16	2014.09.15	LEVELS L4 - L14 PLAN	de Hoog	Kieuff
17	2014.09.15	LEVELS L15 & L16 PLAN	de Hoog	Kieuff
18	2014.09.15	LEVEL L17 PLAN	de Hoog	Kieuff
19	2014.09.15	LOWER ROOF PLAN	de Hoog	Kieuff
20	2014.09.15	UPPER ROOF PLAN	de Hoog	Kieuff
21	2014.09.15	SOUTH & EAST ELEVATIONS	de Hoog	Kieuff
22	2014.09.15	NORTH & WEST ELEVATION	de Hoog	Kieuff
23	2014.09.15	SIGNAGE DETAILS	de Hoog	Kieuff
24	2014.09.15	BUILDING SECTIONS	de Hoog	Kieuff
25	2014.09.15	BUILDING SECTIONS	de Hoog	Kieuff

de Hoog & Kieuff architects	
977 Fort Street Victoria, BC V8V 3K3 P: 658-3367 F: 658-3337 info@dhk.ca	2014.09.15
960 Yates Street Victoria, BC	
Project Data	
NO.	DATE
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23	2014.09.15
24	2014.09.15
25	2014.09.15



1 Aerial View
A002 1300

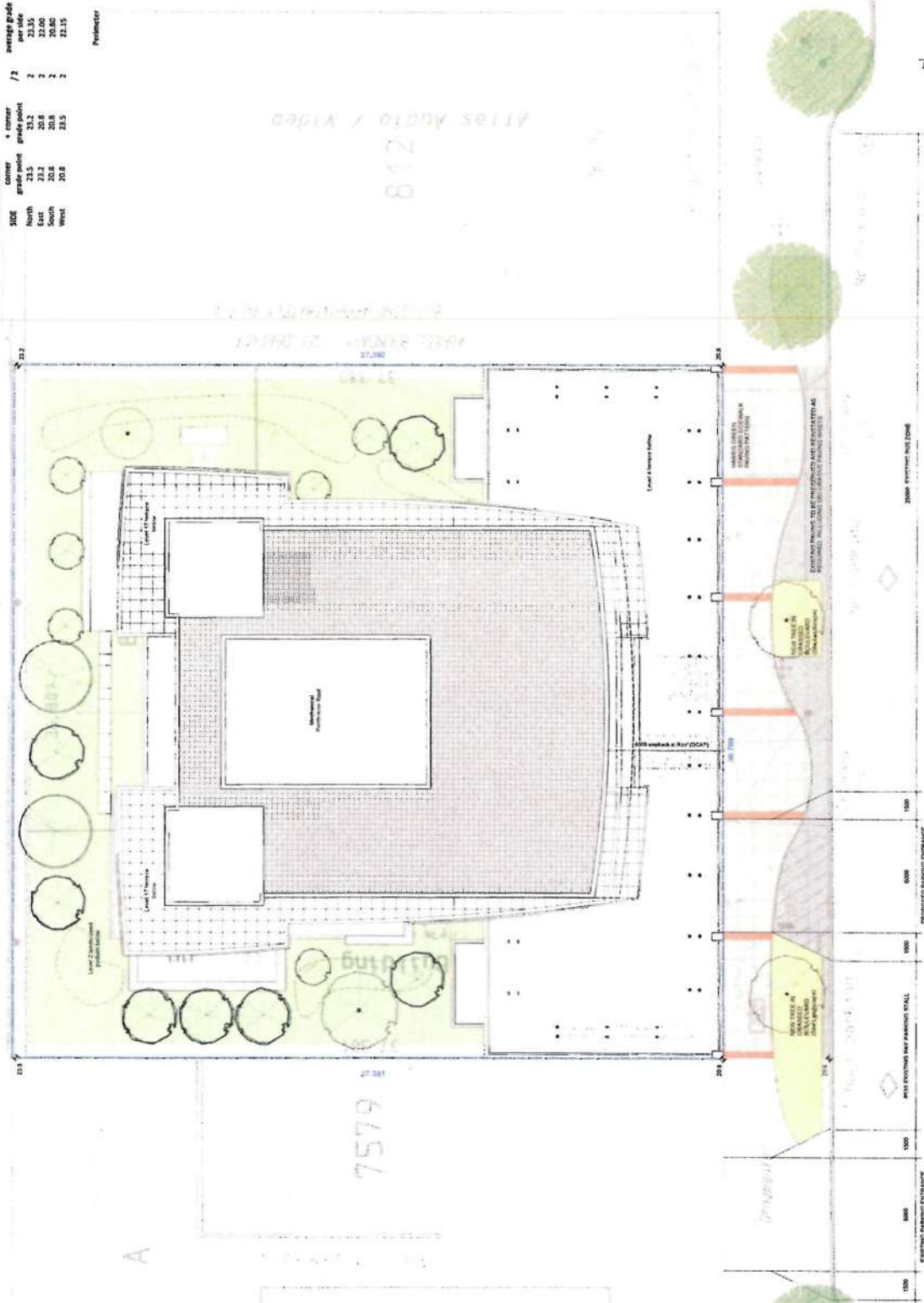
2 Street View
A002 1300



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Grade Calculation

Side	Corner	Grade point	7/2	Average grade	Length of side	TOTAL
North	23.2	20.8	2	21.5	36.1	736.1
East	23.2	20.8	2	22.00	37.23	830.5
South	20.8	20.8	2	20.80	36.75	764.4
West	20.8	23.5	2	22.15	37.23	824.6
GRADE =						3266.2
TOTAL/Perimeter						21.1
Perimeter						147.56



CERTIFIED CORRECT

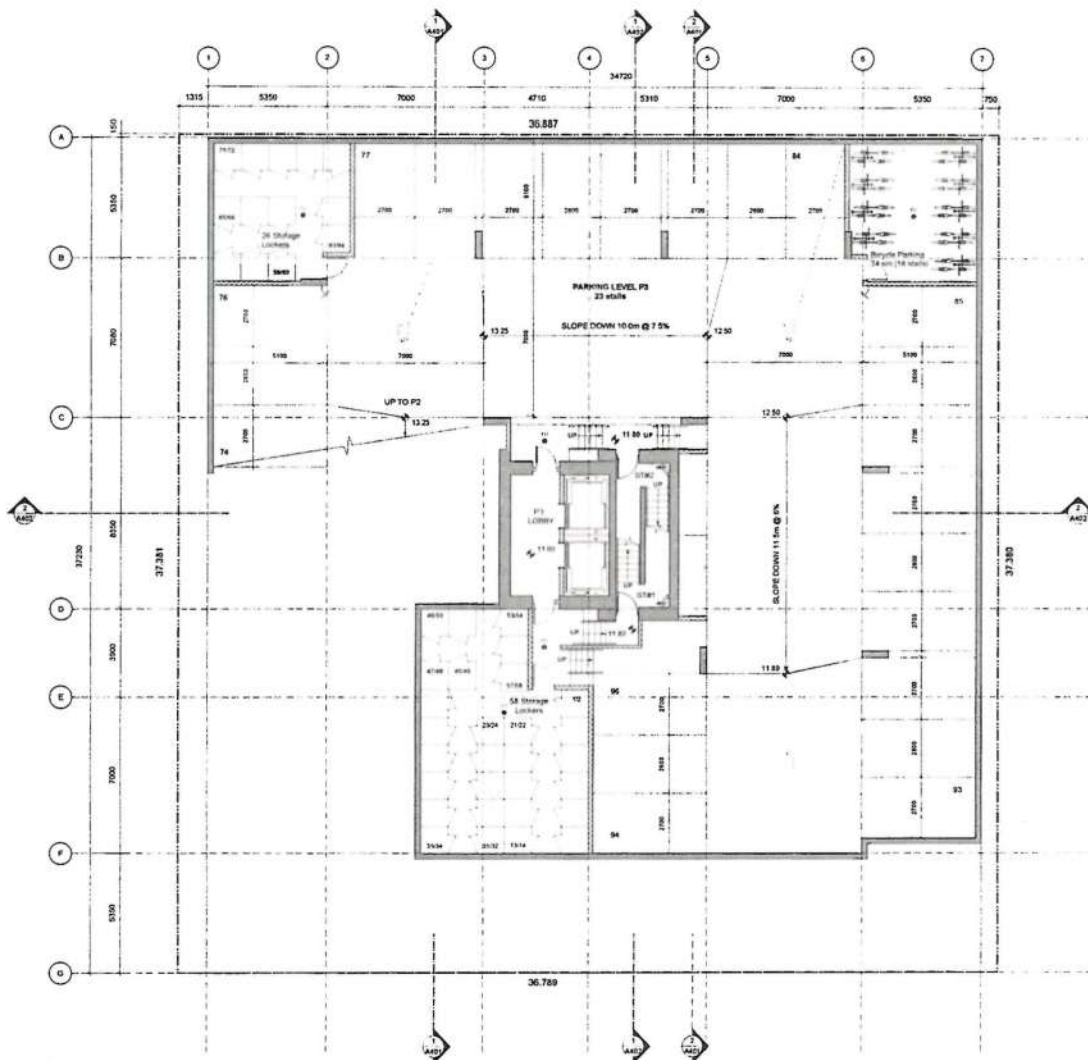
City of York
Government

YATES STREET

DEC 03 2014

Planning & Development Department
Development Services Division

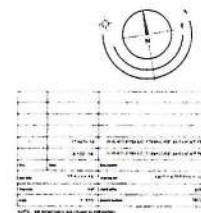
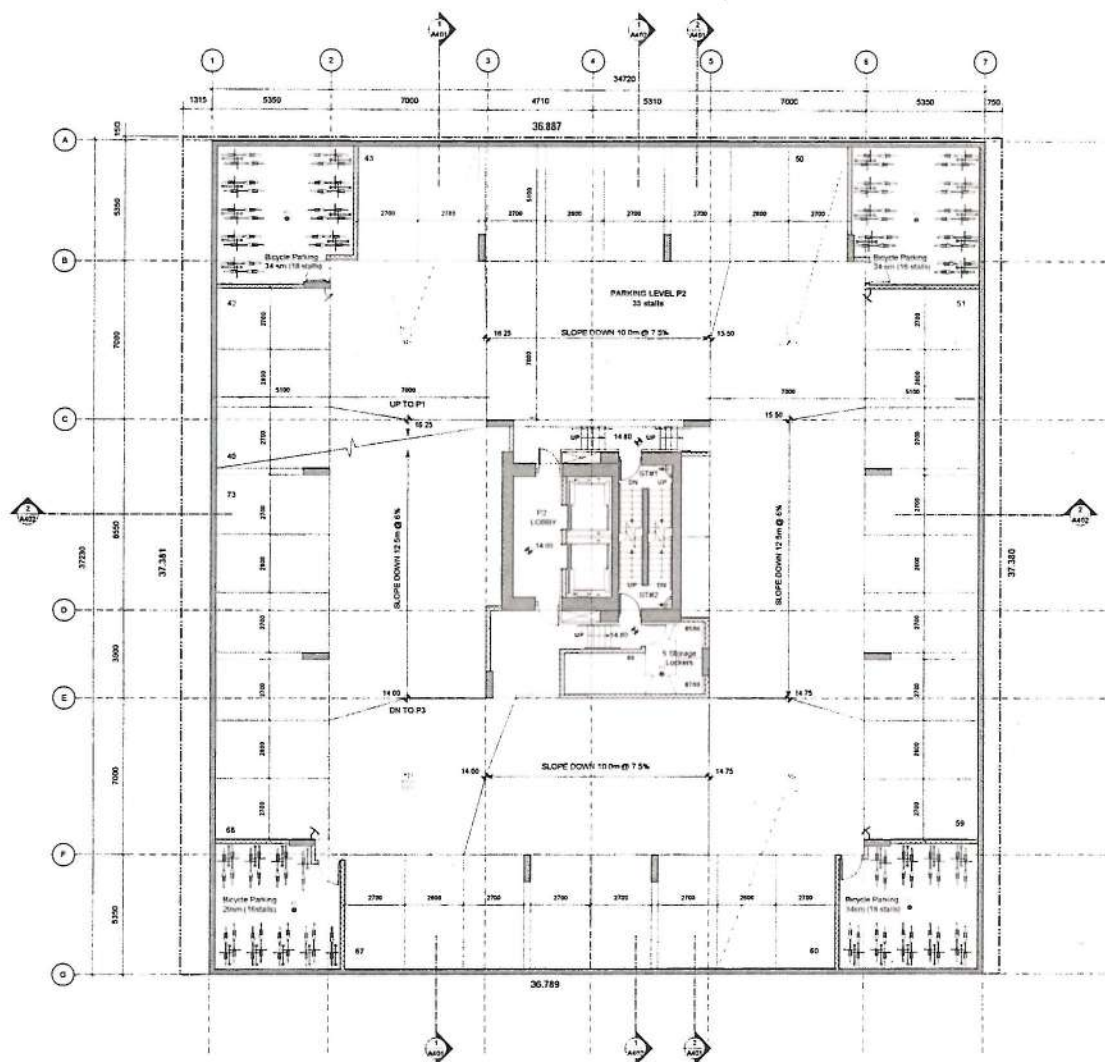
de Hong & Kneiff Architects
960 Yates Street
Victoria BC
Site Plan
A102



DATE	2014/11/10
PROJECT	560 YALES STREET
CLIENT	CHRYSLER
DESIGNER	de Haug & Kierulff architects
ARCHITECT	de Haug & Kierulff architects
ENGINEER	de Haug & Kierulff architects
LANDSCAPE ARCHITECT	de Haug & Kierulff architects
INTERIOR ARCHITECT	de Haug & Kierulff architects
MECHANICAL ENGINEER	de Haug & Kierulff architects
ELECTRICAL ENGINEER	de Haug & Kierulff architects
PLUMBING ENGINEER	de Haug & Kierulff architects
HEATING ENGINEER	de Haug & Kierulff architects
Cooling ENGINEER	de Haug & Kierulff architects
ENVIRONMENTAL ENGINEER	de Haug & Kierulff architects
SAFETY ENGINEER	de Haug & Kierulff architects
STRUCTURAL ENGINEER	de Haug & Kierulff architects
TRANSPORTATION ENGINEER	de Haug & Kierulff architects
WATER ENGINEER	de Haug & Kierulff architects
WASTE ENGINEER	de Haug & Kierulff architects
SOIL ENGINEER	de Haug & Kierulff architects
GEOTECHNICAL ENGINEER	de Haug & Kierulff architects
SEISMIC ENGINEER	de Haug & Kierulff architects
AV ENGINEER	de Haug & Kierulff architects
TELECOMMUNICATIONS ENGINEER	de Haug & Kierulff architects
ENERGY ENGINEER	de Haug & Kierulff architects
ENVIRONMENTAL ENGINEER	de Haug & Kierulff architects
SAFETY ENGINEER	de Haug & Kierulff architects
STRUCTURAL ENGINEER	de Haug & Kierulff architects
TRANSPORTATION ENGINEER	de Haug & Kierulff architects
WATER ENGINEER	de Haug & Kierulff architects
WASTE ENGINEER	de Haug & Kierulff architects
SOIL ENGINEER	de Haug & Kierulff architects
GEOTECHNICAL ENGINEER	de Haug & Kierulff architects
SEISMIC ENGINEER	de Haug & Kierulff architects
AV ENGINEER	de Haug & Kierulff architects
TELECOMMUNICATIONS ENGINEER	de Haug & Kierulff architects
ENERGY ENGINEER	de Haug & Kierulff architects

de Haug & Kierulff architects
560 Yales Street
Washington, DC
P3 Parking Plan
A201

Received
City of Chicago
DEC 03 2014
Planning & Development Department
Development Services Unit



de Hoog & Kierulff architects

900 Yates Street
Victoria BC

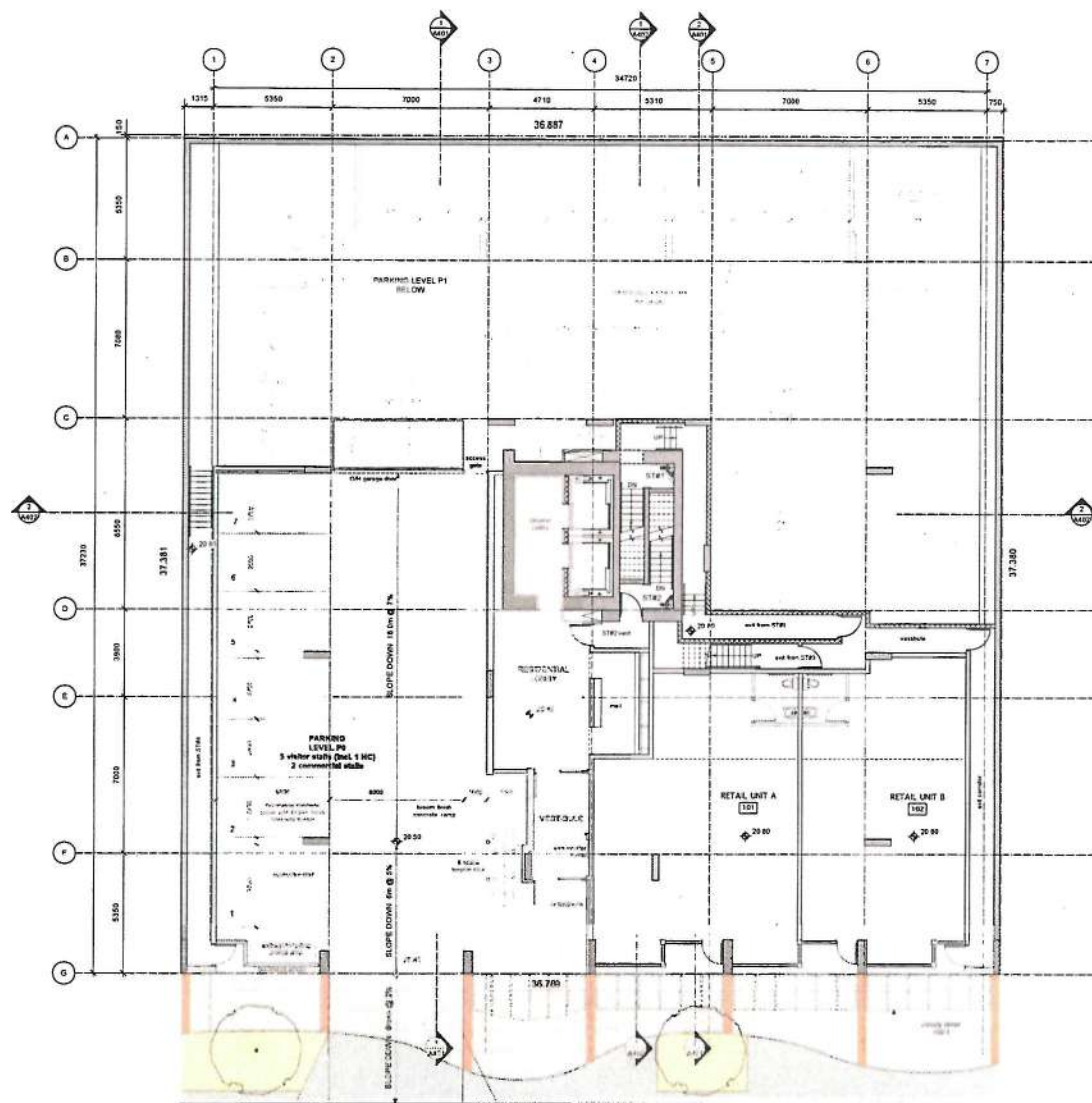
P2 Parking Plan

A202

Received
City of Victoria

DEC 03 2014

Planning & Development Department
Development Services Division



1 Site & L1 Floor Plan
A204/ 1:100

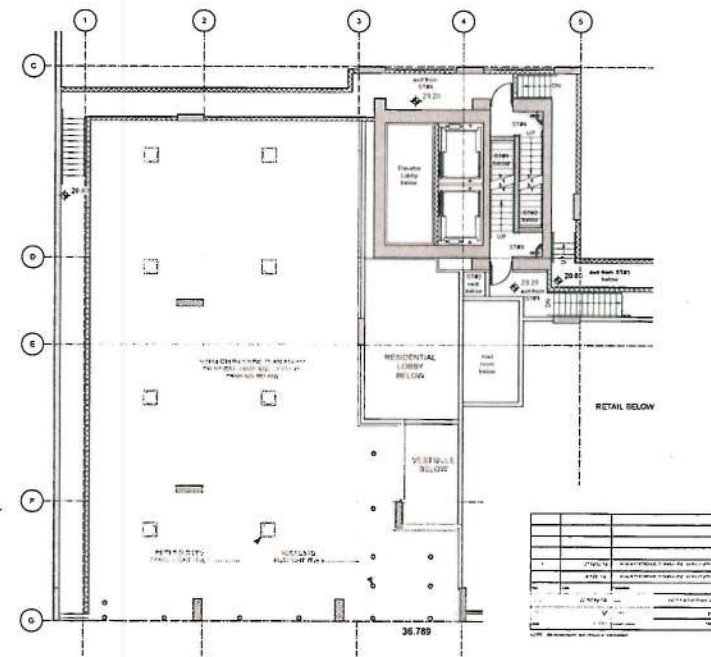
TOTAL MAIN FLOOR AREA (trailing height above 23.90m grade)
= 190 sqm (35 sqm Residential, 145 sqm Commercial)

YATES STREET

RECEIVED
City of Victoria

DEC 03 2014

Planning Department
City of Victoria



2 L1 Exit Mezzanine Detail Plan
A204/ 1:100

TOTAL EXIT MEZZANINE FLOOR
AREA (trailing height above 23.90m
grade) = 36.789 sqm

NO.	REVISION	DATE	BY	CHECKED
1	ISSUED FOR PERMIT	2014/11/10	de Hoog & Kienitz	
2	REVISION			
3	REVISION			
4	REVISION			
5	REVISION			

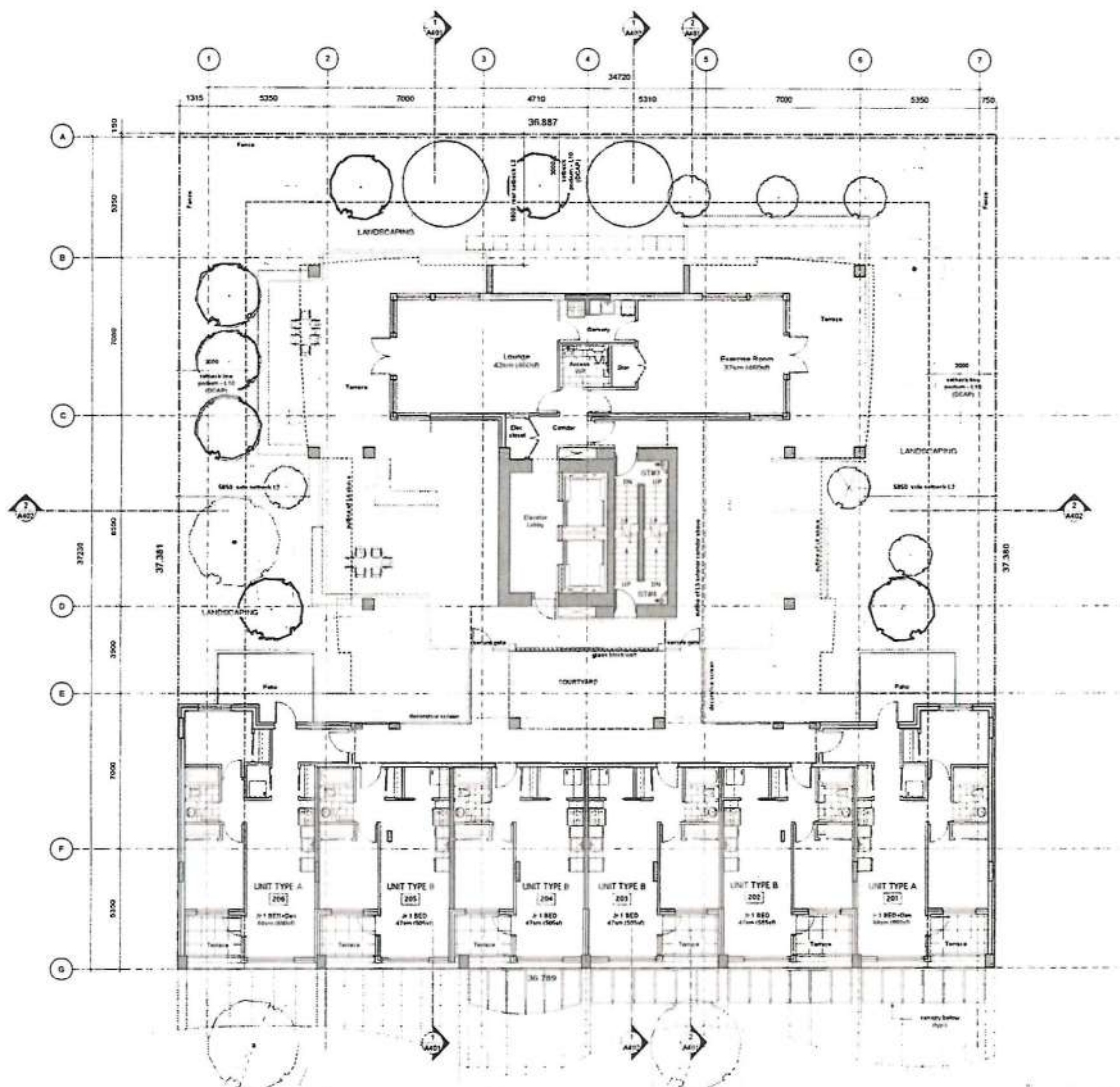


de Hoog & Kienitz architects

960 Yates Street
Victoria BC

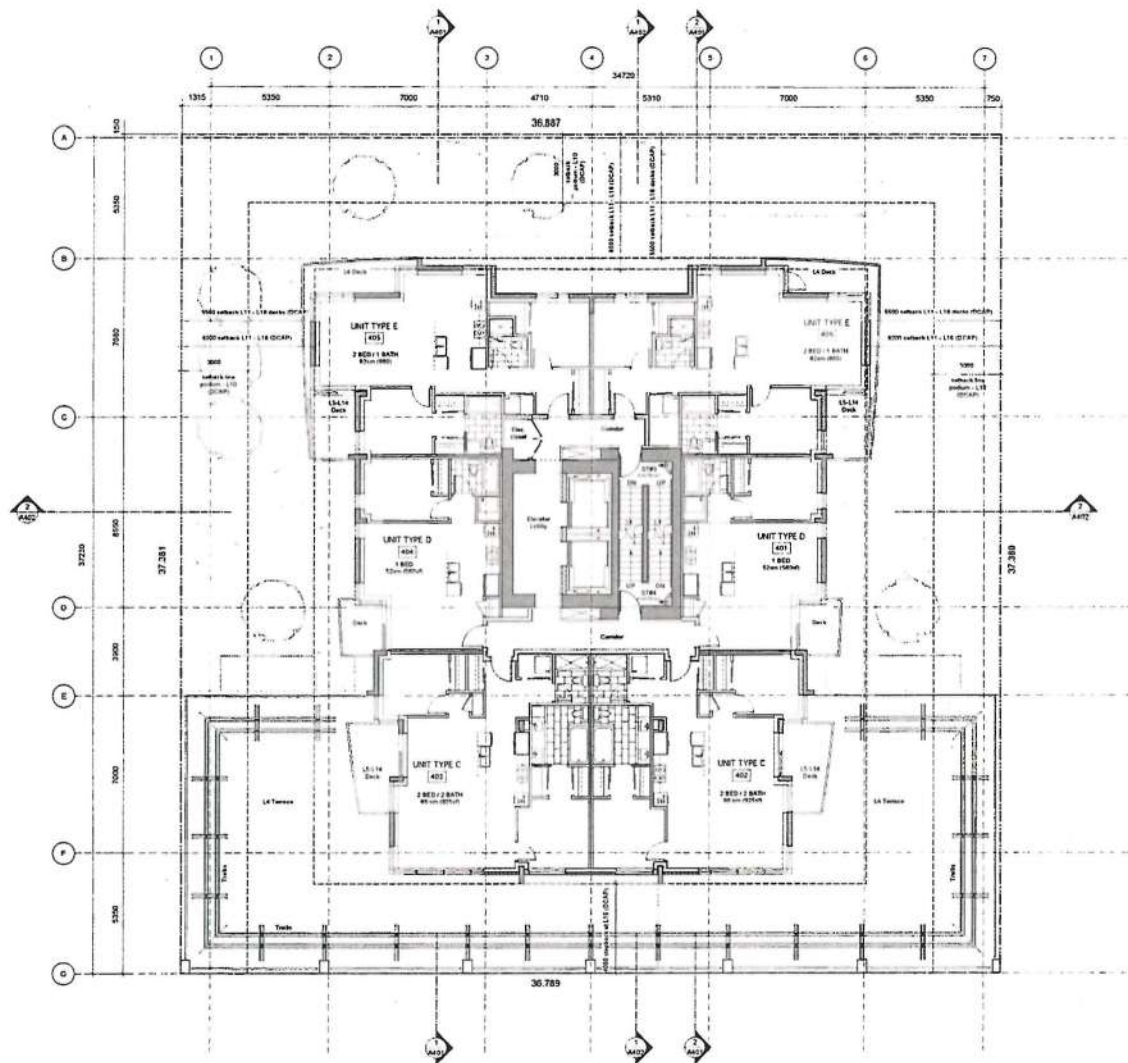
L1 Floor Plan

A204



PROJECT: 960 Yates Street VICTORIA BC L2 Floor Plan A205	de Hoog & Kierulff architects 960 Yates Street Victoria BC L2 Floor Plan A205

960 Yates Street
 City of Victoria
 DEC 03 2011
 Planning & Development Department
 Development Services Section



PROJECT: 960 Yates Street VICTORIA BC DATE: 2004-03-03 DRAWN BY: [Name] CHECKED BY: [Name] APPROVED BY: [Name]	SHEET: A207 OF: 1

de Hoog & Kierulff architects

960 Yates Street
Victoria BC

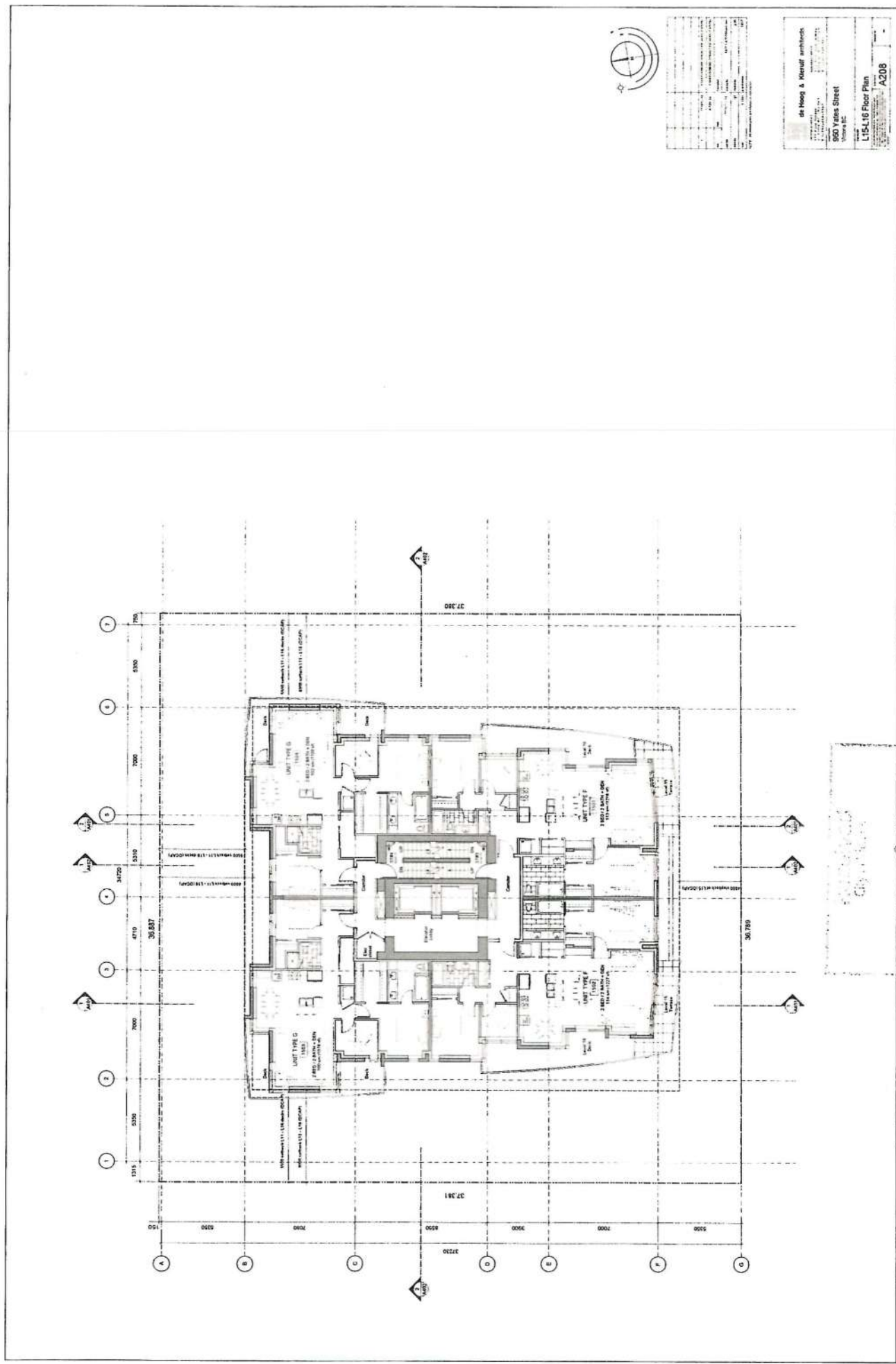
L4-L14 Floor Plan

A207

Received
City of Victoria

DEC 03 2004

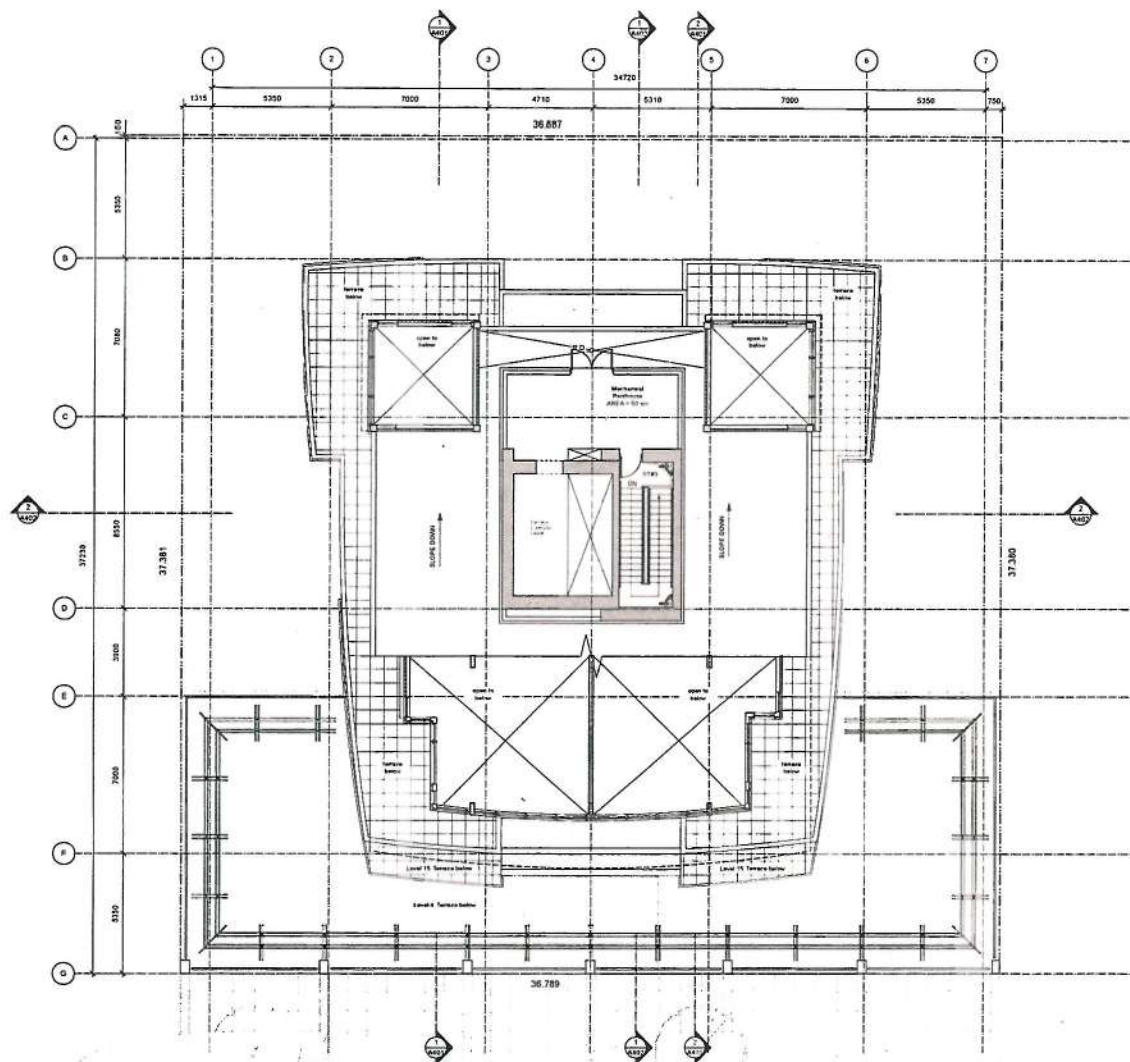
Planning & Development Department
City of Victoria



Project Name	15th Floor Plan
Project No.	A208
Client	de Hoog & Meruff architect
Address	900 Yale Street
City	Victoria BC
Scale	1:100
Date	2003.03.20
Drawn by	[Signature]
Checked by	[Signature]
Approved by	[Signature]

de Hoog & Meruff architect
 900 Yale Street
 Victoria BC
 15th Floor Plan
 A208

DEC 03 2004
 Mapping & Survey Department
 City of Victoria

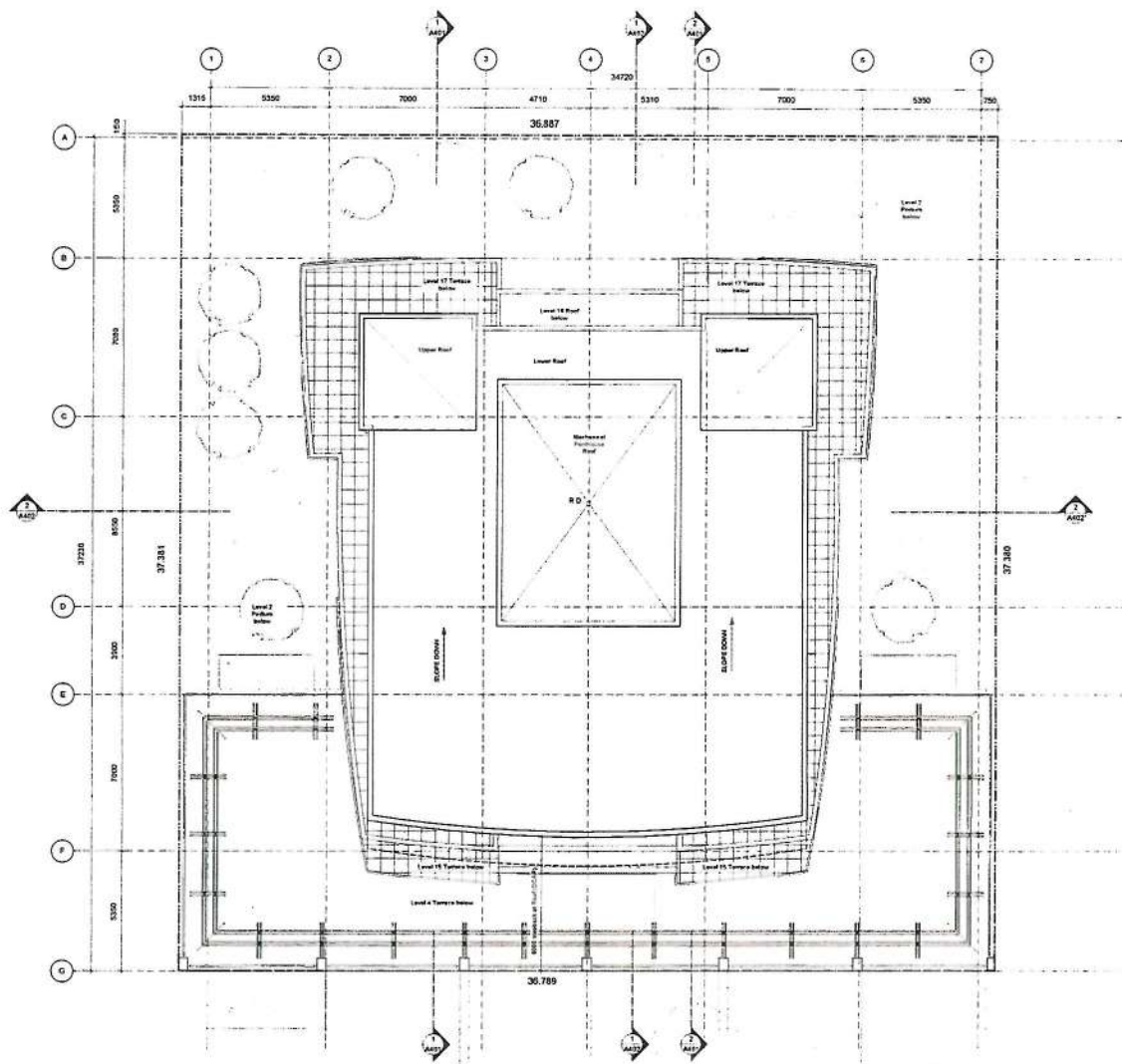


Project Name	
Project Number	
Project Location	
Project Date	
Project Status	
Project Manager	
Project Engineer	
Project Designer	
Project Checker	
Project Approver	

de Hoog & Koushik architects
 960 Yales Street
 Atlanta, GA 30309
 Phone: 404.525.1234
 Fax: 404.525.1235
 Email: info@dehoogkoushik.com
 Website: www.dehoogkoushik.com

Lower Roof Plan
 A210

DEC 03 2004
 Planning & Design Department
 Project: 04-001-001



Project Name	Upper Roof Plan
Project Number	A211
Client	960 Yates Street
Location	Victoria BC
Architect	de Hoog & Kierulff architects
Scale	1:100
Date	DEC 03 2014
Drawn by	
Checked by	
Approved by	

de Hoog & Kierulff architects

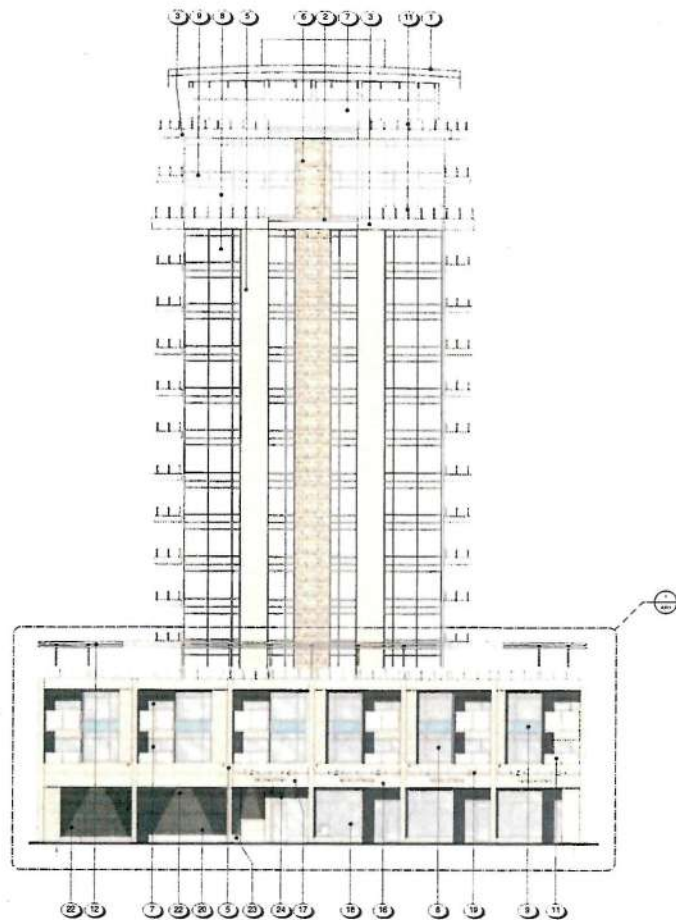
960 Yates Street
Victoria BC

Upper Roof Plan

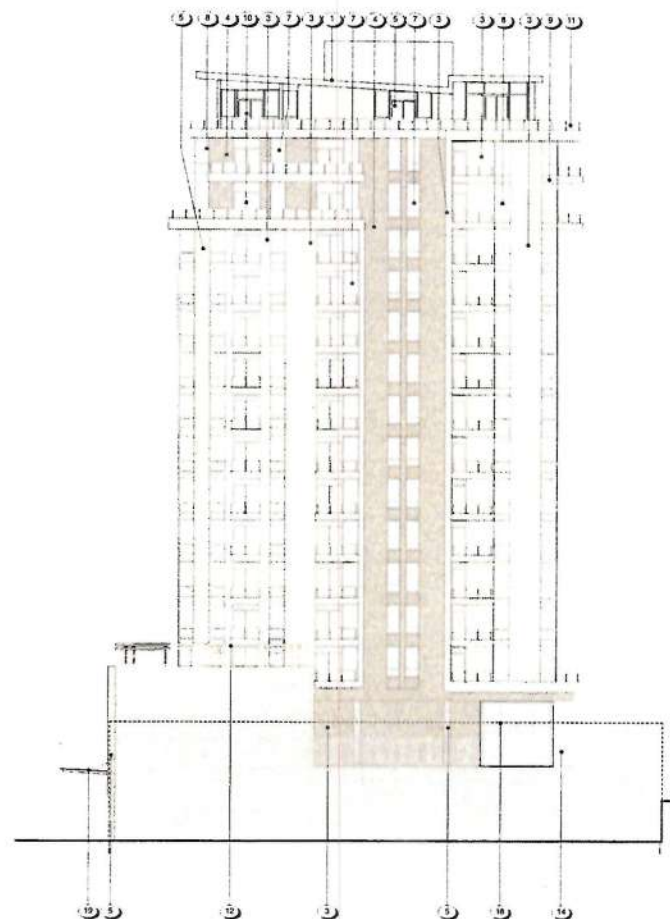
A211

DEC 03 2014

Planning & Development Department
City of Victoria



1 South Elevation (Yates Street)
A301/ 1:150



2 East Elevation
A301/ 1:150

MATERIALS SCHEDULE

- 1 PREFINISHED METAL FASADA
- 2 PREFINISHED METAL FLASHING
- 3 PAINTED CONCRETE - BASE COLOR
- 4 PAINTED CONCRETE - BASE COLOR
- 5 LIMESTONE VENEER - BASE
- 6 LIMESTONE VENEER - BASE
- 7 PRE-FINISHED ALUMINUM WINDOW
- 8 PRE-FINISHED ALUMINUM WINDOW SYSTEM
- 9 SOLIDATED STAINLESS STEEL PANELS
- 10 PRE-FINISHED STAINLESS STEEL GLASS DOORS
- 11 THERMO-BREAK GLASS GLAZING WITH PREFINISHED ALUMINUM FRAME
- 12 PAINTED METAL TRUSS
- 13 GLASS BLOCK
- 14 PREFINISHED METAL FENCE
- 15 THERMO-BREAK TRUSS GLAZING GLASS SECURITY SCREEN
- 16 STAINLESS STEEL
- 17 PRE-FINISHED ALUMINUM WINDOW SYSTEM
- 18 PRE-FINISHED METAL AND THERMO-BREAK GLASS CANOPY
- 19 PREFINISHED METAL GATE SECURITY GATE
- 20 PLASTER
- 21 PREFINISHED METAL SCREEN
- 22 PREFINISHED METAL GATE
- 23 PREFINISHED METAL SCREEN WITH GLASS BLOCK

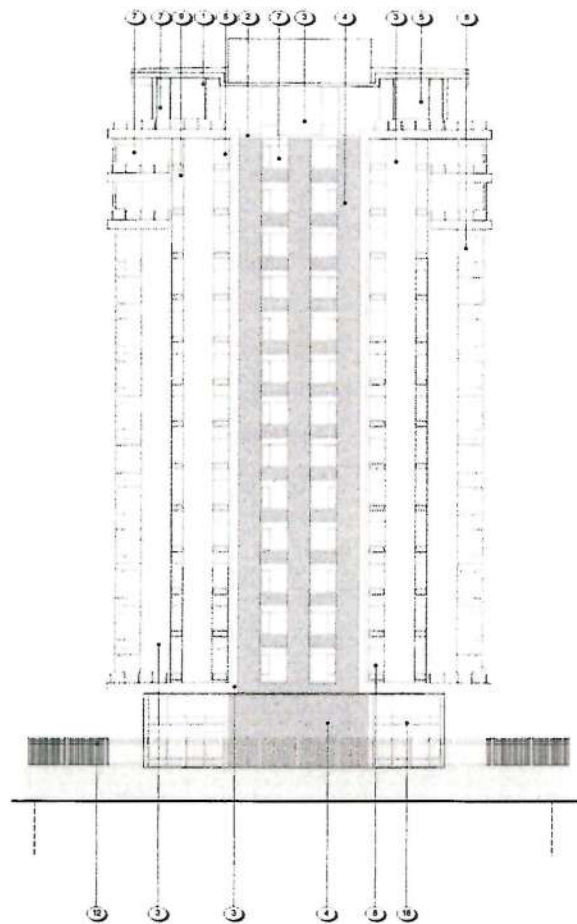
DATE	24 JUL 14	BY	de Hoog & Kleruf
SCALE	1:150	PROJECT	950 Yates Street
CLIENT	City of Vancouver	ARCHITECT	de Hoog & Kleruf
DATE	24 JUL 14	BY	de Hoog & Kleruf
SCALE	1:150	PROJECT	950 Yates Street
CLIENT	City of Vancouver	ARCHITECT	de Hoog & Kleruf

de Hoog & Kleruf architects	
ARCHITECT	24 JUL 14
950 Yates Street	
Vancouver BC	
South & East Elevations	
A301	

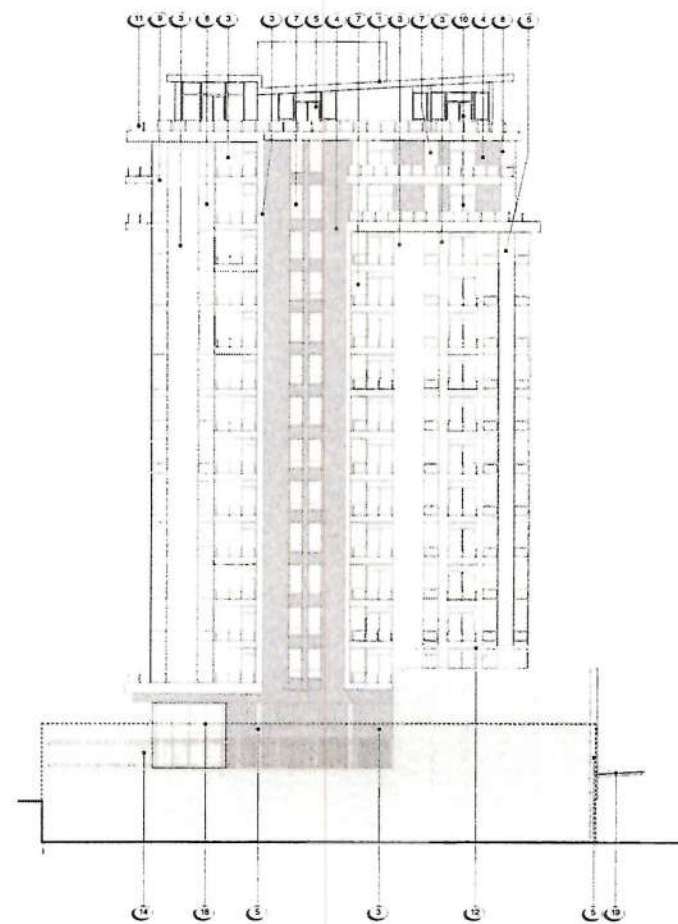
Received
City of Seattle

DEC 03 2014

Director of Planning Department
City of Seattle



1 North Elevation
A302 1:150



2 West Elevation
A302 1:150

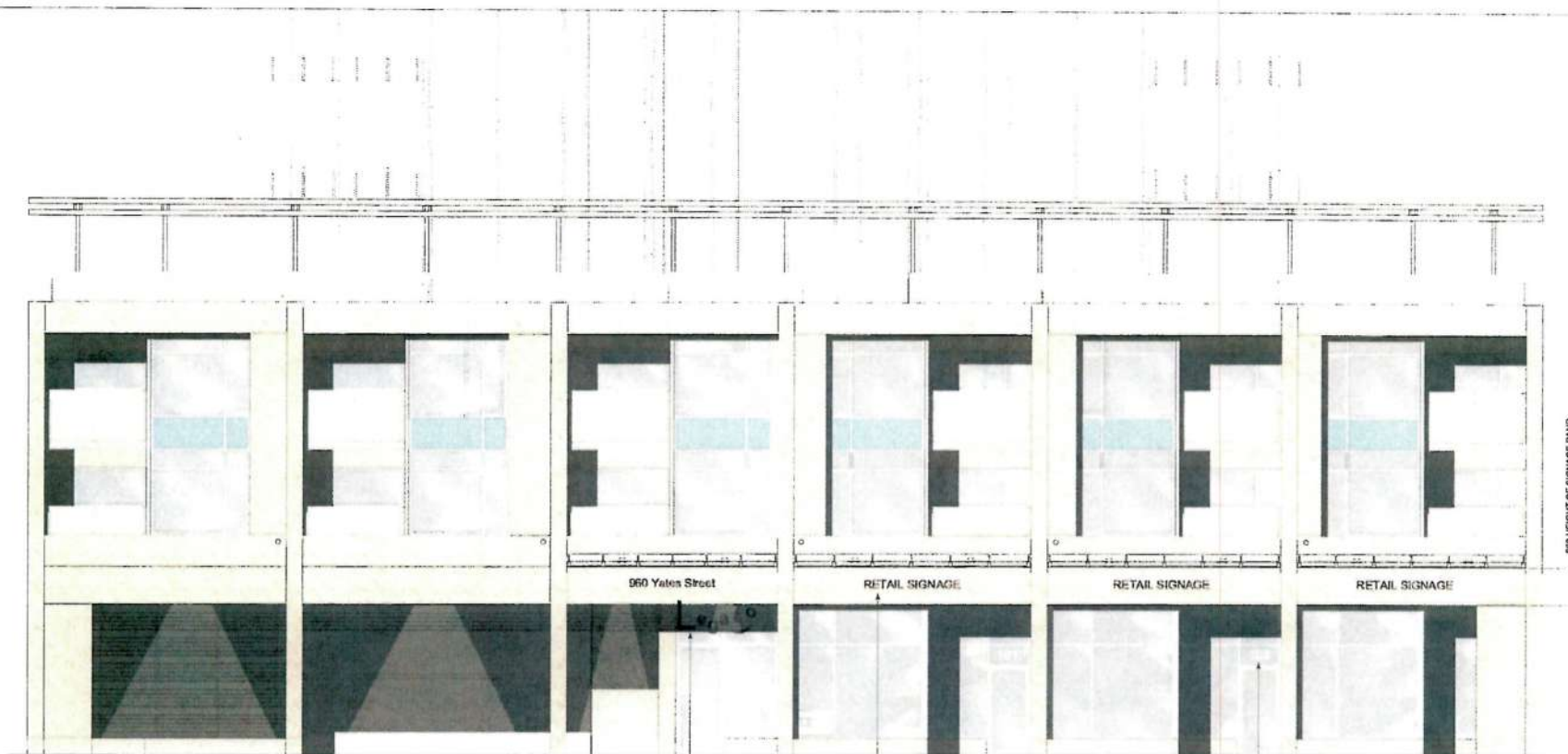
MATERIALS SCHEDULE

- 1 PREPAINTED METAL FASADA
- 2 PREPAINTED METAL FLASKING
- 3 PAINTED CONCRETE - BASE COLOR
- 4 PAINTED CONCRETE - BASE COLOR
- 5 LIMESTONE VINYL - BASE
- 6 LIMESTONE VINYL - HIGH
- 7 PRE-PAINTED ALUMINUM WINDOW
- 8 PRE-PAINTED ALUMINUM WINDOW
- 9 REGULATED SPANDEK PANELS
- 10 PRE-PAINTED TRANSPARENT GLASS DOORS
- 11 TRANSPARENT GLASS DOORS WITH PRE-PAINTED ALUMINUM FRAME
- 12 PAINTED METAL TRUSS
- 13 GLASS BLOCK
- 14 PREPAINTED METAL EDGE
- 15 TRANSPARENT TRANSPARENT GLASS PRIVACY SCREEN
- 16 NOISE BARRIER
- 17 BUILDING STORAGE
- 18 PRE-PAINTED ALUMINUM TRANSPARENT WINDOW
- 19 PRE-PAINTED METAL AND TRANSPARENT GLASS WINDOW
- 20 PREPAINTED METAL GATE SECURITY GATE
- 21 BLANKET
- 22 PREPAINTED METAL SCREEN
- 23 PREPAINTED METAL GATE
- 24 PREPAINTED METAL SCREEN WITH BUILDING NAME

DATE	11/11/14	BY	de Haag & Kleruif
PROJECT	960 Yates Street	CLIENT	de Haag & Kleruif
ARCHITECT	de Haag & Kleruif	ENGINEER	de Haag & Kleruif
CONTRACTOR	de Haag & Kleruif	INSTALLER	de Haag & Kleruif

de Haag & Kleruif architects	
960 Yates Street	1000-1000
1000-1000	1000-1000
North & West Elevations	
A302	1:150

RECEIVED
CIVIL ENGINEER
DEC 03 2014
Planning Department
City of Chicago

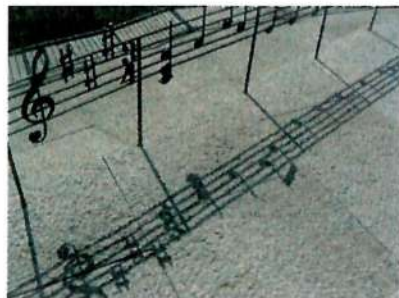


1 South Elevation Detail
A303 1:50

PAINTED CAST LETTERS 500mm MAX HIGH
AFFIXED TO DECORATIVE SCREEN

PREFINISHED RAISED LETTERS 250mm HIGH MAX
TO 4m WIDTH MAX WITHIN SIGNAGE BAND (typ.)

WHITE ADHESIVE ADDRESS NUMBERS
AFFIXED TO INSIDE FACE OF GLASS

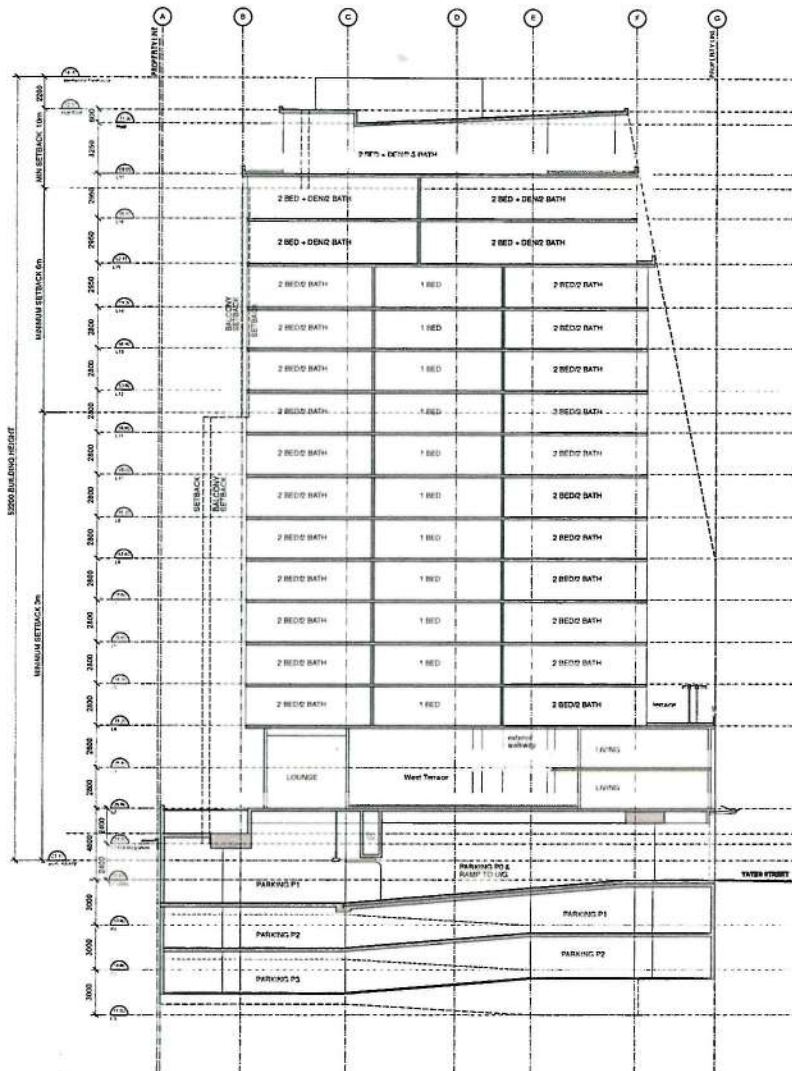


2 Concept for Decorative Screen
NTS

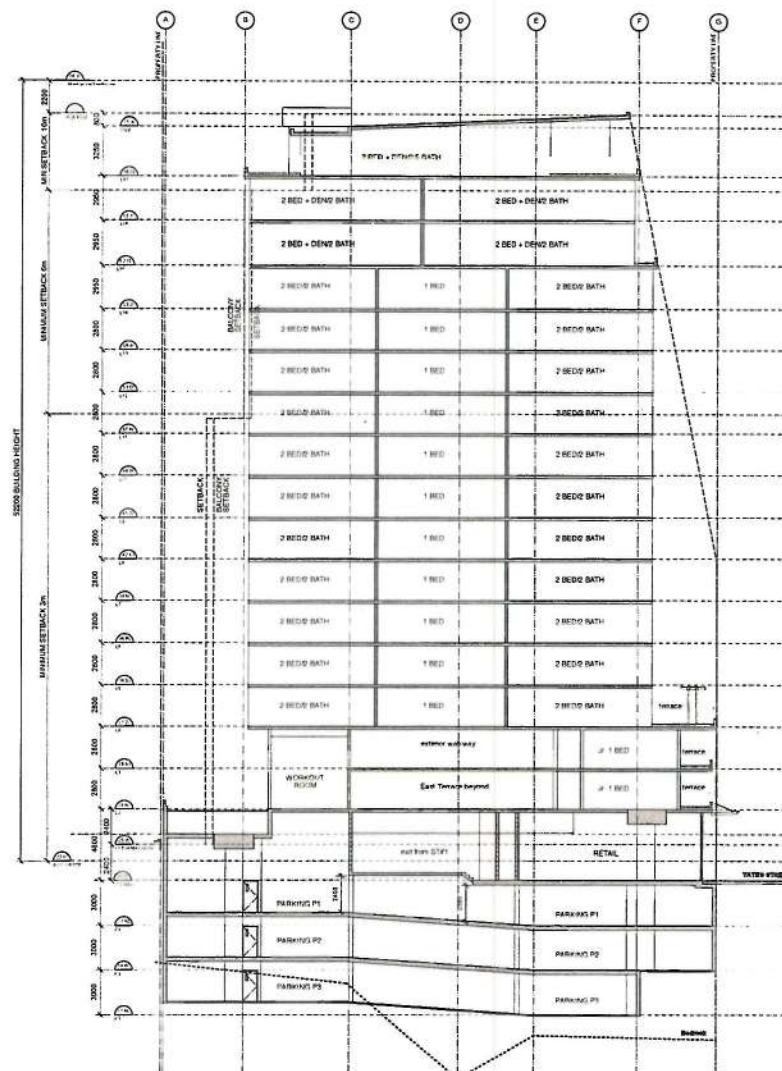
Project Name	960 Yates Street
Client	City of Victoria
Architect	de Haag & Kieruff architects
Project Number	A303
Scale	1:50
Date	2014

de Haag & Kieruff architects	
2111 14th Avenue	Van 10-10-10-10-10
Vancouver, BC	BC
960 Yates Street	
Victoria BC	
Signage Details	
A303	

RECEIVED
City of Victoria
DEC 03 2014
Planning & Development Department
Development Services Division



1 Building Section Through Ramp
A401 1:150



2 Building Section Through Retail
A401 1:150

NO.	DATE	BY	CHKD.	DESCRIPTION
1	2018.10.18	J. de Hoog	J. de Hoog	Initial Design
2	2018.10.18	J. de Hoog	J. de Hoog	Initial Design
3	2018.10.18	J. de Hoog	J. de Hoog	Initial Design
4	2018.10.18	J. de Hoog	J. de Hoog	Initial Design
5	2018.10.18	J. de Hoog	J. de Hoog	Initial Design

de Hoog & Kierulff architects

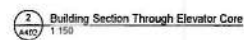
950 Yates Street
Victoria, BC

Building Sections

A401

DEC 03 2018

Planning & Development Department
City of Victoria



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 City of Victoria
 DEC 03 2014
 Planning & Development Department
 Development Services Division

LEGEND

Property line
 Rain garden - TOP OF POOL

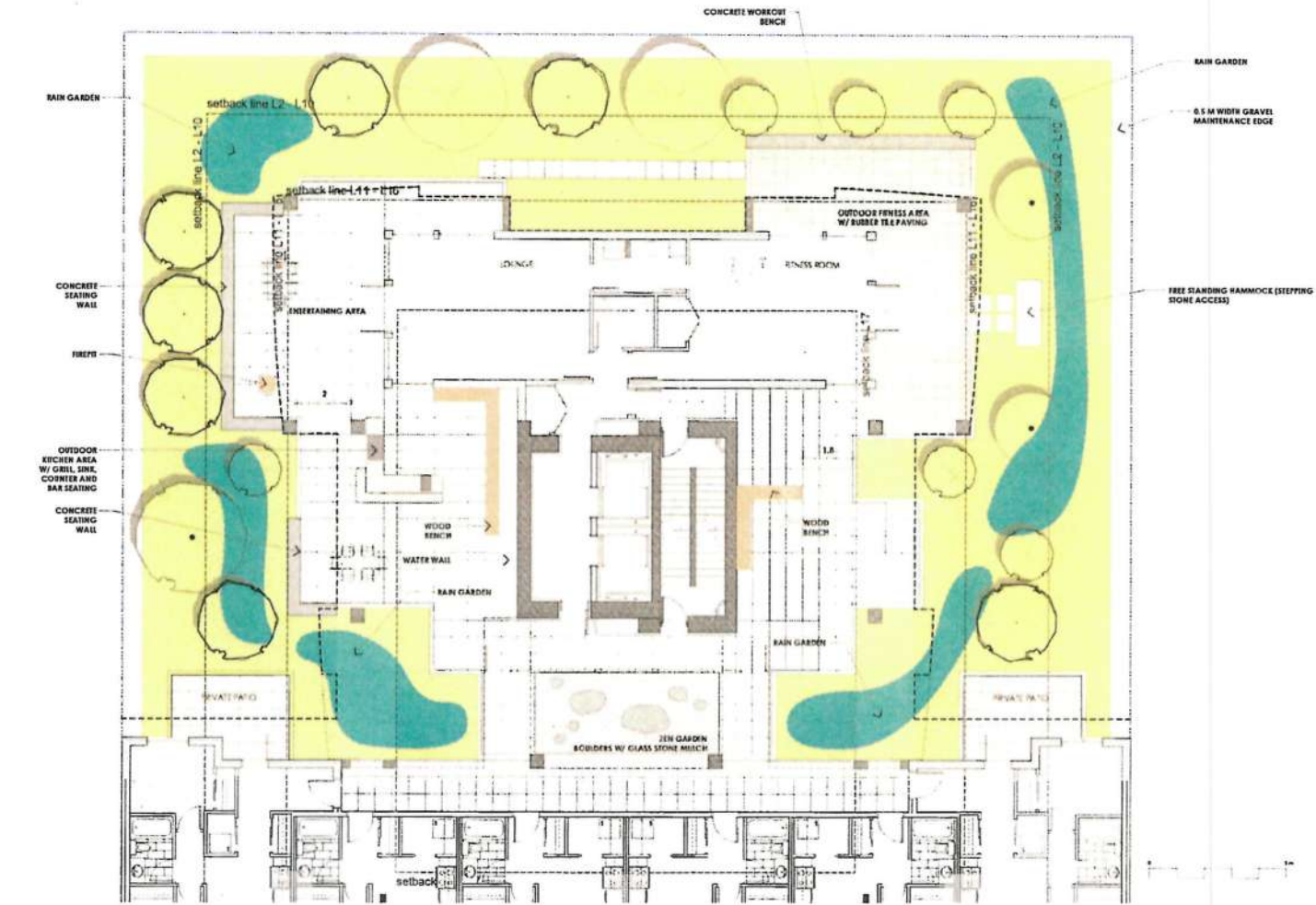
Landscape Materials

Hydraprinted Slab paving
 Rubber Tile Surface - Outdoor Fitness Area
 Planting Area
 Rain Garden Area (SEE DETAIL, L3.01)
 Gravel Maintenance Edge (0.5 m width)

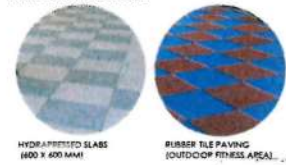
- DRAWING NOTES**
1. DO NOT SCALE DRAWINGS: Verify all property lines and existing structures/vegetation in situ prior to commencing work.
 2. All plan dimensions in metres and all detail dimensions in millimetres.
 3. Plant quantities on Right: shall take precedence over plant list quantities.
 4. Contractor to confirm location and elevation of all existing services and utilities prior to start of construction.
 5. Provide layout of all work for approval by Landscape Architect prior to proceeding with work.
 6. Contractor to provide irrigation system for all plants and structures, to meet SABC Standards and Contract Specifications.
 7. Landscape installation to carry a 1 year warranty from date of acceptance.
 8. Plant material: installation and maintenance to conform to the current edition of the SCSLA/BCSLA Landscape Standards.
 9. General Contractor and/or sub-contractors are responsible for all costs related to production and submission to consultant of all landscape as-built information including irrigation.
 10. Tree protection fencing, for existing trees, to be installed prior to commencement of all site work.



NO.	REVISION	DATE	BY	CHKD BY
1	ISSUED FOR PERMIT	2014/11/20	MD	MD
2	ISSUED FOR CONSTRUCTION	2014/12/03	MD	MD



PAVING MATERIAL PALETTE



LANDSCAPE FEATURES



de Hoog & Klerk architects

960 Yates Street
 Sydney NSW

Landscape Plan - Level 2

Scale: 1:100

Sheet: 1

DEC 03 2014

Planning & Environment Department
 New South Wales Government

--- PAYING AREA 31 SQ. M. TOTAL



ROOF AREA
950 SQ. M.

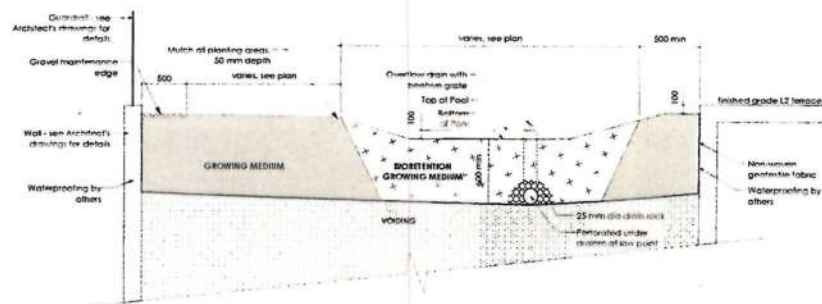
RAIN WATER LEADER

RAIN WATER LEADEN

RAIN GARDEN

LEGEND

- Property line
- Landscape Depression - TOP OF POOL
- Landscape Depression - BOTTOM OF POOL
- Direction of flow
- Rain Garden Area
- Catchment Area A - ROOF AREA 950 SQ. FT.
- Catchment Area A - PAVING (LEVEL 2) AREA 31 SQ. FT.



1 Rain garden detail
Scale: 1:25



Rain Garden Capacity Calculations

Catchment Area	Contributing Impervious Area	Design Storm Runoff Volume Contributing to Rain Garden	Planting Depth Medium Green	Stormwater Treatment Capacity per sq. m. of Rain Garden	Rain Garden Area	Rain Garden Capacity	Excess (H) or Deficient (-) Capacity
	[sq. m.]	[cu. m./day]	[m.]	[cu. m./day]	[sq. m.]	[cu. m./day]	[cu. m./day]
Catchment A	950.5	46.6	0.4	0.7	77.5	54.3	7.7

Assumptions

1 Design storm is a 2 year storm event which equal 5 cm of water, in a 24 hr period.

2 Ron Garden design based on 100 mm live ponding plus 20% of the sand/compact growing medium volume (assuming growing medium has 20% void space) with a minimum infiltration rate of 2 mm/hour (or 48 cm per day), via perforated underdrain.

DRAWING NOTES

- DRAINING NOTES**
1. DO NOT SCALE DRAINAGE. Verify all property lines and existing structures/vegetation to ensure prior to stormwater work.
 2. ALL excavations in marshes and old ditch/drainages in marshes.
 3. Plant quantities on Plans shall take precedence over plant list.
 4. Contractor to ensure location and maintenance of all existing weirs and culverts prior to start of construction.
 5. Final plan of work to be approved by Landscape Architect prior to proceeding with work.
 6. Contractor to provide irrigation system for all planters and containers, to current SABC Standards and Contact Specifications.
 7. Landscape installation to carry a 1 year warranty from date of completion.
 8. Plant material: selection and maintenance to conform to the current edition of the SCLIA/SCJLA Landscape Standards.
 9. Contractor to ensure that all plant material is suitable for each region to production and submission to manufacturer of all landscape as built information including irrigation.
 10. Ensure protective fencing around trees. To be installed prior to commencement of all site work.

